



CITY COUNCIL REGULAR MEETING AGENDA

November 1, 2022 at 6:30 p.m.

City Hall Council Chambers and Virtual

The City of Kennewick broadcasts Council meetings on the City's website at <https://www.go2kennewick.com/CouncilMeetingBroadcasts> and via Zoom. If you are unable to attend in person and wish to comment during one of the Visitors sections or if applicable to provide public testimony for a Public Hearing, please register at https://us02web.zoom.us/webinar/register/WN_rAT9hyobSpCh4mLiyacCQ. Registrations must be received by 4:00 p.m. on the day of the meeting.

The public can also submit comments by either filling out an online form at <https://www.go2kennewick.com/PublicComments> via e-mail to clerkinfo@ci.kennewick.wa.us, or submitting written comments to P.O. Box 6108, Kennewick, WA 99336. Comments must be received no later than 4:00 p.m. on the Monday before the meeting.

WORKSHOP MEETING – Begins at 5:30 p.m.

1. 2023/2024 Biennial Budget Presentation

1. CALL TO ORDER

Pledge of Allegiance/Welcome/Invocation

HONORS & RECOGNITIONS

2. VISITORS

Public comments for item(s) on the agenda not covered under a public hearing. Please limit your comments to three minutes. Records intended for Council (9 copies are required) must be given to the City Clerk by 4:00 p.m. on the Monday before the meeting.

3. APPROVAL OF AGENDA

4. CONSENT AGENDA

All matters listed within the Consent Agenda have been distributed to each member of the Kennewick City Council for reading and study, are considered to be routine, and will be enacted by one motion of the Council with no separate discussion.

- a. Minutes of Regular Meeting of October 18, 2022.
- b. Motion to approve Claims Roster - None.
- c. Motion to approve Payroll Roster for October 15, 2022.
- d. Motion to set the date of November 15, 2022 for the public meeting on Annexation 2022-0001 (Harmony Development, LLC.)
- e. Motion to award Contract P2111-22 Pedestrian Crossing Safety Project to Ellison Earthworks LLC in the amount of \$806,459.49.
- f. Motion to approve the 2023 Tourism Promotion Area (TPA) Budget and Marketing Plan.

5. ORDINANCES/RESOLUTIONS

- a. (1) Ordinance 5989: Water Rate Increase (KMC 14.13.030, 14.13.040, 14.13.050, 14.12.100.)
(2) Ordinance 5990: Sewer Rate Increase (KMC 14.26.010, 14.26.20, 14.26.030, 14.26.040, 14.26.070.)
- b. Ordinance 5992: Comprehensive Plan Amendment CPA-2022-0005 from Low Density Residential to High Density Residential & Low Density Residential to Medium Density Residential at 2701 & 2711 S Sherman St.

6. PUBLIC HEARINGS/MEETINGS

7. NEW BUSINESS

*To assure disabled persons the opportunity to participate in or benefit from City services, please provide twenty-four (24) hour advance notice for additional arrangements to reasonably accommodate special needs.
Please be advised that all Kennewick City Council Meetings are Audio and Video Taped*

8. UNFINISHED BUSINESS

9. VISITORS

Public comments for any item(s) the public wants to bring to Council. Please limit your comments to three minutes. Records intended for Council (9 copies are required) must be given to the City Clerk by 4:00 p.m. on the Monday before the meeting.

10. COUNCIL COMMENTS/DISCUSSION

11. ADJOURNMENT

DRAFT
CITY OF KENNEWICK
CITY COUNCIL
Regular Meeting
October 18, 2022

1. CALL TO ORDER

Mayor Bill McKay called the meeting to order at 6:30 p.m.

City Council and Staff Present:

Mayor Pro Tem Gretl Crawford	Marie Mosley	Nick Farline
John Trumbo	Anthony Muai	Trevor White
Chuck Torelli	Christina Palmer	Chad Michael
Jim Millbauer	Lisa Beaton	Evelyn Lusignan
Brad Beauchamp	Cary Roe	
Loren Anderson	Terri Wright	
Mayor Bill McKay	Dan Legard	

Girl Scout Troops 3518 and 4527 led the Pledge of Allegiance.

HONORS & RECOGNITIONS

- National Day of the Girl Proclamation

Mayor McKay presented the proclamation to Girl Scout Troops #3518, 4527 and Troop 3518 Leader Elysha Pleasant.

2. VISITORS - None

3. APPROVAL OF AGENDA

Mr. Torelli moved, seconded by Mr. Trumbo to approve the Agenda as presented. The motion passed unanimously.

4. APPROVAL OF CONSENT AGENDA

- a. Minutes of Regular Meeting of October 4, 2022.
- b. Motion to approve Claims Roster for September 2022.
- c. Motion to approve Payroll Roster for September 30, 2022.

Mr. Trumbo moved, seconded by Mr. Torelli to approve the Consent Agenda. The motion passed unanimously.

5. ORDINANCE/RESOLUTIONS

- a. Ordinance 5991: Ward Boundaries (Redistricting). Anthony Muai, Planning Director reported.

ORDINANCE NO. 5991

AN ORDINANCE AMENDING CITY OF KENNEWICK WARD BOUNDARIES

Mr. Anderson moved, seconded by Mr. Torelli to adopt Ordinance No. 5991. The motion passed unanimously.

6. PUBLIC HEARINGS/MEETINGS - None

7. NEW BUSINESS - None

8. UNFINISHED BUSINESS - None

DRAFT

9. VISITORS

Tina Gregory, Kennewick – Commented on current events and her opinions on same.

10. COUNCIL COMMENTS/DISCUSSION

Council members reported on their respective activities.

11. ADJOURNMENT

Meeting was adjourned at 7:17 p.m.

Terri L. Wright, MMC
City Clerk

Council Agenda Coversheet



Agenda Item Number	4.c.	Council Date	11/01/2022
Agenda Item Type	General Business Item		
Subject	Payroll Roster for PPE 10/15/2022		
Ordinance/Reso #		Contract #	
Project #		Permit #	
Department	Finance		

Consent Agenda	<input checked="" type="checkbox"/>
Ordinance/Reso	<input type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

That Council approve the Payroll Roster.

Motion for Consideration

I move to approve the Payroll Roster for 10/15/2022 in the amount of \$2,692,777.30 comprised of check numbers 75917 through 75929 and direct deposit numbers 207052 through 207513.

Summary

None.

Alternatives

None.

Fiscal Impact

Total: \$2,692,777.30.

Through

Dept Head Approval

City Mgr Approval

Dan Legard
Oct 26, 12:15:10 GMT-0700 2022

Marie Mosley
Oct 27, 20:28:13 GMT-0700 2022

Attachments: Payroll Roster

☐ Recording
Required?

November 1, 2022

All Departments:

October 15, 2022

ADMINISTRATIVE TEAM	3,094.44
CITY COUNCIL	4,700.00
CITY MANAGER	12,505.15
CIVIL SERVICE	8,229.45
COMMUNITY PLANNING & ECONOMIC DEVELOPMENT	32,944.91
EMPLOYEE & COMMUNITY RELATIONS	54,389.67
ENGINEERING	59,681.63
FACILITIES & GROUNDS	79,538.84
FINANCE	57,825.04
FIRE	96,378.14
LEGAL SERVICES	23,379.46
MANAGEMENT SERVICES	96,133.28
POLICE	453,244.27
Subtotal General Fund	982,044.28
STREETS	25,025.29
TRAFFIC	23,470.63
Subtotal Street Fund	48,495.92
BI-PIN	10,799.33
BUILDING SAFETY	42,987.66
COMMUNITY DEVELOPMENT	3,064.43
CRIMINAL JUSTICE	79,896.10
EQUIPMENT RENTAL	12,521.78
MEDICAL SERVICES	350,090.26
RISK MANAGEMENT	3,847.73
STORMWATER UTILITY	17,613.91
WATER & SEWER	143,739.04
Subtotal Other Funds	664,560.24
Total Salaries and Wages	1,695,100.44
<u>Benefits:</u>	
Dental Insurance	45,262.20
Industrial Insurance	43,399.70
Life Insurance	5,258.63
Long Term Disability Insurance	6,100.90
Medical Insurance	661,906.28
Medical Retirement Account	3,300.00
Retirement	125,746.96
Social Security (FICA)	96,938.72
Vision Insurance	7,231.32
WA Family Leave	2,532.15
Total Benefits	997,676.86
Grand Total	\$2,692,777.30

I, Dan Legard, Finance Director, at the direction of the Council, do hereby certify that the Payroll hereinabove specified is approved for payment in the amount of \$2,692,777.30 comprised of check numbers 75917 through 75929 and direct deposit numbers 207052 through 207513.

Approved for payment:



Dan Legard, Finance Director

Council Agenda Coversheet



Agenda Item Number	4.d.	Council Date	11/01/2022
Agenda Item Type	General Business Item		
Subject	Annexation 2022-0001- Harmony Dev LLC		
Ordinance/Reso #		Contract #	
Project #		Permit #	AZAZ-2022-0001
Department	Planning		

Consent Agenda	<input checked="" type="checkbox"/>
Ordinance/Reso	<input type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

Staff recommends that the City Council set the date for a public meeting for Annexation 2022-0001 for November 15, 2022.

Motion for Consideration

I move to set the date of November 15, 2022 for the public meeting on Annexation 2022-0001 (Harmony Development, LLC).

Summary

Harmony Development, LLC has submitted a notice of intent to petition for annexation was submitted on October 7, 2022. The area proposed to annexed includes two parcels totaling 9.37 acres. The proposed annexation area is located south of W. 10th Ave., east of Leisure Lane (a private road), west of the Hidden Estates subdivision and north of the Highland Feeder Canal.

RCW 35A.14.120 requires that a public meeting be held within 60 days to determine if the City will accept, reject, or geographically modify the proposed annexation, whether it shall require the simultaneous adoption of a proposed zoning regulation, and whether it shall require the assumption of all or of any portion of existing city indebtedness by the area to be annexed.

Alternatives

None Recommend

Fiscal Impact

None at this time.

Through

Steve Donovan
Oct 26, 12:46:50 GMT-0700 2022

Dept Head Approval

Anthony Muai
Oct 26, 14:21:29 GMT-0700 2022

City Mgr Approval

Marie Mosley
Oct 27, 20:30:04 GMT-0700 2022

Attachments:

Notice of Intent
Map

☐ Recording
Required?

NOTICE OF INTENTION TO COMMENCE ANNEXATION PROCEEDINGS

September 9, 2022

The Honorable Mayor and City Council
City of Kennewick
210 W. 6th Ave.
Kennewick, WA 99336



Dear Mayor and City Council:

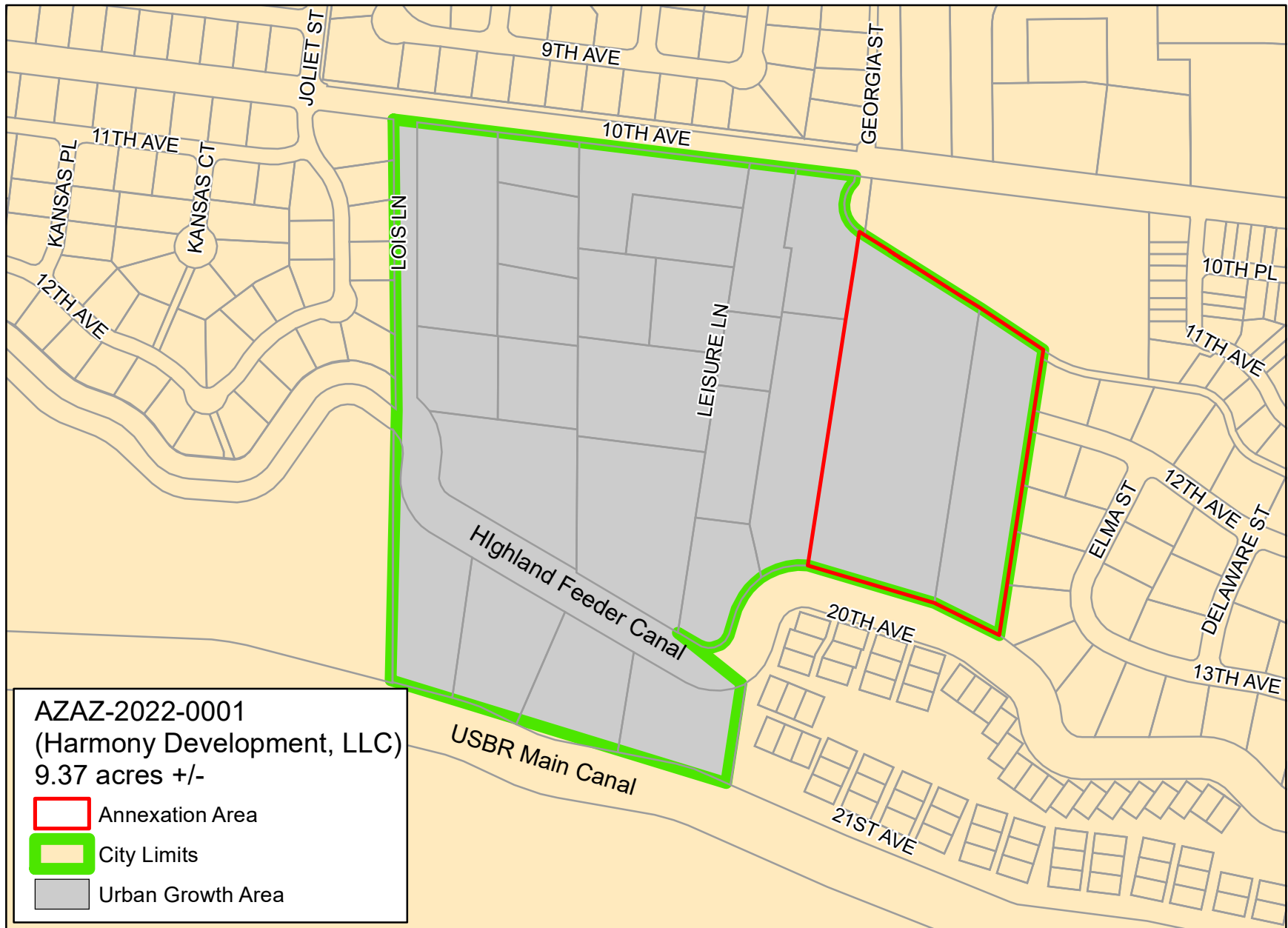
Prodigy Homes, who is the sole owner of the acreage for which annexation is sought, hereby advises the City Council of the City of Kennewick that it is the desire of Prodigy Homes, the owner of the subject property, to commence annexation proceedings:

The property herein referred to is legally described and geographically depicted on the property survey attached hereto.

It is requested that the City Council of the City of Kennewick set a date, not later than sixty (60) days after the filing of this request, for a meeting with the undersigned to determine:

1. Whether the City Council will accept, reject, or geographically modify the proposed annexation.
2. Whether the City Council will require the simultaneous adoption of the zoning for the proposed area in substantial compliance with the Comprehensive Plan as adopted by the City of Kennewick.
3. Whether the City Council will require the assumption of all or any portion of indebtedness by the area to be annexed.

 Owner Signature	<u>Jason Wilkinson</u> Printed Name	<u>107891000004001</u> Address & Tax Parcel Number	<u>9/12/2022</u> Date Signed
 _____	<u>Hasmik (Jasmine) Wilkinson</u> _____	<u>107891000004001</u> _____	<u>9/12/2022</u> _____



Council Agenda Coversheet



Agenda Item Number	4.e.	Council Date	11/01/2022
Agenda Item Type	Contract/Agreement/Lease		
Subject	Pedestrian Crossing Safety Project		
Ordinance/Reso #		Contract #	
Project #	P2111-22	Permit #	
Department	Public Works		

Consent Agenda	<input checked="" type="checkbox"/>
Ordinance/Reso	<input type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

That City Council award Contract P2111-22 Pedestrian Crossing Safety Project to Ellison Earthworks LLC in the amount of \$806,459.49

Motion for Consideration

I move to award Contract P2111-22 Pedestrian Crossing Safety Project to Ellison Earthworks LLC in the amount of \$806,459.49

Summary

Four (4) bids were received on 10/5/2022 at 10:00 a.m.

Allstar Construction Group	\$798,877.84	Engineer's Estimate	\$763,800.00
Ellison Earthworks	\$806,459.49		
ESF Solutions	\$827,312.00		
Double J Excavating	\$837,080.00		

This Contract is for the improvement of Pedestrian Crossing Safety that consists of installing and/or improving nine (9) crossings at separate identified locations city-wide. This project includes the installation of rectangular rapid flash beacons (RRFB) at each crossing, constructing curb and gutter, extruded curbing, sidewalk, concrete flat work, refuge islands, concrete curb ramps, striping, and signage. There will be some storm drainage catch basin and manhole adjustments and minor landscaping and irrigation repair and restoration, and other work. Originally at time of application for funding, we had scoped fifteen (15) RRFBs for this contract work. Due to the high inflation in construction costs that we've seen lately, we were forced to remove six sites in order to make our budget.

State law requires that we award contracts to a responsible bidder with the lowest responsive bid. WSDOT reviewed the bid packages and determined that Allstar did not submit a responsive bid due to inconsistencies with their DBE requirements. The three other bidders were determined to be responsive. We are recommending award of this project to Ellison Earthworks, who both WSDOT and City staff have determined to be a responsive bidder with the lowest responsive bid.

Alternatives

Not award the contract.

Fiscal Impact

This funding is 100% reimbursable through a Federal Safety Grant, but with a maximum contract amount of \$763,800. This creates an overage of \$42,659. In order to meet budget, scope will be reduced by removing one (1) of the nine (9) proposed sites and modifying a second site(see attached Memo).

Through

Heath Mellotte
Oct 12, 08:48:04 GMT-0700 2022

Dept Head Approval

Cary Roe
Oct 17, 10:10:13 GMT-0700 2022

City Mgr Approval

Marie Mosley
Oct 27, 20:34:46 GMT-0700 2022

Attachments:

Map
Memo

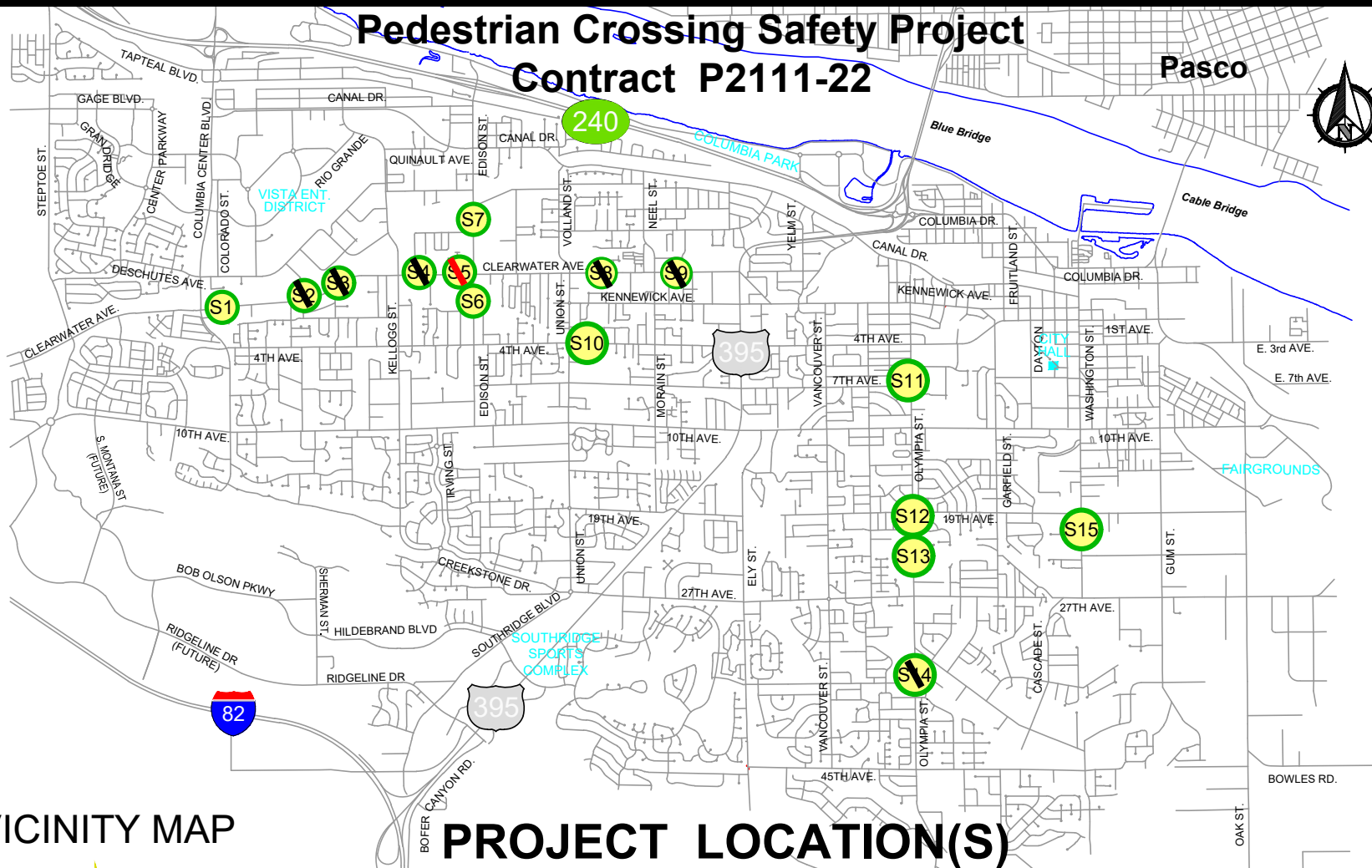
☐ Recording
Required?

Pedestrian Crossing Safety Project Contract P2111-22

Pasco



Richland



VICINITY MAP



PROJECT LOCATION(S)

Location & Description Key:

- | | | |
|--|---|--|
| S1 W. CLEARWATER AVE & N. COLORADO ST | S7 N. EDISON ST & KAMIAKIN HIGH SCHOOL | S13 S. OLYMPIA ST & W. 22ND PL |
| S2 W. CLEARWATER AVE & N. TAFT ST (NIC) | S8 W. CLEARWATER AVE. EAST of N. UNION ST. (NIC) | S14 S. OLYMPIA ST & W. 36TH AVE (NIC) |
| S3 W. CLEARWATER AVE & N. QUAY ST (NIC) | S9 W. CLEARWATER AVE & N. JOHNSON ST (NIC) | S15 WASHINGTON ST. & W. 21ST AVE |
| S4 W. CLEARWATER AVE & N. IRVING PL (NIC) | S10 W. 4TH AVE & S. TWEEDT ST. | |
| S5 W. CLEARWATER AVE & N. FILLMORE ST | S11 S. OLYMPIA ST & W. 7TH AVE | |
| S6 N. EDISON ST & ELECTRICIANS UNION HALL | S12 S. OLYMPIA ST & W. 19TH AVE | |

**NOTE: SITES 2, 3, 4, 8, 9
and 14 ARE NOT IN
CONTRACT (NIC)**



Memorandum

Public Works

To: Cary M. Roe, P.E.
Public Works Director

From: Heath Mellotte, PE *HM*
Design Services Manager

Date: 10/7/22

Subject: P2111-22 Pedestrian Crossing Safety Project
Scope Reduction

The apparent low bid for the P2111-22 Pedestrian Crossing Safety Budget came in at \$806,459.49. The project budget is \$763,800.00. This leaves the City with a difference of \$42,659.49. The Public Works Director directed the Transportation Manager and Engineering to evaluate the plans and bid to reduce scope in order to meet budget.

The following scope reduction is recommended to reduce project cost in order to meet budget:

1. Site 13 Instead of replacing the two existing telespar-mounted RRFB's on Olympia with new RRFB's on 4-inch breakaway posts, just relocate these telespar-mounted RRFB's to the proposed new locations. (Cost Reduction: **\$27,721**)
2. Site 5 In the opinion of the Transportation Manager, this site is the least on the priority list since it is not near a school, and the Edison and Clearwater signalized intersection was nearby to use as a crossing. Therefore, the decision was made to remove this site in it's entirety. (Cost Reduction: **\$79,011**)

This reduction can be accomplished per Section 1-04.4 Changes of the WSDOT Standard Specifications which allows agencies to increase or decrease quantities by 25% without triggering a change order.

Contract amount would be revised to **\$699,727** which would allow a **9%** contingency.

Acknowledgement:

Cary M. Roe
Cary M. Roe PE, Public Works Dir.

Rvwd. By: SJ

SS
10/7/2022

				ELLISON EARTHWORKS				
ITEM	ITEM WITH UNIT PRICE BID	CONTRACT QTY	UNIT	UNIT PRICE	TOTAL	REVISED QTY	REVISED TOTAL	% CHANGED
						Quantities revised by removing Site 5 in its entirety and also not replacing two existing RRFBs at Site 13 that are on telespar poles.		
	SCHEDULE A - STREET (Section 2)							
1	MOBILIZATION	1	LS	\$ 48,381.30	\$ 48,381.30	1	\$ 48,381.30	0%
2	SPCC PLAN	1	LS	\$ 1,129.57	\$ 1,129.57	1	\$ 1,129.57	0%
3	ESC LEAD	18	PER DAY	\$ 112.96	\$ 2,033.28	18	\$ 2,033.28	0%
4	PROJECT TEMPORARY TRAFFIC CONTROL (Min. Bid \$25,000)	1	LS	\$ 32,907.17	\$ 32,907.17	1	\$ 32,907.17	0%
5	FLAGGERS & SPOTTERS (Minimum Bid \$41.94)	540	HRS	\$ 60.94	\$ 32,907.60	480	\$ 29,251.20	-11%
6	PEDESTRIAN TRAFFIC CONTROL (Includes Pedestrian Access Route Plan)	1	LS	\$ 8,360.27	\$ 8,360.27	1	\$ 8,360.27	0%
7	PROJECT MAINTENANCE	1	LS	\$ 11,408.31	\$ 11,408.31	1	\$ 11,408.31	0%
8	CONSTRUCTION SURVEY STAKING	1	LS	\$ 5,637.57	\$ 5,637.57	1	\$ 5,637.57	0%
9	ADA FEATURES SURVEYING	1	LS	\$ 13,325.16	\$ 13,325.16	1	\$ 13,325.16	0%
10	SEQUENTIAL ARROW SIGN	225	HRS	\$ 7.74	\$ 1,741.50	225	\$ 1,741.50	0%
11	CLEARING AND GRUBBING (Includes Sod Removal and Tree Trimming)	1	LS	\$ 21,315.57	\$ 21,315.57	1	\$ 21,315.57	0%
12	REMOVING ASPHALT CONC. PAVEMENT (Includes Saw Cutting)	245	SY	\$ 44.01	\$ 10,782.45	198	\$ 8,713.98	-19%
13	CURB AND GUTTER REMOVAL (Includes Saw Cutting)	425	LF	\$ 13.13	\$ 5,580.25	379	\$ 4,976.27	-11%
14	SIDEWALK /PED. RAMP REMOVAL (Includes Saw Cutting and Stairs/Railing)	308	SY	\$ 29.50	\$ 9,086.00	276	\$ 8,142.00	-10%
15	CONCRETE CURB AND GUTTER	425	LF	\$ 63.92	\$ 27,166.00	379	\$ 24,225.68	-11%
16	TYPE E-1 CONCRETE CURB	444	LF	\$ 48.90	\$ 21,711.60	356	\$ 17,408.40	-20%
17	CONCRETE PEDESTRIAN CURB	341	LF	\$ 38.13	\$ 13,002.33	305	\$ 11,629.65	-11%
18	CONCRETE SIDEWALK (4-Inch Depth)	252	SY	\$ 121.29	\$ 30,565.08	216	\$ 26,198.64	-14%
19	REMOVE AND REINSTALL BLOCK RETAINING WALL	21	LF	\$ 141.73	\$ 2,976.33	21	\$ 2,976.33	0%
20	TOP COURSE (2-Inches)	49	TON	\$ 197.80	\$ 9,692.20	44	\$ 8,703.20	-10%
21	2-INCH COLD MIX PATCHING	56	SY	\$ 80.36	\$ 4,500.16	46	\$ 3,696.56	-18%
22	3.5-Inch HMA PATCHING	56	SY	\$ 118.02	\$ 6,609.12	46	\$ 5,428.92	-18%
23	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes 4.0"d Concrete Sidewalk Transitions)	17	EA	\$ 2,448.13	\$ 41,618.21	15	\$ 36,721.95	-12%
24	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes Stairs, Hand Railing and Fence Restoration)	1	EA	\$ 11,749.02	\$ 11,749.02	1	\$ 11,749.02	0%
25	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes Mod. Retaining Wall and Sidewalk Transitions)	1	EA	\$ 7,051.47	\$ 7,051.47	0	\$ -	-100%
26	24-Inch x 48-Inch DETECTABLE WARNING SURFACE	37	EA	\$ 230.63	\$ 8,533.31	31	\$ 7,149.53	-16%
27	24-Inch x 60-Inch DETECTABLE WARNING SURFACE	4	EA	\$ 256.25	\$ 1,025.00	4	\$ 1,025.00	0%
28	24-Inch X 120-Inch CROSSWALK BLOCKS - PREFORMED THERMOPLASTIC	63	EA	\$ 182.89	\$ 11,522.07	53	\$ 9,693.17	-16%
29	ADJUST SD CATCH BASIN	2	EA	\$ 465.98	\$ 931.96	2	\$ 931.96	0%

ITEM	ITEM WITH UNIT PRICE BID	CONTRACT QTY	UNIT	ELLISON EARTHWORKS		REVISED QTY	REVISED TOTAL	% CHANGED
				UNIT PRICE	TOTAL			
30	ADJUST VALVE BOX	1	EA	\$ 605.03	\$ 605.03	1	\$ 605.03	0%
31	RECTANGULAR RAPID FLASH BEACON SYSTEM (Solar)	25	EA	\$ 13,225.31	\$ 330,632.75	20	\$ 264,506.20	-20%
32	RECTANGULAR RAPID FLASH BEACON SYSTEM (Solar) Mounted on St. Light Pole	1	EA	\$ 9,284.77	\$ 9,284.77	1	\$ 9,284.77	0%
33	PEDESTRIAN PUSHBUTTON, POST and BASE	1	EA	\$ 6,016.00	\$ 6,016.00	1	\$ 6,016.00	0%
34	REMOVAL OF PAVEMENT MARKINGS	1	LS	\$ 7,678.16	\$ 7,678.16	1	\$ 7,678.16	0%
35	REMOVE AND REINSTALL SIGNAGE, POST and BASE	3	EA	\$ 564.18	\$ 1,692.54	5	\$ 2,820.90	67%
36	REMOVE AND DISPOSE EXISTING SIGN AND POST	10	EA	\$ 212.39	\$ 2,123.90	10	\$ 2,123.90	0%
37	REMOVE AND SALVAGE EXISTING RRFB (SOLAR), POST and BASE	2	EA	\$ 1,199.69	\$ 2,399.38	0	\$ -	-100%
38	OBJECT MARKER SIGN: OM3-L 12-Inch x 36-Inch	10	EA	\$ 140.47	\$ 1,404.70	8	\$ 1,123.76	-20%
39	IMPACT RECOVERY SYSTEM (IRS) with DRIVABLE BASE	9	EA	\$ 332.61	\$ 2,993.49	7	\$ 2,328.27	-22%
40	LANDSCAPE AND SITE RESTORATION (Includes Sod, Fence Rest. & Irrigation Repair)	1	LS	\$ 38,378.91	\$ 38,378.91	1	\$ 38,378.91	0%
	PROJECT TOTAL				\$ 806,459.49		\$ 699,727.06	
	CONSTRUCTION BUDGET	\$ 763,800.00						

SUMMARY OF QUANTITIES												
ITEM NO.	TOTAL QUANTITY	UNIT	ITEM	SHEET 3	SHEET 4	SHEET 4	SHEET 4	SHEET 5	SHEET 5	SHEET 5	SHEET 6	SHEET 6
				SITE 1	SITE 5	SITE 6	SITE 7	SITE 10	SITE 11	SITE 12	SITE 13	SITE 15
1	LUMP SUM	L.S.	MOBILIZATION	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.
2	LUMP SUM	L.S.	SPCC PLAN	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.
4	LUMP SUM	L.S.	PROJECT MAINTENANCE	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.
5	540	HRS	FLAGGERS AND SPOTTERS (Min. Bid \$41.94 PER Hour)	60	60	60	60	60	60	60	60	60
11	LUMP SUM	L.S.	CLEARING AND GRUBBING (Includes Sod Removal and Tree Trimming)	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.
12	245	S.Y.	REMOVING ASPHALT CONC. PAVEMENT (Includes Saw Cutting)	47	47	47	0	0	47	10	47	0
13	425	L.F.	CURB AND GUTTER REMOVAL (Includes Saw Cutting)	36	46	36	0	56	36	53	132	30
14	308	S.Y.	SIDEWALK/PED RAMP REMOVAL (Includes saw cutting)	32	32	32	6	42	32	25	83	24
15	425	L.F.	CONCRETE CURB AND GUTTER	36	46	36	0	56	36	53	132	30
16	444	L.F.	TYPE E-1 CONCRETE CURB	88	86	88	4	0	88	0	88	0
17	341	L.F.	CONCRETE PEDESTRIAN CURB	36	36	36	5	36	36	34	84	36
18	252	S.Y.	CONCRETE SIDEWALK 4" DEPTH	36	36	36	6	10	36	13	69	10
19	21	L.F.	REMOVE AND REINSTALL BLOCK RETAINING WALL	0	0	0	0	0	0	0	0	21
20	49	TON	TOP COURSE (2-INCHES)	5	5	5	5	5	5	5	10	4
21	56	S.Y.	2-INCH COLD MIX PATCHING	10	10	10	1	0	10	5	10	0
22	56	S.Y.	3.5-INCH HMA PATCHING	10	10	10	1	0	10	5	10	0
23	17	EACH	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes 4.0" Concrete Sidewalk Transitions)	2	2	2	0	2	2	2	4	1
24	1	EACH	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes Stairs, Hand Railing and Fence Restoration)	0	0	0	1	0	0	0	0	0
25	1	EACH	PARALLEL CURB RAMP TYPE A - MODIFIED (Includes Mod. Retaining Wall and Sidewalk Transitions)	0	1	0	0	0	0	0	0	0
26	37	EACH	DETECTABLE WARNING SURFACE 24" x 48"	6	6	6	0	2	6	2	8	1
27	4	EACH	DETECTABLE WARNING SURFACE 24" x 60"	0	0	0	2	0	0	0	0	2
28	63	EACH	24-INCH x 120-INCH CROSSWALK BLOCKS - PREFORMED THERMOPLASTIC	10	10	10	0	7	6	7	13	0
29	2	EACH	ADJUST SD CATCH BASIN	0	0	0	0	0	0	0	2	0
30	1	EACH	ADJUST VALVE BOX	0	0	0	0	0	0	1	0	0
31	20	EACH	RECTANGULAR RAPID FLASH BEACON SYSTEM (Solar)	3	3	2	3	2	3	2	3	2
32	1	EACH	RECTANGULAR RAPID FLASH BEACON SYSTEM (Solar) Mounted on St. Light Pole	0	0	1	0	0	0	0	0	0
33	1	EACH	PEDESTRIAN PUSHBUTTON, POST and BASE	0	0	1	0	0	0	0	0	0
34	LUMP SUM	L.S.	REMOVAL OF PAVEMENT MARKINGS	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.
35	5	EACH	REMOVE AND REINSTALL SIGN AND POST	0	0	0	0	0	0	1	3	1
36	10	EACH	REMOVE AND DISPOSE SIGN AND POST	0	0	0	4	2	0	0	2	2
37	2	EACH	REMOVE AND SALVAGE EXISTING RRFB (SOLAR), POST and BASE	0	0	0	0	0	0	0	0	0
38	10	EACH	OBJECT MARKER SIGN: OM3-L 12-Inch x 36-Inch	2	2	2	0	0	2	0	2	0
39	9	EACH	IMPACT RECOVERY SYSTEM (IRS) with DRIVABLE BASE	2	2	1	0	0	2	0	2	0
40	LUMP SUM	L.S.	LANDSCAPE AND SITE RESTORATION (Includes Sod, Top Soil & Irrigation Repair)	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.	L.S.

PROPOSAL STATES "2"

- MET WITH TRANSPORTATION MANAGER ON 10/6/22. DECISION WAS MADE TO REMOVE SITE 5 IN ORDER TO MEET BUDGET. ALSO DECIDED TO NOT REPLACE EXISTING RRFB'S AT SITE 13 ON OLYMPIA (CURRENTLY ON TELSPARS) WITH NEW POLE CONFIGURATION W/BASES.

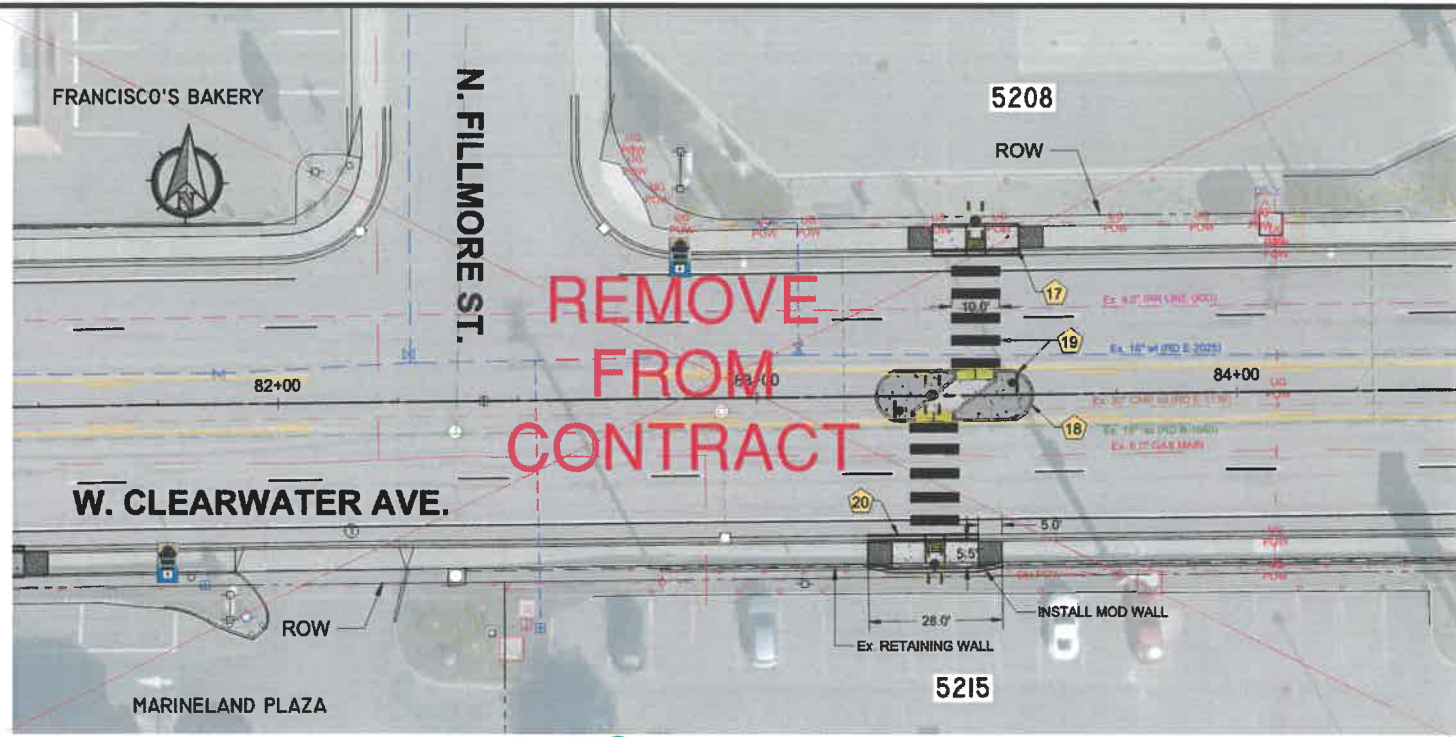
TA-_____	REGION NUMBER	STATE	FEDERAL AID PROJECT NUMBER
	10	WA	HSIP-000S(588)

Drawing Plot / Print Date: 8/2/2022

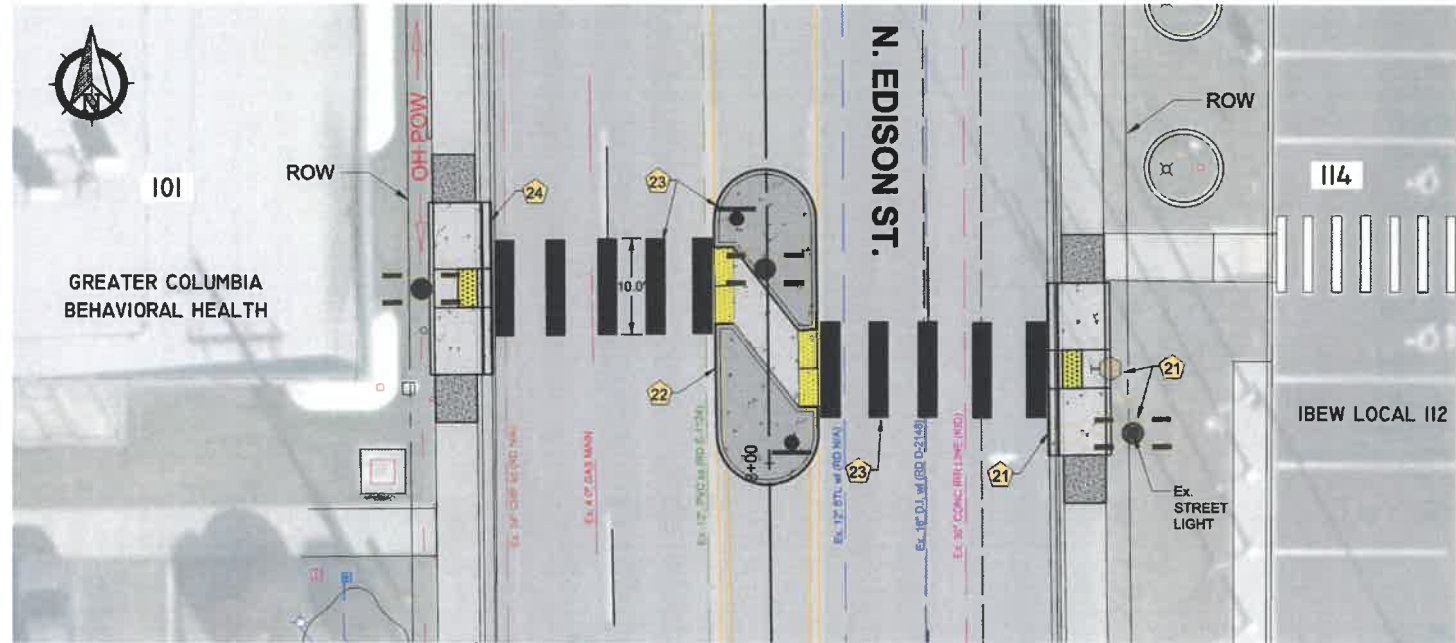


KENNEWICK
WASHINGTON
PEDESTRIAN
CROSSING SAFETY
SUMMARY of QUANTITIES
PROJECT ACCOUNTING #: P2111-22

DATE 7/27/22
DWN JDS/RJD
DES JDS/RJD
CHK SJ
SHEET 02 OF 09
DWG. NO. A-1141



SITE 5 **S5** WEST OF FILLMORE SCALE: 1" = 20'



SITE 6 **S6** ELECTRICIANS UNION HALL SCALE: 1" = 10'

SITE 5 **S5 SCHEDULE:**

- 17** SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of Ex. CONCRETE SIDEWALK
18 LF (Total) of Ex. CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
INCLUDES 6 SY (Total) of 4.0" CONCRETE SIDEWALK
TRANSITION, SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

- 18** RAISED REFUGE ISLAND:
SAW CUT, REMOVE & DISPOSE
47 SY (35' x 12' MIN.) of Ex. ACP
INSTALL: 88 LF (Total) of E-1 CONCRETE CURBING,
36 SY of 4.0" CONCRETE, "BROOM" FINISH
(INCLUDES 2.0" CSTC UNDER CONCRETE)
SEE DETAIL ON SHEET 7.
(4) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

- 19** STRIPING and SIGNAGE, INSTALL:
10 EA of 24" x 120" CROSSWALK BLOCK
2 EA of OM3-L, IRS HAZARD MARKER WITH
DRIVABLE BASE, SEE SHEET 7.

- 20** SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of CONCRETE SIDEWALK
28 LF (Total) of CURB and GUTTER
INSTALL: 28 LF (Total) of CURB and GUTTER,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
INCLUDES 29 LF (Total) of MOD. RETAINING WALL
(0.90h) and 6 SY (Total) of 4.0" CONCRETE SIDEWALK
TRANSITION, SEE TYP. DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.

SITE 6 **S6 SCHEDULE:**

- 21** SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of CONCRETE SIDEWALK
18 LF (Total) of CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
INCLUDES 6 SY (Total) of 4.0" CONCRETE SIDEWALK
TRANSITION, SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) RRFB SIGNAL SYSTEM [SOLAR] and
(1) PED. PUSHBUTTON, POST and BASE (PPB)

- 22** RAISED REFUGE ISLAND:
SAW CUT, REMOVE & DISPOSE
47 SY of Ex. ACP (12" MIN from NEW CURB)
INSTALL: 88 LF (Total) of E-1 CONCRETE CURBING,
36 SY of 4.0" CONCRETE, "BROOM" FINISH
(INCLUDES 2.0" CSTC UNDER CONCRETE)
SEE DETAIL ON SHEET 7.
(4) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

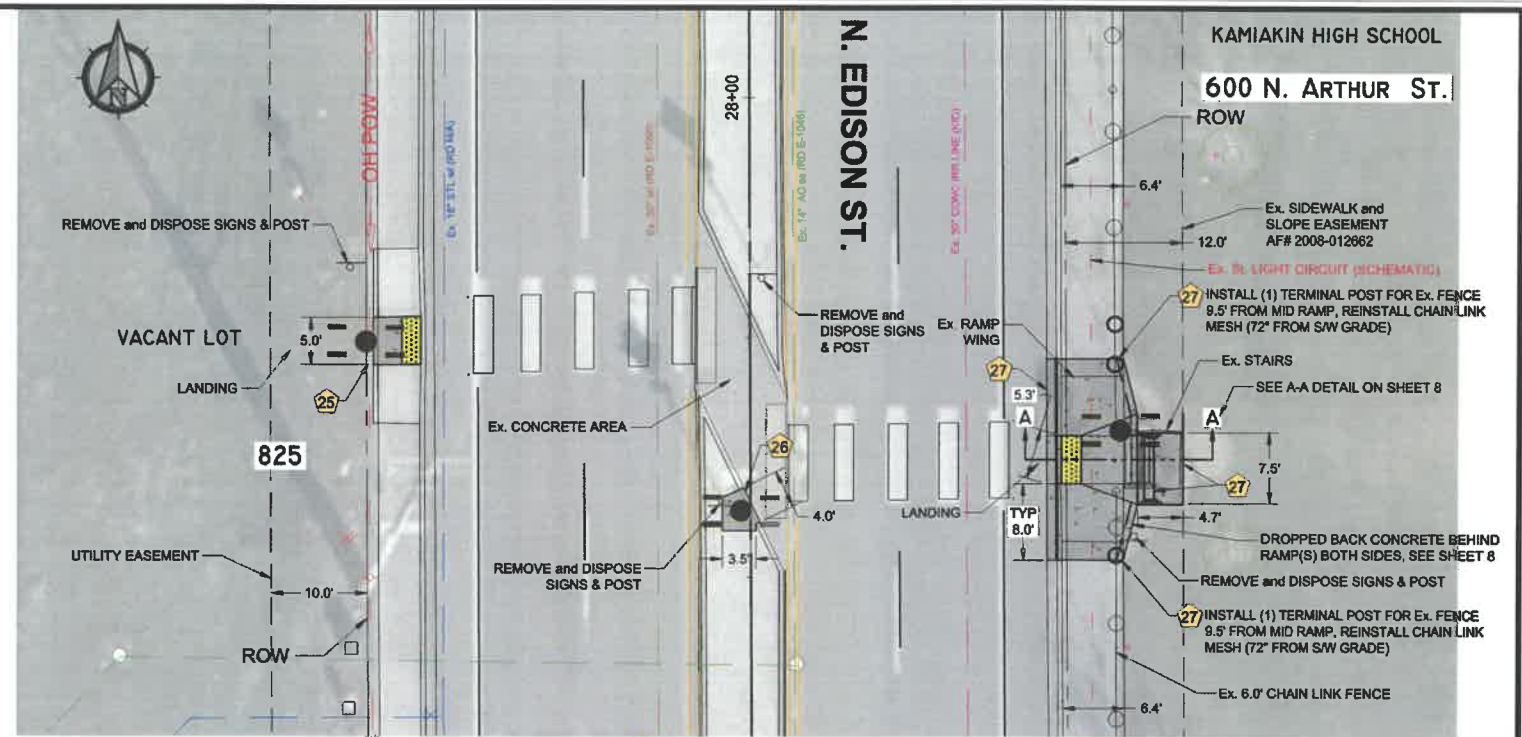
- 23** STRIPING and SIGNAGE, INSTALL:
10 EA of 24" x 120" CROSSWALK BLOCK
2 EA of OM3-L, IRS HAZARD MARKER WITH
DRIVABLE BASE, SEE SHEET 7.
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.

SITE 7 **S7 SCHEDULE:**

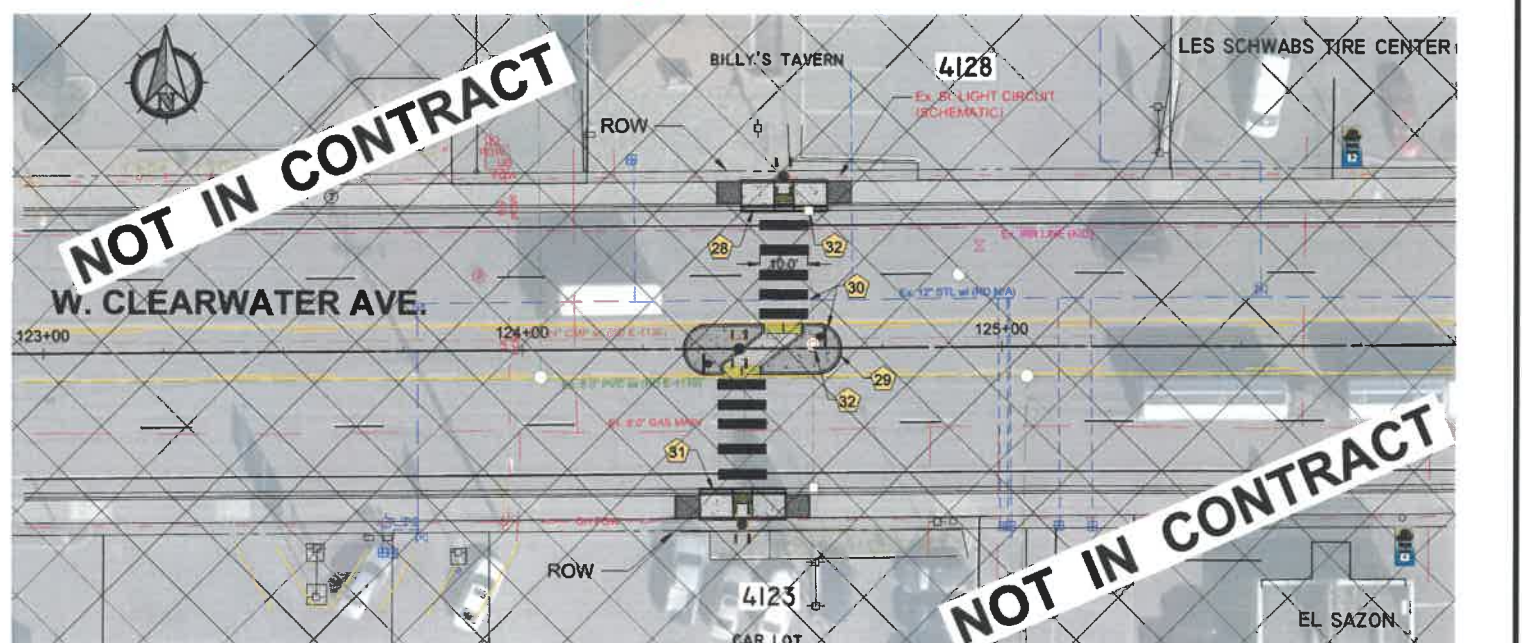
- 25** SAW CUT, REMOVE & DISPOSE:
3 SY of Ex. CONCRETE SIDEWALK
5 LF of PEDESTRIAN CURB
INSTALL: 3 SY of CONCRETE SIDEWALK,
5 LF (Total) of PEDESTRIAN CURBING,
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 60" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

- 26** RAISED REFUGE ISLAND (Existing):
SAW CUT, REMOVE & DISPOSE:
2 SY of Ex. CONCRETE SIDEWALK & Ex. ACP
INSTALL: 2 SY of CONCRETE SIDEWALK,
4 LF (Total) of E-1 CONCRETE CURBING (ISLAND)
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

- 27** SAW CUT, REMOVE & DISPOSE:
15 SY of Ex. CONCRETE SIDEWALK,
(1) Ex. CONCRETE STAIRS and HAND RAILING
INSTALL: (1) EA "PARALLEL CURB RAMPS TYPE A
(MODIFIED), INCLUDES DROPPED BACK CONCRETE,
STAIRS and HAND RAILINGS", 3 SY of 4.0" CONCRETE
STAIR LANDING PAD,
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
REMOVE 14 LF of Ex. 6.0" CHAIN LINK FENCE, INSTALL
(2) FENCE TERMINAL POSTS and FOUNDATION
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.
DWS: DETECTABLE WARNING SURFACE



SITE 7 **S7** KAMIAKIN HIGH SCHOOL SCALE: 1" = 10'



SITE 8 **S8** EAST OF UNION SCALE: 1" = 20'

SITE 8 **S8 SCHEDULE:**

- 28** SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of Ex. CONCRETE SIDEWALK
18 LF (Total) of Ex. CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
INCLUDES 6 SY (Total) of 4.0" CONCRETE SIDEWALK
TRANSITION, SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

- 29** RAISED REFUGE ISLAND:
SAW CUT, REMOVE & DISPOSE
47 SY (35' x 12' MIN.) of Ex. ACP
INSTALL: 88 LF (Total) of E-1 CONCRETE CURBING,
36 SY of 4.0" CONCRETE, "BROOM" FINISH
(INCLUDES 2.0" CSTC UNDER CONCRETE)
SEE DETAIL ON SHEET 7.
(4) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)




- 30** STRIPING and SIGNAGE, INSTALL:
10 EA of 24" x 120" CROSSWALK BLOCK
2 EA of OM3-L, IRS HAZARD MARKER WITH
DRIVABLE BASE, SEE SHEET 7.

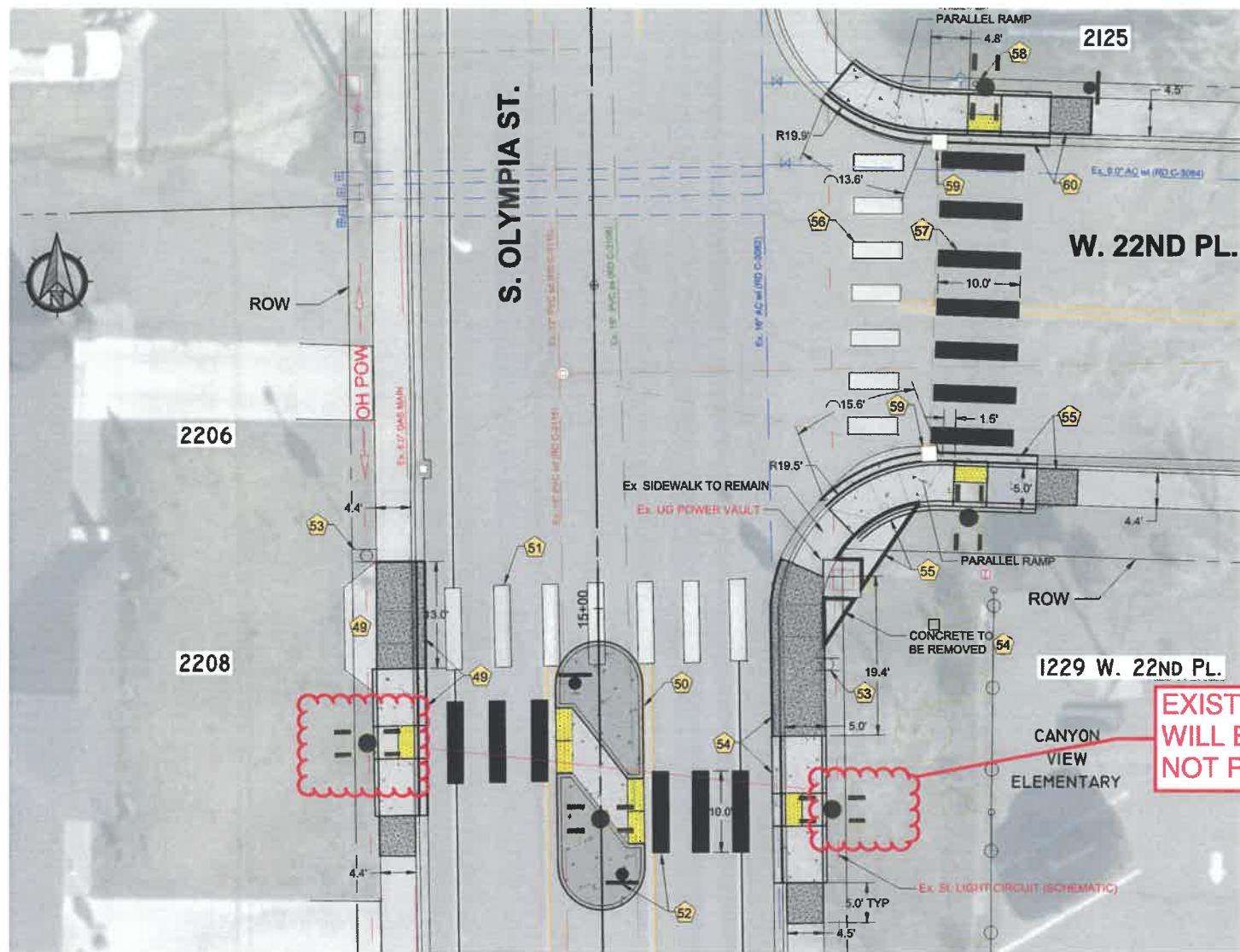
- 31** SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of CONCRETE SIDEWALK
18 LF (Total) of CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
INCLUDES 6 SY (Total) of 4.0" CONCRETE SIDEWALK
TRANSITION, SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.

LANDSCAPE RESTORATION NOTE:
SEE SHEET 1 FOR NOTE.

PED. CROSSING DATA INFORMATION
SEE SHEET 9

RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM NOTE:
SOLAR INSTALL: NEW RECTANGULAR RAPID FLASHING AMBER BEACON (RRFB) SYSTEM. ONE (1) SYSTEM INCLUDES:
(1) RRFB-XL LIGHT BARS WITH HARNESS (LIGHTS ARE TO BE BI-DIRECTIONAL AND SIDE-EMITTING), (1) 50 WATT SOLAR
PANEL WITH HARNESS, (1) CONTROLLER BOX(S) WITH BATTERY (55AH), (1) 4-1/2" x 14" POLE SPUN ALUMINUM SCH. 40, (1)
PEDESTAL BASE ALUMINUM SQUARE WITH DOOR HATCH. INSTALL: (1) FIXED BASE FOUNDATION WITH 3/4" #10 ANCHOR
BOLT KIT (GALV. - SEE FOUNDATION DETAIL). INSTALL: (1) POLARA XAVZE-LED YELLOW HOUSING PUSHBUTTON ASSEMBLY
WITH R10-25 5-INCH x 7-INCH SIGN TO BE MOUNTED ADJACENT TO RAMP and PUSHBUTTON FRAME EXTENDER (8" or 12"),
AND INSTALL: (1) W11-501, 12-INCH x 12-INCH, "CROSS TRAFFIC MAY NOT STOP USE CAUTION WHEN CROSSING", AND ALL
ASSOCIATED HARDWARE AS NEEDED TO COMPLETE THE SYSTEM. INSTALL: (2) W11-2 AND W16-7P (1-LEFT AND 1-RIGHT
ARROWS) SIGNS AND LIGHTS ON BOTH SIDES OF POLE. ALL POLE MOUNTING HARDWARE SHALL BE BAND-IT OR APPROVED
EQUAL. SEE DETAIL SHEET 7 and SHEET 8 FOR RRFB INFORMATION.
SITE #7 SCHOOL CROSSING SIGNAGE EACH POLE: INSTALL: (2) S1-1 AND W16-7P (1-LEFT AND 1-RIGHT ARROWS) SIGNS AND
LIGHTS ON BOTH SIDES OF POLE. ALL POLE MOUNTING HARDWARE SHALL BE BAND-IT OR APPROVED EQUAL. SEE DETAIL
SHEET 7 FOR RRFB INFORMATION.

TA-_____	REGION NUMBER	STATE	FEDERAL AID PROJECT NUMBER
	10	WA	HSIP-000S(588)
 KENNEWICK ENGINEERING DIVISION	 CITY OF KENNEWICK WASHINGTON	 KENNEWICK WASHINGTON PEDESTRIAN CROSSING SAFETY Sites 5 thru 8 PROJECT ACCOUNTING #: P2111-22	DATE 7/27/22 OWN JDS/RJD DES JDS/RJD CHK SJ
			SHEET 04 OF 09 DWG. NO. A-1141



SITE 13 S13 CANYON VIEW ELEMENTARY SCALE: 1" = 10'

SITE 13 S13 SCHEDULE:

- 49 SAW CUT, REMOVE & DISPOSE:
24 SY (Total) of Ex. CONCRETE SIDEWALK / CURB RAMP
31 LF (Total) of Ex. CURB and GUTTER
INSTALL: 31 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
10 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 6 SY of SOD with 4.0" of TOP SOIL
ADJUST IRRIGATION IF REQUIRED.
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 50 RAISED REFUGE ISLAND:
SAW CUT, REMOVE & DISPOSE:
47 SY (35' x 12' MIN.) of Ex. ACP
INSTALL: 88 LF (Total) of E-1 CONCRETE CURBING,
36 SY of 4.0" CONCRETE, "BROOM" FINISH
(INCLUDES 2.0" CSTC UNDER CONCRETE)
SEE DETAIL ON SHEET 7.
(4) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 51 STRIPING and SIGNAGE:
REMOVE 7 EA of Ex. CROSSWALK BLOCKS
INSTALL: 7 EA of 24" x 120" CROSSWALK BLOCK
REMOVE & REINSTALL:
(1) Ex. STOP SIGN, POST & BASE
REMOVE, REINSTALL and ADJUST:
2 EA Ex. CATCH BASIN FRAME & GRATE
SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of Ex. CONCRETE SIDEWALK / CURB RAMP
29 LF (Total) of Ex. CURB and GUTTER
INSTALL: 29 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
18 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 13.6' RAMP (west) and 3 SY of 4.0"
CONCRETE SIDEWALK TRANSITION, SEE DETAIL ON
SHEET 7.
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.
- 52 REMOVE and SALVAGE (2) Ex. SIGN & RRFB's
SAW CUT, REMOVE & DISPOSE:
24 SY (Total) of Ex. CONCRETE SIDEWALK / CURB RAMP
40 LF (Total) of Ex. CURB and GUTTER
INSTALL: 40 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
18 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)

RELOCATE EXISTING
TELESPAR RRFB'S

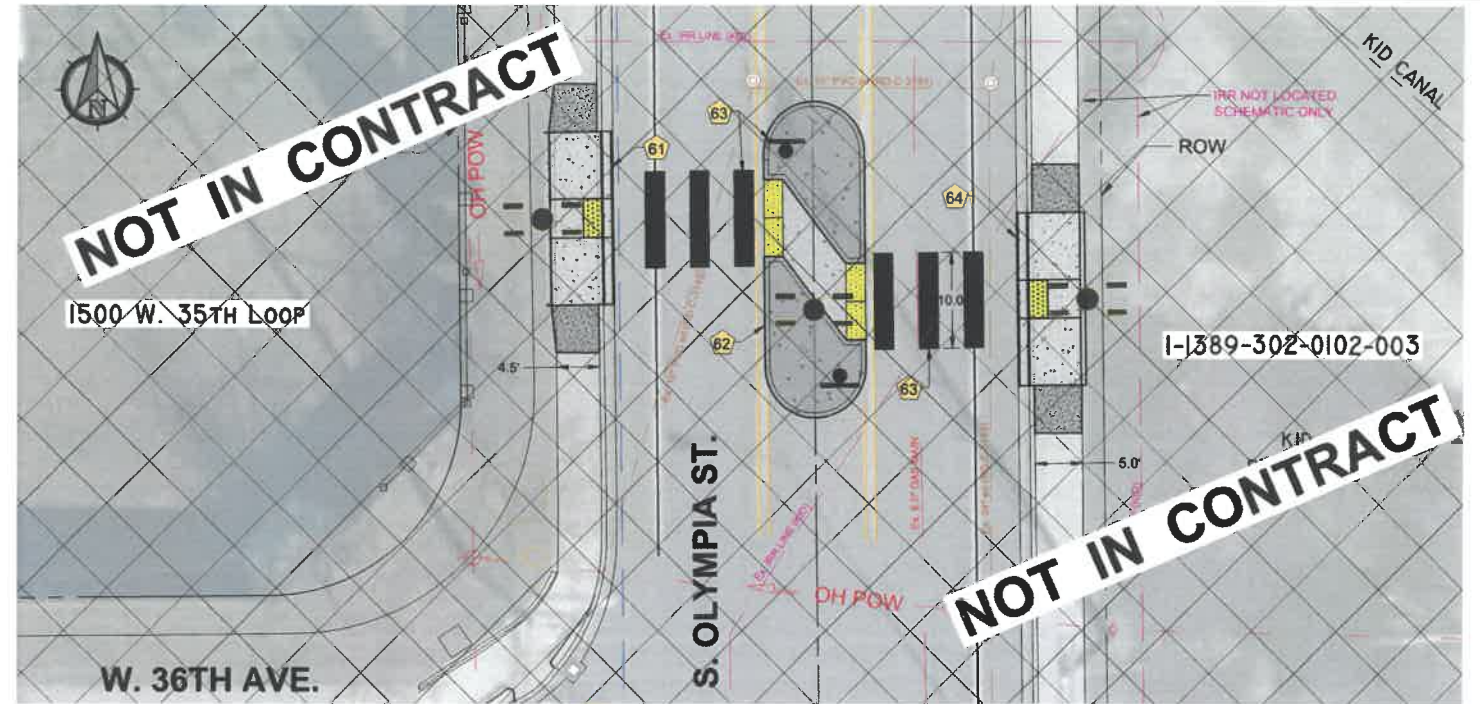
SITE 14 S14 SCHEDULE:

- 61 SAW CUT, REMOVE & DISPOSE:
18 SY (Total) of Ex. CONCRETE SIDEWALK
18 LF (Total) of Ex. CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
10 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 62 RAISED REFUGE ISLAND:
SAW CUT, REMOVE & DISPOSE:
47 SY (35' x 12' MIN.) of Ex. ACP
INSTALL: 88 LF (Total) of E-1 CONCRETE CURBING,
36 SY of 4.0" CONCRETE, "BROOM" FINISH
(INCLUDES 2.0" CSTC UNDER CONCRETE)
SEE DETAIL ON SHEET 7.
(4) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 63 STRIPING and SIGNAGE, INSTALL:
6 EA of 24" x 120" CROSSWALK BLOCK
2 EA of OM3-L IRS HAZARD MARKER WITH
DRIVABLE BASE, SEE SHEET 7.
SAW CUT, REMOVE & DISPOSE:
16 SY (Total) of CONCRETE SIDEWALK
18 LF (Total) of CURB and GUTTER
INSTALL: 18 LF (Total) of CURB and GUTTER,
18 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
18 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.

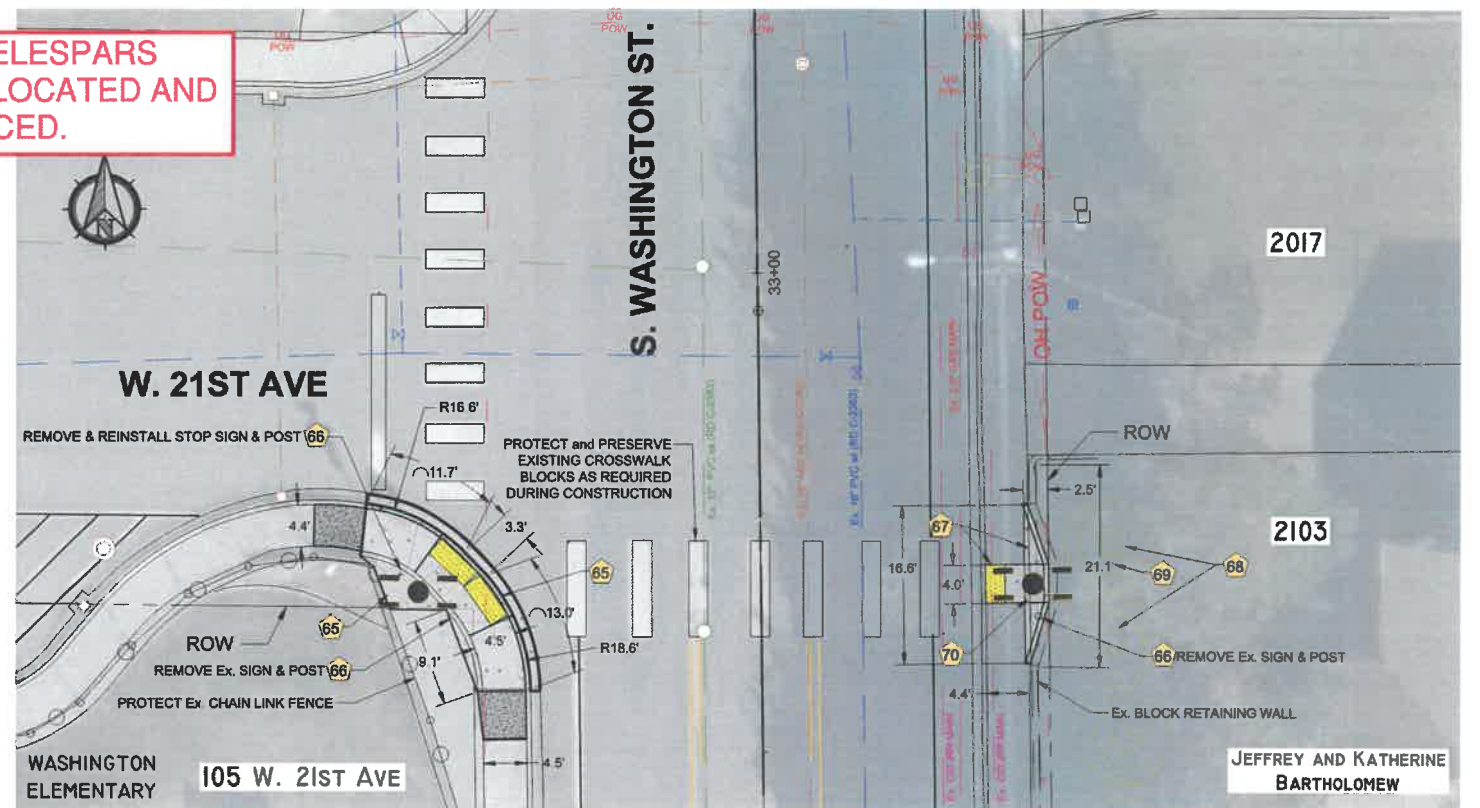
SITE 15 S15 SCHEDULE:

- 65 SAW CUT, REMOVE & DISPOSE:
22 SY (Total) of Ex. CONCRETE SIDEWALK / CURB RAMP
30 LF (Total) of Ex. CURB and GUTTER
INSTALL: 30 LF (Total) of CURB and GUTTER,
21 LF (Total) of PEDESTRIAN CURBING,
(1) EA "PARALLEL CURB RAMPS TYPE A (MODIFIED)",
10 SY of CONCRETE SIDEWALK and TRANSITION,
SEE DETAIL ON SHEET 7.
(INCLUDES 2.0" CSTC UNDER ALL CONCRETE)
INSTALL: 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 66 SIGNAGE:
REMOVE & REINSTALL:
(1) Ex. STOP SIGN, POST & BASE
REMOVE & DISPOSE
(2) Ex. SCHOOL SIGN & POST
SAW CUT, REMOVE & DISPOSE:
2 SY (Total) of CONCRETE SIDEWALK
INSTALL: 5 SY (Total) of CONCRETE SIDEWALK,
(INCLUDES 2.0" CSTC UNDER CONCRETE), POUR
CONCRETE AGAINST REINSTALLED BLOCK WALL.
17 LF (Total) of PEDESTRIAN CURBING,
(1) 24" x 48" DETECTABLE WARNING SURFACE.
(1) "RRFB SIGNAL SYSTEM [SOLAR], PER POLE (EA)
- 67 TREE TRIMMING:
TRIM LIMBS AND BRANCHES ABOVE "RRFB"
STANDARD POLE FOR VISIBILITY AND 14' ABOVE
ROADWAY AND SIDEWALK ON THE EAST SIDE OF THE
EX. CROSSING.
REMOVE & REINSTALL: 21 LF of Ex. BLOCK
RETAINING WALL BEHIND Ex. RAMP TO
RETROFITTED WITH ADDITIONAL CONCRETE.
NOTE: ADDITIONAL BLOCKS MAY BE REQUIRED,
INSTALL LIKE-KIND.
*SEE SOLAR RRFB SYSTEM NOTE THIS SHEET.

EXISTING TELESPARS
WILL BE RELOCATED AND
NOT REPLACED.



SITE 14 S14 36TH AVE. & S. OLYMPIA ST. SCALE: 1" = 10'






SITE 15 S15 21ST AVE. & S. WASHINGTON ST. SCALE: 1" = 10'

**LANDSCAPE
RESTORATION NOTE:**
SEE SHEET 1 FOR NOTE.

RECTANGULAR RAPID FLASH BEACON (RRFB) SYSTEM NOTE:

SOLAR, INSTALL: NEW RECTANGULAR RAPID FLASHING AMBER BEACON (RRFB) SYSTEM. ONE (1) SYSTEM INCLUDES:
(1) RRFB-XL LIGHT BARS WITH HARNESS (LIGHTS ARE TO BE BI-DIRECTIONAL AND SIDE-EMITTING), (1) 60 WATT SOLAR
PANEL WITH HARNESS, (1) CONTROLLER BOX(S) WITH BATTERY (55AH), (1) 4-1/2"Ø x 14' POLE SPUN ALUMINUM SCH. 40, (1)
PEDESTAL BASE ALUMINUM SQUARE WITH DOOR HATCH. INSTALL: (1) FIXED BASE FOUNDATION WITH 3/4" #10 ANCHOR BOLT
KIT (GALV. - SEE FOUNDATION DETAIL). INSTALL: (1) POLARA XAV2E-LED YELLOW HOUSING PUSHBUTTON ASSEMBLY WITH
R10-25 5-INCH x 7-INCH SIGN TO BE MOUNTED ADJACENT TO RAMP AND PUSHBUTTON FRAME EXTENDER (6" or 12"), AND
INSTALL: (1) W11-501, 12-INCH x 12-INCH, "CROSS TRAFFIC MAY NOT STOP USE CAUTION WHEN CROSSING", AND ALL
ASSOCIATED HARDWARE AS NEEDED TO COMPLETE THE SYSTEM. INSTALL: (2) W11-2 AND W16-7P (1-LEFT AND 1-RIGHT
ARROWS) SIGNS AND LIGHTS ON BOTH SIDES OF POLE. ALL POLE MOUNTING HARDWARE SHALL BE BAND-IT OR APPROVED
EQUAL. SEE DETAIL SHEET 7 and SHEET 8 FOR RRFB INFORMATION.
SITE #13 and SITE #15 SCHOOL CROSSING SIGNAGE EACH POLE: INSTALL: (2) S1-1 AND W16-7P (1-LEFT AND 1-RIGHT
ARROWS) SIGNS AND LIGHTS ON BOTH SIDES OF POLE. ALL POLE MOUNTING HARDWARE SHALL BE BAND-IT OR APPROVED
EQUAL. SEE DETAIL SHEET 7 FOR RRFB INFORMATION.

**PED. CROSSING
DATA INFORMATION
SEE SHEET 9**

TA-	REGION NUMBER	STATE	FEDERAL AID PROJECT NUMBER
	10	WA	HSIP-000S(588)
  			
PEDESTRIAN CROSSING SAFETY Sites 13 thru 15 PROJECT ACCOUNTING #: P2111-22			DATE 7/27/22 DWN JDS/RJD DES JDS/RJD CHK SJ SHEET 06 OF 09 DWG. NO. A-1141

Council Agenda Coversheet



Agenda Item Number	4.f.	Council Date	11/01/2022
Agenda Item Type	General Business Item		
Subject	Tourism Promotion Area Budget & Marketing Plan		
Ordinance/Reso #		Contract #	
Project #		Permit #	
Department	City Manager		

Consent Agenda	<input checked="" type="checkbox"/>
Ordinance/Reso	<input type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

That Council approve the 2023 Tourism Promotion Area (TPA) Budget and Marketing Plan.

Motion for Consideration

I move to approve the 2023 Tourism Promotion Area (TPA) Budget and Marketing Plan.

Summary

At the October 25th workshop, Council received an update from Visit Tri-Cities. The 2023 TPA Budget and Marketing Plan was reviewed during the presentation. This plan was previously reviewed and approved by the Tri-City Regional Hotel-Motel Commission.

Alternatives

None.

Fiscal Impact

None.

Through

Terri Wright
Oct 26, 15:20:48 GMT-0700 2022

Dept Head Approval

City Mgr Approval

Marie Mosley
Oct 27, 20:38:43 GMT-0700 2022

Attachments: 2023 Budget & Marketing Plan

☐ Recording
Required?



Tri-Cities Regional Hotel-Motel Commission

2023 BUDGET AND MARKETING PLAN

VISION

Inspire wanderlust for a bold yet casual, geeky but cool, magical experience in wide-open spaces.

MISSION

We make the Tri-Cities bigger, bolder, brighter, better and more cool through tourism.



Tourism Promotion Area Marketing Plan
prepared by Visit Tri-Cities for:

CITY OF KENNEWICK
CITY OF PASCO
CITY OF RICHLAND



Please Note: The attached document reflects only those programs and costs associated with Tourism Promotion Area (TPA) funding. Visit Tri-Cities manages many other aspects of tourism development, which are funded by hotel and lodging taxes and membership investment. Such programs include, Rivershore Enhancement, Media Outreach, Visitor Services and Member Development.

INTRODUCTION

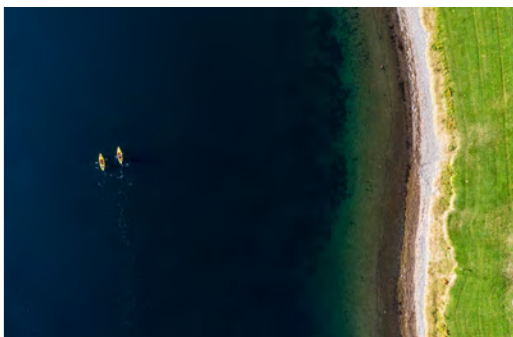
The Tri-City Hotel Motel Commission, established in 2004, was created through an interlocal agreement between the Cities of Kennewick, Pasco, and Richland. The agreement allows for the collection of a tourism promotion assessment, currently \$3, for each night a guest stays in a hotel.

The proceeds of the fees collected within the tourism promotion area are used to bolster efforts to increase tourism spending in the region. The assessment complements the two other sources of financial support for Visit Tri-Cities, hotel-motel lodging tax and membership dues. The interlocal agreement specifically outlines that the proceeds from the assessment are intended to supplement these funding sources, not replace, or become a substitute for them. The assessment was enacted by a vote of those businesses (hotels) that have the fees levied on their products and the model works well because the tourism marketing efforts are supported by a collaboration between the industry, the cities and individual member investors.

In 2023 the proceeds from the tourism promotion assessment are projected to meet or exceed \$2,050,000. The funds are used for advertising across multiple mediums: traditional print, television, digital, social and backyard marketing tactics. Staffing for

marketing, convention and sports sales, media outreach and production are also covered by this source of revenue. Perhaps the most “popular” use of the proceeds is the Tourism Promotion Opportunity Fund, whereby groups are incented to book their events in the Tri-Cities and receive financial support based on the number of hotel rooms they require. Perhaps the only drawback of a tourism promotion assessment is that funding is directly tied to each and every (with a few exceptions) hotel room sold in the region, and the fluctuations in travel, even seasonality can make predicting, and therefore budgeting, tenuous. Take 2020 for example, when travel came to a startling halt in March and remained almost non-existent through the end of the year. The interruption to revenues generated by the assessment was swift and severe. Thankfully, Visit Tri-Cities went into this period with a healthy TPA Reserve Account, which underlines the importance of the need for a well-funded reserve balance and the ability to remain nimble to change directions quickly if required.

The following pages outline the detailed plan for 2023. We are excited about the year ahead and the continued importance of the tourism economy to the Tri-Cities community.





ACKNOWLEDGMENTS

We greatly appreciate the time and dedication of our city partners and hoteliers who meet monthly to provide Visit Tri-Cities with support and direction on how to invest the proceeds from the tourism promotion assessments collected in Kennewick, Pasco, and Richland.

TRI-CITY REGIONAL HOTEL-MOTEL COMMISSION

KENNEWICK

Mark Blotz, Clover Island Inn
Nickolas Woody, Inn at Columbia Center
Marie Mosley, Ex Officio, City of Kennewick

PASCO

Monica Hammerberg, Hampton Inn & Suites Pasco / Tri-Cities
Jerry Beach, A1-Hospitality
Dave Zabell, Ex Officio, City of Pasco

RICHLAND

Linda Hendricks, Hampton Inn Richland
Lacey Stephens, Home2 Suites
Jon Amundson, Ex Officio, City of Richland

TABLE OF CONTENTS

Introduction	4
Acknowledgments	5
Tourism Outlook	7-8
Competitive Situation Analysis	9-10
2023 Group Sales Initiatives	11
Meetings and Conventions	12-13
Sports and Tournaments	14-15
Tourism Development	16-17
DMO Funding Comparative	18-20
Visit Tri-Cities TPA Budget	21

TOURISM 2023

NATIONAL OUTLOOK

The forecast for nationwide growth in the hospitality industry closely mirrors what the Tri-Cities has experienced in the last twelve months. Recently STR, widely recognized as the authority on tourism economics, upgraded the recovery timeline for U.S. hotel revenue per available room (RevPAR). The metric is now expected to surpass 2019 levels in 2022, according to the latest forecast presented in June at the 44th Annual NYU International Hospitality Industry Investment Conference.

Both demand and occupancy have performed as expected in the previous forecast provided by STR, but “pricing continues to exceed expectations due to the influence of inflation as well as the economic fundamentals supporting increased guest spending,” said Amanda Hite, STR’s president. “This latest forecast acknowledges the risk of a light recession with no anticipation of mass layoffs and household finances in a strong position to mitigate recession impacts.

The traveling public is less affected by recession, and right now, we are forecasting demand to reach historic levels in 2023 as business travel recovery has ramped up and joined the incredible demand from the leisure sector.” Of course, we must recognize that profitability has only started hitting 2019 levels recently due to concerns that persist around the cost of labor and services.

The other important factor to consider is the popularity of home sharing and the use of products like Airbnb, HomeAway, Homestay and CouchSurfing. In 2014 revenues for this subset of the hospitality industry were \$15 billion, in 2025 they are projected to reach \$335 billion; and while these hospitality choices still allow for increased visitor spending in communities, the trends do indicate that there are changes to the visitor economy underway. In fact, 74% of millennials have used vacation rental housing for business travel, while only 20% of baby boomers have joined this trend.

PROJECTED U.S. HOTEL KEY PERFORMANCE METRICS*

	2019 Actual	2021 Actual	2022 Forecast	2023 Forecast	2024 Forecast
Occupancy	66.1%	57.6%	63.4%	65.1%	66.4%
ADR	\$131	\$125	\$145	\$150	\$155
RevPAR	\$87	\$72	\$92	\$98	\$103
RevPAR Compared to 2019	-	-17%	+6%	+13%	+18%

*STR May 2022 Forecast



LOCAL OUTLOOK

The local hospitality market is not without challenges. Labor shortages and increased costs on goods and services have created havoc, just as tourism related businesses are striving to recover from pandemic induced travel bans and restrictions of public gatherings. This is not uncommon across the country, but in the Tri-Cities where Darigold is scheduled to open a new plant in the fall of 2023 requiring 1,000 workers, just on the heels of Amazon's previously delayed project, now set to open in the spring of 2023, providing an additional 1,500 new jobs; finding staff to cover housekeeping, front desk and food service is a concern.

However, the reduction of hotel room supply coupled with a strong demand created by business, leisure and sports travelers have created an enviable position for hoteliers. Through the first six months of 2022 the Tri-Cities, particularly Richland, is leading the state in occupancy rates, and ranks third in the state in percentage of RevPar growth to prior year.

The Sports market is expected to remain a strong contributor to group business in 2023. The recent investments in facilities being made by each of the cities will allow for expansion of new markets. In Richland the 30-acre West Village Community Park includes eight pickleball courts, three multi-use fields, a softball field and a basketball court. Groundbreaking is anticipated

for late Fall 2022. In Pasco, **The A Street Sports Complex**, will include 10 multi-use fields mostly for soccer, lacrosse, rugby, or ultimate Frisbee use. The first phase should be complete in the spring of 2023 with full build-out in 2025. And in Kennewick, the city is investing \$1.3 million to add 15 new pickleball courts to **Lawrence Scott Park**.

Convention group business is and will be a little slower to return to pre-pandemic levels. Although people's desire to meet face to face continues to be a basic human need, the logistics of conventions and corporate meetings have been forever changed. Technology allows for productive exchange of information and is cheaper and more efficient than traveling to a multi-day conference. Conferences and events will continue to take place, but the size will be reduced, and delegates may take fewer trips as hybrid solutions are offered. The competition to host conventions is fierce and many communities have invested in new or increased the offerings at public meeting facilities.



2022 PERFORMANCE JANUARY - JUNE*

	OCC	ADR	RevPar	RevPar vs Prior Year
SEATTLE	60.6%	\$176	\$72.09	+145%
SPOKANE	60.3%	\$133	\$80.41	+56.5%
TACOMA	58.5%	\$122	\$71.14	+40%
TRI-CITIES	65.7%	\$110	\$72.09	+52%
VANCOUVER	66.8%	\$121	\$81.22	+30.4%
YAKIMA	54.5%	\$104	\$56.71	+31%

*STR June 2022 Destination Report

COMPETITIVE SITUATION ANALYSIS

In order to promote the Tri-Cities as a preferred destination for group, business and leisure travelers, it is important to recognize both the strengths and challenges within our community and to set sales strategies accordingly.

CONVENTION AND SPORTS

Destination Strengths

- Variety of hotels and brands
- Positive relationship between Visit Tri-Cities, hotels and meeting venues
- Three Rivers Convention Center and HAPO Center
- Unique offsite event venues (i.e., wineries, museums, etc.)
- Ample complimentary parking available
- Sports facility infrastructure
- Strong Sports Council
- Opportunity funds available
- Amenities and attractions that appeal to convention groups (i.e., wine experiences, outdoor recreation, etc.)
- Centrally located in the Pacific Northwest and favorable weather conditions

Destination Challenges

- Lack of hotels capable of 150+ room block
- Lack of resort-style hotels with ample meeting space
- Meeting venues with larger meeting space in competing locations
- Limited number of meeting rooms and exhibit space available under one roof
- Number of hotel rooms within walking distance of Convention Center/large facilities
- The number of hotel rooms available to groups due to hotels converting to apartments
- Competing locations have expanded or have plans to expand their Convention Centers
- Lack of sports officials
- Lack of family friendly attractions pending the opening of the Pasco Aquatics Center
- Competing locations have newly developed indoor/outdoor multi-use sports facilities with state-of-the-art amenities
- Distance from I-5 corridor vs. competition

LEISURE TRAVEL

Destination Strengths

- Heart of Washington Wine Country
- Water2Wine Cruise
- Agritourism assets: farmers' markets and festivals
- Development of wine assets such as Columbia Gardens
- Addition of new public market, Public Market at Columbia River Warehouse
- Popularity of outdoor recreation/wide open spaces
- Trail systems
- Weather
- Water recreation
- STEM tourism assets
- Travel writer outreach
- Easy three hour drive from I-5 corridor destinations
- Consumer sentiments about driving vs flying for travel

Destination Challenges

- Weak reputation as a wine destination vs Walla Walla or Woodinville
- Seasonal demand
- Mountain & snow winter driving from key drive markets
- Perceived lack of family activities
- Heavy weekend traffic on I-90
- Price of airfare/inconvenience of air travel
- No centralized downtown area
- Lingering travel safety concerns due to COVID

BUSINESS TRAVEL

Destination Strengths

- Less price sensitive for airfare and hotels
- Mid-week demand complements weekend and convention groups
- Recently expanded Tri-Cities Airport
- Direct flights from Denver, Salt Lake City, Seattle, Mesa/Phoenix, San Francisco, Minneapolis and Burbank

Destination Challenges

- Destination choice not influenced by sales and marketing efforts
- Mostly mid-week travel
- Popularity of Zoom meetings, reducing in person meetings and travel
- Business travel reductions due to Covid-19
- Very dependent on Hanford business trends

GUESTROOM PRODUCTIVITY

	2019 ACTUAL	2020 ACTUAL	2021 ACTUAL	2022 FORECAST	2023 BUDGET
CONVENTION BOOKINGS	20,955	18,953	8,997	18,000	20,000
CONVENTIONS CANCELLED DUE TO COVID	-	-25,978	-7,251	-805	0
SPORTS BOOKINGS	25,085	10,057	15,483	20,000	25,000
SPORTS CANCELLATIONS	-	-23,765	-475	0	0
TOTAL	46,040	-20,733	16,754	37,195*	45,000*

RFP PRODUCTIVITY

LEADS ISSUED	2019	2020	2021	2022	2023
QTR 1	40	60	23	40	45*
QTR 2	64	18	22	44	52*
QTR 3	74	18	36	40*	62*
QTR 4	87	15	39	56*	71*
TOTAL	265	111	120	180*	230*

*Estimated Production



2023 GROUP SALES INITIATIVES

QUARTER 1

JANUARY - MARCH

- Host Sports Planner Customer Event in Portland
- Launch ad campaign on PlayEasy
- Customer Event in Olympia
- Olympia Sales Blitz
- Establish quarterly meeting planner e-newsletter, similar to Your Weekend Starts Here
- MPI Cascadia Conference
- Launch digital ads targeting MPI attendees
- Launch meeting planner incentive package

QUARTER 2

APRIL - JUNE

- Customer Event at ETA Event Symposium
- E-Sports Conference
- Washington Society of Association Executives Convention
- Society of Government Meeting Planners event
- Ad campaign targeting WSAE Members
- Launch first time sports event incentive

QUARTER 3

JULY - SEPTEMBER

- Sports Relationship Conference
- Publish Sports Facilities Map
- Host Customer Event for Tri-Cities Sports Council
- Convention and corporate sales mission to Seattle/Greater Puget Sound Region
- Host Super FAM event for meeting planners

QUARTER 4

OCTOBER - DECEMBER

- TEAMS Conference
- US Sports Conference
- Launch digital ads targeted at TEAMS and US Sports Congress attendees
- Direct mail "Remember Tri-Cities" gift
- Olympia Sales Blitz



MEETINGS & CONVENTIONS



20,000

GUEST ROOMS BOOKED



\$5,250,000

ESTIMATED ECONOMIC IMPACT



\$2,600,000

ESTIMATED DIRECT
HOTEL SPENDING

2023 ACTIONS

SALES BLITZES

Develop two separate multi-day sales blitzes in Spring (Olympia) and Fall (location to be determined).

CUSTOMER EVENTS

Organize meeting planner customer luncheon in the spring to promote the Tri-Cities as a destination. Event to be held in conjunction with Spring Sales Blitz.

MEETING PLANNER FAM TOURS

Host qualified meeting planners for individual and group, customized FAM tours.

RELATIONSHIPS

Continue staff attendance at Washington Society of Association Executives (WSAE) and Meeting Professionals International (MPI) Washington Chapter and Professional Convention Management Association (PCMA) monthly/quarterly meetings to strengthen relationships with key meeting planners. Staff will continue to serve on the WSAE Convention Planning committee. Staff will continue to serve on industry boards and committees such as WSAE, WTA and SGMP. Renew Diamond Level enhanced listing with Cvent.

ADVERTISING

Digital campaigns of industry tradeshows and site campaigns of meeting professionals by geographic location.

MEETING INCENTIVES

Utilize the opportunity fund specifically to offset costs for new group business and groups that block more than 300 rooms per night city-wide. Create meeting planner incentives and develop a "deals" page outlining any hotel sponsored incentives.

REGIONAL/STATE/NATIONAL CONFERENCES AND TRADESHOWS

Attend Washington Society of Association Executives (WSAE) and Meeting Professionals International (MPI) Cascadia Annual Conferences. Participate in additional sponsorship opportunities at each of these events for increased exposure.

SKYNAV AND KUULA

Convention sales department will leverage the SkyNAV and Kuula platforms to promote Tri-Cities and meeting venues to convention professionals. The interactive tour highlights points of interest with image galleries, videos and information to assist meeting planners in selecting the Tri-Cities for their events. The tour link will be included with all electronic proposal packets, digital Meeting Planner Guide as well as communications to meeting groups.

CONVENTION SALES PROGRAM TOTAL:

\$73,569

ADVERTISING

- MPI site retargeting campaign: \$8,450 (Jan)
- Cvent Diamond Listing: \$9,700 (Sept)
- Digital targeting campaigns of industry tradeshows: \$6,000 (\$500 per month)
- Cvent retargeting campaigns: \$12,000 (April)

Total: \$36,150

TRAVEL

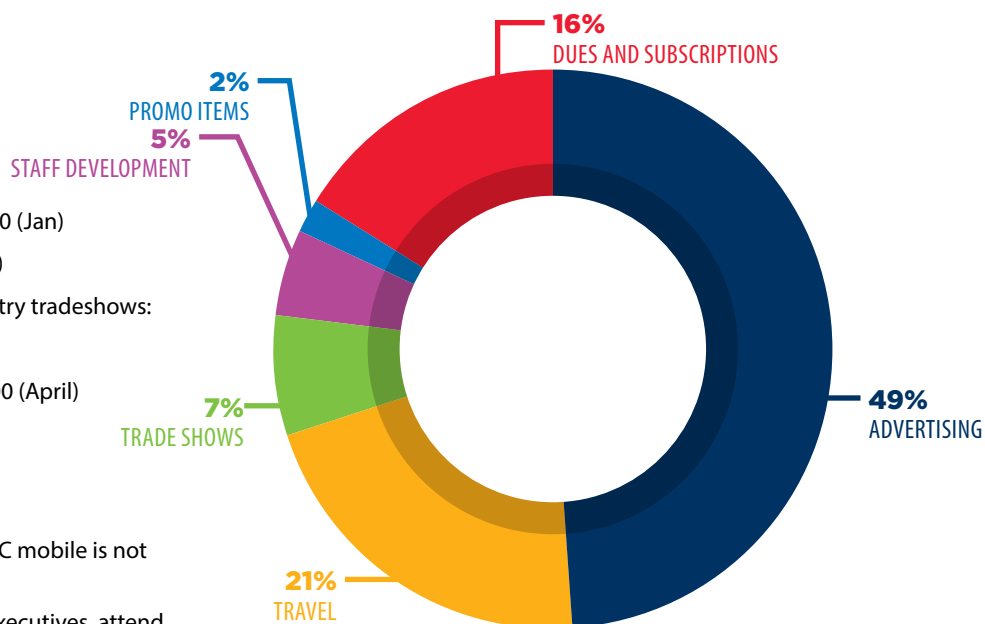
- Mileage for site inspections when VTC mobile is not available: \$480 (\$40 per month)
- Washington Society of Association Executives, attend one association meeting: \$500 (Oct)
- Meeting Planners International Annual Convention: \$600 (March) (hotels and meals)
- Olympia Spring Sales Blitz: \$2,500 (March) (\$500 per staff x 5)
- Fall Sales Blitz: \$2,000 (Oct) (\$500 per staff x 4)
- WSAE Convention: \$800 (June) (hotels, meals, travel at \$400 per staff x 2)
- Continuing education courses: \$8,000 (March and July) (hotels, meals, flight at \$2,000 per session per staff member x 4)
- Society of Government Meeting Professionals Winter Workshop: \$300 (Feb) (hotel, meals, travel)

Total: \$15,180

TRADE SHOWS

- Meeting Planners International (MPI) registration for hosted buyer program: \$1,100 (Jan)
- MPI Conference Sponsorship: \$1,000 (Feb)
- Washington Society of Association Executives Conference Registration: \$450 (May)
- WSAE Conference Sponsorship: \$1,775 (May)
- National trade show targeted at meeting planners, travel media and travel advisors: \$1,000 (March)

Total: \$5,325



STAFF DEVELOPMENT

- CE training courses: \$4,000 (Feb, March, July) (\$1,000 registration per staff x 4)

Total: \$4,000

PROMO ITEMS

- Amenities: \$800 (Feb, Aug)
- Logo'd items: \$700 (Feb, Aug)

Total: \$1,500

DUES AND SUBSCRIPTIONS

- WSAE Sapphire Level Sponsorship: \$2,550 (Jan)
- WSAE dues: \$690 (June) (\$345 per staff x 2)
- Meeting Planners International: \$495 (Dec)
- Society of Government Meeting Professionals: \$400 (April)
- Professional Conference Management Association: \$485 (Nov)
- National Tour Association: \$700 (Feb)
- Religious Conference Management Association Dues: \$195 (July)
- Military Reunion Network: \$699 (May)
- DI MINT subscription: \$5,200 (Jan)

Total: \$11,414

SPORTS AND TOURNAMENTS



25,000

GUEST ROOMS BOOKED



\$7,187,500

ESTIMATED ECONOMIC IMPACT



\$3,125,000

ESTIMATED DIRECT
HOTEL SPENDING

2023 ACTIONS

SPORTS COUNCIL

Organize and administer activities for the Tri-Cities Sports Council.

ADVERTISING

Place print and digital advertising as appropriate in publications such as: Sports Events, Sports Destination Management and Sports Planning Guide.

SALES BLITZ

Conduct sales calls in the Portland or Seattle metropolitan areas in February.

PROMOTE RESOURCES

Update the sports facilities guide, provide e-version and QR code links for consumer access.

VIRTUAL TOURS

SkyNav and Kuula tour links will be promoted in our communications to sports groups. SkyNav and Kuula allows tournament directors to experience the Tri-Cities virtually and the ability to see first-hand the venues, amenities, attractions and accommodations that would be available to their teams. The interactive tours highlight points of interest with image galleries, videos and information to aid tournament directors in understanding the advantages of selecting the Tri-Cities.

OPPORTUNITY FUND

Utilize the opportunity fund specifically to offset tournament costs for groups that block more than 300 rooms per night city-wide.

TRADESHOWS

Promote the Tri-Cities as a premier sports destination at National Tradeshow Events such as TEAMS, S.P.O.R.T.S. Relationship, US Sports Congress and Sports ETA.

CUSTOMER EVENTS

Sponsor event at annual national sports tradeshows, such as TEAMS and Sports ETA Symposium. In addition, host customer event luncheon in conjunction with the Sales Blitz in February.

BIDDING FEES

Bid on new regional/national level tournaments that demand bidding fees to host events.

NEW BUSINESS

With the addition of new pickleball courts in the Tri-Cities, staff will continue to research and focus on pursuing major pickleball tournaments.

SPORTS PROGRAM TOTAL:

\$96,670

ADVERTISING

- Miscellaneous ads to support tournaments: \$925 (Jan: \$100, March: \$150, May: \$325, Nov: \$200, Dec: \$150)
- Sports Events Magazine featured listing: \$1,000 (Dec)
- Sports Planning Guide site inspection feature: \$2,000 (July)
- Playeasy platform destination spotlight: \$3,250
- Printed Sports Facilities Map: \$600 (Jun)

Total: \$7,775

BIDDING FEES

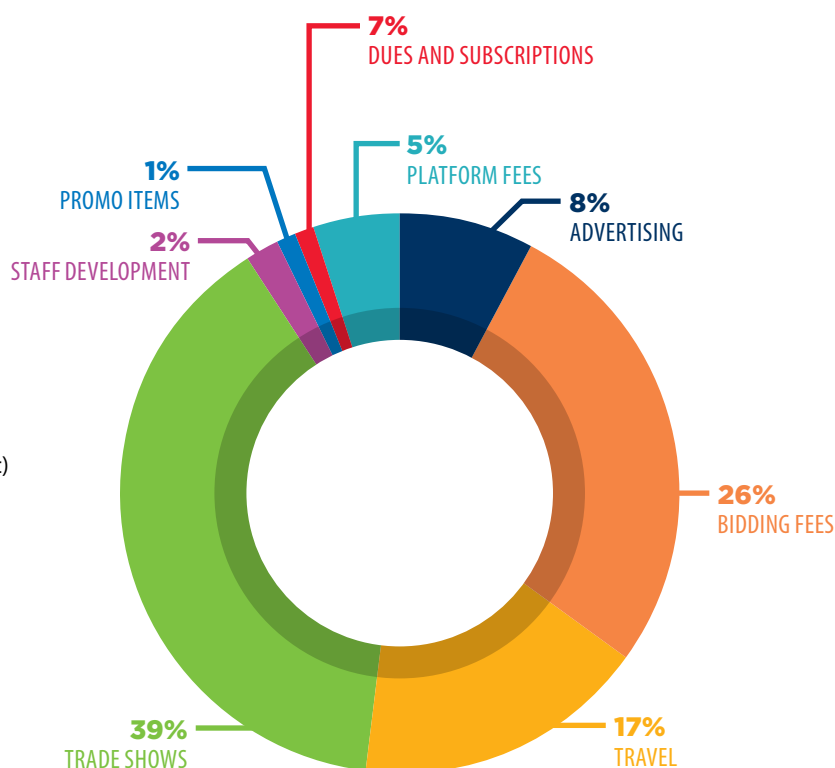
- USTA Tennis Championships: \$5,000 (Aug)
- National Association of Intercollegiate Athletics, Softball Opening Rounds: \$5,000 (May)
- B.A.S.S. Nation: \$15,000 (July)

Total: \$25,000

TRAVEL

- Sports ETA Symposium: \$2,840 (May) (\$1,420 per staff x 2; hotel: \$180 x 4 = \$720, airfare: \$575, meals: \$125)
- TEAMS Conference: \$3,490 (Oct) (\$1,745 per staff x 2; hotel: \$180 x 4 = \$720, airfare: \$875, meals: \$150)
- S.P.O.R.T.S - Relationship Conference: \$1,745 (Sept) (hotel: \$180 x 4 = \$720, airfare: \$875, meals: \$150)
- US Sports Congress: \$1,800 (Dec) (hotel: \$950, airfare: \$700, meals: \$150)
- EsportsTravel Summit: \$1,490 (June) (hotel: \$180 x 3 = \$540, airfare: \$800, meals: \$150)
- Sports Sales Blitz/Customer Event: \$1,120 (\$560 per staff x 2; hotel \$180 x 2 = \$360, meals: \$125, fuel: \$75)
- Staff development, continuing education travel: \$4,000 (May, July) (\$2,000 per staff x 2)
- Mileage for site inspections when VTC mobile is not available: \$300

Total: \$16,785



TRADE SHOWS

- TEAMS Conference: \$12,750 (Feb) (two staff members: \$2,750, TEAMS Conference sponsorship: \$10,000)
- EsportsTravel Summit registration: \$3,000 (Feb)
- S.P.O.R.T.S Relationship Conference: \$6,025 (March) (registration: \$1,500, sponsorship: \$4,525)
- Sports ETA Symposium: \$12,790 (Dec) (registration for two staff members: \$2,790, sponsorship: \$10,000)
- US Sports Congress registration: \$3,150 (April)

Total: \$37,715

STAFF DEVELOPMENT

- Continuing Education Training Courses: \$2,000 (May and July) (\$1,000 registration per staff x 2)

Total: \$2,000

PROMO ITEMS

- Sports Amenities: \$1,200 (April: \$600 and Sept: \$600)

Total: \$1,200

DUES AND SUBSCRIPTIONS

- Sports ETA Membership: \$1,195 (Dec.)

Total: \$1,195

PLATFORM FEES

- Playeasy Digital Platform: \$5,000 (March)

Total: \$5,000

TOURISM DEVELOPMENT

2023 ACTIONS

WEBSITE

The Visit Tri-Cities website, VisitTri-Cities.com, is the foundation of the organization's marketing efforts as all campaigns "call to action" direct users to the website. The website has been redesigned and augmented with new technology, such as an itinerary builder and Crowdriff (user generated content platform) and SkyNav (virtual 360 tour) to enhance the visitor experience. Website content is continually optimized to ensure a first-rate visitor experience when trip planning.

PROMOTE THE DESTINATION

Promote increased leisure travel through development of a cohesive year-long campaign targeting wine aficionados, foodies, golfers, outdoor adventurers, multi-generational families and STEM enthusiasts. The campaigns will be geographically focused on metropolitan areas that are within a 300-mile radius of the Tri-Cities, primarily the Puget Sound and Greater Spokane.

TRAVEL TRADE SHOWS

Target wine enthusiasts at consumer-focused wine and food events such as Taste Washington to increase destination awareness as a world-class wine region.

MATERIALS

Develop compelling marketing materials including, but not limited to, Official Tri-Cities Visitor Guide and wine map. Publications will be available online as many visitors access information digitally, and consumers are increasingly aware of environmental impacts and concerns of public health. Additionally, digital publications may be updated at-will, which benefits the visitor as well as tourism-based businesses and attractions.

ADVERTISE

Digital and social media advertising are the primary marketing tactics, with complementing print and broadcast advertising campaigns. The advertising budget calls for a flexibility in target not only seasonal needs but to reach a targeted audience that is most likely to visit the Tri-Cities.

SOCIAL MEDIA

Social media are ever evolving marketing tools that can be used to connect and engage potential visitors. Visit Tri-Cities will promote the destination through Tri-Cities, WA (30,968 followers) on Facebook, as well as through Instagram and Twitter. In 2023 we will increase social content with compelling photos and video, and engage with popular platforms (e.g. TikTok) to boost engagement.

TECHNOLOGY

We will continue to lead the industry in technology and improve visitor experience by investing in programs and platforms:

- TrueOmni Digital Kiosks (touchscreen information centers with itinerary builders)
- Bandwango (visitor passes and packages)
- SkyNav (virtual 360 tours of community attractions)
- Kuula (virtual site inspections for meeting and sports facilities)

TRI-CITIES WINE TOURISM COUNCIL

Visit Tri-Cities staff provides management and leadership for the Tri-Cities Wine Tourism Council, which works on marketing projects designed to increase wine tourism.

PACKAGES

Work with member hotels to showcase their existing packages to travel media and on the Visit Tri-Cities website.

SEGMENTED CONSUMER NEWSLETTERS

User generated content on the website and social media channels will be supplemented seasonal consumer newsletters targeted to interest groups (outdoor recreation, wine, science) to compel readers to plan leisure travel to the region.

TRI-IDEAS

Visit Tri-Cities will continue to highlight, share and celebrate the diversity of the Tri-Cities that can be experienced through travel and tourism activities.

TOURISM DEVELOPMENT TOTAL:

\$501,810

ADVERTISING

- WTA State Official Visitor Guide: \$7,500 (Nov)
- Cohesive multi-channel campaign to include a mix of broadcast television, digital and social ads targeting leisure travelers in the geographic markets of the Puget Sound Region and Greater Spokane throughout 2023: \$400,000 (\$47,000 Jan/Feb/March, \$165,000 Apr/May/June, \$145,000 July/Aug/Sept, \$43,000 Oct/Nov/Dec)

Total: \$407,500

TRAVEL

- Tri-Cities Wine Tourism Council: \$1,200 (Mar, Sept) (\$600 each show for 1 staff member)
- Continuing education courses: \$10,000 (Mar, May, and Sept) (hotel, meals and travel at \$2,000 per session per staff member x 5)
- Spokane Golf & Travel Show: \$1,400 (Feb) (\$700 per staff member for hotel, meals and travel x 2)

Total: \$12,600

TRADE SHOWS

- Spokane Golf & Travel Show exhibit fees: \$1,300 (Oct)
- Taste Washington is expected to return in 2023

Total: \$1,300

WEBSITE/INTERNET MARKETING

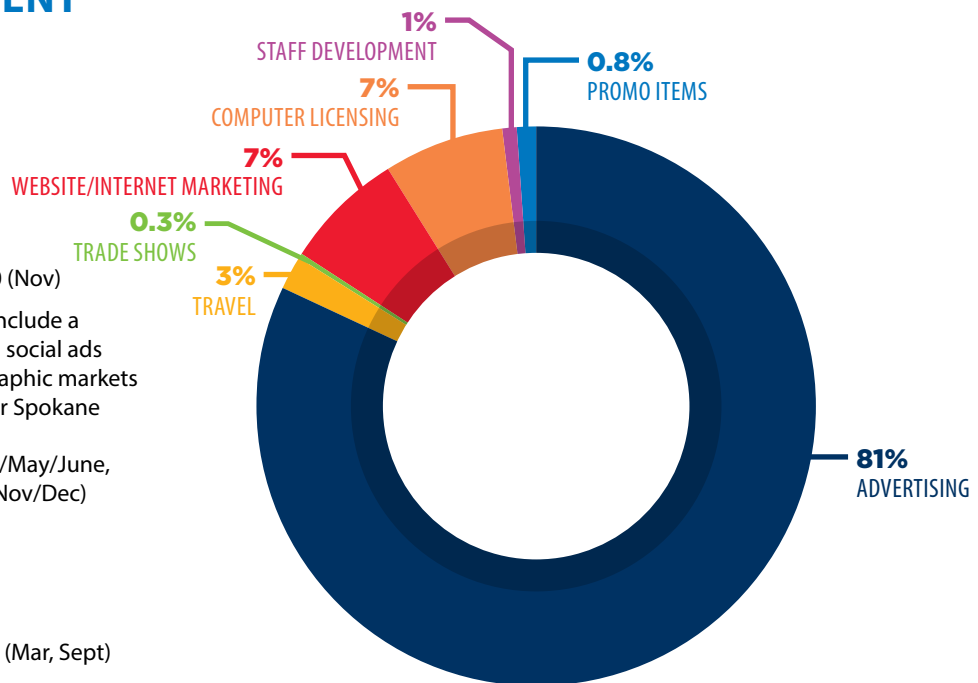
- Invest in Search Engine Optimization for new website: SEO Package: \$36,000 (Jan, April, July, Oct) (\$9,000 a quarter)

Total: \$36,000

COMPUTER LICENSING FEE:

- Website Hosting Fee: \$25,000 (March, June, Sept, Dec) (\$6,250 a quarter)
- iDSS: \$9,000 (March, June, Sept, Dec) (\$2,250 per quarter)
- Blue Buzzard: \$1,560 (\$130 monthly)

Total: \$35,560



STAFF DEVELOPMENT

- Continuing Education Training Courses: \$5,000 (March, May, July, Sept) (\$1,000 registration per staff member x 5)

Total: \$5,000

PROMO ITEMS

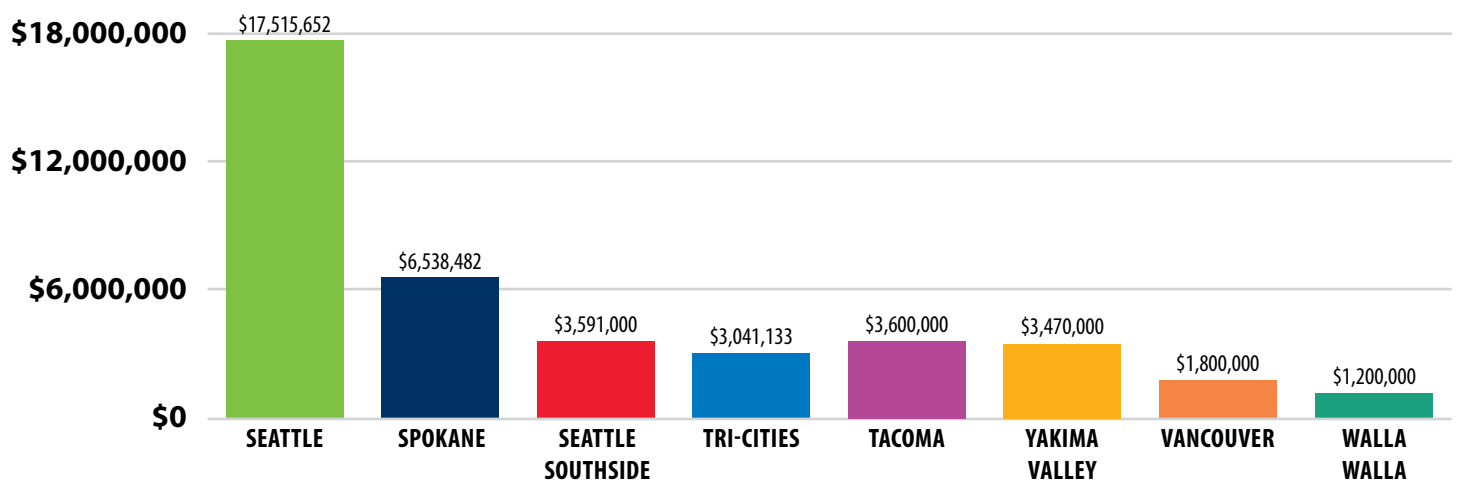
- Registration Bags: \$1,450 (Feb)
- Logo'd Swag: \$2,400 (April, Aug)

Total: \$3,850

DESTINATION MARKETING ORGANIZATION (DMO) FUNDING COMPARATIVE

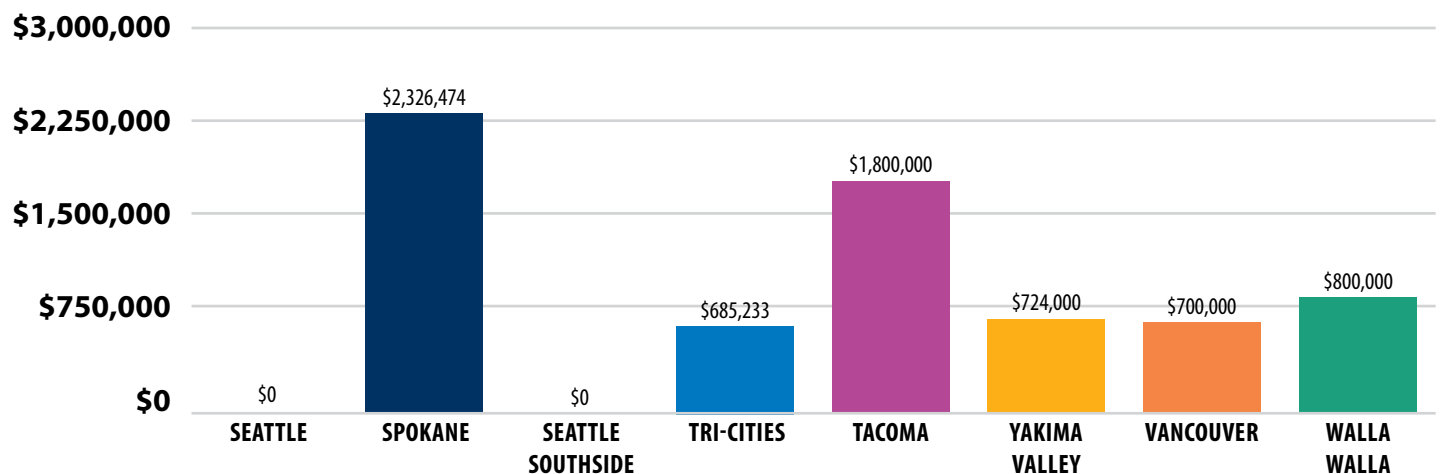
OVERALL BUDGET

The following chart provides historical look at a total budget comparative (including Hotel Motel Tax, Membership Investments and Tourism Promotion Assessment) in key competitive markets.



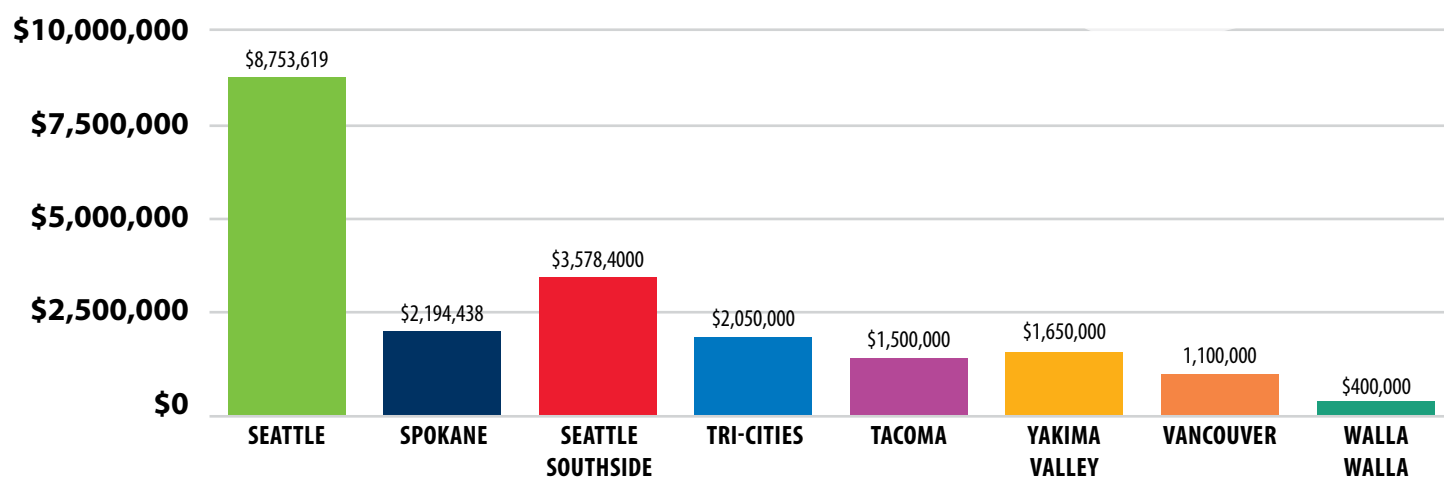
HOTEL-MOTEL TAX INVESTMENTS

The following chart provides historical look at hotel-motel tax investments in key competitive markets.



TPA BUDGET

The following chart provides historical look at a tourism promotion assessments in key competitive markets.



DESTINATION MARKETING ORGANIZATION (DMO) FUNDING COMPARATIVE

The following chart provides historical look at a total budget comparative (including Hotel-Motel Tax, Membership Investments and Tourism Promotion Assessment) in key competitive markets.

COMPETING CITY	DMO BUDGET	NUMBER OF HOTEL ROOMS	\$ SPENT PER ROOM
Visit Seattle	\$17,515,652	14,565	\$1,203
Visit Spokane & Sports Commission	\$6,538,482	7,169	\$912
Seattle Southside Tourism Authority	\$3,591,000	9,394	\$382
Visit Tri-Cities	\$3,041,133	3,670	\$829
Travel Tacoma	\$3,600,000	7,000	\$514
Yakima Valley Tourism	\$3,470,000	3,063	\$1,133
Visit Vancouver USA	\$1,800,000	3,332	\$540
Walla Walla Tourism	\$1,200,000	987	\$1,216

The following chart provides historical look at hotel-motel tax investments in key competitive markets.

COMPETING CITY	HOTEL-MOTEL TAX	NUMBER OF HOTEL ROOMS	\$ SPENT PER ROOM
Visit Seattle	\$0	14,565	\$0
Visit Spokane & Sports Commission	\$2,326,474	7,169	\$325
Seattle Southside Travel Authority	\$0	9,394	\$0
Visit Tri-Cities	\$685,233	3,670	\$187
Travel Tacoma	\$1,800,000	7,000	\$257
Yakima Valley Tourism	\$724,000	3,063	\$236
Visit Vancouver USA	\$700,000	3,332	\$210
Walla Walla Tourism	\$800,000	752	\$1,064

The following chart provides historical look at a tourism promotion assessments in key competitive markets.

COMPETING CITY	TPA	NUMBER OF HOTEL ROOMS	\$ SPENT PER ROOM
Visit Seattle	\$8,753,619	14,545	\$601
Visit Spokane & Sports Commission	\$2,194,438	7,169	\$306
Seattle Southside Tourism Authority	\$3,578,400	9,394	\$381
Visit Tri-Cities	\$2,050,000	3,670	\$559
Travel Tacoma	\$1,500,000	7,000	\$214
Yakima Valley Tourism	\$1,650,000	1,858	\$888
Visit Vancouver USA	\$1,100,000	3,332	\$330
Walla Walla Tourism	\$400,000	752	\$532

VISIT TRI-CITIES 2022 TPA BUDGET

REVENUE		2022 BUDGET	2022 FORECAST		2023 PROJECTED BUDGET		VARIANCE TO 2022 BUDGET	VARIANCE TO 2022 F/C
	City of Kennewick	\$804,923	\$906,045		\$881,500		76,577	\$(24,545)
	City of Richland	\$692,608	\$723,471		\$738,000		45,392	\$14,529
	City of Pasco	\$374,383	\$421,599		\$430,500		56,117	\$8,901
1000	Total Revenues:	\$1,871,914	\$2,051,115		\$2,050,000		178,086	\$(1,115)

EXPENSES		2021 BUDGET	2021 FORECAST		2022 PROJECTED BUDGET		VARIANCE TO 2022 BUDGET	VARIANCE TO 2022 F/C
5010	Salaries	\$696,526.00	\$682,444		\$774,811		78,285	\$92,367
5012	401k	\$33,527.00	\$24,826		\$38,741		5,214	\$13,915
5015	Health Insurance	\$101,558.00	\$102,481		\$137,688		36,130	\$35,207
5030	Payroll Taxes	\$58,508.00	\$62,441		\$75,157		16,649	\$12,716
5035	Accounting Services	\$24,747.00	\$24,744		\$24,747		0	\$3
5050	Telephone & Toll free	\$7,500.00	\$6,844		\$7,000		-500	\$156
5060	Office Expense	\$9,000.00	\$9,141		\$9,000		0	\$(141)
5070	Office Insurance	\$6,000.00	\$5,198		\$6,000		0	\$802
5080	Postage	\$4,000.00	\$2,861		\$3,000		-1,000	\$139
5090	Equipment Upkeep	\$22,000.00	\$19,399		\$20,000		-2,000	\$601
5092	Visit Mobile	\$3,660.00	\$3,048		\$3,600		-60	\$552
5095	Capital Expenditures	\$15,000.00	\$15,628		\$15,000		0	\$(628)
5100	Office Rent	\$63,000.00	\$41,390		\$45,500		-17,500	\$4,110
5105	Legal/Professional	\$2,500.00	\$1,500		\$2,500		0	\$1,000
5110	Dues & Subscriptions	\$7,200.00	\$7,090		\$7,200		0	\$110
5112	Platform Fees	\$58,500.00	\$58,028		\$61,000		2,500	\$2,972
5115	Computer Licensing Fee	\$36,000.00	\$35,187		\$37,000		1,000	\$1,813
5225	Website Maintenance	\$38,000.00	\$37,672		\$38,000		0	\$328
5230	Training	\$10,000.00	\$9,234		\$11,000		1,000	\$1,766
5290	Advertising/Marketing	\$440,988.00	\$440,031		\$512,056		71,068	\$72,025
5300	Promo Items *	\$5,000.00	\$4,542		\$6,000		1,000	\$1,458
5310	Bid Fees	\$25,000.00	\$25,000		\$25,000		0	\$-
5315	Travel	\$37,200.00	\$24,290		\$25,000		-12,200	\$710
5317	Trade Shows	\$46,500.00	\$37,610		\$45,000		-1,500	\$7,390
5500	Opportunity Fund	\$120,000.00	\$111,775		\$120,000		0	\$8,225
Total Expenses		\$1,871,914	\$1,792,404		\$2,050,000			



7130 W. Grandridge Blvd., Ste. B
Kennewick, WA 99336
(509) 735-8486 | (800) 254-5824
VisitTri-Cities.com

Council Agenda Coversheet



Agenda Item Number	5.a.(1)	Council Date	11/01/2022
Agenda Item Type	Ordinance		
Subject	Water Rate Increase		
Ordinance/Reso #	5989	Contract #	
Project #		Permit #	
Department	Public Works		

Consent Agenda	<input type="checkbox"/>
Ordinance/Reso	<input checked="" type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

Staff recommends adoption of Ordinance 5989 amending section 14.13 Charges (Water) of Kennewick Municipal Code (KMC)

Motion for Consideration

I move to adopt Ordinance 5989.

Summary

The City of Kennewick completed a Council authorized water-sewer rate study in 2016 with the City's selected consultant FCS Group. The primary purpose of the study was to determine whether the utility's current rate structure will provide funding to adequately meet the utility's financial obligations and capital improvements required to maintain the current system and meet additional needs required to serve new growth.

The General Sewer Comprehensive Plan, Wastewater Treatment Plant Facility Plan, both approved and adopted by the City Council in 2015, along with the Water Comprehensive Plan in 2016, were used to assist in the development of the rate study.

The current rate study is a multi-year financial plan that will provide revenue requirements to meet the following obligations: Operating Expenses, Debt Service, Capital Expenditures, and Fiscal Policy Achievement.

The rate study evaluated the sufficiency of the current rates on a standalone basis and developed a rate implementation strategy. Council adopted this strategy for the 2021/2022 biennial budget with a rate increase in 2021 and 2022.

Staff recommendation is for a rate implementation in 2023 of 5.15% for Water and then implement the same rates again in 2024.

In 2024 the plan will be reevaluated for the 2025/2026 biennial budget.

Alternatives

None recommended.

Fiscal Impact

N/A

Through

Jeremy Lustig
Oct 24, 14:21:47 GMT-0700 2022

Dept Head Approval

Cary Roe
Oct 24, 15:19:43 GMT-0700 2022

City Mgr Approval

Marie Mosley
Oct 27, 20:40:52 GMT-0700 2022

Attachments:

Ordinance
Ordinance Redline
Presentation

☐ Recording
Required?

CITY OF KENNEWICK
ORDINANCE NO. 5989

AN ORDINANCE RELATING TO WATER CHARGES AND AMENDING
SECTIONS 14.13.030, 14.13.040, 14.13.050 AND 14.13.100 OF THE
KENNEWICK MUNICIPAL CODE

THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, DO ORDAIN AS
FOLLOWS:

Section 1. Section 14.13.030 of the Kennewick Municipal Code, be, and the same hereby is,
amended to read as follows:

14.13.030: - Residential Water Rates—Inside the City Limits.

This schedule applies to all residences and individual apartment houses within the City
limits where service is furnished through a separate meter for each such individual apartment:

- (1) Effective January 1, 2023, the service fee for a bi-monthly billing period will be \$29.21
plus any modification authorized under Section 14.13.100. Effective January 1, 2024,
the service fee for a bi-monthly billing period will be increased by an additional 5.15
percent, plus any modification authorized under Section 14.13.100.
- (2) Effective January 1, 2023, all water use during the bi-monthly billing period will be
charged at \$1.661 per 100 cubic feet of water metered, plus any modification authorized
under Section 14.13.100. Effective January 1, 2024, the charge for water use during the
bi-monthly billing period will be increased by an additional 5.15 percent, plus any
modification authorized under Section 14.13.100.

(Ord. 5989 Sec. 1, 2022; Ord. 5881 Sec. 1, 2020; Ord. 5786 Sec. 1, 2018; Ord. 5677 Sec. 1,
2016; Ord. 3619 Sec. 5, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord.
2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481
Sec. 2(part), 1980)

Section 2. Section 14.13.040 of the Kennewick Municipal Code, be, and the same hereby is,
amended to read as follows:

14.13.040: - Residential Water Rates—Outside the City Limits.

This schedule applies to all residences, and the individual apartments in apartment houses
outside the City limits where service is furnished through a separate meter for each such
individual apartment:

- (1) Effective January 1, 2023, the service fee for a bi-monthly billing period will be \$64.28,
plus any modification authorized under Section 14.13.100. Effective January 1, 2024,

the service fee for a bi-monthly billing period will be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

- (2) Effective January 1, 2023, all water use during the bi-monthly billing period will be charged at \$3.660 per 100 cubic feet of water metered, plus any modification authorized under Section 14.13.100. Effective January 1, 2024, the charge for water use during the bi-monthly billing period will be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

(Ord. 5989 Sec 2, 2022; Ord. 5881 Sec 2, 2020; Ord. 5786 Sec. 2, 2018; Ord. 5677 Sec. 2, 2016; Ord. 3619 Sec. 6, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord. 2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481 Sec. 2(part), 1980)

Section 3. Section 14.13.050 of the Kennewick Municipal Code, be, and the same hereby is, amended to read as follows:

14.13.050: - Multi-family, Commercial and Industrial Water Rates.

- (1) Effective January 1, 2023, the following schedule, plus \$1.661 per 100 cubic feet of water, plus any modification to these rates authorized under Section 14.13.100, applies to all nonresidential water users within the City limits:

Meter Size	Rate For Two Months
¾"	\$56.47
1"	\$84.42
1½"	\$150.18
2"	\$197.28
3"	\$300.54
4"	\$441.39
6"	\$666.84

Effective January 1, 2024, rates authorized under this section shall be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

- (2) Effective January 1, 2023, the following schedule, plus \$3.660 per 100 cubic feet of water, plus any modification to these rates authorized under Section 14.13.100, applies to all nonresidential water users outside the City limits:

Meter Size	Rate For Two Months
¾"	\$124.22
1"	\$185.74
1½"	\$330.37
2"	\$434.03
3"	\$661.19
4"	\$971.06
6"	\$1,467.05

Effective January 1, 2024, rates authorized under this section shall be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

(Ord. 5989 Sec 3, 2022; Ord. 5881 Sec 3, 2020; Ord. 5786 Sec. 3, 2018; Ord. 5677 Sec. 3, 2016; Ord. 3619 Sec. 7, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord. 2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481 Sec. 2(part), 1980)

Section 4. Section 14.13.100 of the Kennewick Municipal Code, be, and the same hereby is, amended to read as follows:

14.13.100: - Indexing Fees and Charges.

The fees and charges for water services set out in Sections 14.13.010, 14.13.030, 14.13.040, 14.13.050, and 14.13.090 shall be indexed annually by the Treasurer to reflect 100 percent of any change from the Consumer Price Index (U.S. Cities - Urban Wage Earners and Clerical Workers - CPI-W) for October, or other comparable index if not published. Effective beginning with rates that will be in effect on January 1, 2025, the fees and charges for water services set out in Sections 14.13.030, 14.13.040, and 14.13.050 shall also be indexed annually by the Treasurer to reflect 100 percent of any change from the Consumer Price Index (U.S. Cities - Urban Wage Earners and Clerical Workers - CPI-W) for October, or other comparable index if not published. The Treasurer shall adjust the fees and publish them each December to take effect on all goods and services delivered or contracted after the beginning of each year and all utility bills mailed after the first of each year. In no event shall the cumulative change in rates be less than zero

percent nor more than four percent per year, nor may the change in any given year be less than zero percent nor more than four percent. Unit prices for fees and charges set out in Sections 14.13.030, 14.13.040 and 14.13.050 shall be to the nearest 1/10¢ and all other charges to the nearest cent. Fees and charges set out in Sections 14.13.010 and 14.13.090 will only be adjusted in increments of \$5.00 and will not be adjusted until such time that the cumulative annual increase based on annual changes to the Consumer Price Index (CPI) is at least \$5.00.

(Ord. 5989 Sec 4, 2022; Ord. 5881 Sec. 4, 2020; Ord. 5465 Sec. 3, 2012; Ord. 4078 Sec. 1, 2002; Ord. 3619 Sec. 15, 1995)

Section 5. This ordinance shall be in full force and effect on January 1, 2023.

PASSED BY THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, this 1st day of November, 2022, and signed in authentication of its passage this 1st day of November 2022.

Attest:

W.D. MCKAY, Mayor

TERRI L. WRIGHT, City Clerk

ORDINANCE NO. 5989 filed and recorded
in the office of the City Clerk of the City of
Kennewick, Washington this 2nd day of
November, 2022.

Approved as to Form:

LISA BEATON, City Attorney

TERRI L. WRIGHT, City Clerk

DATE OF PUBLICATION_____

CITY OF KENNEWICK
ORDINANCE NO. 5989

AN ORDINANCE RELATING TO WATER CHARGES AND AMENDING
SECTIONS 14.13.030, 14.13.040, 14.13.050 AND 14.13.100 OF THE
KENNEWICK MUNICIPAL CODE

THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, DO ORDAIN AS
FOLLOWS:

Section 1. Section 14.13.030 of the Kennewick Municipal Code, be, and the same hereby is,
amended to read as follows:

14.13.030: - Residential Water Rates—Inside the City Limits.

This schedule applies to all residences and individual apartment houses within the City
limits where service is furnished through a separate meter for each such individual apartment:

- (1) Effective January 1, ~~2021~~2023, the service fee for a bi-monthly billing period will be
\$~~26.42~~29.21 plus any modification authorized under Section 14.13.100. Effective
January 1, ~~2022~~2024, the service fee for a bi-monthly billing period will be increased by
an additional 5.15 percent, plus any modification authorized under Section 14.13.100.
- (2) Effective January 1, ~~2021~~2023, all water use during the bi-monthly billing period will
be charged at \$~~1.50~~1.661 per 100 cubic feet of water metered, plus any modification
authorized under Section 14.13.100. Effective January 1, ~~2022~~2024, the charge for
water use during the bi-monthly billing period will be increased by an additional 5.15
percent, plus any modification authorized under Section 14.13.100.

(~~Ord. 5989 Sec. 1, 2022~~; Ord. 5881 Sec. 1, 2020; Ord. 5786 Sec. 1, 2018; Ord. 5677 Sec. 1,
2016; Ord. 3619 Sec. 5, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord.
2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481
Sec. 2(part), 1980)

Section 2. Section 14.13.040 of the Kennewick Municipal Code, be, and the same hereby is,
amended to read as follows:

14.13.040: - Residential Water Rates—Outside the City Limits.

This schedule applies to all residences, and the individual apartments in apartment houses
outside the City limits where service is furnished through a separate meter for each such
individual apartment:

- (1) Effective January 1, ~~2021~~2023, the service fee for a bi-monthly billing period will be
\$~~58.14~~64.28, plus any modification authorized under Section 14.13.100. Effective

January 1, ~~2022~~2024, the service fee for a bi-monthly billing period will be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

- (2) Effective January 1, ~~2021~~2023, all water use during the bi-monthly billing period will be charged at ~~\$3.31~~3.660 per 100 cubic feet of water metered, plus any modification authorized under Section 14.13.100. Effective January 1, ~~2022~~2024, the charge for water use during the bi-monthly billing period will be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

(Ord. 5989 Sec 2, 2022; Ord. 5881 Sec 2, 2020; Ord. 5786 Sec. 2, 2018; Ord. 5677 Sec. 2, 2016; Ord. 3619 Sec. 6, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord. 2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481 Sec. 2(part), 1980)

Section 3. Section 14.13.050 of the Kennewick Municipal Code, be, and the same hereby is, amended to read as follows:

14.13.050: - Multi-family, Commercial and Industrial Water Rates.

- (1) Effective January 1, ~~2021~~2023, the following schedule, plus ~~\$1.50~~1.661 per 100 cubic feet of water, plus any modification to these rates authorized under Section 14.13.100, applies to all nonresidential water users within the City limits:

Meter Size	Rate For Two Months
¾"	\$51.07 56.47
1"	\$76.36 84.42
1½"	\$135.82 150.18
2"	\$178.43 197.28
3"	\$271.82 300.54
4"	\$399.21 441.39
6"	\$603.12 666.84

Effective January 1, ~~2022~~2024, rates authorized under this section shall be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

- (2) Effective January 1, ~~2021~~2023, the following schedule, plus \$~~3,3063.660~~ per 100 cubic feet of water, plus any modification to these rates authorized under Section 14.13.100, applies to all nonresidential water users outside the City limits:

Meter Size	Rate For Two Months
¾"	\$ 112.35 <u>124.22</u>
1"	\$ 167.99 <u>185.74</u>
1½"	\$ 298.80 <u>330.37</u>
2"	\$ 392.55 <u>434.03</u>
3"	\$ 598.01 <u>661.19</u>
4"	\$ 878.27 <u>971.06</u>
6"	\$ 1,326.87 <u>1,467.05</u>

Effective January 1, ~~2022~~2024, rates authorized under this section shall be increased by an additional 5.15 percent, plus any modification authorized under Section 14.13.100.

(~~Ord. 5989 Sec 3, 2022~~; Ord. 5881 Sec 3, 2020; Ord. 5786 Sec. 3, 2018; Ord. 5677 Sec. 3, 2016; Ord. 3619 Sec. 7, 1995; Ord. 3121 Sec. 1(part), 1987; Ord. 3084 Sec. 1(part), 1987; Ord. 2973 Sec. 1(part), 1985; Ord. 2910 Sec. 1(part), 1985; Ord. 2888 Sec. 3(part), 1985; Ord. 2481 Sec. 2(part), 1980)

Section 4. Section 14.13.100 of the Kennewick Municipal Code, be, and the same hereby is, amended to read as follows:

14.13.100: - Indexing Fees and Charges.

The fees and charges for water services set out in Sections 14.13.010, 14.13.030, 14.13.040, 14.13.050, and 14.13.090 shall be indexed annually by the Treasurer to reflect 100 percent of any change from the Consumer Price Index (U.S. Cities - Urban Wage Earners and Clerical Workers - CPI-W) for October, or other comparable index if not published. Effective beginning with rates that will be in effect on January 1, ~~2023~~2025, the fees and charges for water services set out in Sections 14.13.030, 14.13.040, and 14.13.050 shall also be indexed annually by the Treasurer to reflect 100 percent of any change from the Consumer Price Index (U.S. Cities - Urban Wage Earners and Clerical Workers - CPI-W) for October, or other comparable index if not published. The Treasurer shall adjust the fees and publish them each December to take effect on all goods and services delivered or contracted after the beginning of each year and all utility bills mailed after the first of each year. In no event shall the cumulative change in rates be less than zero

percent nor more than four percent per year, nor may the change in any given year be less than zero percent nor more than four percent. Unit prices for fees and charges set out in Sections 14.13.030, 14.13.040 and 14.13.050 shall be to the nearest 1/10¢ and all other charges to the nearest cent. Fees and charges set out in Sections 14.13.010 and 14.13.090 will only be adjusted in increments of \$5.00 and will not be adjusted until such time that the cumulative annual increase based on annual changes to the Consumer Price Index (CPI) is at least \$5.00.

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Section 5. This ordinance shall be in full force and effect on January 1, 2023.

PASSED BY THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, this 1st day of November, 2022, and signed in authentication of its passage this 1st day of November 2022.

Attest:

W.D. MCKAY, Mayor

TERRI L. WRIGHT, City Clerk

ORDINANCE NO. 5989 filed and recorded
in the office of the City Clerk of the City of
Kennewick, Washington this 2nd day of
November, 2022.

Approved as to Form:

LISA BEATON, City Attorney

TERRI L. WRIGHT, City Clerk

DATE OF PUBLICATION_____



City Council Meeting



Water and Wastewater Rate Study Update

Cary Roe, P.E. Public Works Director, City of Kennewick
Angie Sanchez Virnoche, Principal FCS GROUP
Chase Bozett, Senior Analyst FCS GROUP

November 1, 2022



Discussion Outline

- **Background**
- **Rate setting overview**
- **Summary of findings**
- **Staff recommendations**

Background

- **Water and Wastewater initial Rate Study completed in 2016**
 - » Covered 20-year time period
- **City practice to revisit rate plan during biennium budget process for next two-year rate period**
- **2020 study resulted in maintaining original rate path**

Utility	2021	2022	2023	2024
Water	5.15%	5.15%	5.15%	5.15%
Wastewater	7.40%	6.65%	6.65%	6.65%

- **City Council rate workshop held on October 11**



Background (cont.)

- **Each update incorporates current financial, operating and engineer needs informed by:**
 - » Utility budgets
 - » General Sewer Plan adopted in 2015
 - » Wastewater Treatment Facilities Plan adopted in 2015
 - » Facilities Plan amended in 2020
 - » Water Comprehensive Plan in 2016
 - » Water Capacity Analysis 2018
 - » Water Treatment Plant Condition Assessment 2019
 - » Reservoir Condition Assessment
 - » UGA Study Update

Deferred Capital

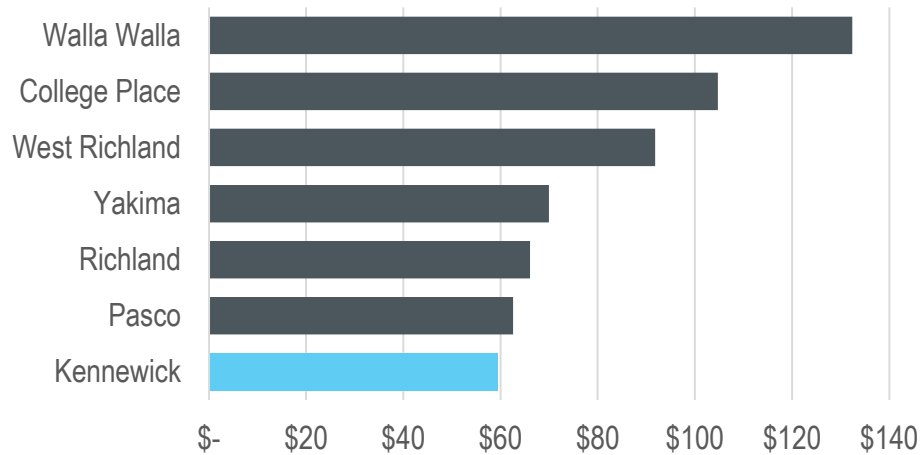
- Water Treatment Plant Repairs and Improvements
- Transmission Mains and Pump Station Upgrades
- Wastewater Treatment Plant Upgrades





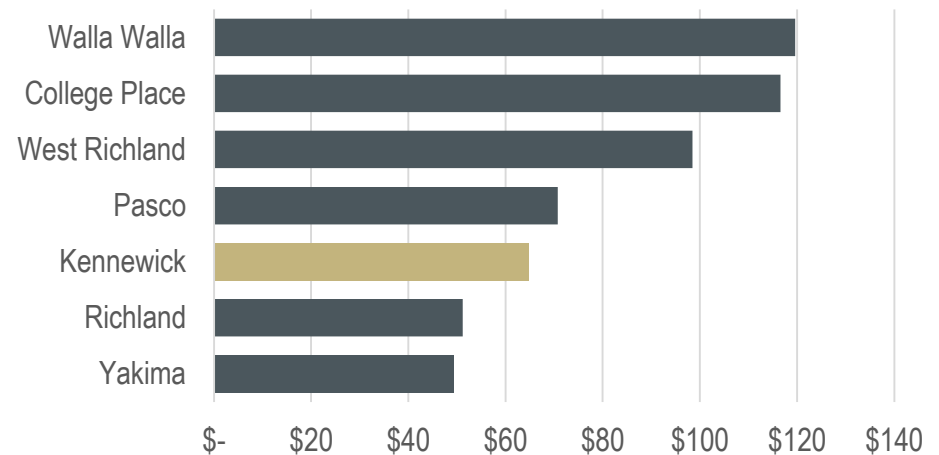
2022 Residential Bimonthly Bill Comparisons

Water



Note: Assumes residential meter 1" and 20 ccf bi-monthly usage

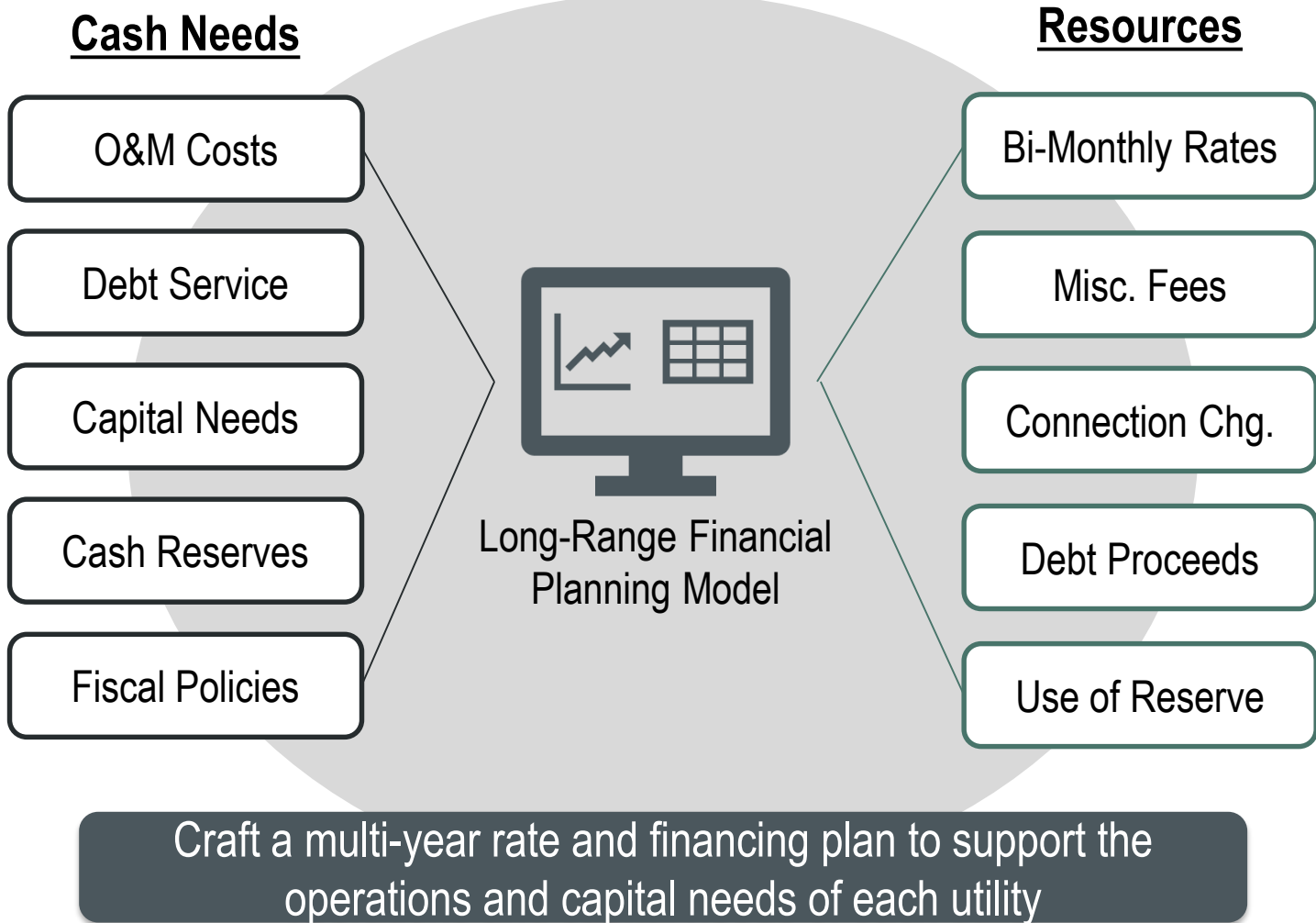
Wastewater



Note: Assumes 11 ccf bi-monthly usage where applicable



Overview of Rate Study





Rate Update Key Factors

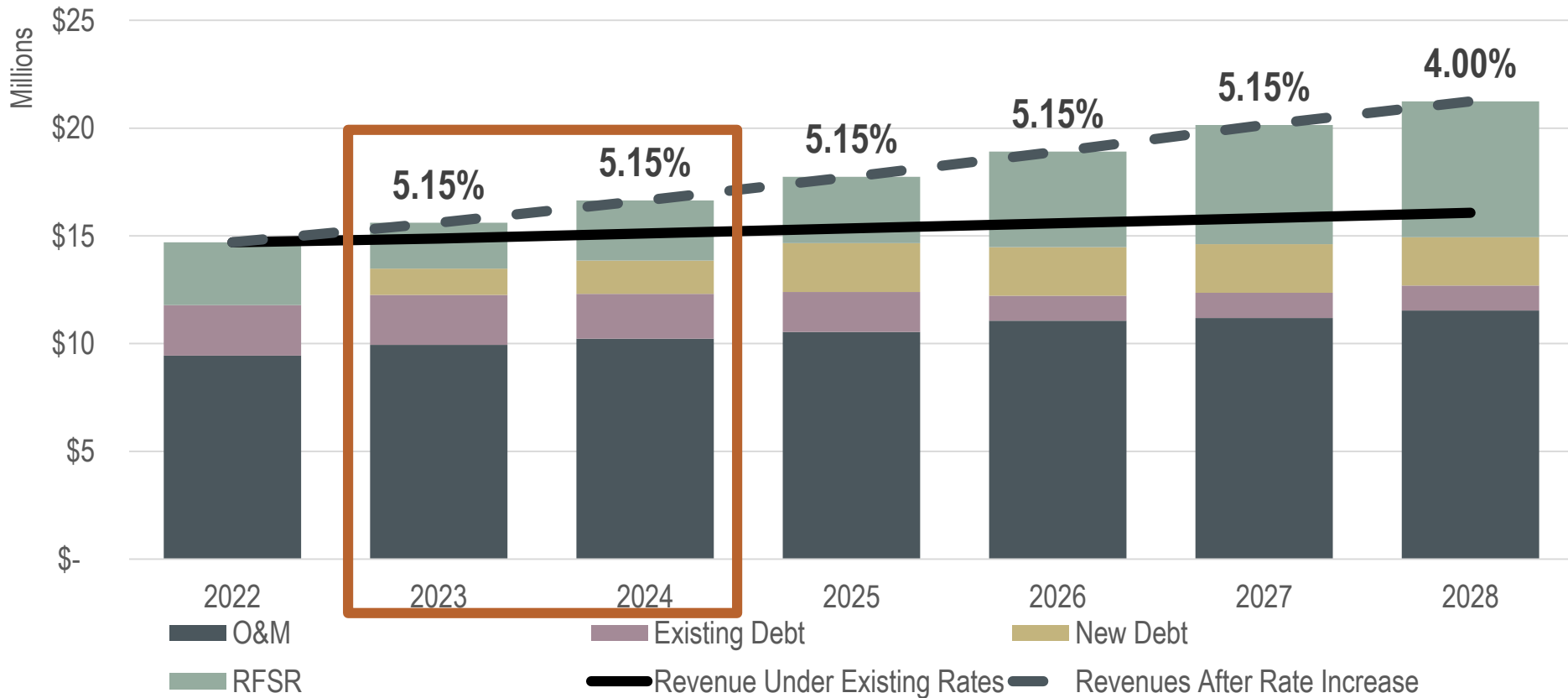
- **Rate update for 2023/2024 rate-setting period (previewing to 2028)**
- **Operating budget used as baseline for revenue and expenses**
 - » 1.50% annual growth rate
- **Existing Debt**
 - » Water - \$2.3M reducing to \$1.2M in 2026
 - » Sewer - \$890K reducing to \$550K 2029
- **Incorporated City provided Capital Plans (~\$110M combined through 2028)**
- **Fiscal policy targets**
 - » Operating reserve 90 days O&M water, 60 days O&M sewer
 - » Capital reserve 1% of original cost asset value
 - » System reinvestment target equal to annual depreciation
 - » Debt service coverage target 1.50

A black and white photograph showing water being poured from a source above into a clear glass. The water is captured mid-pour, creating a dynamic splash with many small bubbles and droplets visible within the glass. The background is a light, textured surface.

Water Revenue Requirement



Revenue Requirement Summary - Water



- **Maintain original rate path**

- » Allows City to continue to meet operating costs and \$60M in upcoming capital needs
- » Support \$25M in debt financing and new debt service 2023 and 2025
- » Continue phase in of system reinvestment policy - allowing for larger cash funding of capital



Water Residential Bill Impacts

Water Sample Bill	Existing	2023	2024	2025	2026	2027	2028
Historically Assumed Policy Based Increase*		2.00%	2.00%	2.00%	2.00%	2.00%	2.00%
<u>Additional Proposed Increase</u>		<u>3.15%</u>	<u>3.15%</u>	<u>3.15%</u>	<u>3.15%</u>	<u>3.15%</u>	<u>2.00%</u>
Total Annual Increase		5.15%	5.15%	5.15%	5.15%	5.15%	4.00%
Sample Residential Bi-Monthly Bill	\$ 59.38	\$ 62.44	\$ 65.65	\$ 69.03	\$ 72.59	\$ 76.33	\$ 79.38
Sample Residential Monthly Bill	\$ 29.69	\$ 31.22	\$ 32.83	\$ 34.52	\$ 36.30	\$ 38.16	\$ 39.69
Monthly Increase		\$ 1.53	\$ 1.61	\$ 1.69	\$ 1.78	\$ 1.86	\$ 1.53

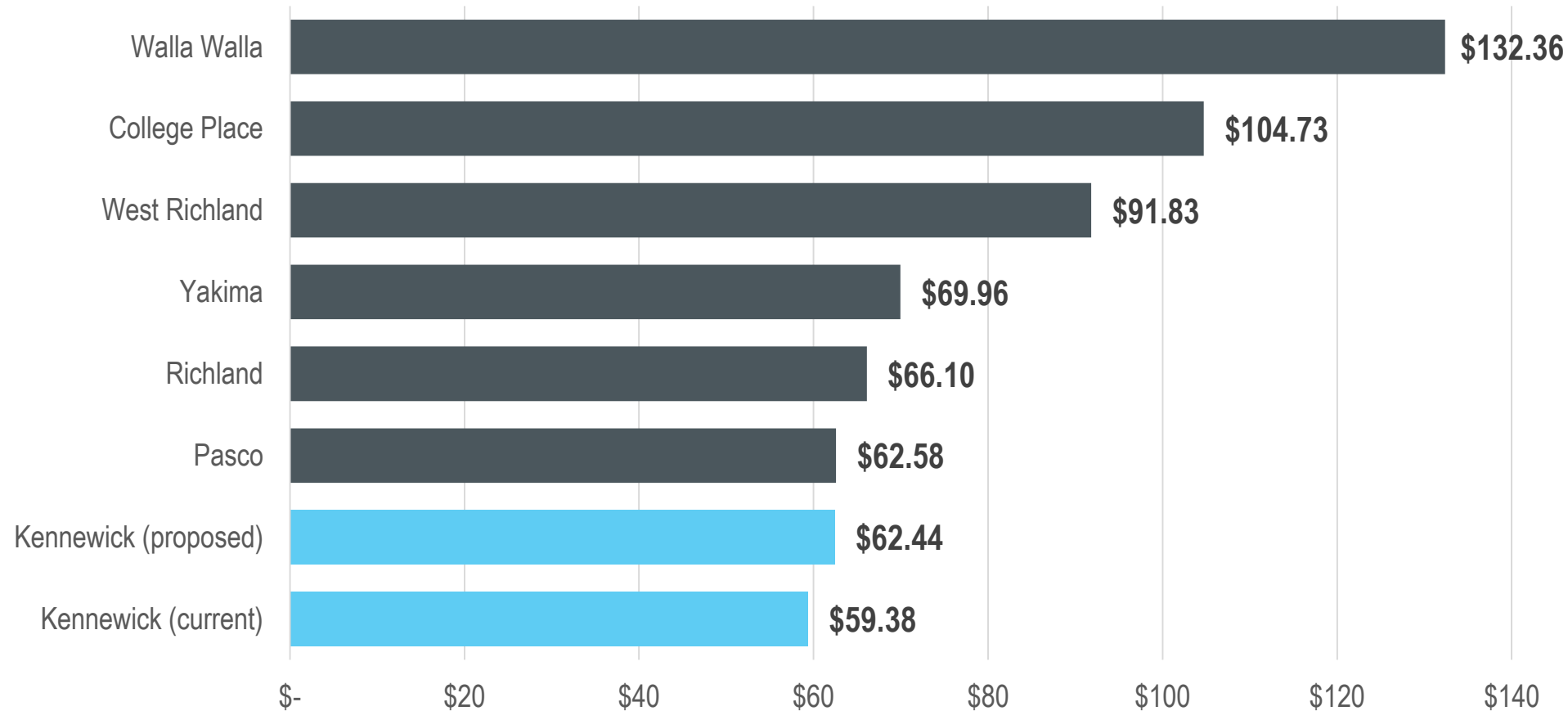
Assumes 1" meter and 20 ccf bimonthly usage

- **City practice to annually index fees and Charges to CPI**
 - » Rate study has historically assumed 2.0% for CPI
 - » Sept-Sept Urban Wage Earners and Clerical Workers CPI-W: 8.46%
- **Rate plan can be maintained for 2023/2024 biennium**
 - » Approximately \$1.55 monthly average increase



Jurisdictional Survey

Residential Bi-Monthly Bill (1" Meter, 20 ccf)

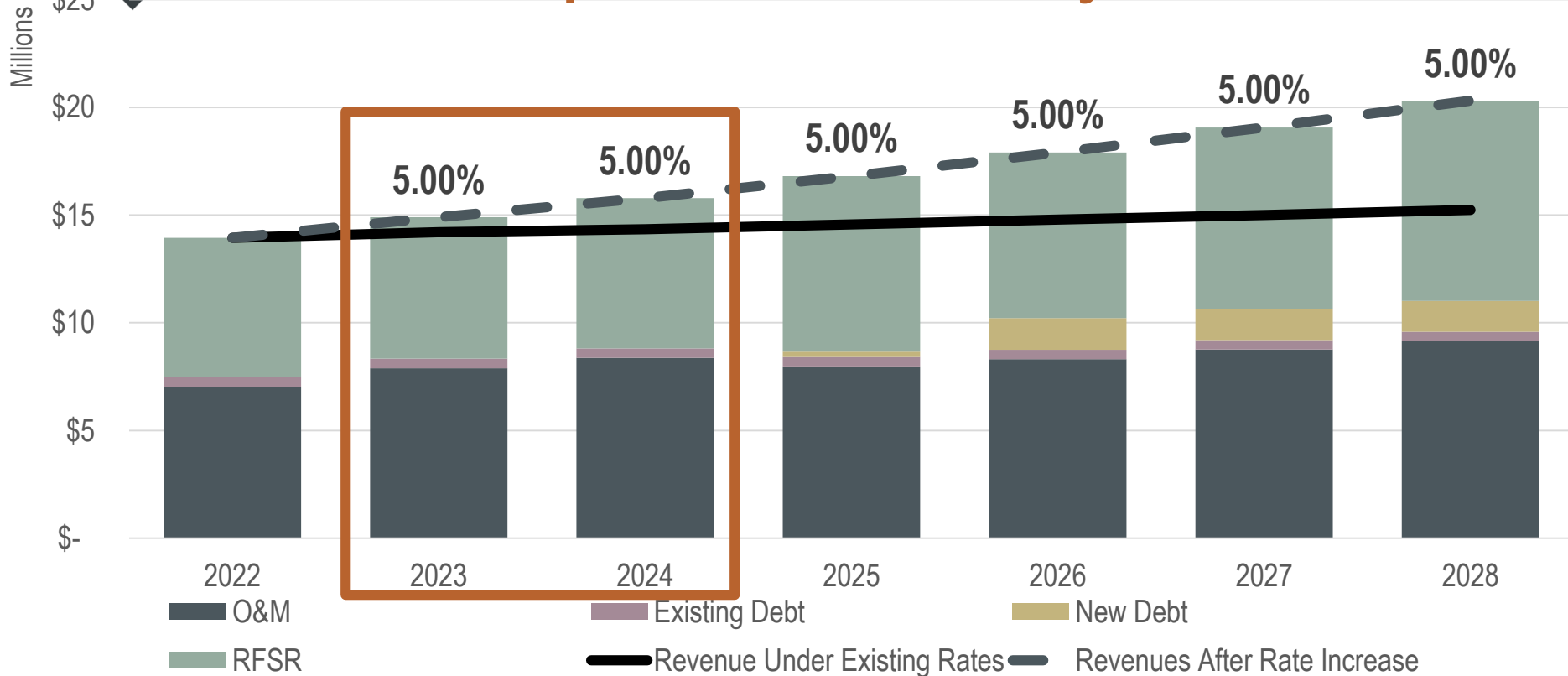




Wastewater Revenue Requirement



Revenue Requirement Summary - Wastewater



- **Adjust original rate path down**

- » Allows City to continue to meet operating costs and \$49M in upcoming capital needs
- » Continue phase in of system reinvestment policy - increased cash funding of capital
- » Reduced WWTP capital costs with progressive design build
- » Support \$29M in debt financing - reduced borrowing costs with City secured low interest loans (\$6.0M of partial loan forgiveness)



Wastewater Residential Bill Impacts

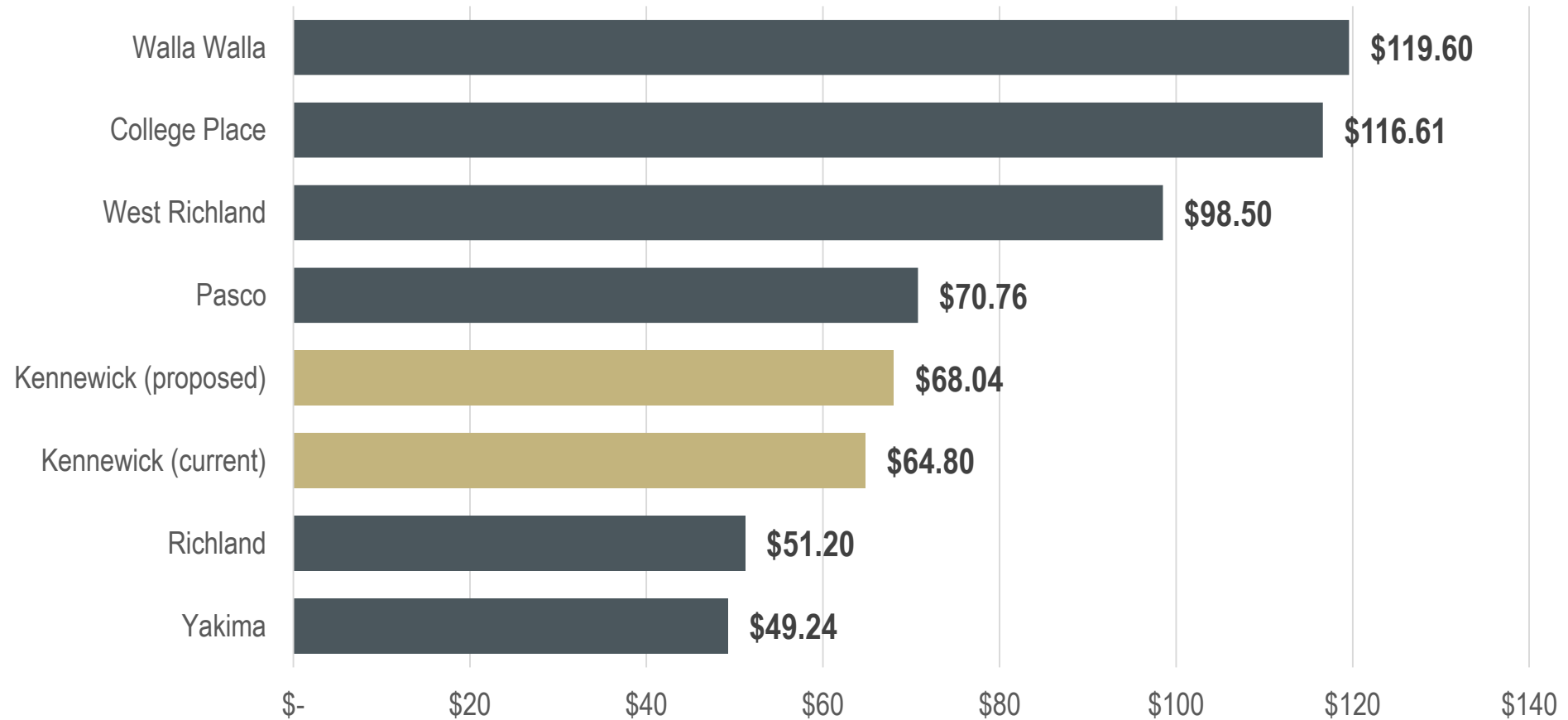
Wastewater Sample Bill		Existing	2023	2024	2025	2026	2027	2028
Historically Assumed Policy Based Increase*			2.00%	2.00%	2.00%	2.00%	2.00%	2.00%
<u>Additional Proposed Increase</u>			<u>3.00%</u>	<u>3.00%</u>	<u>3.00%</u>	<u>3.00%</u>	<u>3.00%</u>	<u>3.00%</u>
Total Annual Increase			5.00%	5.00%	5.00%	5.00%	5.00%	5.00%
Sample Residential Bi-Monthly Bill	\$ 64.80		\$ 68.04	\$ 71.44	\$ 75.01	\$ 78.76	\$ 82.70	\$ 86.84
Sample Residential Monthly Bill	\$ 32.40		\$ 34.02	\$ 35.72	\$ 37.51	\$ 39.38	\$ 41.35	\$ 43.42
Monthly Increase			\$ 1.62	\$ 1.70	\$ 1.79	\$ 1.87	\$ 1.97	\$ 2.07

- **City practice to annually index fees and Charges to CPI**
 - » Rate study has historically assumed 2.0% for CPI
 - » Sept-Sept Urban Wage Earners and Clerical Workers CPI-W: 8.46%
- **Rate plan can be reduced for the 2023/2024 biennium**
 - » Approximately \$1.65 monthly average increase



Jurisdictional Survey

Residential Bi-Monthly Bill (11 ccf water usage if applicable)





Staff Recommendation



Staff Recommendation


- **Maintain existing rate path of 5.15% annually for the water utility**
 - » Monthly increase to average residential bill of \$1.53 in 2023 and \$1.61 in 2024
- **Lower wastewater rate increases from 6.65% to 5.00% annually**
 - » Monthly increase to average residential bill of \$1.62 in 2023 and \$1.70 in 2024
- **Implement rate changes effective January 1st, 2023**

Thank you!

Questions?

Angie Sanchez Virnoche, Principal
(425) 336-4157
AngieS@FCSGroup.com

www.fcsgroup.com

Council Agenda Coversheet 	Agenda Item Number	5.a.(2)	Council Date	11/01/2022	Consent Agenda	<input type="checkbox"/>
	Agenda Item Type	Ordinance			Ordinance/Reso	<input checked="" type="checkbox"/>
	Subject	Sewer Rate Increase			Public Mtg / Hrg	<input type="checkbox"/>
	Ordinance/Reso #	5990	Contract #		Other	<input type="checkbox"/>
	Project #		Permit #		Quasi-Judicial	<input type="checkbox"/>
	Department	Public Works				

Recommendation

Staff recommends adoption of Ordinance 5990 amending section 14.26 Sanitary Sewer User Charges of the Kennewick Municipal Code (KMC)

Motion for Consideration

I move to adopt Ordinance 5990.

Summary

The City of Kennewick completed a Council authorized water-sewer rate study in 2016 with the City's selected consultant FCS Group. The primary purpose of the study was to determine whether the utility's current rate structure will provide funding to adequately meet the utility's financial obligations and capital improvements required to maintain the current system and meet additional needs required to serve new growth.

The General Sewer Comprehensive Plan, Wastewater Treatment Plant Facility Plan, both approved and adopted by the City Council in 2015, along with the Water Comprehensive Plan in 2016, were used to assist in the development of the rate study.

The current rate study is a multi-year financial plan that will provide revenue requirements to meet the following obligations: Operating Expenses, Debt Service, Capital Expenditures, and Fiscal Policy Achievement.

The rate study evaluated the sufficiency of the current rates on a standalone basis and developed a rate implementation strategy. Council adopted this strategy for the 2021/2022 biennial budget with a rate increase in 2021 and 2022.

Staff recommendation is for a rate implementation in 2023 of 5.0% for Sewer and then implement the same rates again in 2024.


In 2024 the plan will be reevaluated for the 2025/2026 biennial budget.

Alternatives

None recommended.

Fiscal Impact

N/A

Through	Jeremy Lustig Oct 24, 14:23:20 GMT-0700 2022	Attachments: 
Dept Head Approval	Cary Roe Oct 24, 15:20:18 GMT-0700 2022	
City Mgr Approval	Marie Mosley Oct 27, 20:41:59 GMT-0700 2022	
		<input type="checkbox"/> Recording Required?

CITY OF KENNEWICK
ORDINANCE NO. 5990

AN ORDINANCE RELATING TO SANITARY SEWER CHARGES
AND AMENDING SECTIONS 14.26.010, 14.26.020, 14.26.030,
14.26.040 AND 14.26.70 OF THE KENNEWICK MINICIPAL CODE

THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, DO ORDAIN AS
FOLLOWS:

Section 1. Section 14.26.010 of the Kennewick Municipal Code, be, and the same hereby is
amended to read as follows:

14.26.010: - User Charge Basis.

Each connection to the sanitary sewer from single-family dwelling unit, multi-family dwelling unit, commercial and industrial accounts shall have a user charge assessed and collected bi-monthly by the City Treasurer on the following basis:

- (1) Single-Family Dwelling Unit Accounts: A flat fee charge rate shall be charged.
- (2) Multi-Family Dwelling Unit Accounts: A variable charge rate based upon metered water consumption shall be charged. The rate shall be determined as follows:
 - (a) A rate, never less than a minimum charge, equal in amount to the single-family dwelling unit flat fee charge rate, normally;
 - (b) Effective January 1, 2023, a rate determined by multiplying the user charge rate (dollars per 100 cubic feet discharged) by the metered water consumption in hundred cubic feet and an \$17.58 per unit service fee, plus any modifications authorized under Section 14.26.070, for each two-month billing cycle and effective January 1, 2024, a rate determined by multiplying the user charge rate (dollars per 100 cubic feet discharged) by the metered water consumption in 100 cubic feet and a rate that has been increased by an additional 5.0 percent, plus any modifications authorized under Section 14.26.070 per unit service fee for each two-month billing cycle; but
 - (c) Never higher than a maximum charge determined by multiplying the number of dwelling units in the account by the single-family dwelling unit flat fee rate.
- (3) Commercial/Industrial Accounts: A variable charge rate based upon metered water consumption shall be charged. The rate shall be determined as follows:
 - (a) A rate never less than a minimum charge equal in amount to the single-family dwelling unit flat fee charge rate, normally;
 - (b) Effective January 1, 2023, a rate determined by multiplying the user's charge rate (dollars per 100 cubic feet discharged) by the metered water consumption in hundred cubic feet and an \$17.58 per unit service fee, plus any modifications authorized under Section 14.26.070, for each two-month billing cycle and effective January 1, 2024, a rate determined by multiplying the user charge rate (dollars per 100 cubic feet discharged) by the metered water consumption in hundred cubic feet

and a rate that has been increased by an additional 5.0 percent, plus any modifications authorized under Section 14.26.070 per unit service fee for each two-month billing cycle; but

- (c) If the metered water consumption is not representative of the wastewater discharged into the sanitary sewer, a factor "K" may be applied to the rate determined in subdivision (b) of this subsection. The factor "K" is a ratio found by dividing the volume of wastewater discharged by the metered water consumption, both determined simultaneously during a common time period. It is the responsibility of the City that their proposed "K" factor is a valid value.
- (4) Outside City Limits Accounts: All accounts outside the City limits shall be assessed a 50-percent surcharge over and above the respective inside City limits rates.
- (5) The usual charge for sanitary sewer service shall be billed to the user, when possible, on the same bill as for domestic water. Sewer service charges shall become delinquent 15 days after date of billing.
- (6) Quantity discounts to large-volume users shall not be allowed.

(Ord. 5990 Sec. 1, 2022; Ord. 5882 Sec. 1, 2020; Ord. 5787 Sec. 1, 2018; Ord. 5678 Sec. 1, 2016; Ord. 3619 Sec. 11, 1995; Ord. 2480 Sec. 8, 1980; Ord. 1919 Sec. 2(part), 1976)

Section 2. Section 14.26.020 of the Kennewick Municipal Code, be, and the same hereby is amended to read as follows:

14.26.020: - Single-Family Dwelling Unit Rate—Schedule 1.

This schedule applies to all single-family dwelling units connected or required to be connected to the sanitary sewer. Effective January 1, 2023, within the City limits, the rate shall be a flat fee bi-monthly user charge of \$68.04, plus any modification authorized under Section 14.26.070. Effective January 1, 2024, within City limits, the rate shall be a flat fee bi-monthly user charge that has been increased by an additional 5.0 percent, plus any modification authorized under Section 14.26.070.

(Ord. 5990 Sec. 2, 2022; Ord. 5882 Sec. 2, 2020; Ord. 5787 Sec. 2, 2018; Ord. 5678 Sec. 2, 2016; Ord. 5678 Sec. 2, 2016; Ord. 3619 Sec. 12, 1995; Ord. 3121 Sec. 2(part), 1987; Ord. 3084 Sec. 2(part), 1987; Ord. 2973 Sec. 2(part), 1985; Ord. 2910 Sec. 2(part), 1985; Ord. 2888 Sec. 6(part) 1985; Ord. 2480 Sec. 9(part), 1980; Ord. 1919 Sec. 2(part), 1976)

Section 3. Section 14.26.030 of the Kennewick Municipal Code, be, and the same hereby is amended to read as follows:

14.26.030: - Multi-family Dwelling Unit Rate—Schedule 2.

This schedule applies to all multi-family dwelling units connected or required to be connected to the sanitary sewer. Effective January 1, 2023, within the City limits, the bi-monthly rate shall be calculated separately for each account using a user charge rate of \$3.187, plus any modification authorized under Section 14.26.070, for each 100 cubic feet of metered water

consumption, plus the service charge in Section 14.26.010. Effective January 1, 2024, the bi-monthly rate shall be calculated separately for each account using a user charge rate that has been increased by an additional 5.0 percent, plus any modification authorized under Section 14.26.070, for each 100 cubic feet of metered water consumption, plus the service charge in Section 14.26.010.

(Ord. 5990 Sec. 3, 2022; Ord. 5882 Sec. 3, 2020; Ord. 5787 Sec. 3, 2018; Ord. 5678 Sec. 3, 2016; Ord. 3619 Sec. 13, 1995; Ord. 3121 Sec. 2(part), 1987; Ord. 3084 Sec. 2(part), 1987; Ord. 2973 Sec. 2(part), 1985; Ord. 2910 Sec. 2(part), 1985; Ord. 2888 Sec. 6(part), 1985; Ord. 2480 Sec. 9(part), 1980; Ord. 1919 Sec. 2(part), 1976)

Section 4. Section 14.26.040 of the Kennewick Municipal Code, be, and the same hereby is amended to read as follows:

14.26.040: - Commercial/Industrial Accounts—Schedule 3.

This schedule applies to all other accounts not classified in Schedules 1 or 2 that are connected or required to be connected to the sanitary sewer. Effective January 1, 2023, within the City limits, the bi-monthly rate shall be calculated separately for each account using a user charge rate of \$3.187 plus any modification authorized under Section 14.26.070, for each 100 cubic feet of metered water consumption, plus the service charge in Section 14.26.010. Effective January 1, 2024, the bi-monthly rate shall be calculated separately for each account using a user charge rate that has been increased by an additional 5.0 percent, plus any modification authorized under Section 14.26.070, for each 100 cubic feet of metered water consumption, plus the service charge in Section 14.26.010.

(Ord 5990 Sec. 4, 2022; Ord 5882 Sec. 4, 2020; Ord. 5787 Sec. 4, 2018; Ord. 5678 Sec. 4, 2016; Ord. 3619 Sec. 14, 1995; Ord. 3121 Sec. 2(part), 1987; Ord. 3084 Sec. 2(part), 1987; Ord. 2973 Sec. 2(part), 1985; Ord. 2910 Sec. 2(part), 1985; Ord. 2888 Sec. 6(part), 1985; Ord. 2480 Sec. 9(part), 1980; Ord. 1919 Sec. 2(part), 1976)

Section 5. Section 14.26.070 of the Kennewick Municipal Code, be, and the same hereby is amended to read as follows:

14.26.070: - Indexing Fees and Charges.

Effective beginning with rates that will be in effect on January 1, 2025, the fees and charges for sewer services set out in Sections 14.26.010, 14.26.020, 14.26.030, and 14.26.040, shall be indexed annually by the Treasurer to reflect 100 percent of any change from the Consumer Price Index (U.S. Cities - Urban Wage Earners and Clerical Workers - CPI-W) for October, or other comparable index if not published. The Treasurer shall adjust the fees and publish them each December to take effect on all goods and services delivered or contracted after the beginning of each year and all utility bills mailed after the first of each year. In no event shall the cumulative change in rates be less than zero percent nor more than four percent per year nor may the change in any given year be less than zero percent nor more than four percent. Unit prices shall be to the nearest 1/10¢ and all other charges to the nearest cent.

(Ord. 5990 Sec. 5, 2022; Ord. 5882 Sec. 5, 2020; Ord. 4078 Sec. 2, 2002; Ord. 3619 Sec. 16, 1995)

Section 5. This ordinance shall be in full force and effect on January 1, 2023.

PASSED BY THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, this 1st day of November, 2022 and signed in authentication of its passage this 1st day of November, 2022.

W.D. MCKAY, Mayor

Attest:

TERRI L. WRIGHT, City Clerk

ORDINANCE NO. 5990 filed and recorded
in the office of the City Clerk of the City of
Kennewick, Washington this 2nd day of
November, 2022.

Approved as to form:

LISA BEATON, City Attorney

TERRI L. WRIGHT, City Clerk

DATE OF PUBLICATION: _____

CITY OF KENNEWICK
ORDINANCE NO. 5990

AN ORDINANCE RELATING TO SANITARY SEWER CHARGES
AND AMENDING SECTIONS 14.26.010, 14.26.020, 14.26.030,
14.26.040 AND 14.26.70 OF THE KENNEWICK MINICIPAL CODE

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water consumption shall be charged. The rate shall be determined as follows:
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dwelling unit flat fee charge rate, normally;
 - (b) Effective January 1, ~~2021~~2023, a rate determined by multiplying the user charge
rate (dollars per 100 cubic feet discharged) by the metered water consumption in
hundred cubic feet and an ~~\$15.70~~17.58 per unit service fee, plus any modifications
authorized under Section 14.26.070, for each two-month billing cycle and effective
January 1, ~~2022~~2024, a rate determined by multiplying the user charge rate (dollars
per 100 cubic feet discharged) by the metered water consumption in 100 cubic feet
and a rate that has been increased by an additional ~~6.65~~5.0 percent, plus any
modifications authorized under Section 14.26.070 per unit service fee for each two-
month billing cycle; but
 - (c) Never higher than a maximum charge determined by multiplying the number of
dwelling units in the account by the single-family dwelling unit flat fee rate.
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feet and a rate that has been increased by an additional ~~6.655.0~~ percent, plus any modifications authorized under Section 14.26.070 per unit service fee for each two-month billing cycle; but

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(~~Ord. 5990 Sec. 2, 2022~~; Ord. 5882 Sec. 2, 2020; Ord. 5787 Sec. 2, 2018; Ord. 5678 Sec. 2, 2016; Ord. 5678 Sec. 2, 2016; Ord. 3619 Sec. 12, 1995; Ord. 3121 Sec. 2(part), 1987; Ord. 3084 Sec. 2(part), 1987; Ord. 2973 Sec. 2(part), 1985; Ord. 2910 Sec. 2(part), 1985; Ord. 2888 Sec. 6(part) 1985; Ord. 2480 Sec. 9(part), 1980; Ord. 1919 Sec. 2(part), 1976)

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of metered water consumption, plus the service charge in Section 14.26.010. Effective January 1, ~~2022~~2024, the bi-monthly rate shall be calculated separately for each account using a user charge rate that has been increased by an additional ~~6.655.0~~ percent, plus any modification authorized under Section 14.26.070, for each 100 cubic feet of metered water consumption, plus the service charge in Section 14.26.010.

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(Ord. 5990 Sec. 5, 2022; Ord. 5882 Sec. 5, 2020; Ord. 4078 Sec. 2, 2002; Ord. 3619 Sec. 16, 1995)

Section 5. This ordinance shall be in full force and effect on January 1, 2023.

PASSED BY THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, this 1st day of November, 2022 and signed in authentication of its passage this 1st day of November, 2022.

W.D. MCKAY, Mayor

Attest:

TERRI L. WRIGHT, City Clerk

ORDINANCE NO. 5990 filed and recorded
in the office of the City Clerk of the City of
Kennewick, Washington this 2nd day of
November, 2022.

Approved as to form:

LISA BEATON, City Attorney

TERRI L. WRIGHT, City Clerk

DATE OF PUBLICATION: _____

Council Agenda Coversheet



Agenda Item Number	5.b.	Council Date	11/01/2022
Agenda Item Type	Ordinance		
Subject	Land Use Designations from LDR to MDR and HDR		
Ordinance/Reso #	5992	Contract #	
Project #		Permit #	CPA-2022-0005
Department	Planning		

Consent Agenda	<input type="checkbox"/>
Ordinance/Reso	<input checked="" type="checkbox"/>
Public Mtg / Hrg	<input type="checkbox"/>
Other	<input type="checkbox"/>
Quasi-Judicial	<input type="checkbox"/>

Recommendation

The Planning Commission recommends denial of CPA-2022-0005.

Motion for Consideration

I move to adopt Ordinance 5992.

Summary

The applicant, Jose Chavallo, requests to change the land use designation of 4.31-acres from Low Density Residential to High Density Residential and 33.93-acres from Low Density Residential to Medium Density Residential. The property is generally located at 2701 and 2711 S Sherman Street.

The Planning Commission held a public hearing for this application on September 19, 2022. At the hearing, the applicant and his representative spoke in favor of the proposal and multiple members of the public spoke against the proposal.

The Planning Commission voted 4-0 to recommend denial to City Council.

On October 4, 2022, Council reviewed the record and voted to 5-2 to approve the proposed amendment and hold a workshop on October 25, 2022 to create findings and conclusions to support approval. The findings and conclusions to support approval of this proposal were generated that night. The findings and Ordinance 5992 are included in the packet for Council's consideration and adoption.

Alternatives

Recommend denial of CPA-2022-0005.

Fiscal Impact

None

Through

Steve Donovan
Oct 26, 09:10:49 GMT-0700 2022

Dept Head Approval

Anthony Muai
Oct 26, 09:51:56 GMT-0700 2022

City Mgr Approval

Marie Mosley
Oct 27, 20:44:43 GMT-0700 2022

Attachments: Council Findings and Conclusions
Ordinance
Staff Report
Site Map

☐ Recording
Required?

City Council Findings and Conclusions
CPA-2022-0005 – José Chavallo

The Kennewick City Council conducted a public meeting on October 4. After reviewing the record from the September 19, 2022 Planning Commission Public Hearing, 5-2 to approve CPA-2022-0005 and scheduled a workshop on October 25, 2022 to establish findings and conclusions to support approval of CPA-2022-0005. The following are the findings and conclusions established by the City Council at that workshop.

Findings of Fact

1. The applicant is José Chavallo, 5927 W Quinault Avenue, Kennewick, WA 99336.
2. The owners is José Chavallo and Tammy Steele-Chavallo, 5927 W Quinault Avenue, Kennewick, WA 99336.
3. The request is to change the land use designation for the subject parcels from Low Density Residential to High Density Residential and Medium Density Residential.
4. The City received the application on April 19, 2022 and routed it for review to various City Departments and other local, state and federal agencies for comment on June 8, 2022.
5. City water and sewer will need to be extended to the site.
6. New access from S Sherman Street will need to be extended and built to public standards to the site.
7. The proposed amendment is adjacent to property designated Low Density Residential, Medium Density Residential and Public Facility.
8. The proposed amendment will change the land use designation for the subject sites from Low Density Residential to High Density Residential and Medium Density Residential.
9. The City issued a Revised Mitigated Determination of Non-Significance on August 10, 2022.
10. No appeal was filed for the Revised Mitigated Determination of Non-Significance.
11. The Revised Mitigation Determination of Non-Significance was adopted on August 31, 2022.
12. A public hearing notification sign was posted at the access site on September 1, 2022.
13. The Notice of Public Hearing was published in the Tri-City Herald on September 4, 2022.
14. Staff mailed or emailed the hearing notice to property owners within 300 feet of the site and parties of record on September 1, 2022.
15. Council held a meeting on October 4, 2002 and decided to approve the proposed amendment.
16. Council held a workshop on October 25, 2022 to develop findings and conclusions in favor of approving the amendment.
17. The proposed amendment bears a substantial relationship to the public health, safety, welfare, and protection of the environment. The proposed amendment has the potential to create positive impacts, such as providing additional housing types and amenities to the public. Negative impacts to the surrounding area may include increased traffic and density to the area. Increased density may also have negative impacts to geologically

hazardous areas and possible cultural resources sites. Future development will be subject to all applicable development regulations.

18. This proposed amendment is consistent with the review/processing requirements of Chapter 36.70A RCW and with the portion of the City's adopted comprehensive plan not affected by the amendment.
19. The proposed amendment does not correct an obvious mapping error.
20. The amendment to High Density Residential will allow for the development of a hotel that will address the need for additional hotel rooms in the Southridge Area.
21. The proposed Medium Density Residential land is adjacent to land that is currently designated Medium Density Residential.
22. The proposed amendment will provide housing options for future residents.

Conclusions of Law

1. Pursuant to Chapter 4.08 of the Kennewick Municipal Code, the lead agency has determined that mitigating measures are required to reduce probable significant adverse impacts on the environment.
2. The proposed amendment complies with Kennewick Municipal Code Section 4.12.110 (7) (d) by addressing the deficiency of High Density Residential lands designated in the comprehensive plan.
3. The proposed amendment complies with Kennewick Municipal Code Section 4.12.110 (8) (c) by having the same Medium Density Residential land use designation as property to the south of the site.
4. The proposed amendment complies with Kennewick Municipal Code Section 4.12.110 (8) (e) by addressing the need for land that allows for high densities.

CITY OF KENNEWICK
ORDINANCE NO. 5992

AN ORDINANCE AMENDING THE CITY OF KENNEWICK'S COMPREHENSIVE
PLAN (CPA 2022-0005, Jose Chavallo and Tammy Steele-Chavallo)

WHEREAS, the City of Kennewick, by and through its City Council, and pursuant to the Growth Management Act, directed the Planning Commission of the City of Kennewick to review and update the Comprehensive Plan for the purposes of coordinating all plans and programs relating to the physical and social development of the Kennewick Urban Growth Area and the people therein; and

WHEREAS, the City of Kennewick, in accord with the Growth Management Act and RCW 36.70A.130 and implementing municipal regulations, has directed the Department of Community Planning and the Planning Commission to review and update the plan annually; and

WHEREAS, appropriate public notice has been given and a public hearing held by the Planning Commission on September 19, 2022, concerning the proposed changes, and the same has been reviewed by the Department of Commerce for the review required under RCW 36.70A.106;
NOW, THEREFORE:

THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, DO ORDAIN AS
FOLLOWS:

Section 1. The following amendment is made to the City of Kennewick Comprehensive Plan Land Use Map as adopted by Resolution 07-12:

1. CPA 2022-0005 – 38.24 acre parcel located at 2701 and 2711 S. Sherman Street (4.31 acres from Low Density, Residential (LDR) to High Density, Residential (HDR) and 33.93 acres from Low Density, Residential (LDR) to Medium Density, Residential (MDR)).

Section 2. The property is legally described as follows:

HIGH DENSITY LEGAL DESCRIPTION

That portion of Lot 5 Short Plat 1681 according to the survey thereof recorded in Volume 1 of Surveys Page 1681 under Auditor's file number 90-3102 records of Benton County, Washington and a portion of; the East 300 feet of the South 1089.45 feet as measured along the South and East lines of the Southwest quarter of the Southeast quarter of Section 8, Township 8 North, Range 29 East, W.M. and a portion of lot 2 Short Plat 3081 according to the survey thereof recorded in Volume 1 of Short Plats, Page 3081 recorded under Auditor's file number 2008-005341 records of Benton County, Washington. All being in the Southeast quarter of Section 8 Township, 8 North, Range 29 East, W.M. and described as follows:

Commencing at the Southeast corner of said Southeast quarter of Section 8; thence along the South line of said Section 8 South 88°03'21" West for 1956.90 feet to the Southwest corner of said Lot 2 Short Plat 3081; thence along the West line of said Lot 2 Short Plat 3081 North 00°25'28" West for 862.26 feet to the True Point of Beginning thence continuing North 00°25'28" West for 124.71 feet to the start of a 250.00 foot Non-Tangent curve (whose radius bears South 02°03'31" West); thence Southeasterly along said curve to the right through a central angle of 26°01'01" an arc length of 113.52 feet; thence South 61°55'27" East for 249.35 feet to intersect the East line of said Lot 2 Short Plat 3081; thence along said East line North 00°27'37" West for 260.60 feet to the Northeast corner of the East 300 feet of the South 1089.45 feet as measured along the South and East lines of the Southwest quarter of the Southeast quarter of Section 8, Township 8 North, Range 29 East, W.M.; thence along the North line thereof North 88°03'21" East for 300.10 feet to intersect the West line of said Lot 5 Short Plat 1681; thence continuing North 88°03'21" East for 25.00 feet; thence South 00°27'37" East for 460.72 feet; thence South 88°34'46" West for 167.69 feet; thence North 66°08'58" West for 532.08 feet to the True Point of Beginning.

Containing 4.31 acres

MEDIUM DENSITY LEGAL DESCRIPTION

That portion of Lot 5 Short Plat 1681 according to the survey thereof recorded in Volume 1 of Surveys Page 1681 under Auditor's file number 90-3102 records of Benton County, Washington and a portion of; the East 300 feet of the South 1089.45 feet as measured along the South and East lines of the Southwest quarter of the Southeast quarter of Section 8, Township 8 North, Range 29 East, W.M and a portion of lot 2 Short Plat 3081 according to the survey thereof recorded in Volume 1 of Short Plats, Page 3081 recorded under Auditor's file number 2008-005341 records of Benton County, Washington. All being in the Southeast quarter of Section 8 Township, 8 North, Range 29 East, W.M. and described as follows:

Beginning at the Southeast corner of said Southeast quarter of Section 8; thence along the East line of said Section 8, North 00°29'45" West for 455.53 feet; thence South 47°15'59" West for 36.83 feet; thence North 54°45'35" West for 150.88 feet; thence South 89°05'10" West for 150.60 feet; thence North 46°55'35" West for 640.89 feet; thence North 57°31'34" West for 670.62 feet to intersect the West line of said Lot 5 Short Plat 1681; thence along said West line South 00°27'37" East for 268.23 feet; thence North 88°03'21" East for 25.00 feet; thence South 00°27'37" East for 460.72 feet; thence South 88°34'46" West for 167.69 feet; thence North 66°08'58" West for 532.08 feet to intersect the West line of said Lot 2 Short Plat 3081; thence along said West line South 00°25'28" East for 521.17 feet to intersect the Southerly Right of Way of Kennewick Irrigation District Amon Pump Lateral; thence along said Right of Way the following courses and distances, South 65°02'36" East for 402.18 feet to the start of a 607.55 foot radius curve (whose radius bears North 24°57'26" East); thence Southeasterly along said

curve to the left through a central angle of 07°56'00" an arc length of 84.12 feet; thence South 72°58'34" East for 132.10 feet to the start of a 155.58 foot radius curve (whose radius bears South 17°01'26" West); thence Southeasterly along said curve to the right through a central angle of 12°05'00" an arc length of 32.81 feet; thence South 60°52'40" East for 34.96 feet to intersect the West line of said Lot 5 Short Plat 1681; thence along said West line South 00°27'16" East for 51.44 feet to the Southwest corner of the Southeast quarter of the Southeast quarter of said Section 8; thence along the South line thereof North 88°03'21" East for 1328.57 feet to the True Point of Beginning.

Containing 33.93 acres

Section 3. This ordinance shall be in full force and effect five days from and after its passage, approval and publication as required by law.

PASSED BY THE CITY COUNCIL OF THE CITY OF KENNEWICK, WASHINGTON, this 1st day of November, 2022, and signed in authentication of its passage this 1st day of November, 2022.

Attest:

W.D. MCKAY, Mayor

TERRI L. WRIGHT, City Clerk

ORDINANCE NO. 5992 filed and recorded
in the office of the City Clerk of the City of
Kennewick, Washington this 2nd day of
November, 2022.

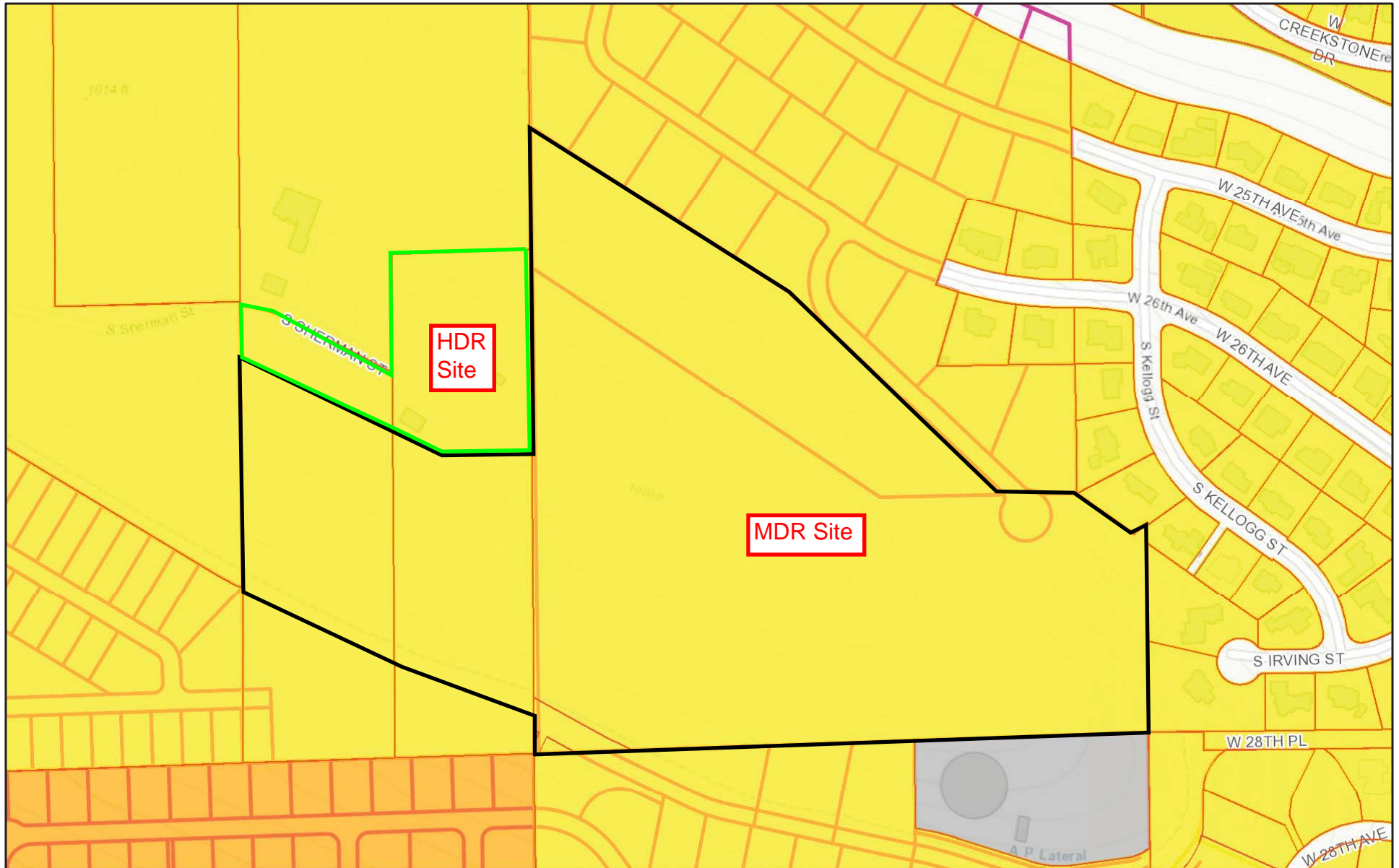
Approved as to Form:

LISA BEATON, City Attorney

TERRI L. WRIGHT, City Clerk

DATE OF PUBLICATION:_____

Site Map




May 5, 2022


This plan is suitable for informational use only. City of Kennewick accepts no liability for any error whatsoever.

CountyParcelLayer **SurveyAddressPoint**

StreetName

 <all other values>

 Apartment

 Building

 Condo

 Mobile Home

Parcel



1 inch = 300 feet

0 0.0275 0.055 0.11 mi

0 0.0425 0.085 0.17 km

Sources: Esri, HERE, Garmin, Intermap, increment P Corp.,

ArcGIS WebApp Builder
City of Kennewick

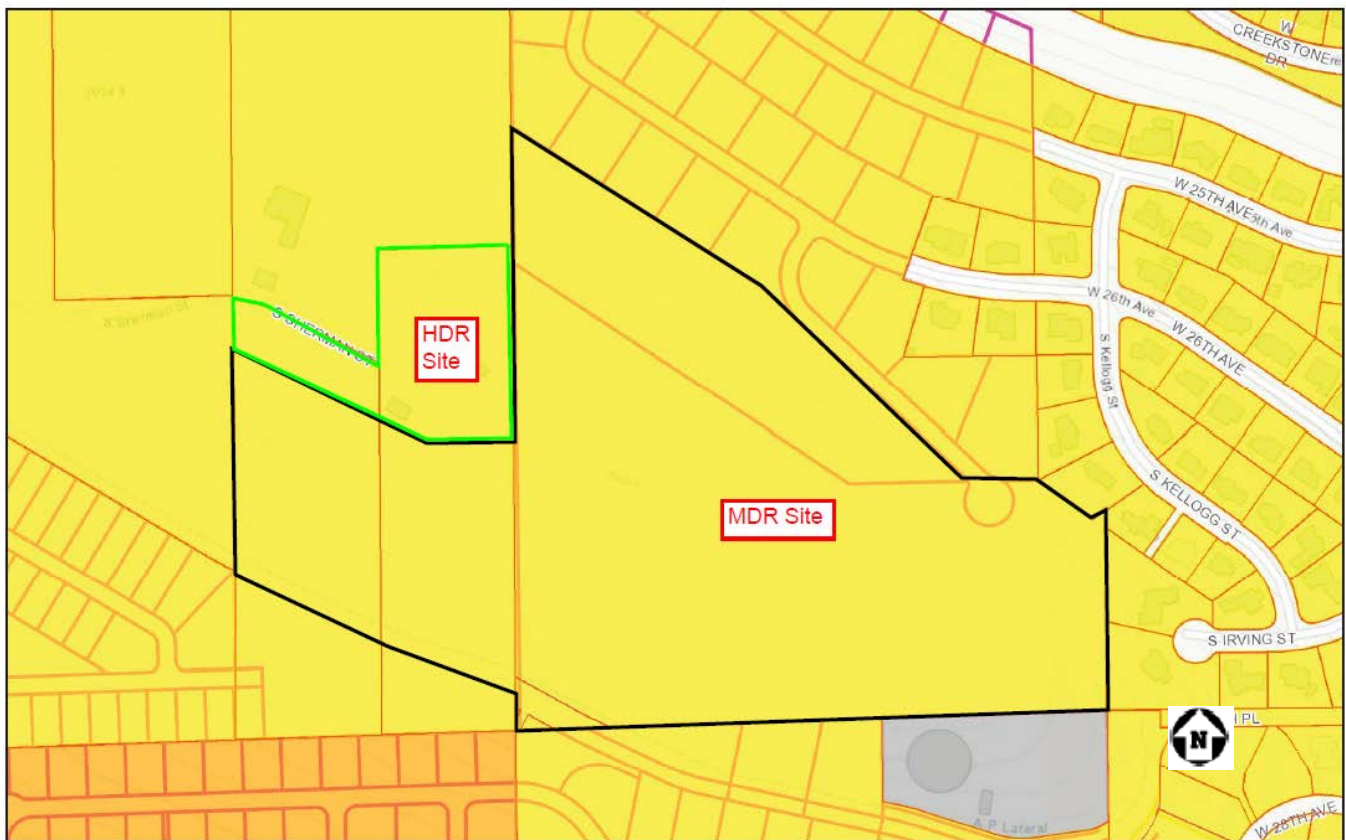


Comprehensive Plan Amendment 2022-0005

REQUEST: Change 4.31 acres from Low Density Residential (LDR) to High Density Residential (HDR) and change 33.93 acres from Low Density Residential (LDR) to Medium Density Residential (MDR).

APPLICANT: José Chavallo

OWNERS: José Chavallo and Tammy Steele-Chavallo



Not to scale

SITE INFORMATION

- **Size:** 38.24 acres
- **Location:** 2701 and 2711 S Sherman Street
- **Topography:** Steep Slopes
- **Existing Comprehensive Plan Designation:** Low Density Residential
- **Existing Zoning:** Residential, Suburban (RS) and Residential, Low Density (RL)
- **Existing Land Use:** Single-Family Residence and Vacant Land

EXHIBITS

- **Exhibit A-1:** Aerial Map, with existing and proposed developments
- **Exhibit A-2:** Land Use Map
- **Exhibit A-3:** Application/Supplemental Questions
- **Exhibit A-4:** Traffic Engineering Division Comments, 8/16/22
- **Exhibit A-5:** Kennewick Irrigation District Comments, 6/15/22 and 8/29/22
- **Exhibit A-6:** Bonneville Power Administration Comments, 6/24/22
- **Exhibit A-7:** Conceptual Access and Layout Design
- **Exhibit A-8:** Michal Black, P.E., White Shield, Inc., Stormwater Comments, 8/24/22
- **Exhibit A-9:** Chavallo Response, 8/26/22
- **Exhibit A-10:** Geotechnical Investigation 8/21/17
- **Exhibit A-11:** Critical Areas Report, 7/1/09 and Critical Areas Report – Citadel, 10/15/10
- **Exhibit A-12:** Harms Engineering – Stormwater, 12/14/20
- **Exhibit A-13:** Sunburst Engineering – Traffic, 12/22/20
- **Exhibit A-14:** 2010 SEPA Checklist and MDNS
- **Exhibit A-15:** Revised SEPA Checklist, 2020
- **Exhibit A-16:** SEPA Analysis, 2/23//21 and SEPA Review 8/10/22
- **Exhibit A-17:** Revised MDNS, 8/10/22
- **Exhibit A-18:** Adoption of Existing Environmental Document, 8/31/22
- **Exhibit A-19:** Public Comments, 53 Letters

APPLICATION PROCESS

- Application Submitted: April 19, 2022
- Application routed for comments: June 8, 2022
- A Revised Mitigated Determination of Non-Significance (MDNS) ED 20-14/PLN-2020-01014 was issued August 10, 2022.
- No appeal was filed for the Revised Mitigated Determination of Non-Significance (MDNS) ED 20-14/PLN-2020-01014
- The Revised Mitigated Determination of Non-Significance (MDNS) ED 20-14/PLN-2020-01014 was adopted for CPA-2022-0005 on August 31, 2022.
- A property posting sign notifying the public of a public hearing on this request was posted at the access point on S Sherman Street on September 1, 2022.
- Notice of Public Hearing was published in the Tri-City Herald on September 4, 2022.
- Notice of Hearing mailed September 1, 2022.

SURROUNDING COMPREHENSIVE PLAN, ZONING AND LAND USES

North	Comprehensive Plan – Low Density Residential Zoning – Residential Suburban (RS) and Residential, Low Density (RL) Existing Land Uses – Single-Family Residential, Vacant Land and Undeveloped Preliminary Plat
South	Comprehensive Plan – Low Density Residential, Medium Density Residential and Public Facility Zoning – Residential, Low Density (RL), Residential, Medium Density (RM) and Public Facility, (PF) Existing Land Uses – Vacant Land, Water Reservoir and 2 Partially Developed Preliminary Plats
East	Comprehensive Plan – Low Density Residential Zoning – Residential, Low Density (RL) Existing Land Uses – Single-Family Residential
West	Comprehensive Plan – Low Density Residential Zoning – Residential, Low (RL) Existing Land Uses – Vacant Land and Single-Family Residential

REGULATORY CONTROLS

- City of Kennewick Comprehensive Plan
- Kennewick Municipal Code (KMC) Title 4
- Kennewick Municipal Code (KMC) Title 18

DESCRIPTION OF REQUEST

The applicant has requested to change the land use designation of 4.31 acres from LDR to HDR and 33.93 acres from LDR to MDR. The KMC contains specific criteria and additional factors to consider in order to approve the requested land use designation amendment. An evaluation of the criteria and additional factors follow:

COMPLIANCE WITH TITLE 4 (ADMINISTRATIVE PROCEDURES)

KMC 4.12.110(7): Approval Criteria. The City may approve Comprehensive Plan Amendments and area-wide zone map amendments if it finds that the request meets one or more of the following:

1. The proposed amendment bears a substantial relationship to the public health, safety, welfare, and protection of the environment;
At this point, the applicant intends to develop a condominium development and a boutique hotel consisting of approximately 60-units at the site. All permitted uses in the Residential, High Density (RH) and Residential, Medium Density (RM) zoning districts may take place at the sites if the requested amendment is approved. The proposed amendment site is surrounded by Low Density Residential Development and has varying degrees of steep slopes, erosion hazard areas, uncertified shrub steppe and an Aquifer Recharge Area. Future development of the site will be required to meet applicable critical area and infrastructure regulations.
2. The proposed amendment is consistent with the requirements of Chapter 36.70A RCW and with the portion of the City's adopted comprehensive plan not affected by the amendment;
This amendment is consistent with the requirements of Chapter 36.70A RCW and with the portion of the City's adopted comprehensive plan not affected by the amendment.
3. The proposed amendment corrects an obvious mapping error; or
This request does not correct a mapping error.
4. The proposed amendment addresses an identified deficiency in the Comprehensive Plan.
The proposed amendment does not significantly address an identified deficiency in the Comprehensive Plan. The comprehensive plan states that it has a deficit of 159.2 acres for land designated HDR. That deficiency has been reduced by approximately 45 acres over the last few amendment cycles.
Additionally, the city currently has a 1,387.2 acre surplus of lands designated LDR and a 248.1 acre surplus of lands designated MDR. However, location and land use context are critical issues to consider when evaluating a comprehensive plan amendment and ultimately rezone of this nature and magnitude.

KMC 4.12.110(8): Additional Factors. The City must also consider the following factors prior to approving Comprehensive Plan Amendments:

1. The effect upon the physical environment;
The applicant has submitted information on what type of development may take place at the site. Depending on the building type and road design, a significant amount of grading work may be needed for the site.

In 2020, the applicant's engineer estimated that approximately an additional 17% of storm water runoff might be generated by a high-density residential development. Although the applicant is no longer, proposing that the entire site be designated HDR, the proposed increase in density will

increase stormwater runoff, but it will most likely be less than what was estimated for High Density Residential.

The applicant provided documentation, see Exhibit A-6, that the site's storm water may be conveyed to a site at the NW corner of the S Sherman Street and Bob Olson Parkway. Now, it is only a possibility, no specific design work or agreements have been completed.

2. The effect on open space and natural features including, but not limited to topography, streams, rivers, and lakes;

All of the property involved in the amendment is under private ownership with no public access. At this time, it is unknown what impact future site development will have on the existing slopes, but the site will be impacted in some way.

The site contains slopes greater than 40%, slopes greater than 15% and erosion hazard areas. Slopes greater than 40% are not allowed to be developed. Geo-technical studies, a cultural resource review and an aquifer recharge review will be required to determine the needed mitigation measures for future development.

3. The compatibility with and impact on adjacent land uses and surrounding neighborhoods;

It is staff's opinion that the proposed amendment will not be compatible with the majority of the surrounding properties. At most, the current maximum density of the site is determined by the minimum lot size of 7,500 square feet or approximately 5 units per acres. The applicant is proposing two land use designations, one (HDR) that allows a density of up to 27 units per acre and one (MDR) that allow a density of up to 13 units per acre.

The proposed land use designation will allow for implementing zoning districts that will allow for a wide variety of land uses including single-family residential, multi-family residential, mini-storage and hotels/motels.

The applicant is considering a 60-unit hotel at the proposed 4.31-acre HDR site. Additionally, based on 4.31 acres at 27 units per acre, the possible development levels could be 116 dwelling units.

The applicant is proposing a condominium development, which may consist of approximately 360 dwelling units for the 33.93-acre MDR site. The maximum density allowed for 33.93-acres at 13 units per acre, is 441 dwelling units.

The majority of the 38.24 acres site is surrounded by property designated LDR. The property adjacent to the southwest corner of the site is designated MDR, the property was developed as a low-density subdivision, but the land use designation was changed to MDR so the owner could construct duplexes. Additionally, the property adjacent to the southeast corner of the site is designated Public Facility (PF) with a city water reservoir.

4. The adequacy of, and impact on community facilities, including utilities, roads, public transportation, parks, recreation, and schools;

The site developer will need to construct water, sewer and stormwater utility extensions and required access to the site.

The following trip analysis is based on the maximum density allowed at the site, not the density proposed by the applicant:

- Current LDR Density: 1,605 Average Daily Trips (ADT)
- Proposed HDR and MDR Densities: A net increase of 1,432 ADT could occur if the HDR and MDR maximum densities are reached, totaling 3,037 ADT. The applicant has not proposed developing the site to the maximum density allowed.
- A PM peak increase of 89 vehicles per hour is estimated because of the proposed density increase.

See Exhibit A-4 for a more detailed evaluation of traffic generation.

Roadway improvements and Traffic Impact Fees will be required once development occurs at the site.

Park Fees will be applicable to future development and required to be paid prior to issuing the Certificate of Occupancy.

The Kennewick School District has reviewed the proposal and has no comment at this time.

5. The quantity and location of land planned for the proposed land use type and density and the demand for such land;
Comprehensive Plan Table 2: Land Inventory; shows that the City has a deficit of 159.2 acres for lands designated HDR projected until 2037. Recent amendments to HDR have reduced the deficit of those lands. The proposed addition of 4.31 acres of HDR land is minimal and using that land for a hotel will not decrease the need for HDR land for housing.
6. The current and projected project density in the area; and
The RL Zone maximum density of 5 units/acres is based on the required minimum lot size of 7,500 square feet. The RS Zone maximum density of 4 units/acres is based on the minimum lot size of 10,500 square feet. The HDR implementing zoning district permits a maximum density of 27 units/acre and the MDR implementing zoning district permits a maximum density of 13 units per acre.
7. The effect, if any upon other aspects of the Comprehensive Plan.
The proposed amendment will not affect any other aspects of the Comprehensive Plan.

PUBLIC COMMENT

Staff has received comments from numerous members of the public with concerns about possible construction of a hotel, multi-family residences, access, traffic, utilities and critical areas. All received public comments are included in Exhibit A-18.

AGENCY COMMENTS

The Kennewick Irrigation District submitted standard comments on not encumbering its easements with permanent structures, protecting irrigation facilities and that site is not considered irrigable. Additionally, it stated that it is open to a discussion on future development using its easement to access S Sherman Street, see Exhibit A-5.

The Bonneville Power Administration stated that the proposal would not have a direct impact on its facilities, see Exhibit A-6.

ANALYSIS OF REQUEST

The following Residential Site Development Standards will apply to the site if approved:

- 4.31-acre HDR site's density will be increased to 27 units per acre. Minimum lot size for the RH Zone is 4,000 square feet for single-family residences and 1,600 square feet for a Rowhouse/Townhouse. A maximum of 116 units could be developed on the site if zoned RH.
- 33.93-acre MDR site's density will be increased to 13 units per acre. Minimum lot size for the RM Zone is 4,000 square feet for single-family residences and 1,800 square feet for Rowhouse/Townhouse. A maximum of 441 units could be developed on the site if zoned RM

Below are the current Residential Site Development Standards:

- No maximum density for LDR designated property, must meet minimum lot area.
- Minimum lot size for the RS Zone is 10,500 square feet
- Minimum lot size for the RL Zone is 7,500 square feet.

The City has a 1,387.2-acre surplus of LDR and a 248.1-acre surplus of MDR designated lands. The proposed amendment to HDR, along with the proposed use will unlikely reduce the need for more dense residential

development. When considering the proposed changes to the comprehensive plan and ultimately rezoning the property, other factors and issues must be considered rather than simply adding higher density residential land to the city's land inventory.

Single-family residential is the primary use permitted in both the RS and RL Zones. The applicant has proposed a boutique hotel and 360-unit condo/townhome development for the site and it is the first of its type proposed on Thompson Hill. In addition to single-family residences and multi-family residential being permitted in the RM and RH Zones, mini-storage and hotels/motels are also permitted in the RH Zone. If approved, any uses in the RM and RH Zones could also take place on the sites.

In 2020, the applicant proposed to amend the land use designation of the entire site from LDR to HDR. A Mitigated Determination of Non-Significance was issued for the proposal in February 2021. Staff reviewed the current application and determined that the request was similar since the contemplated development was the same, but at a lesser intensity since only 4.31 acres were proposed for HDR and 33.93 acres were proposed for MDR. Staff concluded; that with the addition of two more mitigation measures, the previous SEPA Threshold Determination and supporting documents would be sufficient to meet the SEPA review requirements for the current application, see Exhibits A-10 thru A-16.

It is staff's opinion that the proposed amendment is an abrupt transition between existing land use designations, since the single family residential developments are adjacent and in the immediate vicinity of the site. The closest HDR designated property is along W Hildebrand Boulevard and Bob Olson Parkway. As stated earlier, the property to the southeast is designated MDR, but that property is separated by a canal and additional LDR land from the proposed site. The same MDR land was originally designated LDR and it is currently being developed as a single-family residential subdivision.

FINDINGS

1. The applicant is José Chavallo, 5927 W Quinault Avenue, Kennewick, WA 99336.
2. The owners is José Chavallo and Tammy Steele-Chavallo, 5927 W Quinault Avenue, Kennewick, WA 99336.
3. The request is to change the land use designation for the subject parcels from Low Density Residential to High Density Residential and Medium Density Residential.
4. The City received the application on April 19, 2022 and routed it for review to various City Departments and other local, state and federal agencies for comment on June 8, 2022.
5. City water and sewer will need to be extended to the site.
6. New access from S Sherman Street will need to be extended and built to public standards to the site.
7. The proposed amendment is adjacent to property designated Low Density Residential, Medium Density Residential and Public Facility.
8. The City issued a Revised Mitigated Determination of Non-Significance on August 10, 2022.
9. No appeal was filed for the Revised Mitigated Determination of Non-Significance.
10. The Revised Mitigation Determination of Non-Significance was adopted on August 31, 2022.
11. A public hearing notification sign was posted at the access site on September 1, 2022.
12. The Notice of Public Hearing was published in the Tri-City Herald on September 4, 2022.
13. Staff mailed or emailed the hearing notice to property owners within 300 feet of the site and parties of record on September 1, 2022.
14. The proposed amendment bears a substantial relationship to the public health, safety, welfare, and protection of the environment. The proposed amendment has the potential to create positive impacts, such as providing additional housing types and amenities to the public. Negative impacts to the

surrounding area may include increased traffic and density to the area. Increased density may also have negative impacts to geologically hazardous areas and possible cultural resources sites. Future development will be subject to all applicable development regulations.

15. This proposed amendment is consistent with the review/processing requirements of Chapter 36.70A RCW and with the portion of the City's adopted comprehensive plan not affected by the amendment.
16. The proposed amendment does not correct an obvious mapping error.
17. The 4.31 acres amended to HDR and possible hotel will have a minimal impact the City's HDR deficit.

CONCLUSIONS

1. Pursuant to Chapter 4.08 of the Kennewick Municipal Code, the lead agency has determined that mitigating measures are required to reduce probable significant adverse impacts on the environment.
2. The proposed amendment will change the land use designation for the subject sites from Low Density Residential to High Density Residential and Medium Density Residential.
3. The proposed amendment is not consistent with the City of Kennewick Comprehensive Plan in regard to its compatibility with surrounding properties that are designated Low Density Residential. The proposed amendment will have a minimal impact on other aspects of the plan.
4. The proposed amendment will permit an increase to residential and commercial activities in the area.
5. Future development of the site has the potential to affect the park and traffic system. Future development of the site is subject to applicable Park Impact Fees, Traffic Impact Fees and improvements to the existing road network in the immediate area.

Recommendation

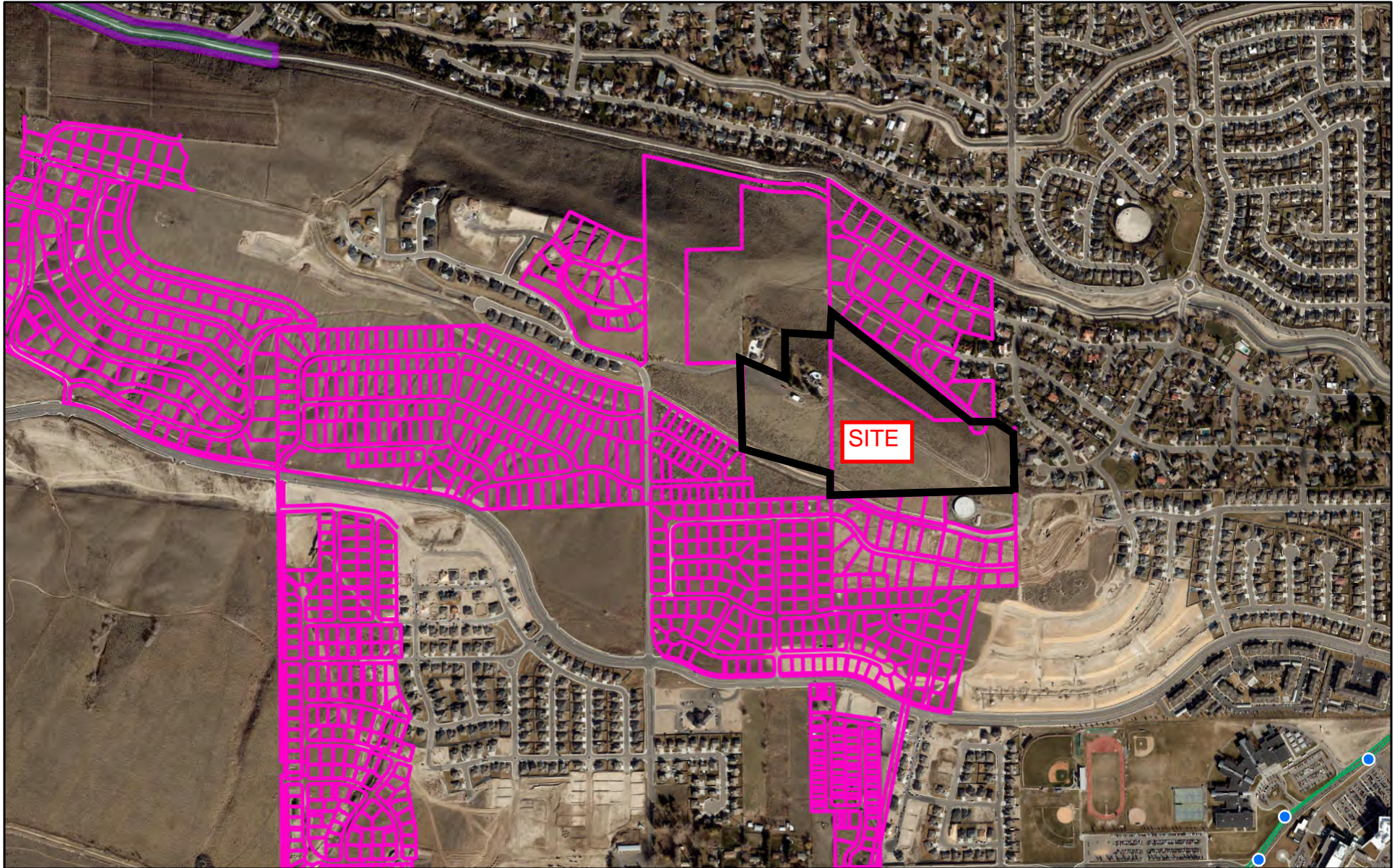
Staff recommends that the Planning Commission concur with the findings and conclusions of CPA 22-05 contained in the staff report and recommend denial to City Council.

Motion

I move that the Planning Commission concur with the findings and conclusions of CPA 22-05 contained in the staff report and recommend to City Council denial of the request.

Aerial Map

Exhibit A-1



September 6, 2022 This plan is suitable for informational use only. City of Kennewick accepts no liability for any error whatsoever.



1 inch = 1,000 feet 1:12,000

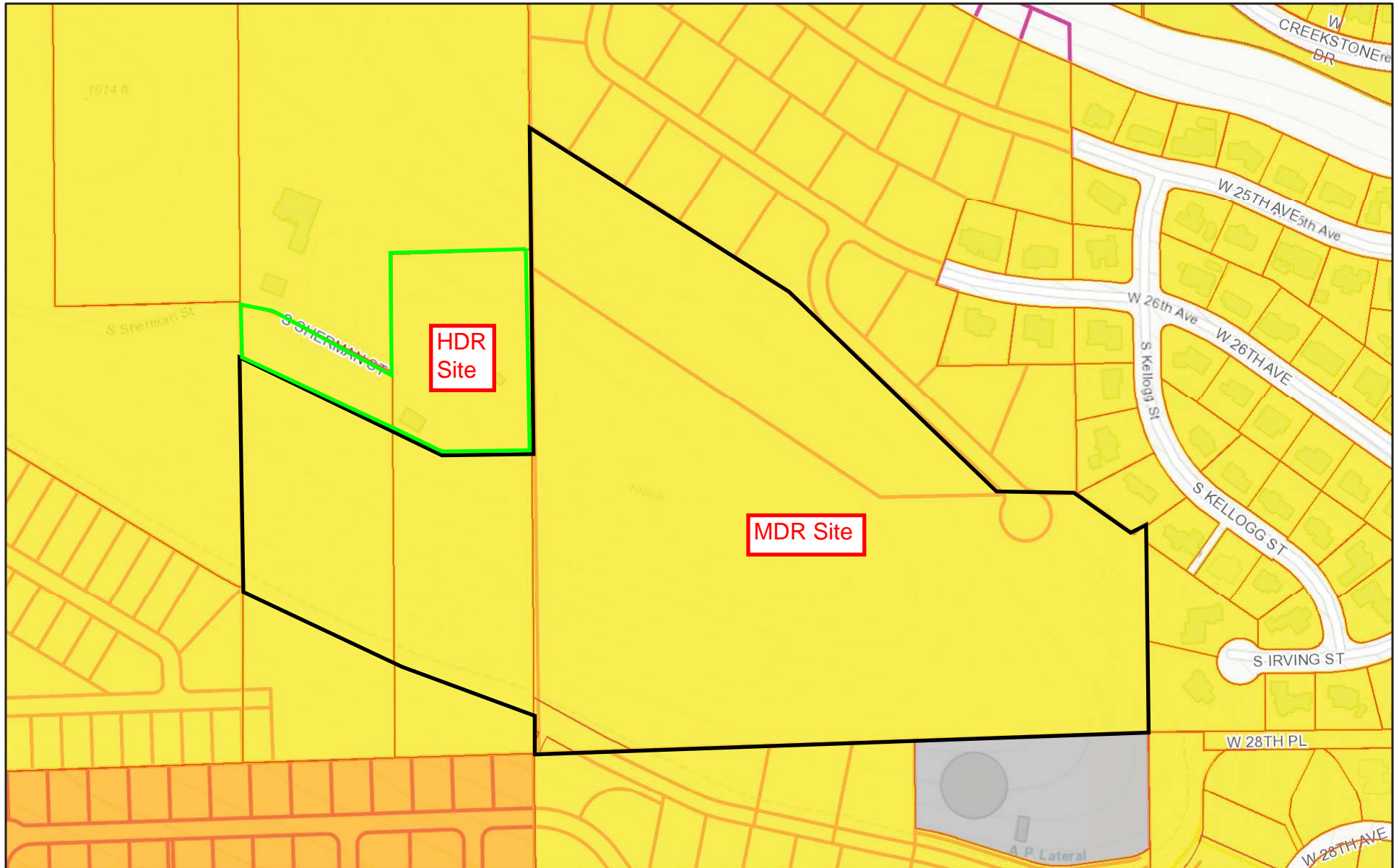
0 0.075 0.15 0.3 mi

0 0.125 0.25 0.5 km

Sources: Esri, HERE, Garmin,
Intermap, increment P Corp.,

ArcGIS WebApp Builder
City of Kennewick

Utilities Map



May 5, 2022

This plan is suitable for informational use only. City of Kennewick accepts no liability for any error whatsoever.

CountyParcelLayer **SurveyAddressPoint**

StreetName



<all other values>



Apartment



Building



Condo



Mobile Home

Parcel



1 inch = 300 feet

1:3,600

0 0.0275 0.055 0.11 mi

0 0.0425 0.085 0.17 km

Sources: Esri, HERE, Garmin, Intermap, increment P Corp.,

ArcGIS WebApp Builder
City of Kennewick

CITY OF KENNEWICK
COMMUNITY PLANNING & DEVELOPMENT SERVICES
APPLICATION (general form)

PROJECT # _____ - _____ PLN- _____ - _____ FEE \$ _____

Please completely fill out this form and return it to Community Planning & Development Services, PO Box 6108, Kennewick, WA 99336, along with the application fee (see fee schedule). Attach a copy of the checklist for the land use application you are submitting. The application submittal must contain all of the information requested on the checklist in order to be processed. **Incomplete applications will not be accepted.**

Check one of the following for the type of application you are submitting:

Site Plan Tier 1 ☐ Tier 2 ☐ Tier 3 ☐ Binding Site Plan ☐

Short Plat ☐ Conditional Use ☐ Other 2022 Comprehensive Plan Amendment

Environmental Determination PLN- _____ - _____ Pre Application Meeting PLN- _____ - _____

Applicant: Jose A. Chavallo

Address: 5927 W. Quinault Avenue, Kennewick, WA

Telephone: _____ Cell Phone: 509-539-1067 Fax: _____ E-mail: NewEnvCorp@gmail.com

Property Owner (if other than applicant): Jose A. Chavallo and Tammy K. Steele-Chavallo

Address: 5927 W. Quinault Avenue, Kennewick, WA

Telephone: _____ Cell Phone: 509-539-1067 E-mail: NewEnvCorp@gmail.com

SITE INFORMATION

Parcel No. 1-0889-400-0004-000, 1-0889-301-3081-002 (*) Acres 39.76 Ac. Zoning: RL & RS

Address of property: 2701 & 2711 S. Sherman Street, and two unaddressed lots

Number of Existing Parking Spaces N/A Number of Proposed (New) Parking Spaces N/A

Present use of property One residential lot and three vacant properties

Size of existing structure: N/A sq. ft. Size of Proposed addition/New structure: N/A sq. ft.

Height of building: N/A Cubic feet of excavation: N/A Cost of new construction n/A

Benton County Assessor Market Improvement Value: _____

Description of Project: Change portions of the aforementioned parcels from Low Density Residential to Medium and High Density Residential

(*) Parcel No. lots 37 & 38 Citadel Estates Preliminary Plat

I, the undersigned, do hereby certify that, to the best of my knowledge, the information provided above is true and correct.

Applicant's Signature

Signature of owner or owner's authorized representative

Date: 4/21/22

Comprehensive Plan Amendment Supplemental Information

The following questions will be reviewed by both the Planning Commission and City Council as a means of assisting in their consideration of the Comprehensive Plan Amendment request. Use additional pages if necessary.

1. State the requested amendment:

Amend 2022 Comprehensive Plan
From Low Density Residential (LDR)
to 35.45 Ac. Medium Density Residential (MDR), and
4.31 Ac. High Density Residential (HDR)

2. What are the reasons for the requested amendment:

Allow amending zoning area to Medium Density and High Density Residential

3. Which elements of the Comprehensive Plan will be affected and how. Include detailed information on the provision of utilities such as water, sewer, power, etc., and how such utilities correspond with the City's various utility plans. Detailed information must also be submitted regarding what effect the proposed change will have on such services as fire, police, parks, schools, etc:

Development of the areas will use the same public utilities and services as current Comprehensive and Zoning areas.

4. Indicate how the requested amendment will implement the Comprehensive Plan and be in the best interest of the Kennewick area, reference specific Comprehensive Plan policies that will be implemented:

Hillside development areas require alternative design standards to increase density and preserve more open areas within the development. A condensed development requires land usage for roads and utilities.

5. Include any other substantiated information in support of the requested amendment:

The City of Kennewick is needing additional areas for Medium and high Density housing.
The City of Kennewick needs more variety in housing styles and types of construction.
A more flexible design area will facilitate growth and attractive livability within the Southridge area.



MEMORANDUM

PUBLIC WORKS

DATE: 8/16/2022

TO: Cary M. Roe, P.E. – Public Works Director

CC: John Cowling, P.E. – Assistant Public Works Director

FROM: Sorin Juster, P.E., PTOE – Transportation Systems Manager

SUBJECT: **Evaluation of Traffic Generation Letter of December 22, 2020**
 Comparison of Traffic Generation Based on Current Comp Plan Zoning to
 Traffic Generation Based on the Proposed Comp Plan Zoning Change

PROJECT NO.: PLN-2020-2960

As stated in the subject of this memo above, the purpose of this memo is twofold:

1. Evaluation of Traffic Generation, based on the current comp plan zoning (low density residential), or single family detached housing (ITE Land Use)
2. Comparison of Traffic Generation Based on Current Comp Plan Zoning (Low density residential), or single family detached housing (ITE Land Use) to Traffic Generation Based on the Proposed Comp Plan Zoning Change, hotel (ITE Land Use) (high density residential) and Mid-rise multi-family (ITE Land Use), (medium density residential).

For comparison purpose, all trip generation calculations were based on the ITE's, Trip Generation Manual, 10th Generation

- a) Based on the current comp plan zoning single-family detached housing (low density residential):
 - a. At 5 units/ acre x 34 acres (6 acres reduction for steep slopes) = 170 units
 - b. ADT Volume 170 units x 9.44 Trips/ unit = 1,605 ADT
 - c. PM Peak Volume 170 units x 0.99 trips/ unit = 168 vph

PUBLIC WORKS

Name of Recipient
Company or Title of Recipient

Date
Page 2 of 3

- b) Based on the proposed comp plan zoning change:
- a. The hotel portion 4.30 acres at 27 rooms/ acre = 116 rooms.
ADT $116 \text{ room} \times 8.36 \text{ trips/ room} = 970 \text{ ADT}$
PM Peak $116 \text{ room} \times 0.6 \text{ trips/ room} = 90 \text{ vph}$
 - b. The multi-family housing (mid-rise)
35.7 gross acres – 6 acres reduction for steep slopes
28.7 acres at 13 units/ acre = 380 units
ADT $380 \text{ units} \times 5.44 \text{ trips/ unit} = 2,067 \text{ ADT}$
PM Peak $380 \text{ units} \times 0.44 \text{ trips/ unit} = 167 \text{ vph}$
 - c. Total proposed comp plan zoning change trip generation
ADT (116 rooms hotel) $970+2,067=3037 \text{ ADT}$
PM Peak (116 room hotel) $90+167=257 \text{ vph}$
- c) Total net trip change from current comp plan:
- ADT (116 rooms hotel) $3,037-1,605=1,432 \text{ ADT net increase}$
- PM Peak (116 room hotel) $257-168=89 \text{ vph}$

Name of Recipient
Company or Title of Recipient

Date
Page 3 of 3

PUBLIC WORKS

1010 E. Chemical Drive * PO Box 6108 * Kennewick, WA 99336
(509) 585-4419 * Fax (509) 585-4451



2015 South Ely Street
Kennewick, WA 99337
Customer Service 509-586-9111
Business 509-586-6012
FAX 509-586-7663
www.kid.org

June 15, 2022

Steve Donovan
City of Kennewick/Development Services Division
PO Box 6108
Kennewick, WA 99336

Subject: Review Comments for CPA-2022-0005/SEPA ED-2022-0013

Dear Mr. Donovan:

The Kennewick Irrigation District has received your Comprehensive Plan Amendment and SEPA documents submitted by Jose Chavallo and Tammy Steele-Chavallo, 5927 W Quinault Ave, Kennewick, WA 99336, for a Comprehensive Plan Amendment to change the land designation of 38.24 acres located at 2701 and 2711 S Sherman Street from Low Density Residential (LDR) to Medium Density Residential (MDR) 33.93 acres and High Density Residential (HDR) 4.31 acres.

1. This parcel is within the Kennewick Irrigation District (KID) boundaries, but is not considered irrigable lands; therefore, the Kennewick Irrigation District does not assess them.
2. Please note that permanent structures are not allowed within irrigation easements.
3. Please protect all existing irrigation facilities.

If you have any questions regarding these comments, please contact me at the address/phone number listed above.

Sincerely,

Chris D. Sittman
CAD Specialist

cc: LB\correspondence\File 08-08-29
Applicant via mail – Jose Chavallo and Tammy Steele-Chavallo, 5927 W Quinault Ave, Kennewick, WA 99336

From: [Jason McShane](#)
To: [Steve Donovan](#); [Anthony Muai](#)
Subject: RE: Comprehensive Plan Amendment #22-05 (Citadel Estates and Citadel South)
Date: Monday, August 29, 2022 6:17:33 PM

Steve,

Took the subject from a previous email. Please see revised.

From: Jason McShane
Sent: Monday, August 29, 2022 5:37 PM
To: 'steve.donovan@ci.kennewick.wa.us' <steve.donovan@ci.kennewick.wa.us>;
'anthony.muai@ci.kennewick.wa.us' <anthony.muai@ci.kennewick.wa.us>
Subject: Comprehensive Plan Amendment #20-06 (Citadel Estates and Citadel South)

Please add this to the record for KID:

Regarding the CPA # 22-05

Regarding access from S. Sherman St. to the subject property above the Kennewick Irrigation District Amon Pump Lateral Canal.

KID's understanding of the proposal is as follows:

The applicant and property owner is proposing a Comprehensive Plan Amendment for a portion of the property from Low Density Residential to High Density Residential.

Regarding Citadel Estates Pre-Plat:

1. This change of zone does not change the Citadel Estates Pre-plat that is proposed as a gated community from the existing streets of W. 25th Ave. and W. 26th Ave. on the Eastern boundary of your property.
2. The Citadel Estates Pre-Plat is for 38 lots with access from these stated existing roadways. In addition to these accesses, the City of Kennewick has required that the applicant provide a Secondary Emergency Vehicle Access (SEVA) road that provides access to the top of the applicant's property. This is principally required for fire protection. This will not be used for normal vehicular access.

Regarding the Citadel South Project that is the subject of the Change of Zone:

1. The proposed Comprehensive Plan Amendment does look at the property which is primarily on the south side of Thompson hill, and access for the property related to the change of zone is proposed to only come from S. Sherman St., and will not access the existing streets of W. 25th Ave. and W. 26th Ave.

2. This project is proposed to be subdivided into Condo or Townhome units, with street access from S. Sherman St.
3. The applicant has requested to potentially access this property utilizing those portions of the property that is lying within the Kennewick Irrigation District Amon Pump Lateral Canal easement; With KID's recent title transfer from the United States Bureau of Reclamation, this type of use can be permitted within the easement, subject to the continued use of the easement for to support's KID's infrastructure needs and KID Board's approval. This proposed new access would be in concept some type of road with landscaping and pedestrian access while maintaining KID's ability to provide water.
4. The applicant is working with the property owners between your property and S. Sherman St. on finalizing access to the property that is the Citadel South Project.

As stated above, KID is open to discussion on the proposed access from S. Sherman St. utilizing the existing KID canal easement. Please let me know if you need any additional information or clarification.

Sincerely,

Jason McShane



Jason McShane, P.E.
Engineering and Operations Manager

Kennewick Irrigation District
Office: (509) 586-6012 [Ext. 103]
Direct: (509) 460-5421
jmcsbane@kid.org



Department of Energy

Bonneville Power Administration
2211 North Commercial Avenue
Pasco, WA 99301

TRANSMISSION SERVICES

June 24, 2022

In reply refer to: CPA-2022-0005

Located within a Portion of Section 8, Township 8 North,
Range 29 East, W.M., Benton County, Washington

Steve Donovan, Senior Planner
City of Kennewick
Community Planning/Planner
210 W. 6th Avenue - PO Box 6108
Kennewick, WA 99336

Dear Steve:

Bonneville Power Administration (BPA) has had the opportunity to review CPA-2022-0005. The plan amendment will designate 38.24 acres from High Density Residential to Medium Density. The property is located at 2701 and 2711 S Sherman Street in Kennewick, WA.

In researching our records, we have found that this proposal will not directly impact BPA facilities approximately 4,498 feet South of the subject property. BPA does not have any objections to the approval of this request at this time.

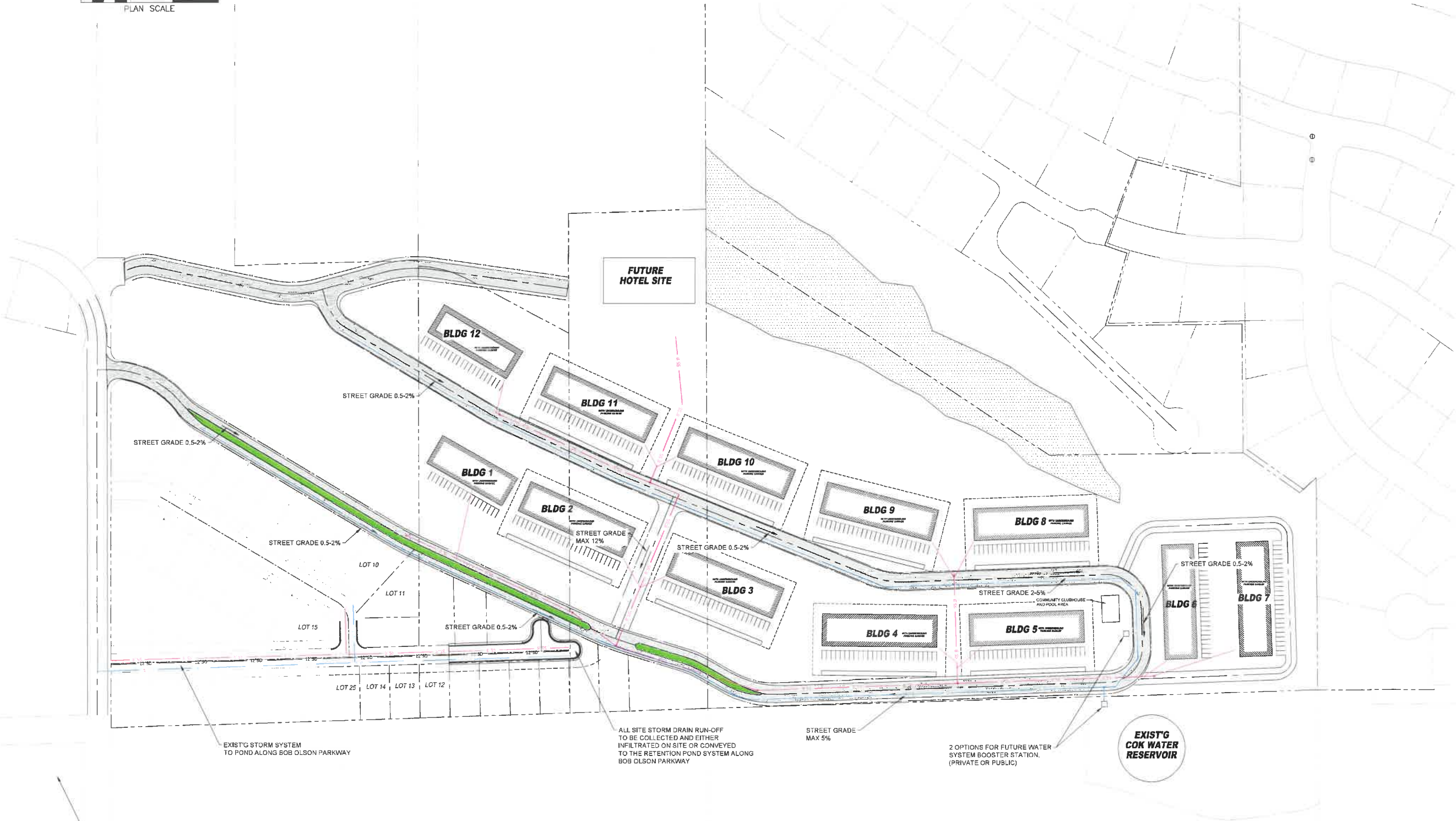
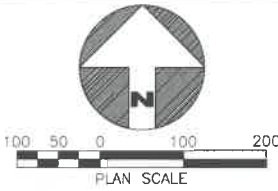
Thank you for the opportunity to review this application. If you have any questions regarding this request or need additional information, please feel free to contact a BPA representative at (509) 544-4747 or by email at jecottrell@bpa.gov.

Sincerely,

A handwritten signature in blue ink that reads "Joseph E. Cottrell II".

Joseph E. Cottrell
Realty Specialist

CITADEL SOUTH ESTATES CONCEPTUAL DESIGN



CONCEPTUAL LAYOUT
SCALE: 1"=100'

CITY OF KENNEWICK PUBLIC WORKS DATE

NOTE: ALL UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL VERIFY EXACT LOCATIONS WITH UTILITY COMPANIES PRIOR TO TRENCHING.

CALL 2 BUSINESS DAYS BEFORE YOU DIG: 811

J.A.C.E.
JENSEN & ASSOCIATES CONSULTING ENGINEERS
MEDFORD OR & KENNEWICK WA
JEFF-609 302-7824
Web Site: WWW.JENSEN-ENGINEERING.NET
Email: JENSEN-ENGINEERING@OUTLOOK.COM
Designed By: JS
Drawn By: JS
File Path: C:\Users\JEFF\Documents\45002\2022\11\Citadel South Estates Plan DWG.d 8/1/2022

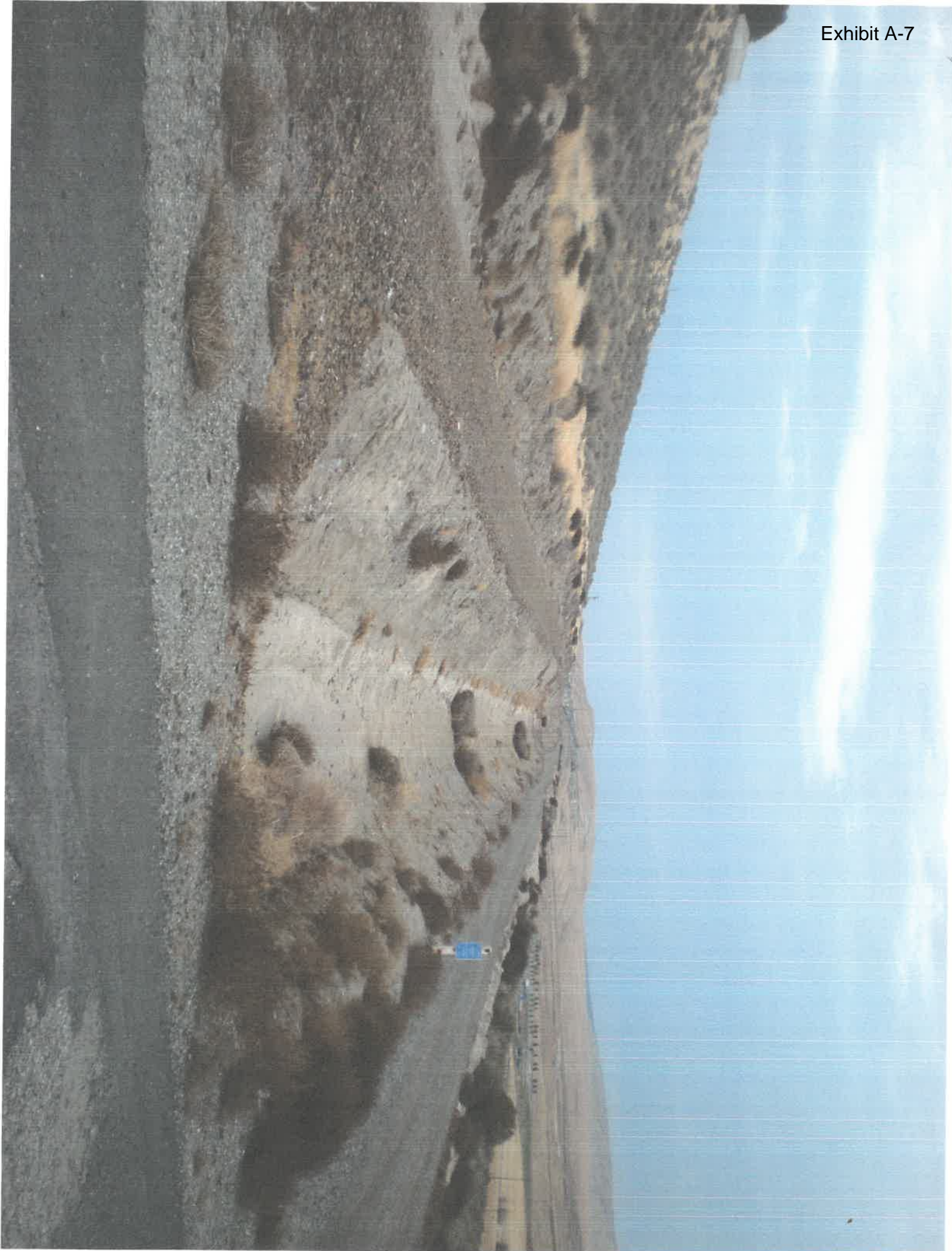
1977 - 2022
45
YEARS OF QUALITY
AND VALUE
Jensen & Associates
Consulting Engineers

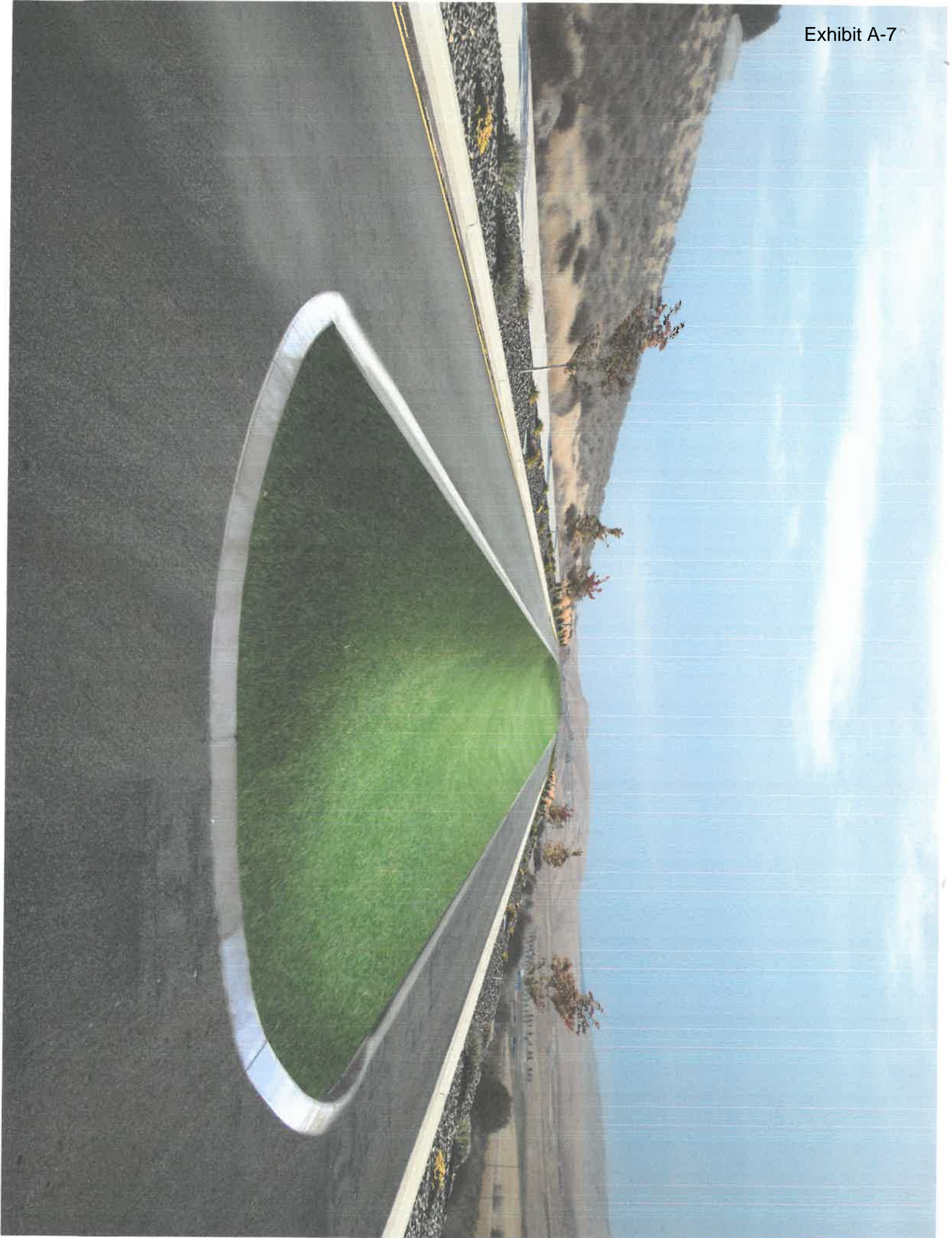
Rev	Date	Description

CITADEL SOUTH
OVERALL UTILITY PLAN
PROPOSED STREET LAYOUT
JOSE CHAVALLO

DATE: 8/1/2022
PROJECT NUMBER: 2022-13
SHEET NUMBER: C1 OF 1

LEAPFROG S. JENSEN
JEFF-609
PROFESSIONAL ENGINEER
8/1/2022





From: [Jose Chavallo](#)
To: [Steve Donovan](#)
Cc: [Anthony Muai](#)
Subject: Fwd: PROPERTY AT NW CORNER OF HILDERBRAND AND SHERMAN, KENNEWICK, WA
Date: Wednesday, August 24, 2022 10:36:05 AM
Attachments: [image001.png](#)
[image001.png](#)

Sent from my iPad

Begin forwarded message:

From: Michael Black <mtb-engineer@dwrengineer.com>
Date: August 22, 2022 at 10:57:00 AM PDT
To: fredc@owt.com
Subject: **PROPERTY AT NW CORNER OF HILDERBRAND AND SHERMAN, KENNEWICK, WA**

Matt Smith called me back and said he has been limited by failing eye sight for some time and just had surgery in his best eye.

Regardless, he confirmed the following:

1. No formal planning work has been done on his property to date.
2. He still considers your need to stormwater capacity a viable consideration, but has not determined the use or arrangement of his property. He did say that the COK has proposed using some of the land for stormwater management as well. Still, the fact remains that he does not reject the idea of your requested stormwater management capacity need, but certainly cannot commit to anything at this time.

Michael Black, P.E.
Principal Engineer
White Shield Inc.
320 N. 20th Avenue
Pasco, WA 99301
[509.547.0100](tel:509.547.0100) Office
[509.551.1897](tel:509.551.1897) Mobile
mtb-engineer@dwrengineer.com
www.whiteshield.com

Oasis Development Corporation
PO Box 4766
Pasco Washington 99302

Email: PDChristensen@charter.net
Tel: (509) 492-4050
Cell: (509) 460-1202

August 26, 2022

Steve Donavon
City of Kennewick Planning
210 W. 6th Street
Kennewick, WA, 99336

RE: **CPA-2022-0005**
Comprehensive Plan Amendment

I received the following three questions from John Cowling, Deputy Public Works Director. These questions were generated at a meeting held at the Frost Building between the Public Works' staff and Jose Chavallo.

- *Provide a narrative on how Stormwater will be detained and conveyed from the site including proposed locations of potential ponds at the bottom of the hill (it would be helpful if you could provide documentation that the property owners are in agreement to provide additional Stormwater storage). Narrative should be consistent with recommendations from any geotechnical information for the site.*

We are working with the Kennewick Irrigation District to reconstruct the abandoned Amon Pump Lateral for a future access road. The proposal is to develop this area as a divided roadway (Parkway), providing access to the subject property, connectivity of a walking / bicycling trail, and a corridor for utilities. One of our options for stormwater is to use this corridor to provide piping to the lower pond owned by Matt Smith at the southwest corner at the intersection of Bob Olsen Parkway and Sherman Street. Future design considerations will determine if the flow will be directly to the pond or retained and metered through an orifice to regulate the flow. If the KID corridor permitted any infiltration, we could allow some infiltration there, and the balance directed to the lower pond system.

You should receive support documentation from Mike Black, Geotechnical Engineer who has discussed this with Matt Smith, and Jason McShane, Engineer for the KID.

- *Provide a narrative on how secondary access will be provided (it would be helpful if KID provided written documentation that they are in agreement of use of the canal ROW).*

As stated in the previous response the secondary access would be by use of the abandoned Amon Pump Lateral easement area. The land use area subject to this comprehensive plan amendment WILL NOT have access to the north through the road system within Panoramic Heights. The Fire Department has asked if it would be possible to provide a SEVA (Secondary Emergency Vehicle Access) through the Citadel Estates Development. Should this be done, it would be through a locked gated roadway. As the developer of Citadel Estates, we also do not want traffic through the gated community.

- *As you are aware the City constructed a Zone 6 booster pump station to provide water to these properties and the City's preferred delivery method would be through the extension of Zone 6 water from the west of your site. Please provide a narrative on how you will provide Zone 6 water to the site.*

We attended meetings during the development of the Zone 6 pressure system, originally slated to serve this property. The extension from the west needed to be a loop system, crossing three large private parcels west of this proposed development area. The north loop would be close to the critical slope areas located at the top of the mountain. We have had discussions with the property owners to the west regarding the possibility of obtaining a utility easement corridor, and at this time they are unwilling to sell an easement that could affect their areas of future development.

We realize it may take several years to develop construction plans within the area of this amendment. Should the western properties propose a development prior to our development, we could extend the Zone 6 water system.

As we looked to the future of development of this area, we implemented plans to build a future booster pumping system to serve this area. With City guidance, a booster pump system could be constructed within the development to serve the entire area and facilities. Another option would be to construct the booster pump on City property near the Zone 5 reservoir to serve the development area. If, or when, the Zone 6 system is extended to the property from the west, the property could be tied into that system. All development at the top of the hill will take time, but water can be made available by any of these options.

Thank you for the opportunity to provide this additional information to help clarify our development. Should you have any further questions, please do not hesitate to contact me.

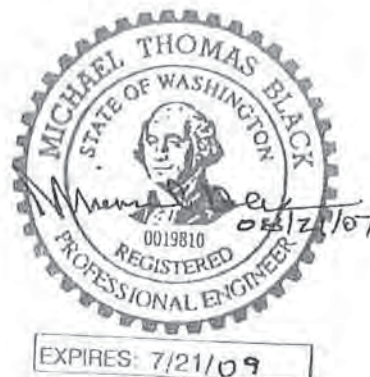
Sincerely,

Paul Christensen, P.E.

Cc: Anthony Muai, Planning Director
 Kary Roe, Public Works Director
 John Cowling, Deputy Public Works Director
 Jose Chavallo, Citadel Estates Developer
 Jeff Jensen, Jensen & Associates Consulting Engineers

Geotechnical Investigation for Panoramic Hts. 4, SE 1/4, Section 8, T8N, R29E Kennewick, WA

August 21, 2007



Prepared for:
Jose Chavallo
102 North Lincoln
Kennewick, WA 99336

Prepared By:
Michael Black, P.E.
Ashley-Bertsch Group, Inc.
5803 W. Metaline Ave.
Kennewick, WA 99336

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

Introduction

This report provides our findings and recommendations for the proposed residential development. The site has been undeveloped land prior to our investigation.

The site lies west of the current developed land at Panoramic Heights. The east boundary of the site lies west of the termination of 25th and 28th Avenues. The enclosed topographic map shows the general location of the site.

Scope of Work

Our investigation included drilling, soil classification, soil testing, and measurement of groundwater elevations. Our design recommendations, covered by this report, include the allowable soil bearing pressures, cut and fill recommendations, sub-grade preparation, seismic design information, and stormwater design parameters.

Summary Conclusions

This site contains mostly windblown sand and silt overlying gravel or remnants of a basalt flow bottom below. This lithology is normally relatively thin as compared to the elastic silt interbed below. The only exception was Borehole No. 5 that contained a fairly thick layer of fine sand and silt.

For most of the site, slope stability and foundation bearing, with conventional footings, is not an issue. However, excessive water introduced into the soil can cause soil heave and near surface sliding. Therefore, irrigation and stormwater management is a crucial element of the site design and each constructed home.

The portion of the site near Borehole No. 5 (southeast corner) will require special consideration for slope stability and soil collapse. However, this can be accommodated by deep foundations and careful control of irrigation and stormwater.

Investigation

Regional Stratigraphy

Most of the geologic features in this area consist of sedimentary deposits overlying Tertiary volcanic bedrock. The bedrock has been moved, and is continuing to move, by a general north-south compression of the basalt flows forming anticline structures (long narrow hills) with an east-west general bearing. This movement includes Thompson Hill. The site is located on the north flank of Thompson Hill. These structures are still moving in the upward direction at an imperceptibly slow rate.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

The enclosed Generalized Stratigraphy¹ provides details for the major individual stratigraphic units.

Local Stratigraphy

Referring to the Generalized Stratigraphy enclosure, we encountered the following general geologic units starting at the surface:

- Recent Aeolian (wind deposited) silt and fine sand
- The Touchet beds consisting of silt with fine sand or fine sand with silt with various degrees of cementation from calcium carbonate. We also found cemented gravel with calcium carbonate cement that is locally referred to as "caliche." These deposits occur in beds as they settled from the waters of the Lake Missoula floods.
- The Ice Harbor member of the Saddle Mountains basalt occurred in borehole numbers 1, 2, and 4. The basalt is fairly thin on this site and has largely eroded away.
- The Levy Interbed occurred below the Ice Harbor basalt and the Elephant Mountain basalt below. This interbed contains primarily elastic silt soil and resulted from sedimentation between the time the Elephant Mountain basalt was deposited and the successive Ice Harbor flow.

Site Geotechnical Findings

The field of geotechnical engineering sometimes used esoteric terms and the following two tables present a definition of major descriptions explained in layman's terms.

Soil and Rock Strength Descriptions

Geotechnical Description	Layman Description
Very loose	Very weak soil that generally requires special treatment or foundations
Loose	Weak soil that generally can only support light loads and may settle when saturated
Compact	Supports most loads without soil replacement or deep foundations. Heavy loads generally require relatively large spread footings
Dense	Supports most heavy loads
Very Dense	Near the support capability of a medium strength bedrock
Massive Basalt	Very hard and competent basalt capable of supporting just about any load without settlement.

¹ Washington State University, T.-C. (Department of Environmental Engineering). (2006). *Groundwater as a source for small flow drinking water in Benton County, WA*, Project Report, M. T. Black, P.E., trans.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

A number of soil classification systems have been put into service over the years. The "Unified Soil Classification System"² (USCS) is concise and used by engineers but it is not "layman friendly" nor does the description include the detail offered by some other systems. We generally use the Burmister Soil Classification System for our field descriptions. The following table, presenting the system, provides a more detailed, yet layman friendly, description of the soil.

Burmister Soil Classification System Essential Features and Comparison

Unified Soil Classification System*	Burmister Designation	Burmister Visual Criteria
Boulder (>12-inch)	Boulder	Greater than 1 ft. along longest axis.
Cobble (3-12-inch)	Cobble	Greater than 3-inch and less than 1 ft. along longest axis.
G (3/4 to 3-inch)	Coarse Gravel (<u>c</u> G)	Upper limit size of billiard ball. Lower limit size of golf ball.
G (no medium class)	Medium Gravel (<u>m</u> G)	Olive or marble sized gravel.
G (<3/4 to 1/4-inch)	Fine Gravel (<u>f</u> G)	Upper limit raisin and lower limit a BB.
S (<1/4 to 1/10-inch)	Coarse Sand (<u>c</u> S)	Less than size of BB and greater than table salt.
S (<1/10 to 1/40-inch)	Medium Sand (<u>m</u> S)	Table salt.
S (<1/40 to 1/200-inch)	Fine Sand (<u>f</u> S)	Just able to distinguish individual grains with naked eye.
M,C,O & PT	Silt, clay, organic (clay) and peat.	Use dilatancy, dry strength, toughness, and test tube tests.

Burmister Descriptions

1. "and" means 35-50% by visual inspection
2. "some" means 20-35% by visual inspection
3. "little" means 10-20% by visual inspection
4. "trace" means 0-10% by inspection
5. List description in following order (1) density, (2) color, (3) underline " " designates prominence, and (4) some shown as "-"

A typical Burmister description may read "Dense brown c-f SAND, some (-)f gravel, trace silt; rounded Gravel, occ. rounded boulders".

² American Society For Testing and Materials. (1999). *D 2487-98*. Vol. 04.08 Soil and Rock, 1999 Annual book of ASTM Standards, Section 4, Construction; Standard practice for classification of soils for engineering purposes (Unified Soil Classification System), Staff, ed.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

We constructed five boreholes on this site. The logs are enclosed in this report along with a map of the approximate locations. The table below presents a summary of our findings.

Approximate Upper Elevations of Major Soil/Rock Horizons

NE = Not encountered

Note: The above table is generalized and the reader is encouraged to review the logs.

Laboratory Results

No.	Surface EL (ft.)	Bottom EL (ft.)	Silt and sand	Cemented gravel or gravel	Hard older silt and sandy silt	Gravel	Flow top or bottom of basalt	Layered silt and gravel
BH1	739	710	Surface	734	722	731.5	730	NE
BH 2	734	714	Surface	729		736.5	727	NE
BH 3	772	738	Surface	769.5	758	NE	NE	NE
BH 4	780	760	Surface	756	751.5	NE	753	NE
BH 5	816	796	Surface	NE	801	NE	NE	811
S&W-BH 1	~690 (% or & 10)	643.5	Surface	NE	665	NE	NE	NE
DWR PZ1	592	578	NE	NE	592	NE	NE	NE
DWR PZ2	613	591	Surface	NE	604.5	NE	NE	NE
DWR PZ3	614	602	Surface	NE	602	NE	NE	NE
DWR PZ4	612	598	Surface	NE	603	NE	NE	NE

Laboratory results consisted of Atterberg Limits and grain size analysis using a hydrometer. Results are provided in the enclosed QSI testing results and on the borehole logs.

For Atterberg Limits, tests for both the "Liquid Limit" and "Plastic Limit", are performed for "fine-grained" soils. The following generalized definitions are provided to aid in understanding of their importance:

- Liquid Limit (LL) measures the amount of water attracted to the soil particle surface by electrostatic charges and absorption onto the soil surface. A viscous liquid such as a slurry or pea soup would be at or beyond the liquid limit. Soil near LL deforms easily with little load. Pumping during compaction is often caused by exceeding the liquid limit.
- Plastic Limit (PL) measures the amount of water that becomes part of the soil structure itself plus some absorption. A soft butter to stiff putty is near the plastic limit. Soil above the PL deforms without cracking under load.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

- Plasticity Index (PI) = LL-PL. This gives the range at which a soil is plastic before exceeding the liquid limit.

The liquid limit is particularly useful in determining the response of low-density, fine-grained soils that are cemented with calcium carbonate cement. These types of soils occur widely south and west of the Columbia River in the Tri-Cities area. Obviously, higher liquid limits allow for the absorption of more water before becoming fluid. The table below presents our results:

Borehole Number	Sample depth (ft.)	Moisture %	Liquid Limit	Plastic Limit	% sand	% silt	% clay
1	20	18.5	46	36	48.9	2.5	48.6
3	19.5	33.8	74	59	6.4	44.9	48.7
3	25	26.9	96	45	48.9	1.5	49.6*

*Of the 49.6% clay, 49.2% were colloids.

Analysis

We consider most of the soil and rock, rippable with a medium sized trackhoe, say a Cat 330, using "tiger teeth." The very stiff to hard silt and elastic silt formations require special consideration. Although the Atterberg Limit tests show these soils act as a silt or elastic silt, the clay content is consistently near 50%. For the purposes of this preliminary evaluation, it is important to discuss the geologic occurrence of clays and their major sub-groups as follows as swelling occurs when water infiltrates between and within the clay particles:

- *Kaolinite Clay* - This clay is essentially non-expansive because strong hydrogen bonds hold the individual clay particles together.
- *Illite Clay* - This clay has weaker potassium bonds that hold the individual clay particles together.
- *Montmorillonite Clay* - Individual clay particles are only weakly linked and water can easily flow in and separate the particles. Field observations, over many areas, confirm that the greatest problems occur in montmorillonite clay.

Tables are available to assist with determining the expansion potential for clays based on common geotechnical tests presented under the "Findings" section. The modified tables below³ present two such tables:

³ Coduto, D. P. (2001). *Foundation design: principles and practices-2nd Ed.* Upper Saddle River, NJ: Printice-Hall, Inc., pgs. 670-671

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

Correlations with Common Soil Tests (USBR)

Percent colloids	Plasticity Index	Liquid Limit	Swelling Potential
<15	<18	<39	low
13-23	15-28	39-50	medium
20-31	25-41	50-63	high
>28	>35	>63	very high

Correlations with Common Soil Tests (Chen, Rocky Mountain Area)

% passing #200 sieve	Liquid Limit	Probable expansion (%)	Swelling Potential
<30	<30	<1	low
30-60	30-40	1-5	medium
60-95	40-60	3-10	high
>95	>60	>10	very high

Using the tests for the clay presented under the "Investigation" section, it is possible to classify the clay from "low" to "high" expansion depending on the criteria used. The sample from Borehole No. 1, at 20 ft. deep, generally falls in the "low to medium" range. The sample from Borehole No. 3, at 19.5 ft. deep shows a "high to very high" range with the exception of the "% passing the 200 sieve," "plastic limit," and colloid criteria. The sample from Borehole No.3, at 25 ft. deep shows mostly "very high" potential with the exception of the "% passing the 200 sieve" criteria. These are not "fat clays" which often exhibit severe expansion potential. However, it is prudent to carefully plan and construct the development to minimize water saturation of the soil column.

Much of Kennewick and Richland are covered with silt and fine sand in various proportions of each. "Silt is inherently unstable, particularly when moisture is increased, with a tendency to become quick when saturated. It is relatively impervious, difficult to compact, highly susceptible to frost heave, easily erodible and subject to piping and boiling. Bulky grains reduce compressibility; flaky grains, i.e. mica and diatoms, increase compressibility and produce 'elastic silt'."⁴ The silt deposits from slow settling from water and windblown silt tend to be bulky grained and subject to collapse under load. The elastic silts most likely were produced by degraded volcanic ash. The low permeability for water and the reduction in strength, when wetted, requires special care for this development.

A slope stability analyses for the entire development is included in the attached sketches based on borehole numbers 1, 3 and 4. The section is shown on the attached drawing. I examined three scenarios using GSLOPE™ V. 4.03 by Mitre Software. The results are discussed below:

- For an unsaturated condition, the factor of safety (FOS) is 8.5. Normally a 1.5 FOS is considered satisfactory for most residential developments

⁴ US Department of Agriculture (Natural Resources Conservation Service). (1999). *Agricultural Waste Management Field Handbook*, Chapter 7-Geologic and groundwater considerations, Appendix 7B, Table 1-1.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

- For a saturated condition, with near surface failure, the FOS = 3.47
- For a saturated condition, with deep seated failure, the FOS = 2.9

I also analyzed the slopes for shallow sloughing or face sliding, which is a near surface phenomena where a shallow sheet slides. Dependent on the direction of water seepage (e.g. 90° is vertical and 0° is horizontal) the FOS changes dependent on the deep of the slide considered. I checked the slide potential in 0.5 ft. increments down to 3.5 ft. deep. The following points summarize the results shown in the attached graph:

- A vertical or 90° seep affords a FOS exceeding 2 in all cases
- A horizontal or 0° seep affords a FOS = 0.86 in all cases or *failure*
- A 26.6° seep has a FOS = 2.9 at 0.5 ft. deep and 1.4 at 3.5 ft. deep
- A 60° seep affords a FOS = 1.7 in all cases

Conclusions and Recommendations

Building on a hillside carries an inherent risk as the soil and rock moves, over time and settles in a low spot. Water added to the soil both exacerbates and accelerates this process. Particularly with the relatively impermeable soil and rock layers near the surface, homeowner negligence with irrigation and outside water use can cause significant damage to their home and the homes below. However, with reasonable design and construction practices, along with homeowner cooperation, these risks can be largely mitigated to prevent damage during the life of the homes.

It is my understanding that at least three retaining walls will be installed to provide level lots. The potential for sliding can be largely mitigated during the design and construction of the retaining walls.

The following general recommendations apply to this site.

Sub-grade Preparation and Protection

For a general guide, the following criteria are appropriate:

- Remove all roots and organic material from all areas to receive foundations or any structural covering (gravel or asphalt). We expect the duff to be 4 to 8-inches thick. The duff may be used for landscaping fill only.
- The very loose, Aeolian (windblown silt and fine sand) should be excavated, water conditioned, and compacted to prevent foundation and pavement failures. This layer is generally no more than 4 ft. thick and is lacking across a significant portion of the site. The north 1/3 of the site or so has these Aeolian deposits.
- The final sub-grade should be scarified to a depth of at least 4-inches, moisture conditioned and compacted with at least six passes of a 10,000 lb.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

vibratory roller. The Geotechnical Engineer, or Civil Engineer of Record, should witness the initial process for compacting the sub-grade and approve the process or make alterations as required. This provides a "proof roll" verification as well. As the site may encounter two different types of soils, compaction equipment recommendations are provided.

Fine sand or silt material is water sensitive (it becomes plastic and pumps when over wetted) and highly erodible. Therefore, construction should proceed during the dry season to prevent excessive moisture accumulation, which prevents proper soil compaction. Furthermore, the soil should be water conditioned to 1-2% below optimum moisture as excessive moisture causes pumping and requires soil dry before compaction can begin. A "kneading" rather than a vibration and force method most efficiently compacts this soil. Either a padded drum (e.g. Cat CP-433C) or tamping foot (e.g. Cat 815F) will best compact this soil.

Gravel -The final sub-grade should be scarified to a depth of at least 4-inches, moisture conditioned and compacted with a medium sized (say 10,000 lb.) vibratory roller imparting at least 350 lbs. dynamic force per inch of drum (e.g. Essick VR-72-T; Ray-Go 300 or Dyna-Pac CA-25T) to an unyielding condition. As an alternative to the unyielding condition the number of passes and water content may be adjusted to provide a sub-grade strength of 40 kips per inch or a modulus of rigidity of at least 995 psf. The Geotechnical Engineer, or Civil Engineer of Record, should witness the initial process for compacting the sub-grade and approve the process or make alterations as required. This provides a "proof roll" verification as well.

- Positive drainage away from the pavement sub-grade should be designed and constructed throughout the project.
- Anticipate 25-35% shrinkage for fine-grained soils and 15-20% for gravelly soils.

Structural Fill Requirements -

Structural fill is any fill that supports structures (e.g. homes, driveways, roads). Native soils may be used for structural fill if properly moisture conditioned and compacted.

The soil 2 ft. or more below finish sub-grade should be compacted to 92% of Maximum Dry Density using the Modified Proctor (ASTM D-1557). The upper 2 ft. shall be compacted to 95% of Maximum Dry Density. All select (crushed gravel from off-site) fill or base coarse should be compacted to at least 95% of maximum dry density per ASTM D1557.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

Fine sand and silt or elastic silt should be compacted in no more than 8-inch deep loose lifts. We also recommend using a soil moisture at 1-2% below optimum to avoid pumping. Gravel should be compacted in no more than 12-inch deep loose lifts at the optimum soil moisture.

General Design Criteria

<u>Item</u>	<u>Criteria</u>
Water Table depth	Not Encountered
Frost depth	2 ft.
Foundations buried at least 2 ft. deep (note basalt and calciche sub-grades controlled by IRC minimum footing size, min. 1 ft. wide continuous and 2 by 2 spread footings)	1,600 lb/ft ²
Bearing pressure increase allowed for short-term loads	33%
Settlement (with recommendations)	<5/8-inch
Anticipated differential settlement (flexible)	1/2 of total settlement
Passive Lateral Earth Pressure above water table	242 lb/ft ³
Active Lateral Earth Pressure above water table	38 lb/ft ³
At-rest lateral pressure	53 lb/ft ³
Near surface soil weight	~90-110 lb/ft ³
Sliding friction coefficient	0.42-gravel; 0.30-sand; 0.25 silt
Soil Profile Type	S _c
Liquefaction Potential	Low

Foundation Sub-grade Preparation and Sizing

Grades have not been selected for the site and the foundation sub-grade recommendations are necessarily general. For discussion, the soils are broken into

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

four basic categories; (1) basalt bedrock or cemented gravel, (2) silt and fine sand mixtures with densities exceeding 90 lb/ft³, (3) silt and fine sand mixtures with densities less than 90 lb/ft³, and (4) elastic silt.

Basalt bedrock and cemented gravel foundation sub-grades possess exceptional bearing capacity and little settlement potential. To reduce potential excavation related settlement, loose gravel and rock fragments should be removed from the sub-grade. A "leveling layer" of crushed gravel should be placed and compacted with a hoe-pack or vibratory compactor.

Silt and fine sand mixtures with densities exceeding 90 lb/ft³ are not considered collapsible when wetted and the foundation sub-grade footprint should be moisture conditioned and compacted with a vibratory compactor or hoe-pack. Our data show that densities increase with depth and the collapsible soils should be a near surface consideration. Each home's excavated footprint should be evaluated for a minimum density of 90 lb/ft³, in at least four places, prior to pouring the foundations.

Silt and fine sand mixtures with densities less than 90 lb/ft³ are potentially collapsible and require some foundation pre-treatment. Two options are available as follows:

- Flood the foundation excavation with approximately 1.5 ft. of water and allow the water to saturate the collapsible soils to a depth of about 5 ft. deep. Once the surface has dried sufficiently, compact the sub-grade.
- Over excavate 2 ft. deep and 1 ft. beyond the foundation perimeter and compact the excavated soil to 92% of maximum dry density per ASTM D-1557.

Elastic silt generally is not collapsible, but it carries a relatively low allowable bearing pressure and becomes unstable when wetted. Strong measures are required to keep water away from the foundation sub-grade on elastic silt sub-grades.

Mixed soil subgrade sites possess the risk of unacceptable differential settlement. For example, a home sited on 50% basalt bedrock and 50% elastic silt will generally suffer no settlement over the basalt and up to 5/8-inch over the elastic silt. This problem can be largely mitigated by some soil replacement of the elastic silt, silt with fine sand, etc. Remove non-basalt or cemented gravel soil to a depth of 18-inches below the foundation subgrade and 1 ft. beyond the outside periphery of the foundation. Install a geotextile (e.g. Amoco 2044) to prevent gravel from migrating into the soft, fine-grained soil, and back fill with 5/8-inch minus compacted gravel.

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

The most positive approach to protecting the foundations is to eliminate or mitigate water from reaching the foundation subgrade. This is accomplished from good site grading, properly compacting the soil against the exterior stemwall, avoiding over-watering from irrigation, and maintaining irrigation and domestic pressure pipes and irrigation heads. The following subsection addresses this crucial item in further detail.

Irrigation & Stormwater and Erosion Control

Given the potential geometry of the lot (slopes), it is imperative that the negative effects of excessive irrigation and storm water be mitigated to protect slopes, water from entering the homes, and foundation damage due to water saturation. Negligent practices from upgradient homeowners will not only impact their homes, but potential water-related damage would geometrically increase as excessive groundwater moves to the north. The attached "Preventing Subsurface Water Damage" should be read thoroughly and applied for this project.

Crushed Gravel Fill

Imported gravel shall meet the following requirements:

Crushed Gravel Base Specifications

Sieve Size	Percent Passing (%)
1-1/4-inch	100
1-inch	80-100
3/4-inch	50-80
1/2-inch	50-80
No. 4	25-45
No. 40	3-18
No. 200	7.5 max.
% fracture	75% min.
Sand equivalent	40% min.

Floors

Slab-on-grade floors may be supported on re-compacted fill materials free of debris and foreign material. Provide a firm and stable sub-grade. Over excavate and replace loose and yielding soils. Install at least 6-inches of free draining material (less than 5% fines) directly beneath the slab.

Pavement Structures

The adequacy of the site pavements responds to the adequacy of the sub-grade. If native soil is to be used for fill under the pavement sections, the sand should be placed in maximum lifts of 8-inches at least 92% of the Modified Proctor dry density. We recommend the following pavement sections:

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

- a. *Lightly loaded areas (parking and driveways):*
2-inches of Asphalt Concrete (AC) over 4-inches of Crushed Rock Base (CRB), or
2-inches of AC over 3-inches of Asphalt Treated Base (ATB) material
- b. *Heavily Loaded Areas (e.g. garbage truck traffic)*
3-inches of AC over 6-inches of CRB, or
3-inches of AC over 4.5-inches of ATB

Stormwater Management

There is little opportunity for the use of drywells, exfiltration trenches, etc. for this project. However, some below ground devices may be used if properly located and the structure will not adversely affect homes. The relatively thin layer of silt and sand soils, over most of the project, should provide an exfiltration rate of about 0.35 ft³ of stormwater per square foot of exfiltration surface per hour in the horizontal direction. According to the Shannon and Wilson borehole log, this soil is thickest in the northeast corner of the site, which is also the lowest. Use an exfiltration rate of about 0.04 ft³ of stormwater per square foot of exfiltration surface per hour in the vertical direction. The effective porosity of this soil is about 0.49 ft³ water per ft³ of soil. Expect near-term water retention (up to three months) of about 0.33 ft³/ft³. This means that successive storms will require up to 6.25 ft³ of soil per 1 ft³ of stormwater. Depending on the amount of stormwater requiring management, the northeast corner could suffer saturated soil, at depth, and a high groundwater table. For example, a successive storm of 2,000 ft³ of water would require 12,500 ft³ of soil. This is a 40 ft. diameter area 10 ft. deep. While stormwater management can be accommodated on-site, with caution, I understand that the City of Kennewick has a stormwater management system that can be used if the water is piped to their system. This may be the most economical alternative.

Ultimately, one large downgrade pond may provide the best solution.

Weather Conditions

It is important to recognize that any geotechnical recommendation assumes that weather conditions are suitable for construction. Excessively hot, cold, or wet conditions can create unsatisfactory conditions for construction. Construction planning must include a consideration of weather related construction difficulties.

Limitations

It is important that the limitations of our work and this report are understood. The recommendations and conclusions documented in this report have been prepared for specific application to your project based on the scope, budget, and schedule constraints. Further, these recommendations and conclusions have been developed



Figure 4. Generalized Stratigraphy

Chavallo, Panoramic Hts. No. 4 - August 21, 2007

in a manner consistent with the level of care and skill normally exercised by members of the engineering profession.

This report is prepared for the use of the CLIENT, design and construction professionals serving the CLIENT, and appropriate regulatory agencies. Ashley-Bertsch Group, Inc. assumes no liability except to the CLIENT and this report remains the property of Ashley-Bertsch Group, Inc. Use of this report by third parties, without our written authorization, is prohibited.

Please contact our office should you have questions or comments, and thank you for your confidence in Ashley-Bertsch Group, Inc.

ABG, Inc.						Boring Log No. B-1 Panaramic Hts. No. 4							
Location: West of Panoramic Hts.										WO#: 0-237.7			
Method: DHH-Mobile B-57										Ground EL: 739			
Hammer: Auto					Hammer weight (lb): 140					Hole depth (ft): 29			
Sampler: Standard Split Spoon					Drop (in): 30		G.W.T. @ Drilling (ft): not encountered					Sampled by: M. Black, P.E.	
Driller: Ethan Hageman					Drill Date: 06/28/07					Logged by: M. Black, P.E.			

Depth	Strata	GWT	No.	Type	Blows Per 6"	USCS	Soil Description	SPT. blow/ft Moisture %				Notes	
								0	20	40	60		
0						TP	Firm, dry, (2.5Y, 6/3) light yellowish brown, SILT WITH FINE SAND. Strong HCL reaction.						
					23/42/29	GP	Loose, dry, (2.5Y, 6/3) light yellowish brown medium to COARSE GRAVEL. Calcium carbonate coating.						
5					39		Very Hard, 2.5 Y (6/3) light yellowish brown (dry); no change damp, CALCRETE/CALICHE. Strong reaction to HCL.						10/18"
10					50+	40	BASALT FLOW BOTTOM- dark grey, calcium carbonate coating, 1/4 in. chips and grey dust from drill rig.						1/18", 50 blows for 5"
15					50+	40	BASALT FLOW BOTTOM- dark grey, calcium carbonate coating, 1/4 in. chips and grey dust from drill rig. Some redish staining from vesicules						3-in. sample; 50 blows for 2"
20					50+	ML	Stiff, dry, (2.5Y, 6/3) light yellowish brown sandy silt. LL=46, PI=10; Hydrometer shows 48.8 % clay sized material and 48.9% sand.						50 blows for 4.5" last 4 in.
25													
30					20/49/+	END	Boring completed at depth of 29.0 ft. This is interbed material between basalt flows.-No HCL reaction.						54 blows for 3" last 6 in.

Remarks:

ABG, Inc.						Boring Log No. B-2 Panaramic Hts. No. 4							
Location: West of Panaramic Hts.										WO#: 0-237.7			
Method: DHH-Mobile B-57										Ground EL: 734			
Hammer: Auto					Hammer weight (lb): 140					Hole depth (ft): 20			
Sampler: Standard Split Spoon				Drop (in): 30		G.W.T. @ Drilling (ft): not encountered				Sampled by: M. Black, P.E.			
Driller: Ethan Hageman						Drill Date: 06/28/07				Logged by: M. Black, P.E.			
Depth	Strata	GWT	No.	Type	Blows Per 6"	USCS	Soil Description	<div style="display: flex; justify-content: space-between;"> ■ SPT. blow/ft ○ Moisture % </div>				Notes	
								0	20	40	60		
0						TP	Firm, dry, (2.5Y, 6/3) light yellowish brown, SILT WITH FINE SAND. Strong HCL reaction.						0
					12/21/+	GP	Loose, dry, (2.5Y, 6/3) light yellowish brown medium to COARSE GRAVEL. Calcium carbonate coating.						
5						39	Very Hard, 2.5 Y (6/3) light yellowish brown (dry); no change damp, CALCRETE/CALICHE. Strong reaction to HCL.						recovery 9/18; 50 blows @ 4"
						40	BASALT FLOW BOTTOM- dark grey, calcium carbonate coating, 1/4 in. chips and grey dust from drill rig. BASALT FLOW BOTTOM- dark grey, VERY HARD; drilling is very slow BASALT FLOW BOTTOM- dark grey, drilling is faster						
					40								
10					40								10
15					50+	40	BASALT FLOW BOTTOM- dark grey, drilling is faster; starting to pick up some silt in drill return. Some calcium carbonate staining.						3-in. sample; 50 blows for 2"
20					50+	END	Boring completed at depth of 20.0 ft. Starting to pick-up some greenish staining (near interbed?)						50 blows for 5" last 6 in.
25													
30													
35													

Remarks:

ABG, Inc.				Boring Log No. B-3 Panaramic Hts. No. 4			
Location: West of Panoramic Hts.						WO#: 0-237.7	
Method: DHH-Mobile B-57						Ground EL: 772	
Hammer: Auto				Hammer weight (lb): 140		Hole depth (ft): 34	
Sampler: Standard Split Spoon			Drop (in): 30		G.W.T. @ Drilling (ft): not encountered		Sampled by: M. Black, P.E.
Driller: Ethan Hageman				Drill Date: 06/29/07		Logged by: M. Black, P.E.	

Depth	Strata	GWT	No.	Type	Blows Per 6"	USCS	Soil Description	SPT. blow/ft ○ Moisture %				Notes	
								0	20	40	60		
0						TP	Firm, dry, (2.5Y, 6/3) light yellowish brown, SILT WITH FINE SAND. Strong HCL reaction.						
5					29/50/50	39	Dense, 2.5 Y (6/3) light yellowish brown (dry); no change damp, CALCRETE/CALICHE. Strong reaction to HCL.						recovery 4/18"
10					50+	39	Very Dense, 2.5 Y (6/3) light yellowish brown (dry); , CALCRETE/CALICHE. Gravel changes from coarse to medium gravel. Strong reaction to HCL.						50 blows for 3.5"
15					12/18/21	MH	Hard, dry, 2.5Y, (6/6) light yellowish brown ELASTIC SILT. Brown mottling.						recovery 18/18"
20					12/21/27	MH	Very hard, dry, 5Y, (7/3) pale yellow ELASTIC SILT. LL=74; PL=59. Silt accounts for 44.9 and clay accounts for 48.7% of the sample						recovery 18/18"
25						MH	Very hard, dry, 5Y, (7/3) pale yellow SANDY ELASTIC SILT. LL=74; PL=59. Sand accounts for 48.9 and clay accounts for 49.6% of the sample						
30						ML	Very Stiff, dry, 2.5Y (4/3) olive brown, INORGANIC SILT WITH SAND (Ringold).						
35					12/50+	BA END	Massive, Hard, BASALT. Boring completed at depth of 34						50 blows for 5.5"

Remarks:

ABG, Inc.						Boring Log No. B-4 Panaramic Hts. No. 4							
Location: West of Panoramic Hts.										WO#: 0-237.7			
Method: DHH-Mobile B-57										Ground EL: 780			
Hammer: Auto					Hammer weight (lb): 140					Hole depth (ft): 20			
Sampler: Standard Split Spoon					Drop (in): 30		G.W.T. @ Drilling (ft): not encountered					Sampled by: M. Black, P.E.	
Driller: Ethan Hageman					Drill Date: 06/28/07					Logged by: M. Black, P.E.			

Depth	Strata	GWT	No.	Type	Blows Per 6"	USCS	Soil Description	<div style="display: flex; align-items: center;"> <div style="width: 10px; height: 10px; background-color: black; margin-right: 5px;"></div> SPT. blow/ft <div style="width: 10px; height: 10px; border: 1px solid black; margin-left: 10px; margin-right: 5px;"></div> Moisture % </div>				Notes	
								0	20	40	60		
0						TP	Firm, dry, (2.5Y, 6/3) light yellowish brown, SILT WITH FINE SAND. Strong HCL reaction.						0
5						39	Very Hard, 2.5 Y (6/3) light yellowish brown (dry); no change damp, CALCRETE/CALICHE. Strong reaction to HCL.						5
						40	BASALT FLOW BOTTOM- dark grey, VERY HARD; drilling is very slow						
10						MH	Hard, dry, 2.5Y, (6/6) light yellowish brown ELASTIC SILT WITH SAND. Brown mottling.						10
15						MH	Hard, damp to wet, 5Y, (5/4) reddish ELASTIC SILT WITH SAND.						15
20						MH	Very hard, dry, 2.5Y, (6/6) light yellowish brown ELASTIC SILT WITH SAND. Brown mottling.						20
						END	Boring completed at depth of 20.0 ft.						20
25													25
30													30
35													35

Remarks:

ABG, Inc.						Boring Log No. B-5 Panaramic Hts. No. 4								
Location: West of Panoramic Hts.								WO#: 0-237.7						
Method: DHH-Mobile B-57								Ground EL: 816						
Hammer: Auto				Hammer weight (lb): 140				Hole depth (ft): 20						
Sampler: Standard Split Spoon				Drop (in): 30		G.W.T. @ Drilling (ft): not encountered				Sampled by: M. Black, P.E.				
Driller: Ethan Hageman						Drill Date: 06/29/07				Logged by: M. Black, P.E.				
Depth	Strata	GWT	No.	Type	Blows Per 6"	USCS	Soil Description	<div style="display: flex; align-items: center;"> <div style="width: 10px; height: 10px; background-color: black; margin-right: 5px;"></div> SPT. blow/ft <div style="width: 10px; height: 10px; border: 1px solid black; margin-left: 10px; margin-right: 5px;"></div> Moisture % </div>				Notes		
								0	20	40	60			
0						TP	Firm, dry, (2.5Y, 6/3) light yellowish brown, SILT WITH FINE SAND. Strong HCL reaction.						0	
5					3/4/5	38	Start layers of SILT WITH FINE SAND and CEMENTED GRAVEL. Most deposits are 6 to 18 inches thick. Layers mostly consist of silt with fine sand						recovery 14/18".	5
10					12/50+								Hit gravel at bottom of sample	10
15						39	Very Hard, 2.5 Y (6/3) light yellowish brown (dry); no change damp, CALCRETE/CALICHE. Strong reaction to HCL.							15
20						END	Boring completed at depth of 20.0 ft.							20
25														25
30														30
35														35
Remarks:														

GRAIN SIZE DISTRIBUTION TEST DATA

Client: ABG # 0-249.7
 Project: Panoramic #4
 Project Number: 20007800025

Sample Data

Source: Panoramic #4
 Sample No.: RLS070049
 Elev. or Depth: BH-1, @ 20' Depth
 Location:
 Description: Sandy silt
 Date: 7/5/07 PL: 36
 USCS Classification: ML
 Testing Remarks:

Sample Length(in./cm.):
 LL: 46 PI: 10
 AASHTO Classification:

Mechanical Analysis Data

	Initial	After wash
Dry sample and tare=	100.00	100.00
Tare =	0.00	0.00
Dry sample weight =	100.00	100.00
Minus #200 from wash=	0.0 %	
Tare for cumulative weight retained=	.00	

Sieve	Cumul. Wt. retained	Percent finer
# 8	0.00	100.0
# 16	41.70	58.3
# 30	44.00	56.0
50	45.70	54.3
# 100	47.40	52.6
# 200	48.90	51.1

Hydrometer Analysis Data

Separation sieve is #10
 Percent -#10 based upon complete sample= 100.0
 Weight of hydrometer sample: 125.0
 Calculated biased weight= 125.00
 Automatic temperature correction
 Composite correction at 20 deg C = 0

Meniscus correction only= 0
 Specific gravity of solids= 2.6
 Specific gravity correction factor= 1.012
 Hydrometer type: 152H

Effective depth $L = 16.294964 - 0.164 \times R_m$

Elapsed time, min	Temp, deg C	Actual reading	Corrected reading	K	Rm	Eff. depth	Diameter mm	Percent finer
2.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0236	49.5
5.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0149	49.4
15.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0086	49.4
30.00	25.0	60.0	61.3	0.0130	60.0	6.5	0.0061	49.6
70.00	25.5	57.0	58.5	0.0130	57.0	6.9	0.0044	47.3
130.00	23.9	43.0	43.9	0.0132	43.0	9.2	0.0025	35.6
1440.00	23.9	42.0	42.9	0.0132	42.0	9.4	0.0011	34.7

Fractional Components

Gravel/Sand based on #4

Sand/Fines based on #200

% PEBBLES = % GRAVEL =

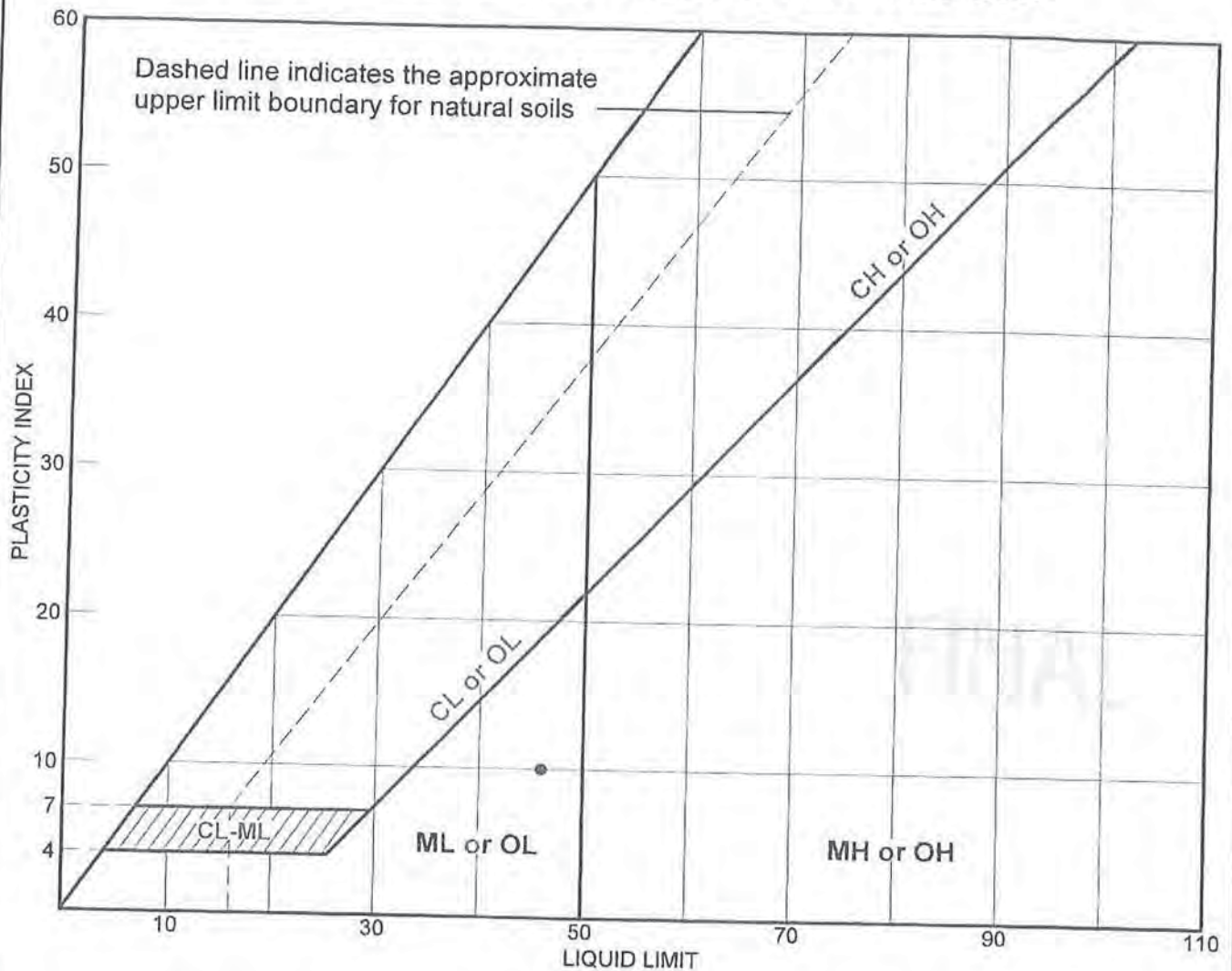
% SAND = 48.9 (% coarse = 12.3 % medium = 32.5 % fine = 4.1)

% SILT = 2.5 % CLAY = 48.6

D₈₅= 1.93 D₆₀= 1.24 D₅₀= 0.04

FINAL

LIQUID AND PLASTIC LIMITS TEST REPORT



SOIL DATA

SYMBOL	SOURCE	SAMPLE NO.	DEPTH (ft.)	NATURAL WATER CONTENT (%)	PLASTIC LIMIT (%)	LIQUID LIMIT (%)	PLASTICITY INDEX (%)	USCS
•	Panoramic #4	RLS070049	BH-1, @ 20'	18.5	36	46	10	ML

LIQUID AND PLASTIC LIMITS TEST REPORT

**QUALITY
INSPECTION
SERVICES, INC.**

Client: ABG # 0-249.7

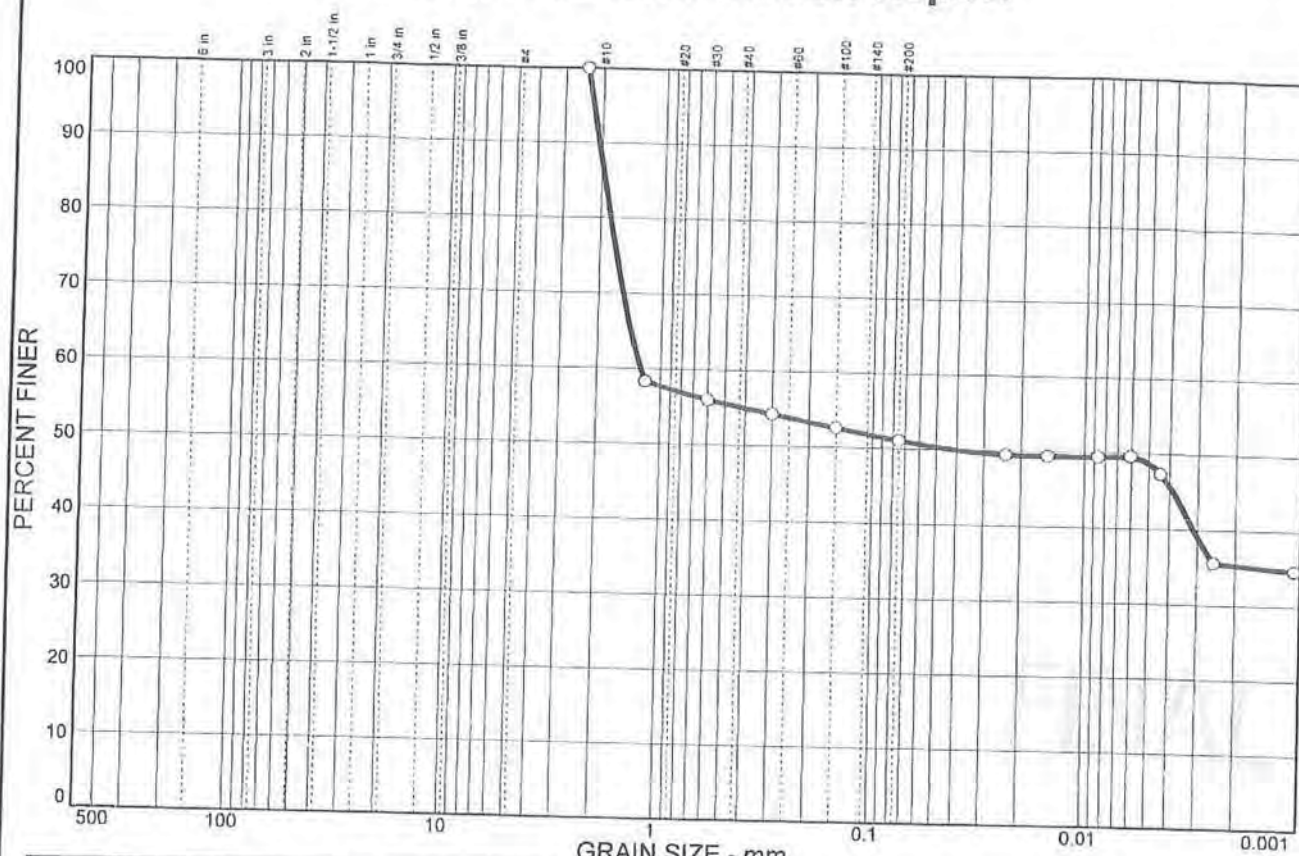
Project: Panoramic #4

Project No.: 20007800025

Rev. C.V. Smith
C.V. Smith

Figure 7-21-07

Particle Size Distribution Report



% COBBLES	% GRAVEL		% SAND			% FINES	
	CRS.	FINE	CRS.	MEDIUM	FINE	SILT	CLAY
0.0	0.0	0.0	12.3	32.5	4.1	2.5	48.6

SIEVE SIZE	PERCENT FINER	SPEC.* PERCENT	PASS? (X=NO)
#8	100.0		
#16	58.3		
#30	56.0		
#50	54.3		
#100	52.6		
#200	51.1		

* (no specification provided)

Soil Description

Sandy silt

Atterberg Limits
 PL= 36 LL= 46 PI= 10

Coefficients
 D₈₅= 1.93 D₆₀= 1.24 D₅₀= 0.0399
 D₃₀= D₁₅= D₁₀=
 C_u= C_c=

Classification
 USCS= ML AASHTO=

Remarks

Sample No.: RLS070049
 Location:

Source of Sample: Panoramic #4

Date: 7/5/07
 Elev./Depth: BH-1, @ 20'

**QUALITY
 INSPECTION
 SERVICES, INC.**

Client: ABG # 0-249.7
 Project: Panoramic #4

Project No: 20007800025

REV. C.V. Smith
 C.V. Smith

Figure 7-26-01

GRAIN SIZE DISTRIBUTION TEST DATA

Client: ABG # 0-249.7
 Project: Panoramic #4
 Project Number: 20007800025

Sample Data

Source: Panoramic #4
 Sample No.: RLS070050
 Elev. or Depth: BH-3, @ 19.5' Depth Sample Length(in./cm.):
 Location:
 Description: Elastic silt
 Date: 7/10/07
 FL: 59 LL: 74 PI: 15
 USCS Classification: MH AASHTO Classification:
 Testing Remarks:

Mechanical Analysis Data

	Initial	After wash
Dry sample and tare=	125.00	125.00
Tare =	0.00	0.00
Dry sample weight =	125.00	125.00
Minus #200 from wash=	0.0 %	
Tare for cumulative weight retained= .00		
Sieve	Cumul. Wt. retained	Percent finer
# 8	0.00	100.0
# 16	1.50	98.8
30	3.30	97.4
# 50	4.80	96.2
# 100	6.50	94.8
# 200	8.00	93.6

Hydrometer Analysis Data

Separation sieve is #10
 Percent -#10 based upon complete sample= 100.0
 Weight of hydrometer sample: 125
 Calculated biased weight= 125.00
 Automatic temperature correction
 Composite correction at 20 deg C = 0

Meniscus correction only= 0
 Specific gravity of solids= 2.6
 Specific gravity correction factor= 1.012
 Hydrometer type: 152H

Effective depth $L = 16.294964 - 0.164 \times R_m$

Elapsed time, min	Temp, deg C	Actual reading	Corrected reading	K	Rm	Eff. depth	Diameter mm	Percent finer
2.00	23.9	60.0	60.9	0.0132	60.0	6.5	0.0237	49.3
5.00	23.9	60.0	60.9	0.0132	60.0	6.5	0.0150	49.3
15.00	23.9	60.0	60.9	0.0132	60.0	6.5	0.0087	49.3
0.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0061	49.4
30.00	25.6	57.0	58.5	0.0130	57.0	6.9	0.0044	47.4
250.00	23.3	43.0	43.7	0.0133	43.0	9.2	0.0026	35.4

Elapsed time, min	Temp, Actual deg C reading	Corrected reading	K	Rm	Eff. depth	Diameter mm	Percent finer
1440.00	23.3 42.0	42.7	0.0133	42.0	9.4	0.0011	34.6

Fractional Components

G. vel/Sand based on #4

Sand/Fines based on #200

% COBBLES =

% GRAVEL =

% SAND = 6.4 (% coarse = 0.3

% medium = 2.8

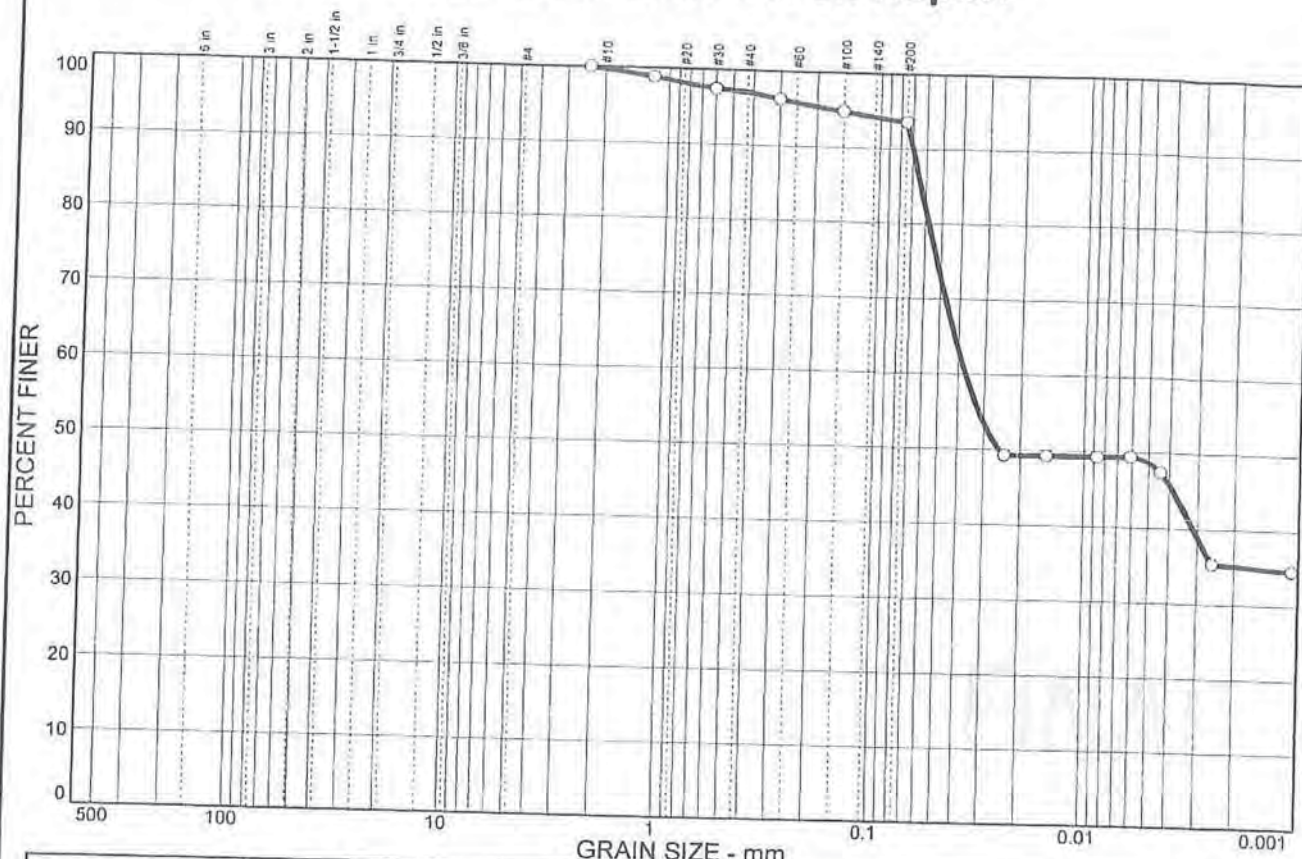
% fine = 3.3)

% SILT = 44.9 % CLAY = 48.7

D₈₅ = 0.06 D₆₀ = 0.04 D₅₀ = 0.03

Rev C.V. SKIBA
C.V. SKIBA
7-26-87

Particle Size Distribution Report



% COBBLES	% GRAVEL		% SAND			% FINES	
	CRS.	FINE	CRS.	MEDIUM	FINE	SILT	CLAY
0.0	0.0	0.0	0.3	2.8	3.3	44.9	48.7

SIEVE SIZE	PERCENT FINER	SPEC.* PERCENT	PASS? (X=NO)
#8	100.0		
#16	98.8		
#30	97.4		
#50	96.2		
#100	94.8		
#200	93.6		

* (no specification provided)

Soil Description		
Elastic silt		
Atterberg Limits		
PL= 59	LL= 74	PI= 15
Coefficients		
D ₈₅ = 0.0637	D ₆₀ = 0.0369	D ₅₀ = 0.0252
D ₃₀ =	D ₁₅ =	D ₁₀ =
C _u =	C _c =	
Classification		
USCS= MH	AASHTO=	
Remarks		

Sample No.: RLS070050
Location:

Source of Sample: Panoramic #4

Date: 7/10/07
Elev./Depth: BH-3, @ 19.5'

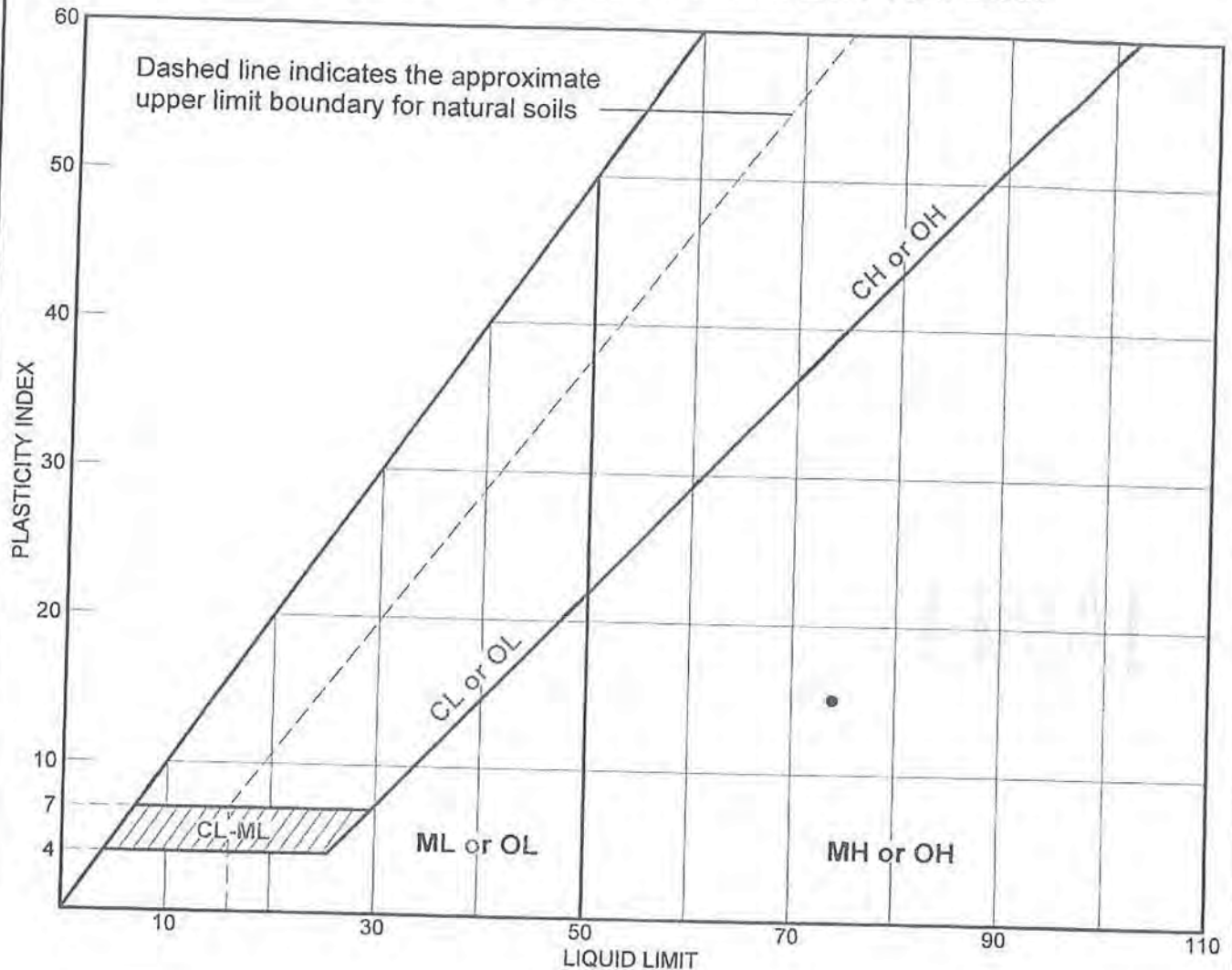
**QUALITY
INSPECTION
SERVICES, INC.**

Client: ABG # 0-249.7
Project: Panoramic #4

Project No: 20007800025

Reviewed: C.V. Seidman
C.V. Seidman
Figure 7-26-62

LIQUID AND PLASTIC LIMITS TEST REPORT



SOIL DATA

SYMBOL	SOURCE	SAMPLE NO.	DEPTH (ft.)	NATURAL WATER CONTENT (%)	PLASTIC LIMIT (%)	LIQUID LIMIT (%)	PLASTICITY INDEX (%)	USCS
●	Panoramic #4	RLS070050	BH-3, @ 19.5'	33.8	59	74	15	MH

LIQUID AND PLASTIC LIMITS TEST REPORT

**QUALITY
INSPECTION
SERVICES, INC.**

Client: ABG # 0-249.7

Project: Panoramic #4

Project No.: 20007800025

RVW. C.V. Skidmore
C.V. Skidmore
Figure J-26-r-7

GRAIN SIZE DISTRIBUTION TEST DATA

Client: ABG # 0-249.7
 Project: Panoramic #4
 Project Number: 20007800025

Sample Data

Source: Panoramic #4
 Sample No.: RLS070051
 Elev. or Depth: BH-3, @ 25'
 Location: Sample Length (in./cm.):
 Description: Sandy elastic silt
 Date: 7/4/07 PL: 45 LL: 96 PI: 51
 USCS Classification: MH AASHTO Classification:
 Testing Remarks:

Mechanical Analysis Data

	Initial	After wash
Dry sample and tare=	100.00	100.00
Tare =	0.00	0.00
Dry sample weight =	100.00	100.00
Minus #200 from wash=	0.0 %	
Tare for cumulative weight retained=	.00	

Sieve	Cumul. Wt. retained	Percent finer
# 8	0.00	100.0
# 16	41.70	58.3
# 30	44.00	56.0
50	45.70	54.3
" 100	47.40	52.6
# 200	48.90	51.1

Hydrometer Analysis Data

Separation sieve is #10
 Percent -#10 based upon complete sample= 100.0
 Weight of hydrometer sample: 125.0
 Calculated biased weight= 125.00
 Automatic temperature correction
 Composite correction at 20 deg C = 0

Meniscus correction only=
 Specific gravity of solids= 2.6
 Specific gravity correction factor= 1.012
 Hydrometer type: 152H
 Effective depth $L = 16.294694 - 0.164 \times R_m$

Elapsed time, min	Temp, deg C	Actual reading	Corrected reading	K	Rm	Eff. depth	Diameter mm	Percent finer
2.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0236	49.4
5.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0149	49.4
15.00	24.4	60.0	61.1	0.0131	60.0	6.5	0.0086	49.4
30.00	25.0	60.0	61.3	0.0130	60.0	6.5	0.0061	49.6
40.00	25.0	60.0	61.3	0.0130	60.0	6.5	0.0043	49.6
50.00	23.3	60.0	60.7	0.0133	60.0	6.5	0.0021	49.2
1440.00	23.3	60.0	60.7	0.0133	60.0	6.5	0.0009	49.2

Fractional Components

Gravel/Sand based on #4

Sand/Fines based on #200

% BOBBLES =

% GRAVEL =

% SAND = 48.9 (% coarse = 12.3

% medium = 32.5 % fine = 4.1)

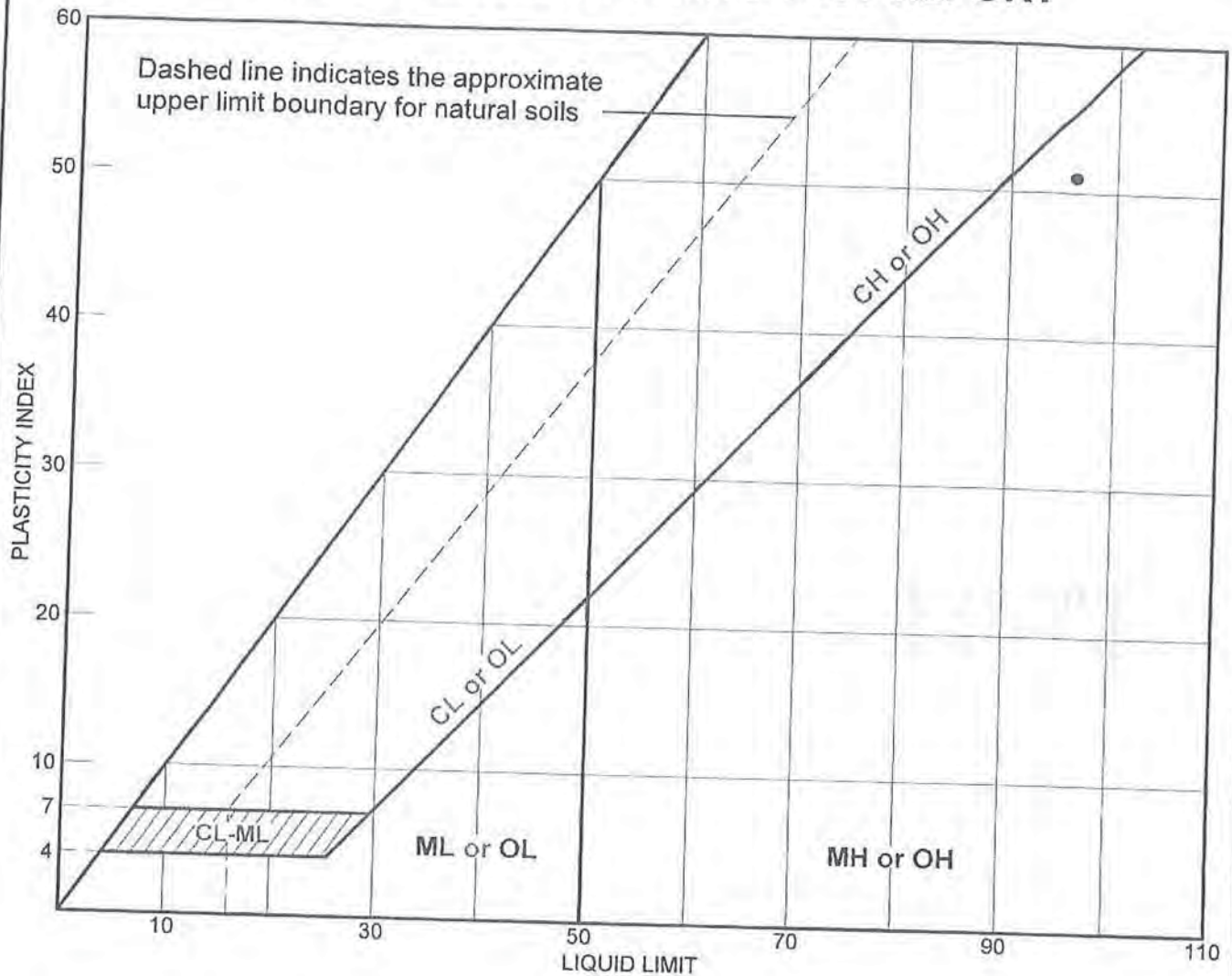
% SILT = 1.5 % CLAY = 49.6

(% CLAY COLLOIDS = 49.2)

D₈₅= 1.93 D₆₀= 1.24 D₅₀= 0.04Rvw C.V. SKIBA
C.V. SKIBA

7-26-07

LIQUID AND PLASTIC LIMITS TEST REPORT



SOIL DATA

SYMBOL	SOURCE	SAMPLE NO.	DEPTH (ft.)	NATURAL WATER CONTENT (%)	PLASTIC LIMIT (%)	LIQUID LIMIT (%)	PLASTICITY INDEX (%)	USCS
•	Panoramic #4	RLS070051	BH-3, @ 25'	26.9	45	96	51	MH

LIQUID AND PLASTIC LIMITS TEST REPORT

**QUALITY
INSPECTION
SERVICES, INC.**

Client: ABG # 0-249.7

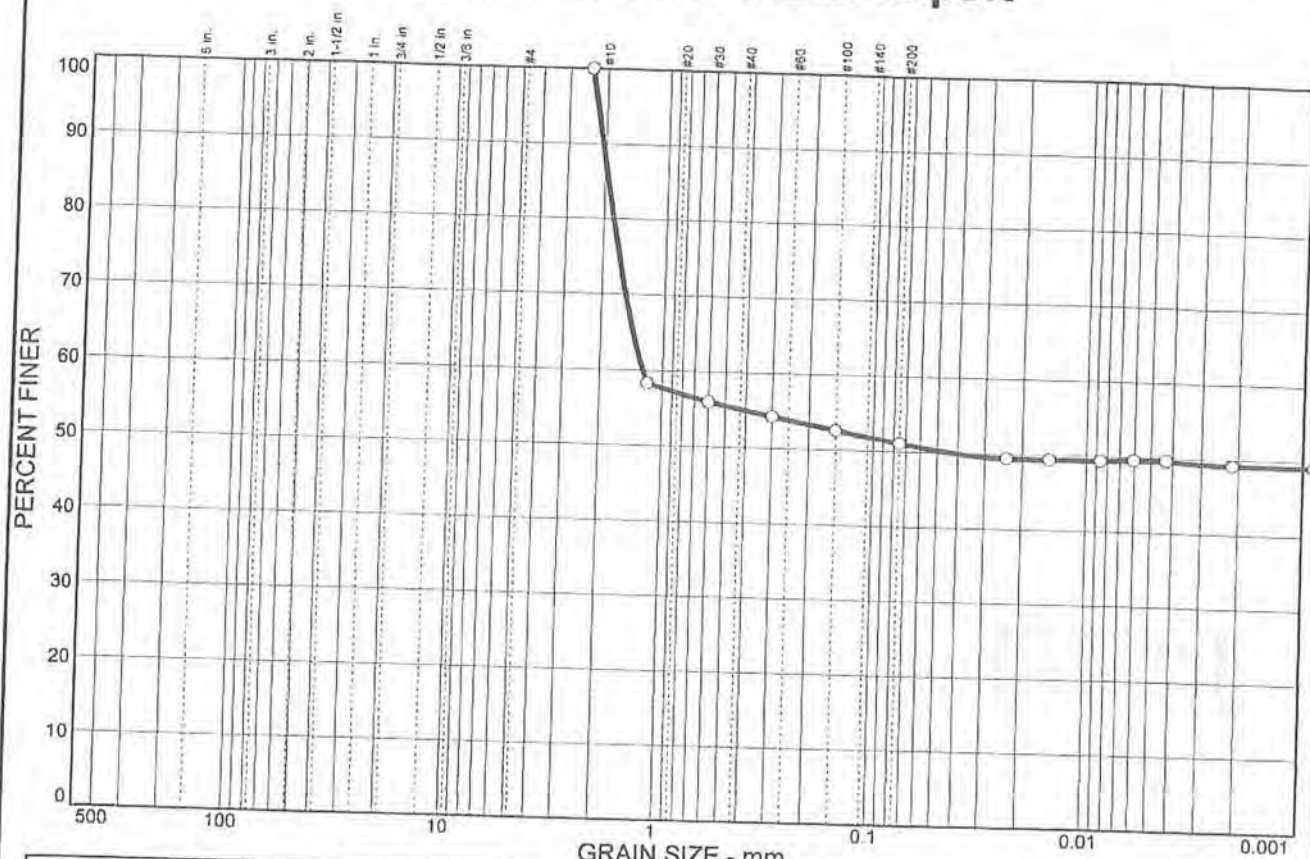
Project: Panoramic #4

Project No.: 20007800025

Figure

7-26-87

Particle Size Distribution Report



% COBBLES	% GRAVEL		% SAND			% FINES	
	CRS.	FINE	CRS.	MEDIUM	FINE	SILT	CLAY
0.0	0.0	0.0	12.3	32.5	4.1	1.5	49.6

SIEVE SIZE	PERCENT FINER	SPEC.* PERCENT	PASS? (X=NO)
#8	100.0		
#16	58.3		
#30	56.0		
#50	54.3		
#100	52.6		
#200	51.1		

* (no specification provided)

Soil Description

Sandy elastic silt

PL= 45

Atterberg Limits

LL= 96

PI= 51

Coefficients

D₈₅= 1.93

D₆₀= 1.24

D₅₀= 0.0421

D₃₀=

D₁₅=

D₁₀=

C_u=

C_c=

Classification

USCS= MH

AASHTO=

Remarks

Sample No.: RLS070051

Location:

Source of Sample: Panoramic #4

Date: 7/4/07

Elev./Depth: BH-3, @ 25'

**QUALITY
INSPECTION
SERVICES, INC.**

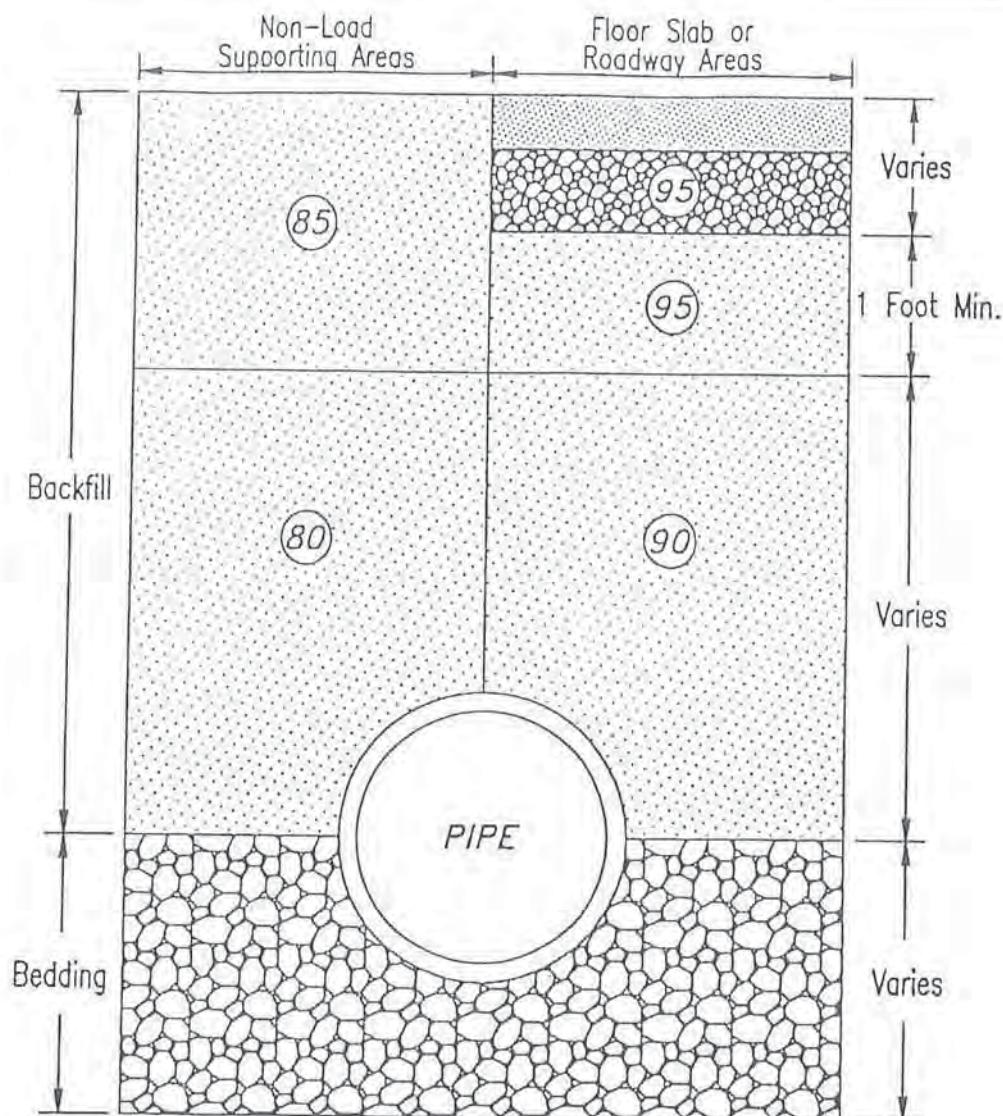
Client: ABG # 0-249.7

Project: Panoramic #4

Project No: 20007800025

Figure

7-26-07



LEGEND:



Asphalt or Concrete Pavement or Concrete Floor Slab



Base Material or Base Rock



Backfill; Compacted On-Site Soil or Imported Select Fill Material as Described in the Site Preparation of the General Earthwork Section of the Attached Report Text.

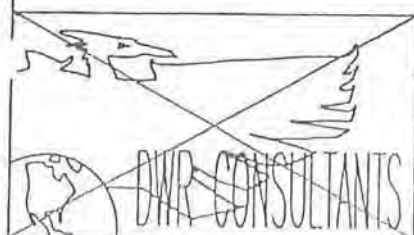


Bedding Material; Material Type Depends on Type of Pipe and Laying Conditions. Bedding Should Conform to the Manufacturers Recommendations for the Type of Pipe Selected.



Minimum Percentage of Maximum Laboratory Dry Density as Determined by ASTM Test Method D 1557-78 (Modified Proctor). Unless Otherwise Specified in the Attached Report Text.

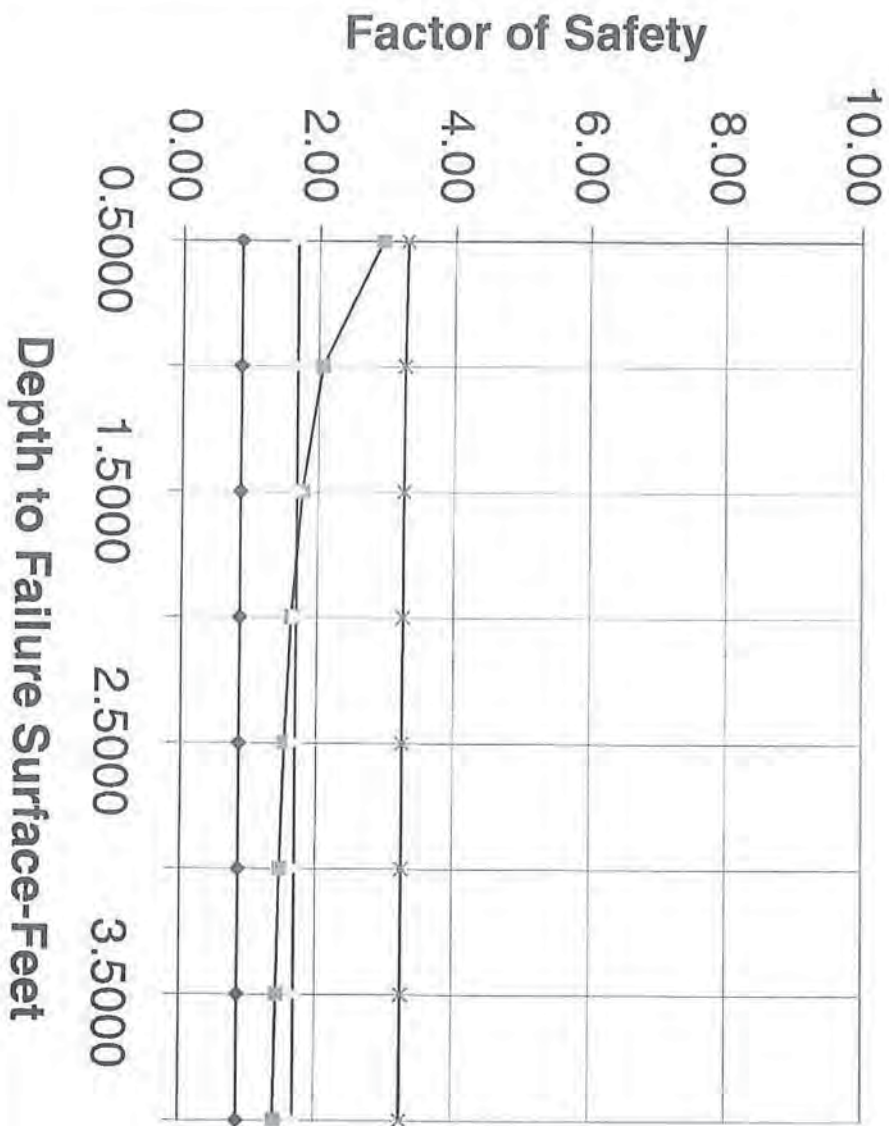
Fig. 3



TYPICAL UTILITY TRENCH
FILL AND FLOOR SLAB
REQUIREMENTS

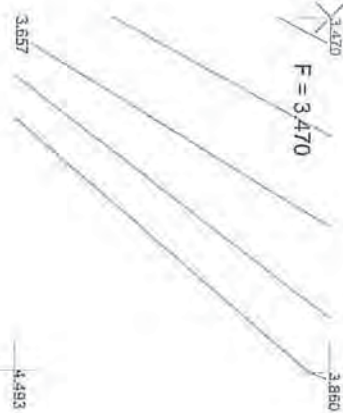
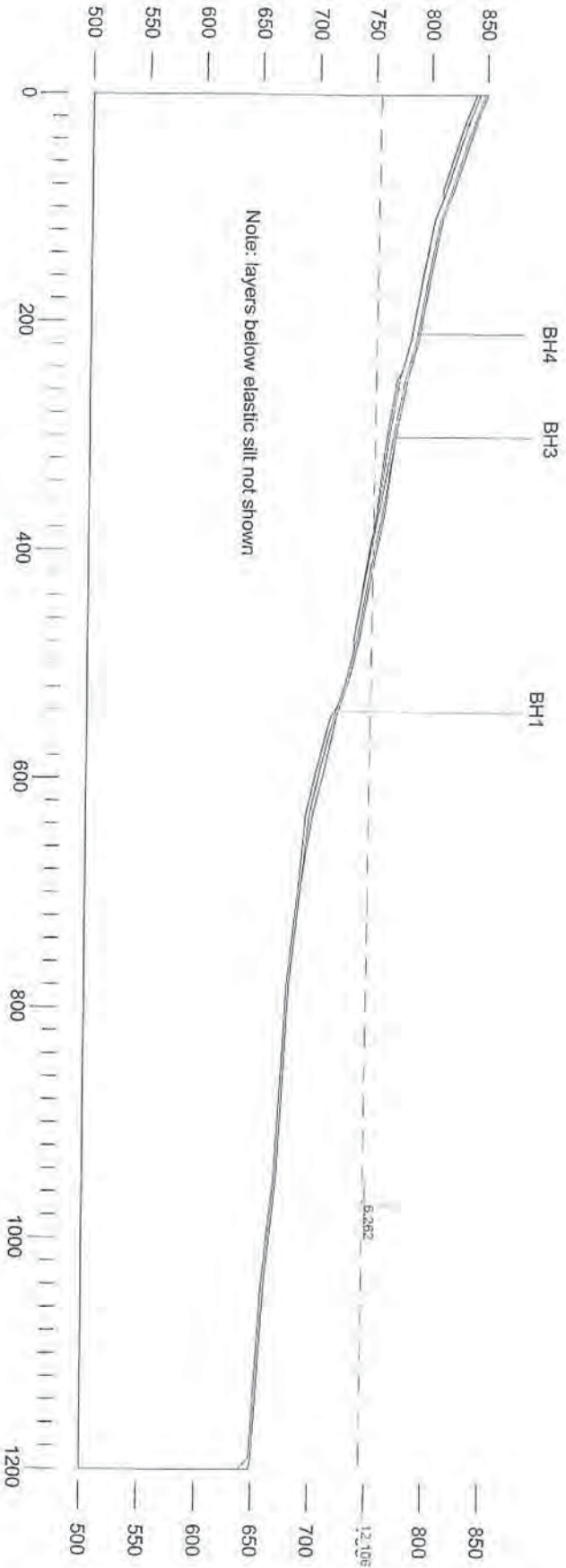
DWR CONSULTANTS, INC.
Pasco, WA
(509) 545-0181
A. Evans 10-28-96

22.0 Degree Slope Stability Against Shallow Sloughing or Face Sliding



- ◆— 0 degree seep
- 26.6 degree seep
- 60 degree seep
- *— 90 degree seep

	Gamma C	Phi	Piezo	Ru
	pcf	psf	deg	Surf.
Silty fine sand	115	0	29	1
Gravel	130	0	34	1
Cemented Gravel	140	200	38	1
BA flow bottom	175	500	45	1
Elastic silt	110	200	30	1



DWR Consultants Inc. - Pasco, WA

Chavallio Panoramic No. 4

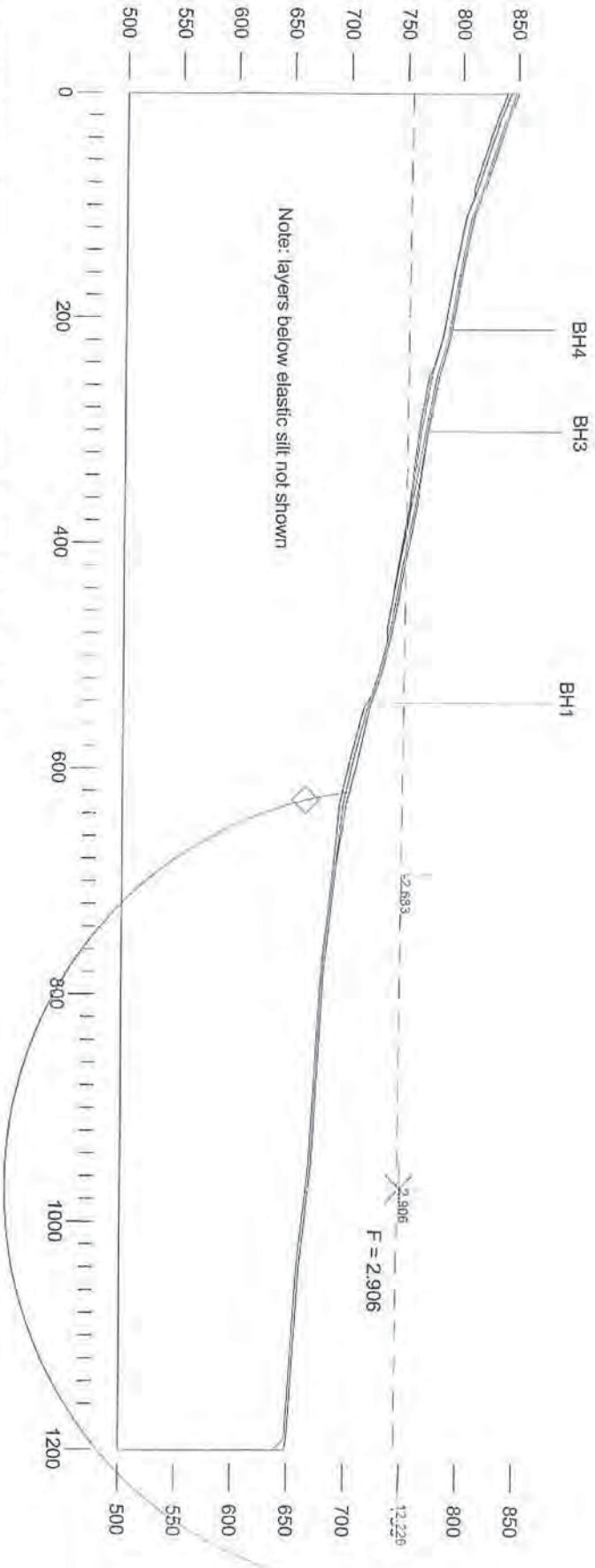
08/15/05:559

	Gamma C	Psi	Piezo	Ru
	pcf	psf	deg	Surf.
Silty fine sand	115	0	29	1
Gravel	130	0	34	1
Cemented Gravel	140	200	38	1
BA flow bottom	175	500	45	1
Elastic silt	110	200	30	1

DWR Consultants Inc. - Pasco, WA

Chavallio Panoramic No. 4

08/15/03.313



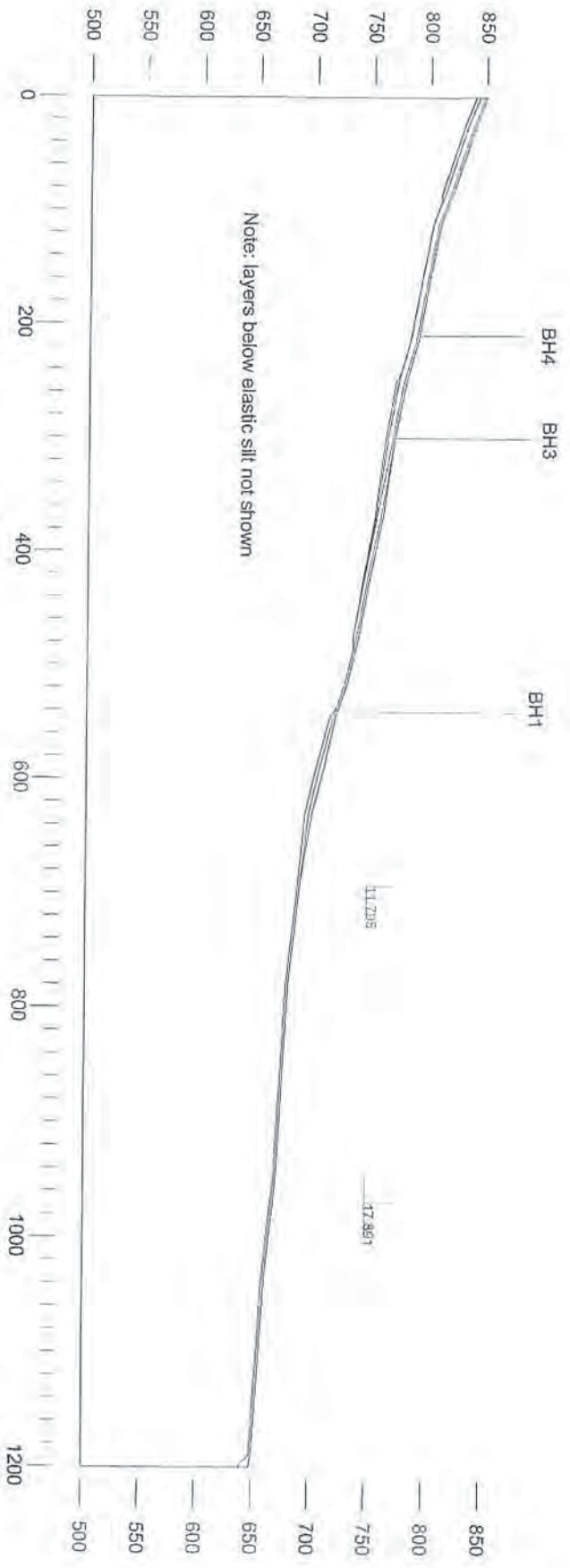
	Gamma C	Phi	Piezo	Ru
	pcf	deg	Surf.	
Silty fine sand	115	0	29	0
Gravel	130	0	34	0
Cemented Gravel	140	0	38	0
BA flow bottom	175	0	45	0
Elastic silt	110	0	30	0

~~8.498~~
F = 8.501

DWR Consultants Inc. - Pasco, WA

Chavallio Panoramic No. 4

08/15/2007 15:585





Columbia Engineers & Constructors, LLC

Critical Areas Report for
Citadel Estates,
SE 1/4 , Section 8, T8N, R29E
Kennewick, WA

Prepared for:
Jose Chavallo
102 No. Lincoln
Kennewick, WA 99336

Prepared By:
Michael Black, P.E.
Columbia Engineers and
Constructors, LLC
1806 Terminal Drive
Richland, WA 99354

July 1, 2009



*Chavallo, Citadel Estates (CAR) - July 1, 2009***Introduction**

This report provides our critical area assessment for the proposed residential development. The site is currently undeveloped land.

The site lies west of the current developed land at Panoramic Heights. The east boundary of the site lies west of the termination of 25th and 26th avenues. The enclosed topographic map shows the general location of the site.

Scope of Work

The Kennewick Municipal Code, Chapter 18, Zoning contains requirements for "Critical Areas" contained in the following sections:

- Critical Areas-General Provisions (18.58)
- Critical Areas- Wetlands (18.59)
- Critical Areas- Critical Aquifer Recharge Area (18.60)
- Critical Areas- Frequently Flooded Areas (18.61)
- Critical Areas- Geologically Hazardous Areas (18.62)
- Critical Areas- Fish and Wildlife Habitat Conservation Areas (18.63)

This assessment specifically addresses Sub-sections 18.60 and 18.62 with the companion Sub-section of 18.58 that is common to all sub-sections. The remaining sub-sections have been addressed as not applicable in the SEPA checklist prepared by others.

Summary Conclusion

We find no impediments to complying with sub-sections 18.60 and 18.62 within the framework of the ordinances. Sub-section 18.60 does not require any action, while Sub-section 18.62 does require some mitigation via engineering structures and operational limitations.

Assessment**Proposed Development**

Christensen¹ shows 27 lots along with a proposed finish floor elevation for the main floor of each home. Access to the development will be served by a looped road tying in with Kellogg Street. The drawing is enclosed with this report. This development does contain existing slopes steeper than 15% that drives the need for an assessment per COK Ordinance 18.62. Using the axiom that water runs downhill and settles in a low spot, the development is near the base of "Thompson Hill" so COK Ordinance 18.60 is considered as well.

¹ Christenson, P.D., *Citadel Estates, Conceptual Grading Plan*, 5/29/09, Oasis Development Corporation

*Chavallo, Citadel Estates (CAR) - July 1, 2009*Surface Conditions

This site is located on the Southeast base of Thompson Hill and contains north sloping terrain with mostly desert shrub vegetation. The extreme north end of the site contains some fill and cut areas associated with home construction to the east and canal construction for the Kennewick Irrigation District (KID). The attached USGS topographical map shows the approximate site location within the site general topography.

Regional Stratigraphy

Most of the geologic features in this area consist of sedimentary deposits overlying Tertiary volcanic bedrock. The bedrock has been moved, and is continuing to move, by a general north-south compression of the basalt flows forming anticline structures (long narrow hills) with an east-west general bearing. This movement includes Thompson Hill. The site is located on the north flank of Thompson Hill. These structures are still moving in the upward direction at an imperceptibly slow rate. Another smaller anticlinal fault, along the south flank of Thompson Hill has been mapped².

Local Stratigraphy

Black³ provided a geotechnical investigation for the site with the following comments on stratigraphy from the starting at the surface:

- Recent Aeolian (wind deposited) silt and fine sand.
- The Touchet beds consisting of silt with fine sand or fine sand with silt with various degrees of cementation from calcium carbonate. We also found cemented gravel with calcium carbonate cement that is locally referred to as "caliche." These deposits occur in beds as they settled from the waters of the Lake Missoula floods.
- The Ice Harbor Member of the Saddle Mountains basalt occurred in borehole numbers 1, 2, and 4. The basalt is fairly thin on this site and has largely eroded away.
- The Levy Interbed occurred below the Ice Harbor basalt and the Elephant Mountain basalt below. This interbed contained primarily lean clay or elastic silt soil and resulted from sedimentation between the time the Elephant Mountain basalt was deposited and the successive Ice Harbor flow.

² Reidel, S. and K. Fecht. (1994). *Geologic Map of the Richland 1:100,000 Quadrangle, Washington*. Olympia, WA: Washing Division of Geology and Earth Resources. 1:100,000.

³ Black, M.T., *Geotechnical Investigation for Panoramic Hts. 4, SE ¼, Section 8, T8N, R29E, Kennewick, WA*, August 21, 2007, Ashley-Bertsch Group, Inc.,

Chavallo, Citadel Estates (CAR) - July 1, 2009

No evidence of groundwater or seasonal groundwater was found in any of the 5 boreholes. We also submitted results from four piezometers for past work done by Black for the KID in the general area and downgradient from the canal. The results and elevations of the data are contained in the table below.

Approximate Upper Elevations of Major Soil/Rock Horizons

NE= not encountered

No.	Surface EL (ft.)	Bottom EL (ft.)	Silt and sand	Cemented gravel or gravel	Hard older silt and sandy silt	Gravel	Flow top or bottom of basalt	Layered silt and gravel
BH1	739	710	Surface	734	722	731.5	730	NE
BH 2	734	714	Surface	729		736.5	727	NE
BH 3	772	738	Surface	769.5	758	NE	NE	NE
BH 4	780	760	Surface	756	751.5	NE	753	NE
BH 5	816	796	Surface	NE	801	NE	NE	811
S&W-BH 1	~690 (+ or - 10)	643.5	Surface	NE	665	NE	NE	NE
DWR PZ1	592	578	NE	NE	592	NE	NE	NE
DWR PZ2	613	591	Surface	NE	604.5	NE	NE	NE
DWR PZ3	614	602	Surface	NE	602	NE	NE	NE
DWR PZ4	612	598	Surface	NE	603	NE	NE	NE

The Geotechnical Investigation found lean clay at depth of around 20 ft. deep in the Geotechnical Investigation. In Borehole No. 1, at 20 ft. deep the clay, generally falls in the "low to medium" expansion range. The sample from Borehole No. 3, at 19.5 ft. deep shows a "high to very high" range with the exception of the "% passing the 200 sieve," "plastic limit," and colloid criteria. The sample from Borehole No.3, at 25 ft. deep shows mostly "very high" potential with the exception of the "% passing the 200 sieve" criteria. These are not "fat clays" which often exhibit severe expansion potential. However, it is prudent to carefully plan and construct the development to minimize water saturation of the soil column.

Much of Kennewick and Richland are covered with silt and fine sand in various proportions of each. "Silt is inherently unstable, particularly when moisture is increased, with a tendency to become quick when saturated. It is relatively impervious, difficult to compact, highly susceptible to frost heave, easily erodible and subject to piping and boiling. Bulky grains reduce compressibility; flaky grains, i.e. mica and diatoms, increase compressibility and produce 'elastic silt'."⁴ The silt

⁴ US Department of Agriculture (Natural Resources Conservation Service). (1999). *Agricultural Waste Management Field Handbook*, Chapter 7-Geologic and groundwater considerations, Appendix 7B, Table 1-1.

Chavallo, Citadel Estates (CAR) - July 1, 2009

deposits from slow settling water and windblown silt tend to be bulky grained and subject to collapse under load. The elastic silts most likely were produced by degraded volcanic ash. The low permeability for water and the reduction in strength, when wetted, requires special care for this development.

The Geotechnical Investigation also examined slope stability of existing topography in the development. The results are discussed below:

- For an unsaturated condition, the factor of safety (FOS) is 8.5. Normally a 1.5 FOS is considered satisfactory for most residential developments.
- For a saturated condition, with near surface failure, the FOS= 3.47
- For a saturated condition, with deep seated failure, the FOS= 2.9

The slopes for shallow sloughing or face sliding were evaluated, which is a near surface phenomena, where a shallow sheet slides. Dependent on the direction of water seepage (E. g. 90° is vertical and 0° is horizontal) the FOS changes dependent on the depth of the slide considered. The slide potential in 0.5 ft. increments down to 3.5 ft. deep resulted in the following results

- A vertical or 90° seep affords a FOS exceeding 2 in all cases.
- A horizontal or 0° seep affords a FOS= 0.86 in all cases or *failure*.
- A 26.6° seep has a FOS= 2.9 at 0.5 ft. deep and 1.4 at 3.5 ft. deep.
- A 60° seep affords a FOS= 1.7 in all cases.

We found no evidence of seeps. Paul Christenson, P.E., stated that all stormwater runoff, including retaining wall drainage water, would be incorporated into the site stormwater system that discharges the water on another property both downgrade and to the north.

Conclusions

None of the slope boreholes showed existing or seasonal groundwater fluctuations. Therefore, the site does not impact a "Critical Aquifer Recharge Area" (18.60). The site does contain hazards associated with "Geologically Hazardous Areas" (18.62). The hazards considered are erosion, landslides, and seismic areas. These items are discussed below, based on the premise that development will not only concentrate stormwater due to impermeable surfaces, but irrigation will add water beyond what the normal rainfall produces.

Erosion Hazard Areas

The surficial silt and fine sand layer is highly erodible and subject to erosion. The existing vegetation currently protects the soil, and mitigation measures will be required during construction.

Chavallo, Citadel Estates (CAR) - July 1, 2009

Landslide Hazard Areas

Based on the analysis discussed above, the entire site, in the native condition, is stable unless considerable water is introduced to the soil column. The site will require grading for the home lots and significant elevation differences will require retaining walls to maximum lot sizes. The weight of the retaining walls will add loads to the slope and storm or irrigation water, if allowed to penetrate the soil column at depth, will add additional loads. Therefore, it is crucial that the retaining walls receive proper design and construction consideration and water entering the soil column be limited.

Seismic Hazard Areas

The aforementioned slope stability analysis is considered seismic loads. Although we judge that the seismic hazard is low, seismic loading must be considered for the design of the retaining walls that includes the global stability of the wall and the slope below.

Mitigation Requirements

COK Critical Areas-General Provisions (18.58) directs mitigation of hazards. Important elements, to this project, follow:

- Mitigation requirements (18.58.160)- As this is to be a residential development, alteration of the geologic hazards is required due to grading requirements for home sites. Properly designed and constructed retaining walls, along with water intrusion control to the soil column, will not only mitigate, but reduce geologic hazards.
- Mitigation sequencing (18.58.170)- The first consideration (1) requires "avoid the impact" which would require leaving the land in its natural state and not doing the development. This is not viable. The second consideration (2) addresses critical recharge areas and wetlands that do not apply to the site. The third consideration (3) requires "Minimizing or eliminating the hazard by restoring or stabilizing the hazard area through approved engineering or other methods." Engineering will be used to reduce the geologic hazards present in the natural state.

Mitigation Plan

Black³ provides substantial recommendations for mitigation of geologic hazards. In addition to the geotechnical report, the following mitigation measures are required:

- The design of all retaining walls shall consider global stability to ensure the slope below the retaining wall(s) remain stable. It is not necessary to design

Chavallo, Citadel Estates (CAR) - July 1, 2009

the retaining walls for an elevated groundwater table given the recommendations that follow.

- Each lot shall provide a "site grading and drainage plan" along with the request for a building permit. The plan should be stamped by a registered professional engineer. As part of the plan, stormwater discharge to the development's stormwater pond shall be detailed. In general, it is important that water not pond adjacent the homes or infiltrate into the soil column.
- If swimming pools or water features are to be installed, a registered professional engineer should review and approve the plans to ensure consistency with the "site grading and drainage plan."
- Based on my experience, the most important feature to ensuring stability of the slopes and retaining walls, with the consequent stability of the homes, is positive control of irrigation. Insofar as practicable, Xeroscape landscaping should be used. Drip irrigation poses less risk than pressurized irrigation. Each property should install a sprinkler controller that can be programmed for weather conditions, slopes, and soil type (E.g. "WeatherTrac"). The setting of the sprinkler controller should be reviewed by a third party to maintain no water saturation below 24 inches deep.
- During construction, the Excavation Safety Standards (Part N) from the Washington State Department of Labor will protect the workers from injury and the slopes from failure. The Washington State Department of Ecology requires a "Stormwater Pollution Prevention Plan (SWPPP)" and a permit (see publication #99-37, October 2008). Adherence to these requirements will mitigate erosion hazards during construction.
- Last, the geotechnical engineer shall review the plat construction drawings to ensure they meet the intent of the recommendations contained in the geotechnical report, and to ensure maximum isolation of the lean clay layer. The enclosed drawings show the preliminary planned elevations vs. existing topography. In most cases the daylight basement of each home will be at or above existing grade. In a few cases the basement will cut into the surficial soil no closer than 15 ft. above the lean clay layers. At this point, in the preliminary stage, the separation is sufficient to inhibit sliding or heaving of the clay layer.

Limitations

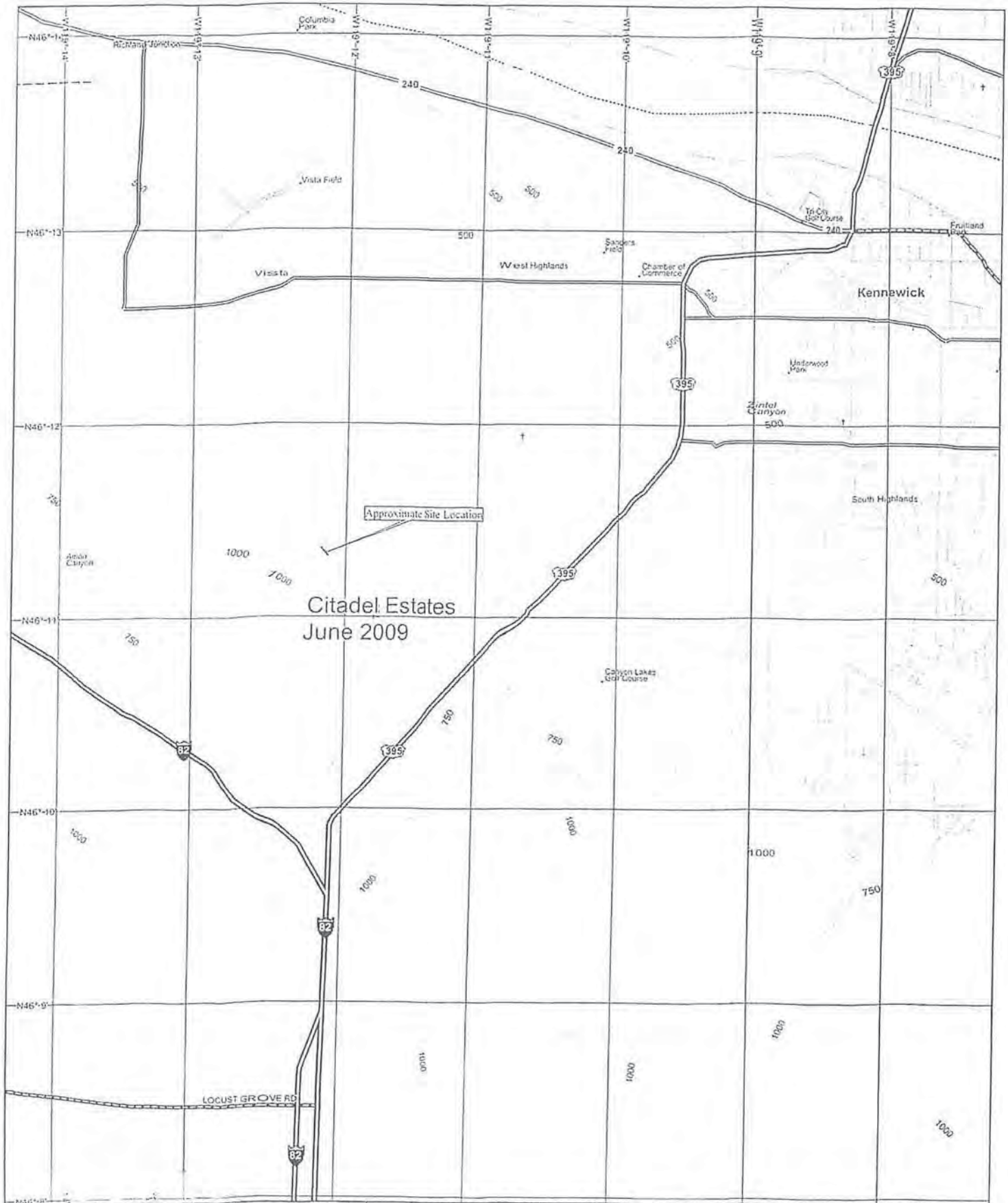
It is important that the limitations of our work and this report are understood. The recommendations and conclusions documented in this report have been prepared for specific application to your project based on the scope, budget, and schedule constraints. Further, these recommendations and conclusions have been developed in a manner consistent with the level of care and skill normally exercised by members of the engineering profession.

This report is prepared for the use of the CLIENT, design and construction professionals serving the CLIENT, and appropriate regulatory agencies. Columbia

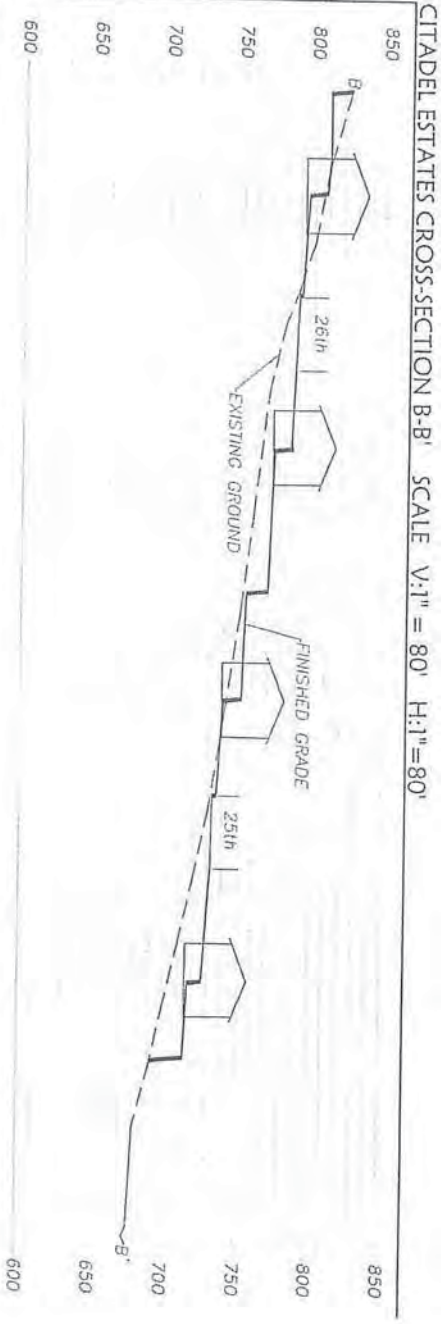
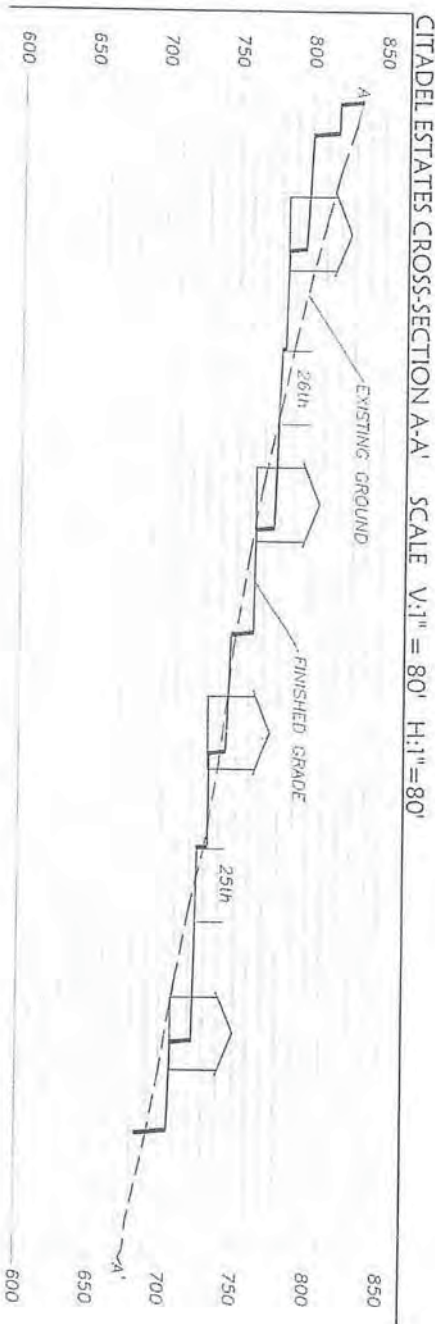
Chavallo, Citadel Estates (CAR) - July 1, 2009

Engineers and Constructors, LLC assumes no liability except to the CLIENT and this report remains the property of Columbia Engineers and Constructors, LLC Use of this report by third parties, without our written authorization, is prohibited.

Please contact our office should you have questions or comments, and thank you for your confidence in Columbia Engineers and Constructors, LLC







2008-04 07/01/2009 C2 OF 2	DATE _____ _____ _____	CITADEL ESTATES CROSS-SECTIONS	DESIGNED FOR: JOSE CHAVALLO KENNEWICK, WASHINGTON	OASIS DEVELOPMENT CORPORATION PO BOX 4766, PASCO, WASHINGTON 99302 PHONE: (509) 492-5495 FAX: (509) 492-4136 EMAIL: POCHRISTENSEN@CHARTER.NET
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Columbia Engineers & Constructors, LLC

October 15, 2010

Jose Chavallo
102 No. Lincoln
Kennewick, WA 99336

Subject: Addendum to Critical Areas report for Citadel Estates

References:

1. Black, M.T., *Geotechnical Investigation for Panoramic Hts. 4, SE ¼ Section 8, T8N, R29E, Kennewick, WA*, Ashley-Bertsch Group, Inc, August 21, 2007.
2. Black, M.T., *Critical Areas Report for Citadel Estates, SE ¼, Section 8, T8N, R29E, Kennewick, WA*, Columbia Engineers and Constructors, LLC, July, 1, 2009.
3. E-mail of 12Oct10, Paul Christenson to Jason Cushing; attachment drawing showing updated site plan with 36 lots, no date, no subject.

Dear Jose:

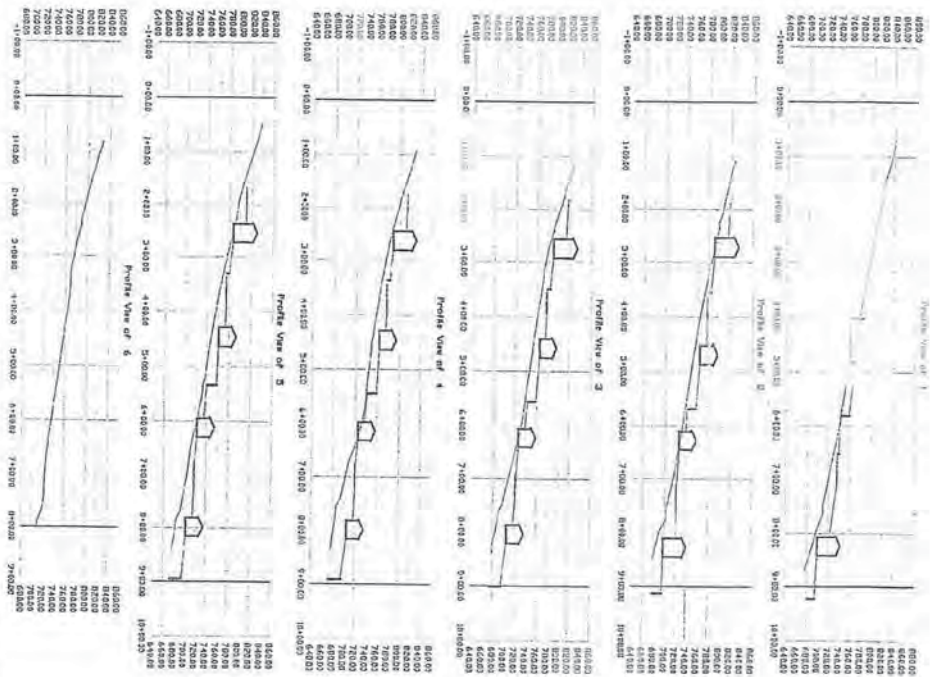
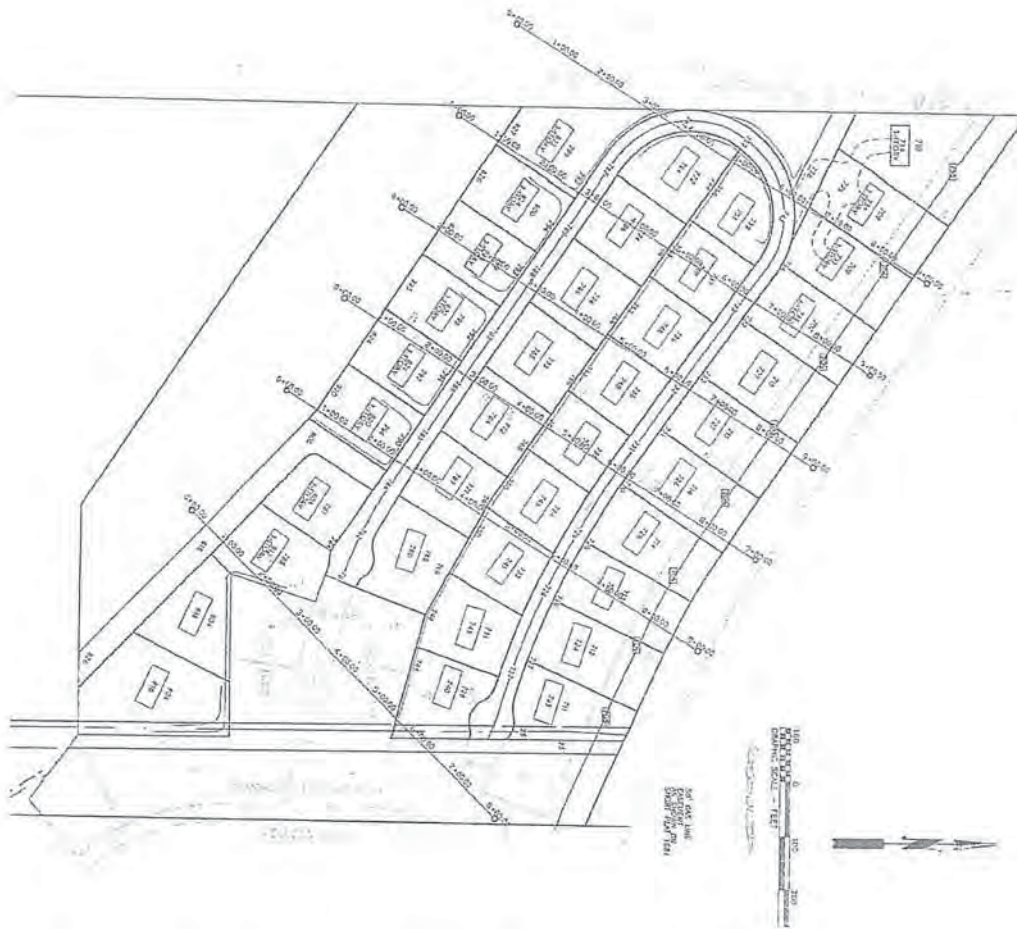
Reference 1 served as the basis for presentation of the Critical Areas Report provided to you in Reference 2. Reference 2 also provided a "Citadel Estates conceptual grading plan," prepared by Paul Christenson, P.E., Oasis Development Corporation, no date your Engineer-of-Record. After conversations with Mr. Christenson, we were advised that the spot elevations on the electronic drawing, Reference 3, represented planned elevations in accordance with what will be the Site Grading Plan. Using these data, we plotted 5 sections (attached) and I examined them for possible impact to Reference 2.

Based on the data herein, and assuming the final grading plan is representative of our sections, I find that the conclusions of Reference 2 remain valid.

Respectfully Yours,

Michael Black, P.E.
COLUMBIA ENGINEERS





10/15/10

HARMS

ENGINEERING, INC.

December 14, 2020

Gregory McCormick
Community Planning Director
City of Kennewick
210 W 6th Ave.
Kennewick, WA 99336

RE: Appeal 20-02/ PLN-2020-02960 Project # 20-125.1
Comprehensive Plan Amendment No. 20-06
SEPA Determination – Stormwater Impacts
Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000, and portions of 1-0889-401-1681-005

Dear Mr. McCormick:

Harms Engineering, Inc. (HEI) has evaluated the proposed Comprehensive Plan Amendment zoning change from RL and RS to RH for stormwater impacts. The City of Kennewick requires that new development design stormwater facilities to retain and dispose of a 25-year 24-hour design storm on-site.

When designing stormwater facilities, the proposed development is evaluated based on pervious (landscaping) and impervious (buildings/roads/parking) areas. Without a specific development with known roads, parking, driveways, and buildings, the exact areas of pervious and impervious surfaces are unknown. Therefore, HEI estimated the pervious and impervious surfaces for the various zoning types as follows:

- RS: minimum 10,500 sf per lot (50% impervious/50% pervious) plus 2,040 sf per lot of right of way (100% impervious), overall, 58% impervious and 42% pervious.
- RL: minimum 7,500 sf per lot (50% impervious/50% pervious) plus 1,720 sf per lot for right of way (100% impervious), overall, 60% impervious and 40% pervious.
- RH: 27 units/acre, all units assumed to be 2-bedrooms, minimum 1,600 sf per lot (60% impervious/40% pervious) plus 480 sf per lot for right of way (100%). Open space requirements for multi-family developments where density is more than 7 units per acre: 300 sf open area required for 1st bedroom; 200 sf open area required for additional bedrooms. Overall estimated to be 70% impervious and 30% pervious.

The existing property has 33.72 acres in RS zoning and 6.92 acres in RL zoning. If the entire property were fully developed at the maximum density for the existing zoning, it would generate approximately 128,500 cubic feet (cf) of runoff during the 25-year 24-hour storm. That amount of runoff could be contained by a stormwater pond that is 150 ft wide x 160 ft long (base dimensions), 5 ft deep, with 3:1 side slopes (horizontal: vertical).

If the entire property were fully developed at the maximum density for RH zoning, the 40.6 acres would generate approximately 150,400 cf of runoff from the 25-year 24-hour storm. That amount

Gregory McCormick / Appeal 20-02/ PLN-2020-02960
December 14, 2020

Page 2
Project #: 20-125.1

of runoff could be contained by a stormwater pond that is 150 ft wide x 190 ft long (base dimensions), 5 ft deep, with 3:1 side slopes.

A zoning change from RS and RL to RH could result in approximately 17% more runoff that would need to be handled versus the existing zoning. Stormwater facilities for the development will be sized to collect, convey, and retain the runoff of the proposed development in accordance with the City's standards and regardless of zoning designation.

If you have any questions with this stormwater analysis, please contact me at 509-547-2679 or email at christine@harmsengineering.com.

Sincerely,



Christine Batayola
2020.12.14
15:25:38 -08'00'

Christy Batayola, PE



Sunburst
ENGINEERING

16402 E. Valleyway Ave
Spokane Valley, WA 99037
(509) 924-2155 sunburstengr.com

December 22, 2020

W. O. No. 2027

Gregory McCormick, AICP
Community Planning Director
City of Kennewick
210 West 6th Avenue
Kennewick, WA 99336-0108

RE: **Appeal 20-02 / PLN-2020-02960**
Comprehensive Plan Amendment No. 20-06
SEPA Determination - Traffic Impacts
Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000 and portions of
1-0889-401-1681-005

Dear Mr. McCormick:

Sunburst Engineering has evaluated the proposed Comprehensive Plan Amendment (CPA) change from Low Density Residential to High Density Residential for traffic and trip generation impacts. Our analysis was completed for both the existing comprehensive plan designation, and the proposed changes to that designation.

Project Description

The property included in this CPA are:

- Tax Parcel 1-0889-400-00004-000,
- Tax Parcel 1-0889-301-3081-0001, and
- Lots 37 and 38 within the preliminary plat of Citadel Estates.

These parcels together total approximately 40.6 acres.

This CPA is a non-project action, and no site plan is attached to this application. If the amendment is approved, additional land use actions will become necessary, including a rezone. For this project, it is my understanding the property owner has committed to coupling the rezone with a project-specific application. We support this approach because it will include project specific traffic analysis at the right time - with the rezone and the project-specific application.

Appeal 20-02 / PLN-2020-02960
Comprehensive Plan Amendment No. 20-06
SEPA Determination - Traffic Impacts
Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000
and portions of 1-0889-401-1681-005
December 22, 2020
Page 2

Development Potential

The present comprehensive plan designation of these parcels allows up to 5 single family dwelling units per acre. Due to terrain and other factors, it is likely full development would be significantly lower. Based upon these factors, for the site as a whole, 203 single family homes would likely be developed under the existing comprehensive plan.

The proposed change to the site would allow multi-family residential housing units and hotels to locate on this site. Regulations state that up to 27 units per acre would be allowed under the proposed comprehensive plan. Since the ultimate development on this site is currently unknown, several options will be analyzed. All will include a 60 room hotel near the winery located near this site. The additional land uses proposed on the site may also include one of the following development levels.

1. 1,096 multi-family dwelling units.
2. 500 multi-family dwelling units.
3. 300 multi-family dwelling units.
4. 200 multi-family dwelling units.

Trip Generation Methodology

Traffic generation information has been compiled by the Institute of Transportation Engineers in a manual. The current version of this manual is titled, "*Trip Generation Manual, 10th Edition.*" This manual contains the trip generation characteristics of many land use categories (LUC), including

- LUC 210 - Single Family Detached Housing,
- LUC 221, Multi-family Housing (Mid-Rise), and
- LUC 310 - Hotel.

These land use categories will be used in this analysis.

Appeal 20-02 / PLN-2020-02960
 Comprehensive Plan Amendment No. 20-06
 SEPA Determination - Traffic Impacts
 Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000
 and portions of 1-0889-401-1681-005
 December 22, 2020
 Page 3

Trip Generation Characteristics of Site Developed Using Existing Comp Plan

As explained previously, if the site were developed into single family housing, approximately 203 units would be developed on the site. The anticipated traffic generation characteristics of the site under this scenario is shown on Table 1.

Table 1 - Trip Generation Characteristics of Single-Family Detached Housing

Units	AM Peak Hour			PM Peak Hour			ADT
	Vol @ 0.74 Trips per Unit	Directional Distribution		Vol @ 0.99 Trips per Unit	Directional Distribution		Vol @ 9.44 Trips per Unit
		25% In	75% Out		63% In	37% Out	
203	150	37	113	201	127	74	1,916

Trip Generation Characteristics of Site With Proposed Comp Plan Change

As stated previously, the change in the comprehensive plan will allow a hotel to locate on this site. This hotel will be adjacent to the winery next door and is planned to have a restaurant in it. The anticipated traffic generated by the hotel is shown on Table 2.

Table 2 - Hotel Trip Generation Rate & Volume Summary

#	A.M. Peak Hour			P.M. Peak Hour			ADT
	Vol @ 0.47 Trips per Room	Directional Distribution		Vol @ 0.60 Trips per Room	Directional Distribution		Vol @ 8.36 Trips per Room
		59% In	41% Out		51% In	49% Out	
60	28	16	12	36	18	18	502

- Number of Rooms

ADT - Average daily trips

In addition to the hotel, the comprehensive plan change would allow multi-family housing to locate on this site. Table 3 shows the traffic generated by the various options outlined previously in this document.

Appeal 20-02 / PLN-2020-02960
 Comprehensive Plan Amendment No. 20-06
 SEPA Determination - Traffic Impacts
 Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000
 and portions of 1-0889-401-1681-005
 December 22, 2020
 Page 4

**Table 3 - Multi-Family Trip Generation Rates & Volumes for
Options Considered On-site**

Units	AM Peak Hour			PM Peak Hour			ADT
	Vol @ 0.36 Trips per Unit	Directional Distribution		Vol @ 0.44 Trips per Unit	Directional Distribution		Vol @ 5.44 Trips per Unit
		26% In	74% Out		61% In	39% Out	
1,096	395	103	292	482	294	188	5,962
500	180	47	133	220	134	86	2,720
300	108	28	80	132	81	51	1,632
200	72	19	53	88	54	34	1,088

Conclusions

Based upon the information in the preceding tables, the four parcels included in this comprehensive plan amendment could be developed using the parameters of the existing comprehensive plan into 203 single family homes generating:

150 trips in the a.m. peak hour,
 201 trips in the p.m. peak hour, and
 1,916 trips on an average day.

If the comprehensive plan amendment is approved, the site could develop into a hotel and an unknown number of multi-family homes. As such, the site would generate:

between 100 - 423 trips during the a.m. peak hour,
 between 124 - 518 trips during the p.m. peak hour, and
 between 1,590 - 6,464 trips during an average day.

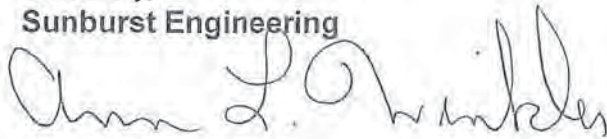
Appeal 20-02 / PLN-2020-02960
Comprehensive Plan Amendment No. 20-06
SEPA Determination - Traffic Impacts
Parcels # 1-0889-301-3081-002, 1-0889-400-0004-000
and portions of 1-0889-401-1681-005
December 22, 2020
Page 5

The net change in traffic volumes due to this comprehensive plan amendment is:

-50 to +273 trips during the a.m. peak hour,
-77 to +317 trips during the p.m. peak hour, and
-326 to +4,548 trips during an average day.

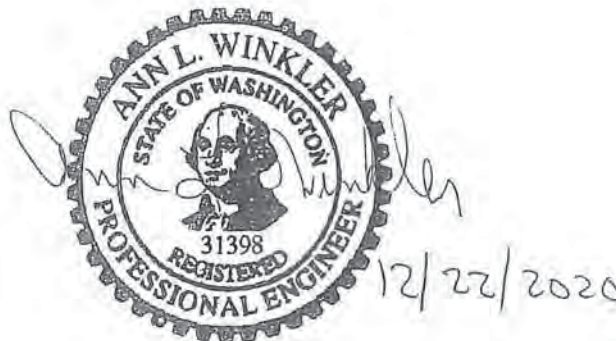
Please let me know if you need additional information.

Sincerely,
Sunburst Engineering



Ann L. Winkler, P. E.
President

encl: Land Use Category Descriptions
Traffic Generation Information for Each Land Use and Time of Day



Land Use: 210

Single-Family Detached Housing

Description

Single-family detached housing includes all single-family detached homes on individual lots. A typical site surveyed is a suburban subdivision.

Additional Data

The number of vehicles and residents had a high correlation with average weekday vehicle trip ends. The use of these variables was limited, however, because the number of vehicles and residents was often difficult to obtain or predict. The number of dwelling units was generally used as the independent variable of choice because it was usually readily available, easy to project, and had a high correlation with average weekday vehicle trip ends.

This land use included data from a wide variety of units with different sizes, price ranges, locations, and ages. Consequently, there was a wide variation in trips generated within this category. Other factors, such as geographic location and type of adjacent and nearby development, may also have had an effect on the site trip generation.

Single-family detached units had the highest trip generation rate per dwelling unit of all residential uses because they were the largest units in size and had more residents and more vehicles per unit than other residential land uses; they were generally located farther away from shopping centers, employment areas, and other trip attractors than other residential land uses; and they generally had fewer alternative modes of transportation available because they were typically not as concentrated as other residential land uses.

Time-of-day distribution data for this land use are presented in Appendix A. For the six general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:00 and 5:00 p.m., respectively. For the two sites with Saturday data, the overall highest vehicle volume was counted between 3:00 and 4:00 p.m. For the one site with Sunday data, the overall highest vehicle volume was counted between 10:15 and 11:15 a.m.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in California, Connecticut, Delaware, Illinois, Indiana, Maryland, Minnesota, Montana, New Jersey, North Carolina, Ohio, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Vermont, and Virginia.

Source Numbers

100, 105, 114, 126, 157, 167, 177, 197, 207, 211, 217, 267, 275, 293, 300, 319, 320, 356, 357, 367, 384, 387, 407, 435, 522, 550, 552, 579, 598, 601, 603, 614, 637, 711, 716, 720, 728, 735, 868, 903, 925, 936

Land Use: 310 Hotel

Description

A hotel is a place of lodging that provides sleeping accommodations and supporting facilities such as restaurants, cocktail lounges, meeting and banquet rooms or convention facilities, limited recreational facilities (pool, fitness room), and/or other retail and service shops. All suites hotel (Land Use 311), business hotel (Land Use 312), motel (Land Use 320), and resort hotel (Land Use 330) are related uses.

Additional Data

Studies of hotel employment density indicate that, on the average, a hotel will employ 0.9 employees per room.¹

Twenty-five studies provided information on occupancy rates at the time the studies were conducted. The average occupancy rate for these studies was approximately 82 percent.

Some properties contained in this land use provide guest transportation services such as airport shuttles, limousine service, or golf course shuttle service, which may have an impact on the overall trip generation rates.

Time-of-day distribution data for this land use are presented in Appendix A. For the one center city core site with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 8:30 and 9:30 a.m. and 3:15 and 4:15 p.m., respectively. On Saturday and Sunday, the peak hours were between 5:00 and 6:00 p.m. and 10:15 and 11:15 a.m., respectively.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in California, District of Columbia, Florida, Georgia, Indiana, Minnesota, New York, Pennsylvania, South Dakota, Texas, Vermont, Virginia, and Washington.

For all lodging uses, it is important to collect data on occupied rooms as well as total rooms in order to accurately predict trip generation characteristics for the site.

Trip generation at a hotel may be related to the presence of supporting facilities such as convention facilities, restaurants, meeting/banquet space, and retail facilities. Future data submissions should specify the presence of these amenities. Reporting the level of activity at the supporting facilities such as full, empty, partially active, number of people attending a meeting/banquet during observation may also be useful in further analysis of this land use.

Source Numbers

170, 260, 262, 277, 280, 301, 306, 357, 422, 507, 577, 728, 867, 872, 925, 951

¹ Buttke, Carl H. Unpublished studies of building employment densities, Portland, Oregon.

Land Use: 221

Multifamily Housing (Mid-Rise)

Description

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors). Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (Land Use 225), and mid-rise residential with 1st-floor commercial (Land Use 231) are related land uses.

Additional Data

In prior editions of *Trip Generation Manual*, the mid-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.46 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 95.7 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 4:45 and 5:45 p.m., respectively.

For the four dense multi-use urban sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:15 and 5:15 p.m., respectively. For the three center city core sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 6:45 and 7:45 a.m. and 5:00 and 6:00 p.m., respectively.

For the six sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.46 residents per occupied dwelling unit.

For the five sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 95.7 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the five center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 1.84 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.94 during Weekday, AM Peak Hour of Generator
- 2.07 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.59 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 32 dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.90 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.90 during Weekday, AM Peak Hour of Generator
- 2.00 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.08 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 13 general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.56 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.88 during Weekday, AM Peak Hour of Generator
- 1.70 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.07 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), British Columbia (CAN), California, Delaware, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, New Hampshire, New Jersey, Ontario, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Utah, Virginia, and Wisconsin.

Source Numbers

168, 188, 204, 305, 306, 321, 357, 390, 436, 525, 530, 579, 638, 818, 857, 866, 901, 904, 910, 912, 918, 934, 936, 939, 944, 947, 948, 949, 959, 963, 964, 966, 967, 969, 970

Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 173

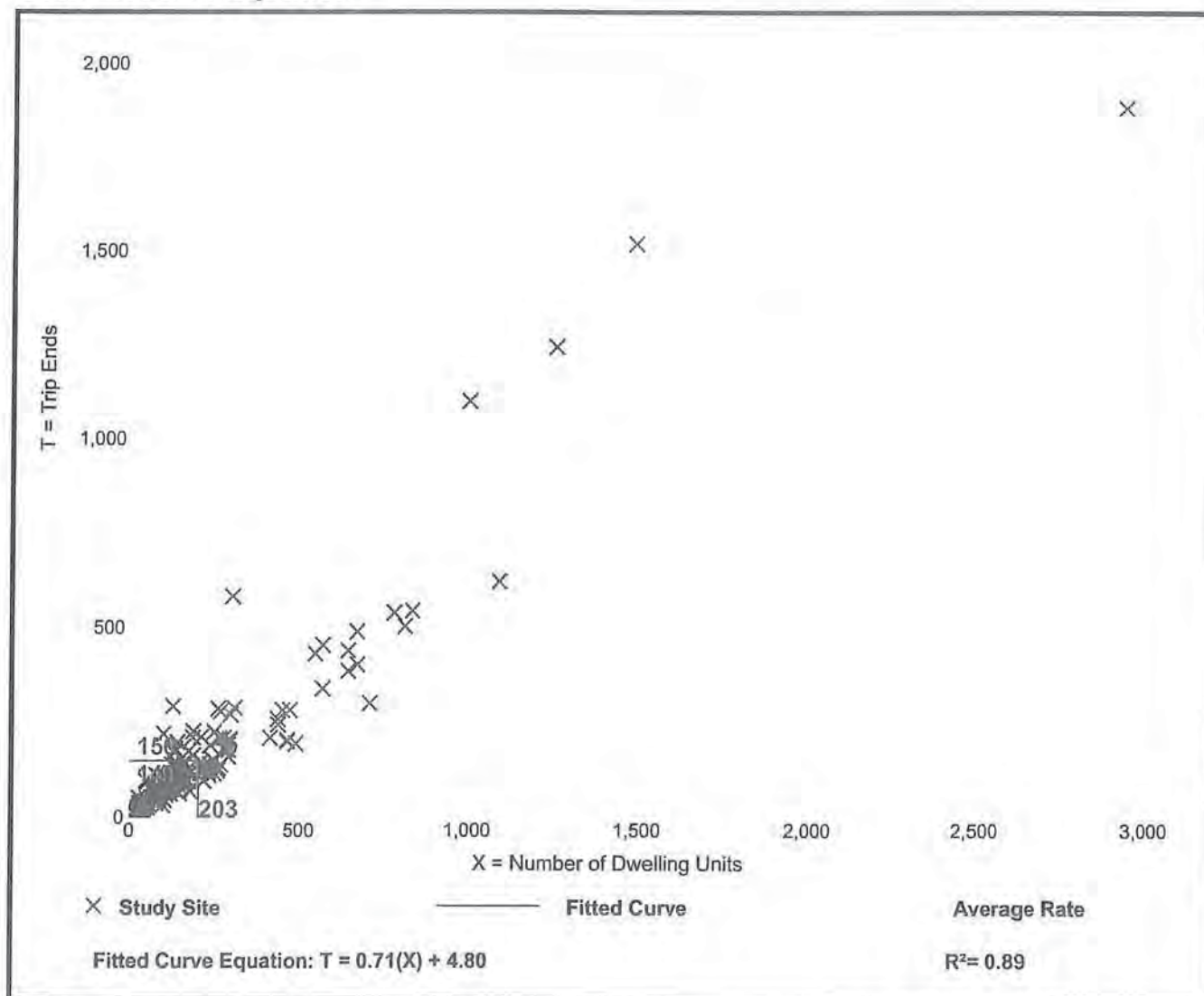
Avg. Num. of Dwelling Units: 219

Directional Distribution: 25% entering, 75% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.74	0.33 - 2.27	0.27

Data Plot and Equation



Single-Family Detached Housing (210)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 190

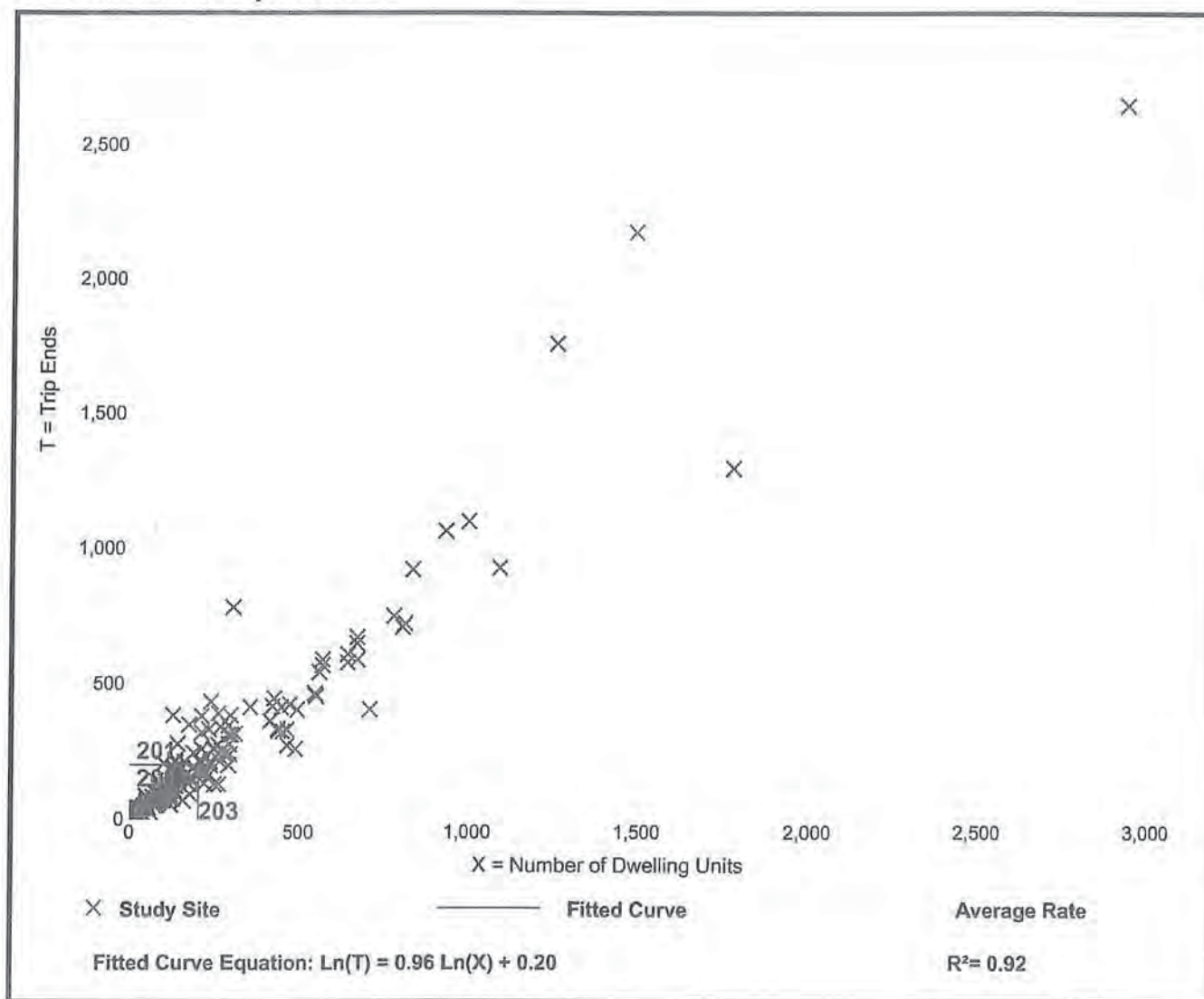
Avg. Num. of Dwelling Units: 242

Directional Distribution: 63% entering, 37% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.99	0.44 - 2.98	0.31

Data Plot and Equation



Single-Family Detached Housing (210)

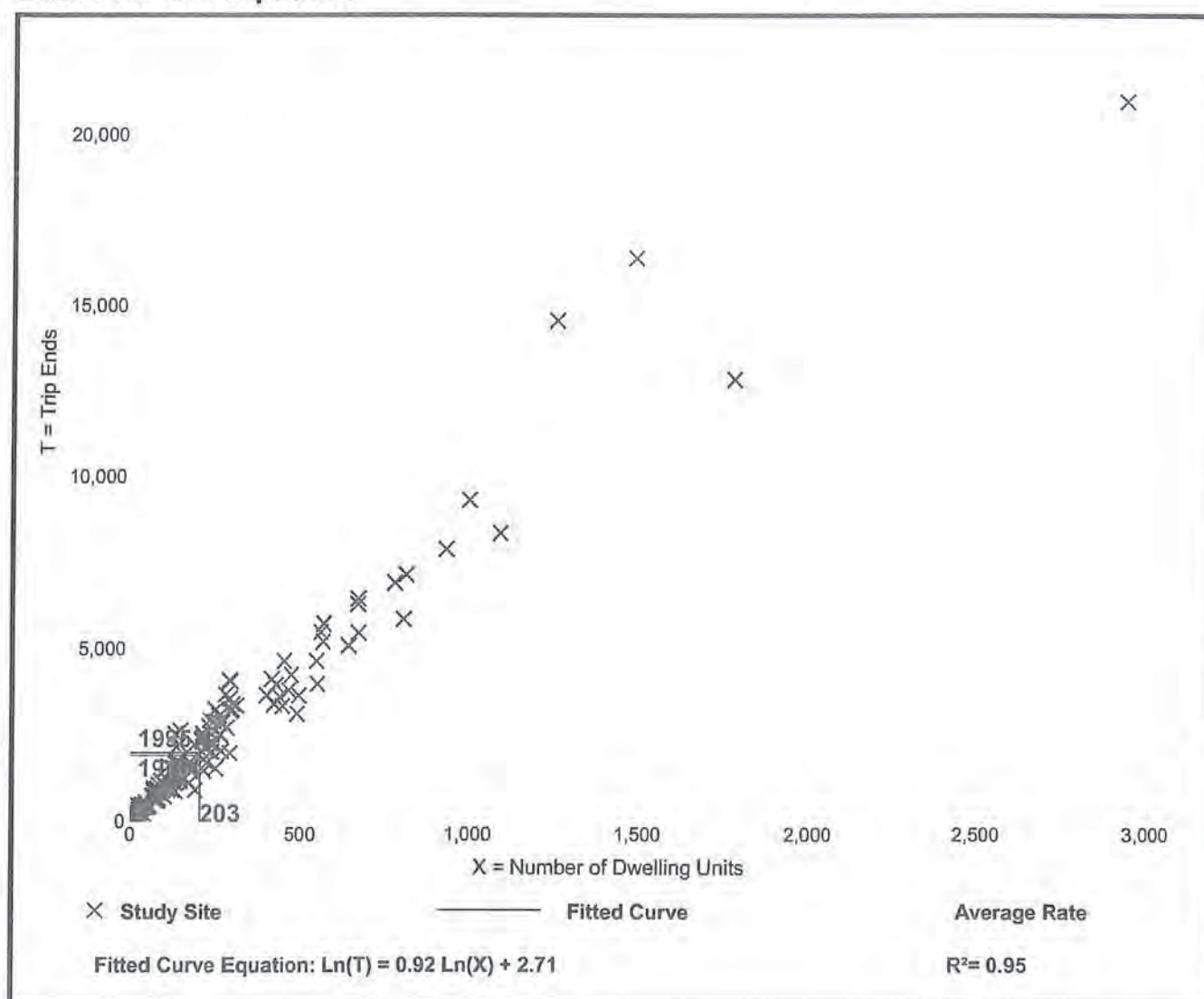
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 159
Avg. Num. of Dwelling Units: 264
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
9.44	4.81 - 19.39	2.10

Data Plot and Equation



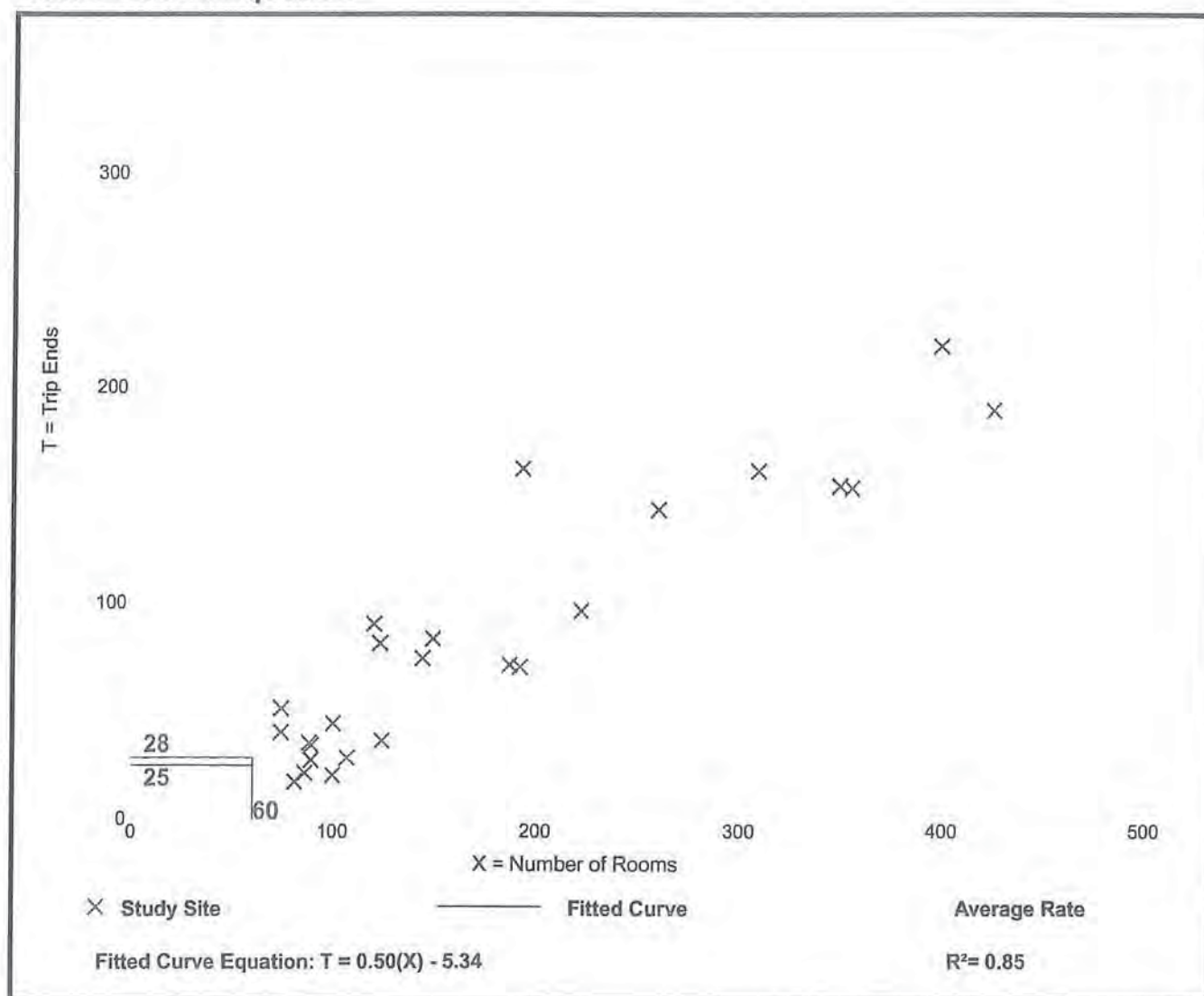
Hotel (310)

Vehicle Trip Ends vs: Rooms
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 25
 Avg. Num. of Rooms: 178
 Directional Distribution: 59% entering, 41% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.47	0.20 - 0.84	0.14

Data Plot and Equation



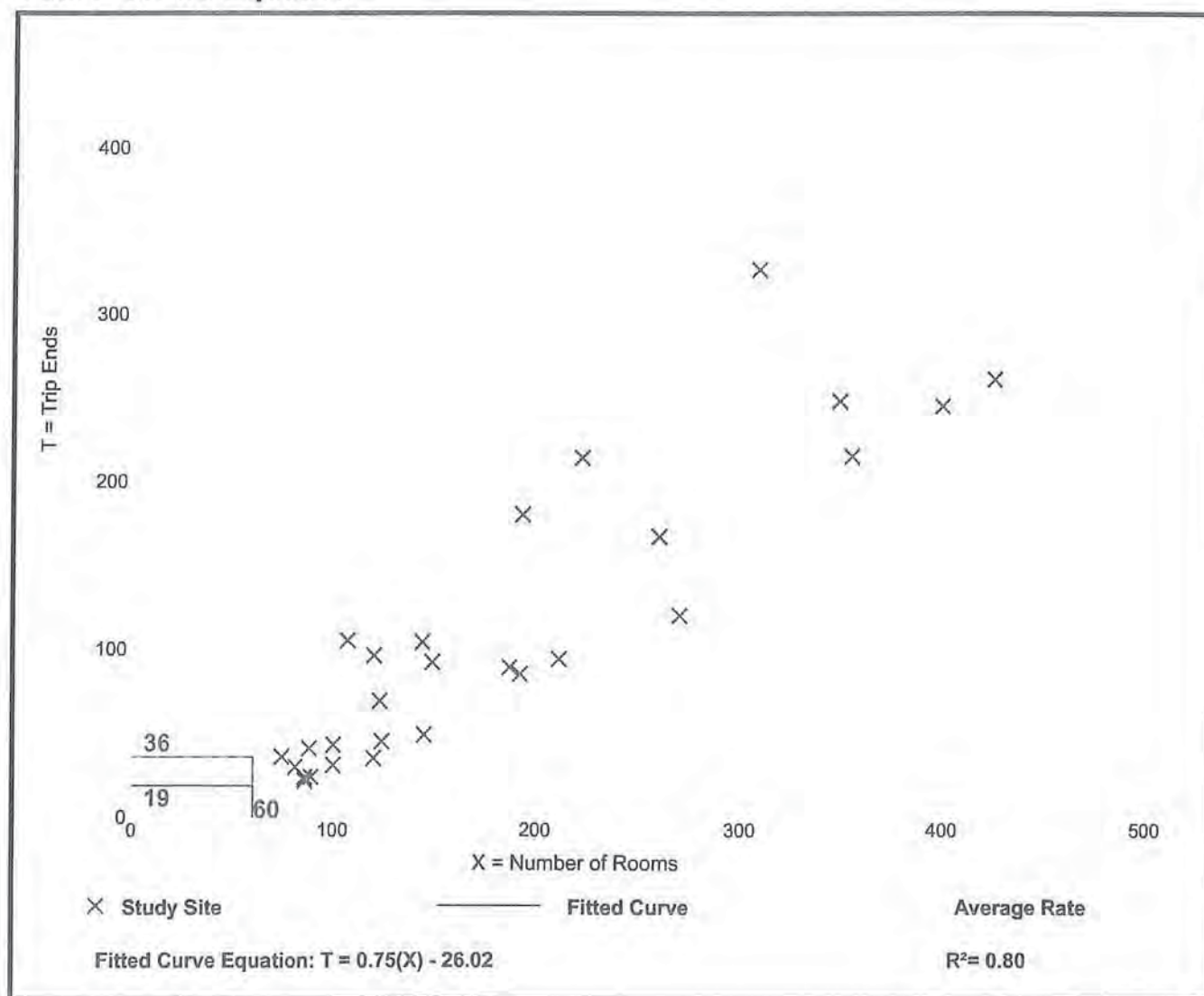
Hotel (310)

Vehicle Trip Ends vs: Rooms
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 28
 Avg. Num. of Rooms: 183
 Directional Distribution: 51% entering, 49% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
0.60	0.26 - 1.06	0.22

Data Plot and Equation



Hotel (310)

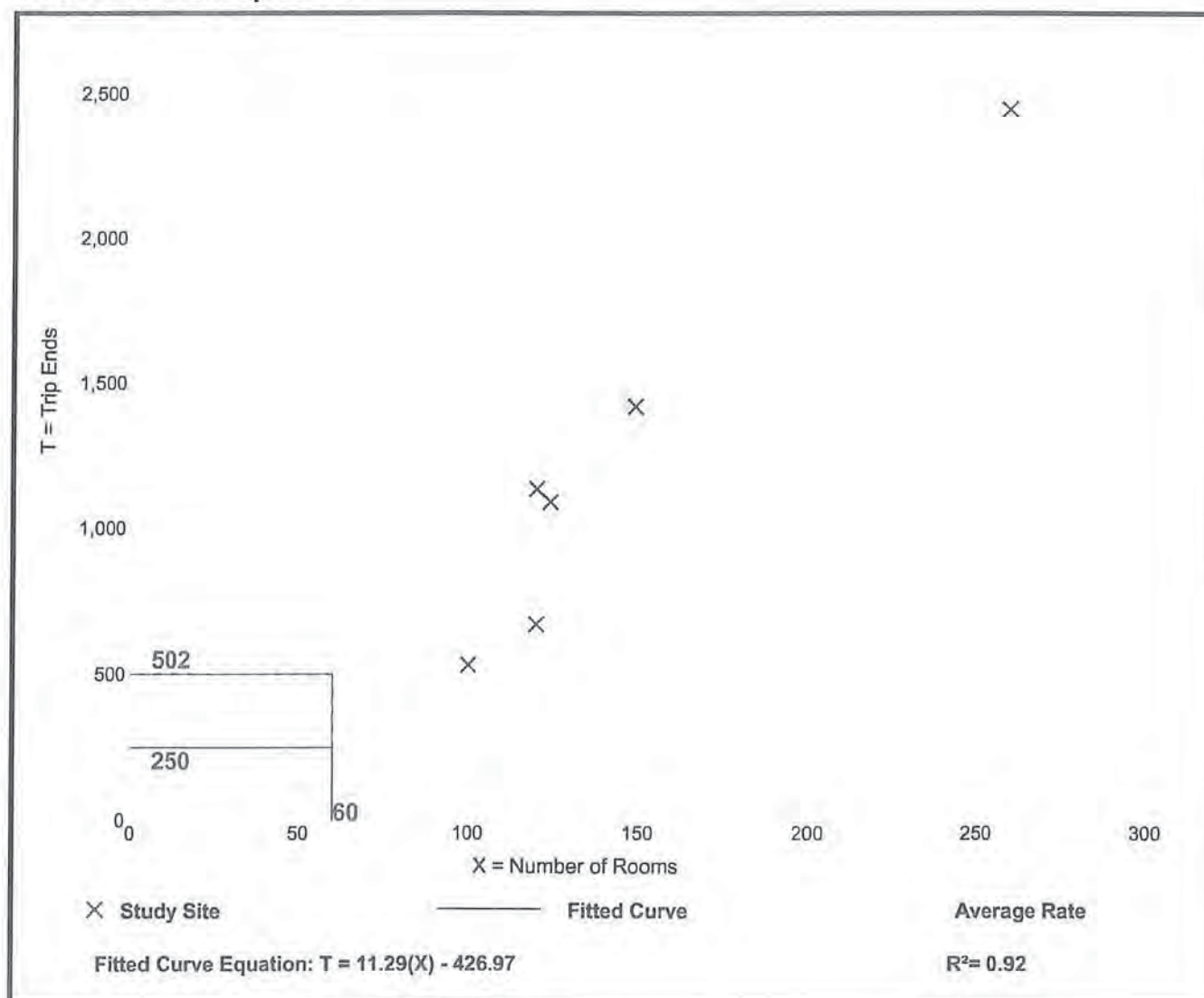
Vehicle Trip Ends vs: Rooms
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 6
Avg. Num. of Rooms: 146
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Room

Average Rate	Range of Rates	Standard Deviation
8.36	5.31 - 9.53	1.86

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

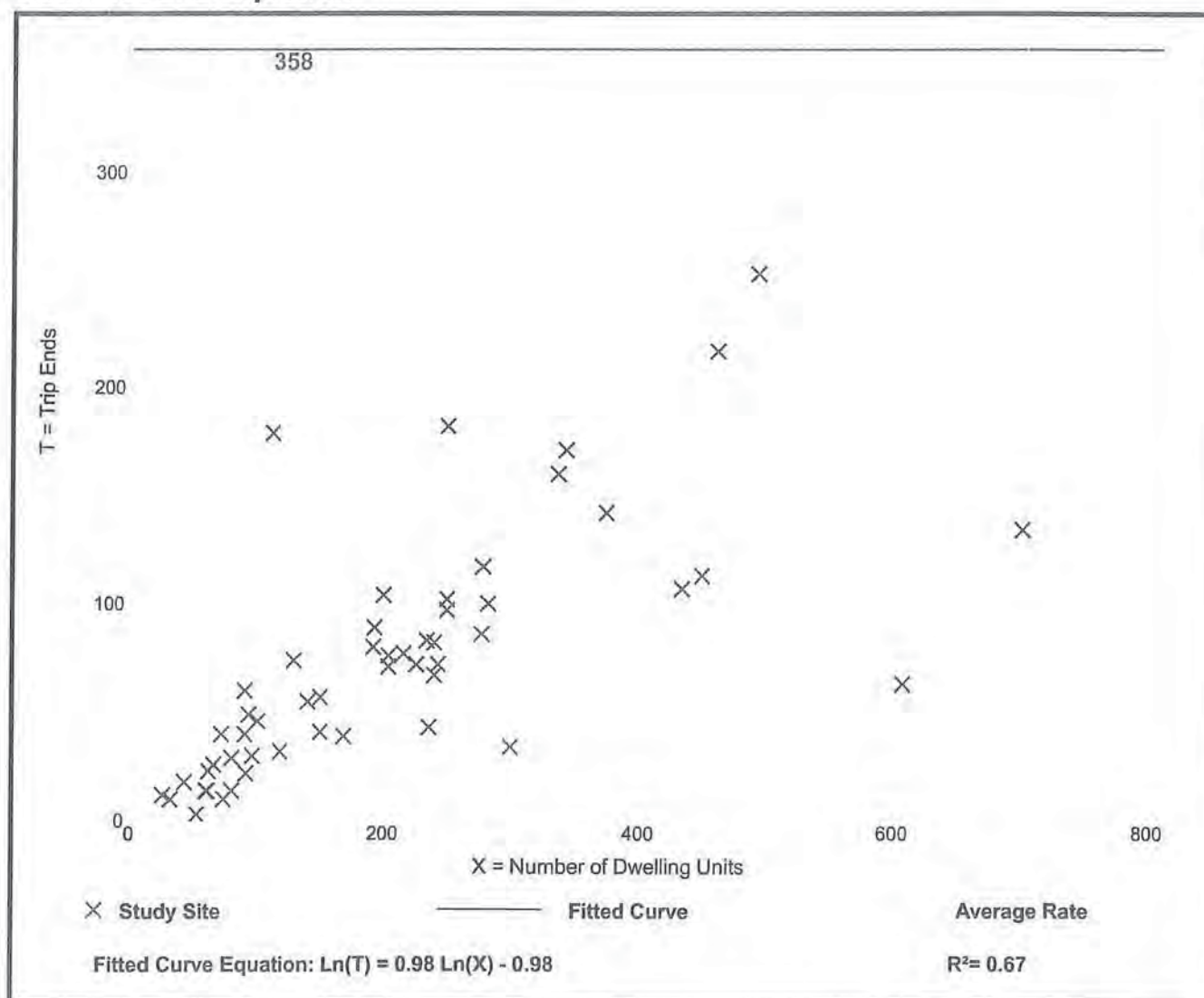
Avg. Num. of Dwelling Units: 207

Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.06 - 1.61	0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 60

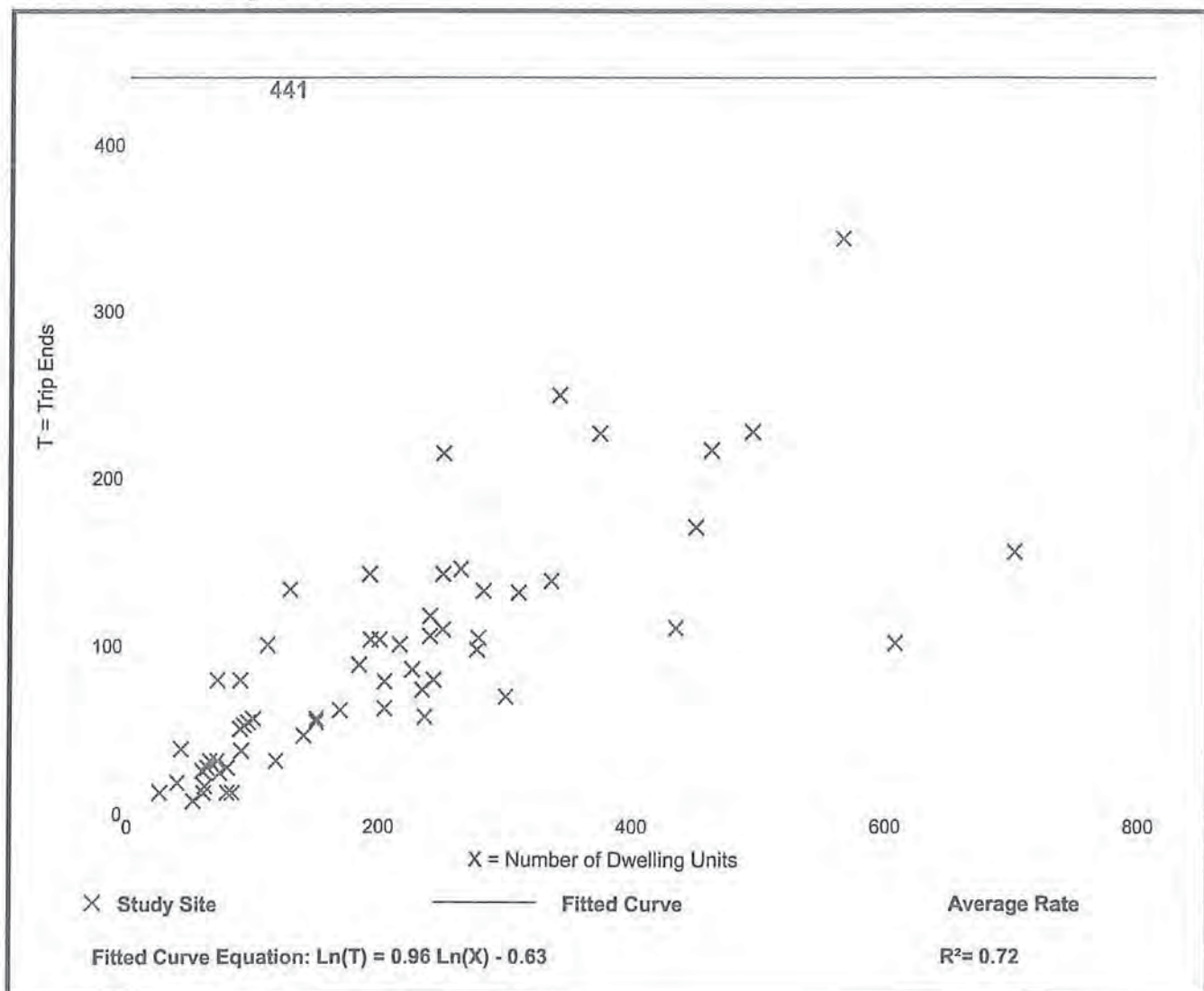
Avg. Num. of Dwelling Units: 208

Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

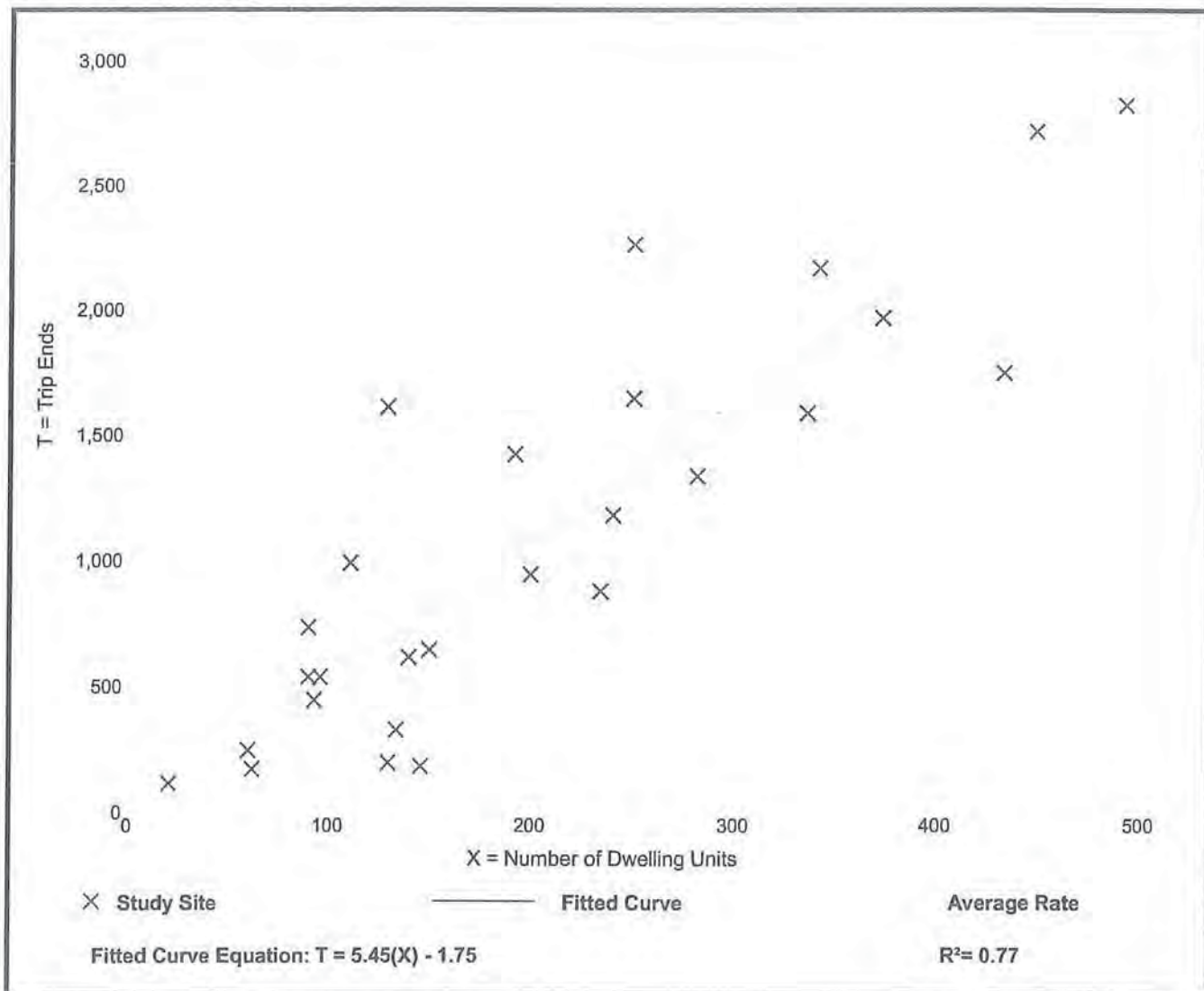
Vehicle Trip Ends vs: Dwelling Units
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 27
Avg. Num. of Dwelling Units: 205
Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 53

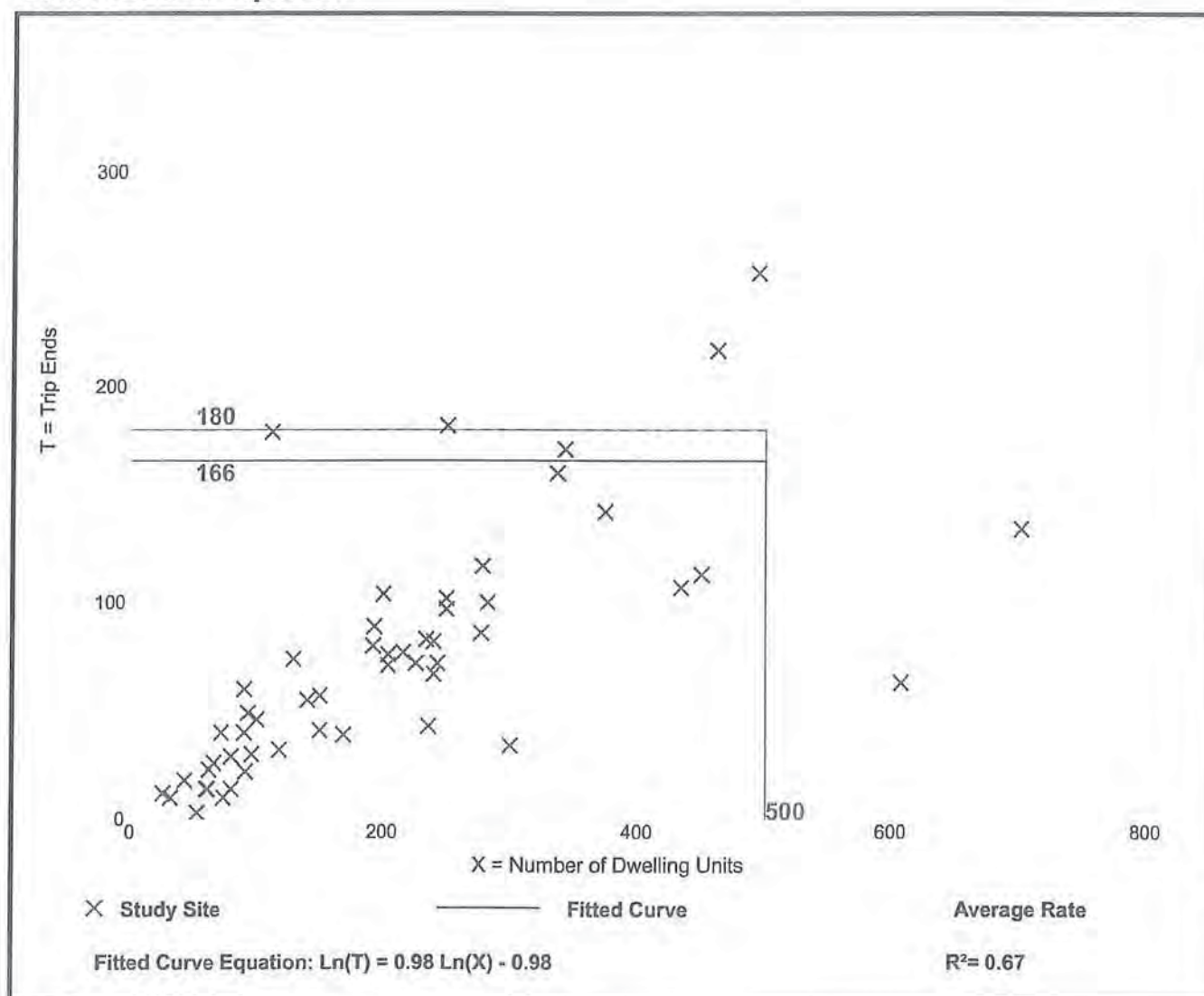
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Directional Distribution: 26% entering, 74% exiting

Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
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Data Plot and Equation



Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 60

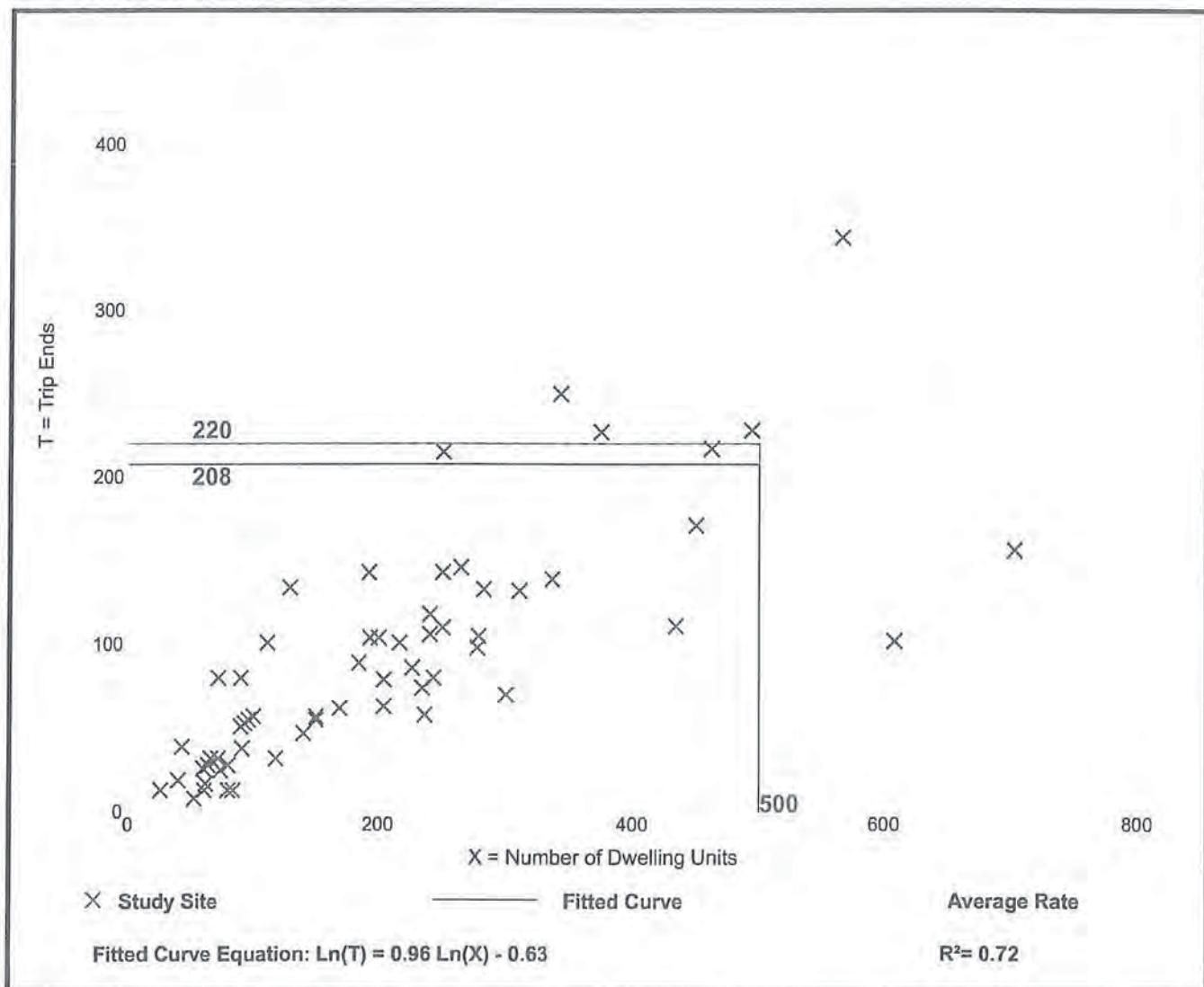
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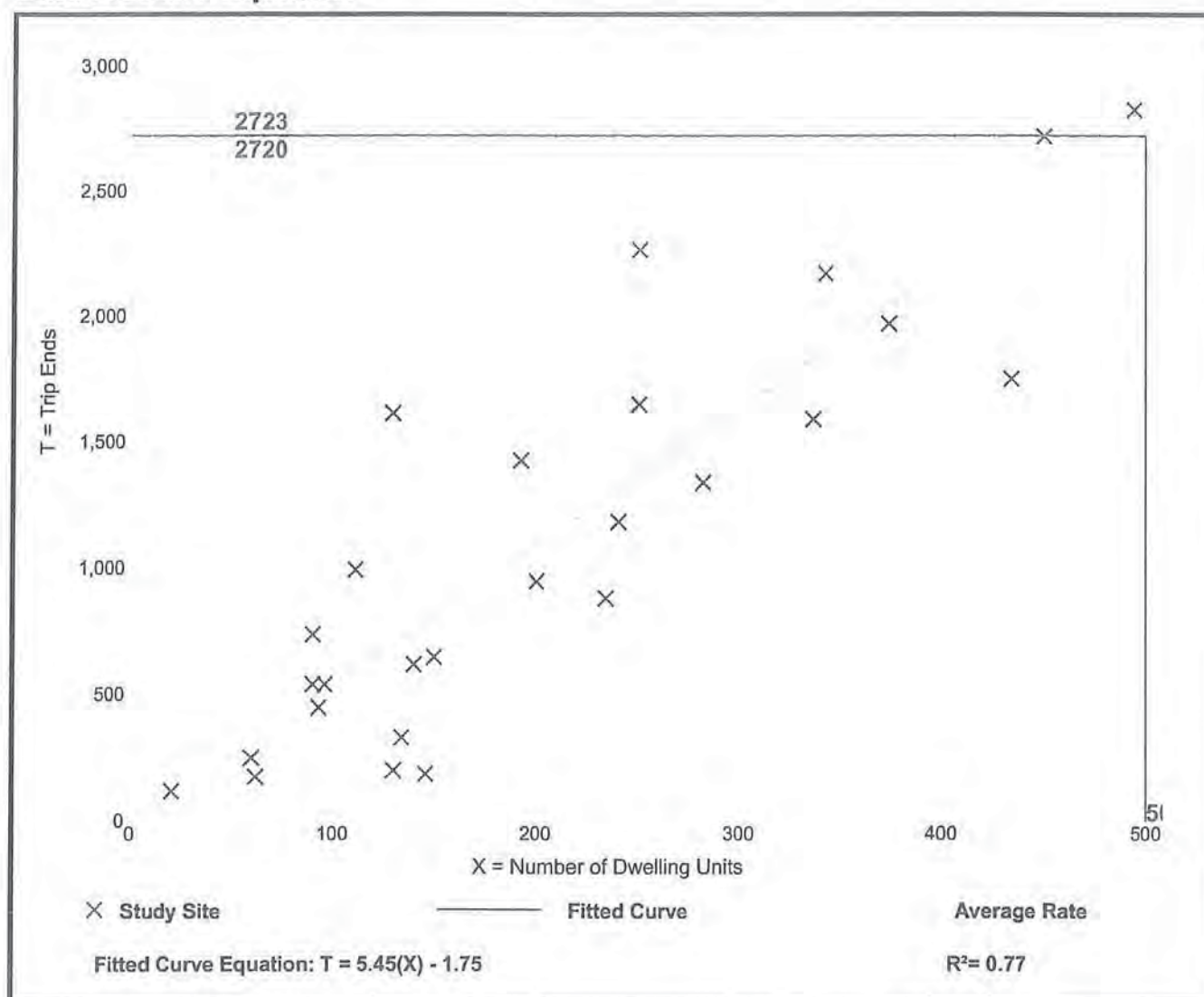
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Setting/Location: General Urban/Suburban
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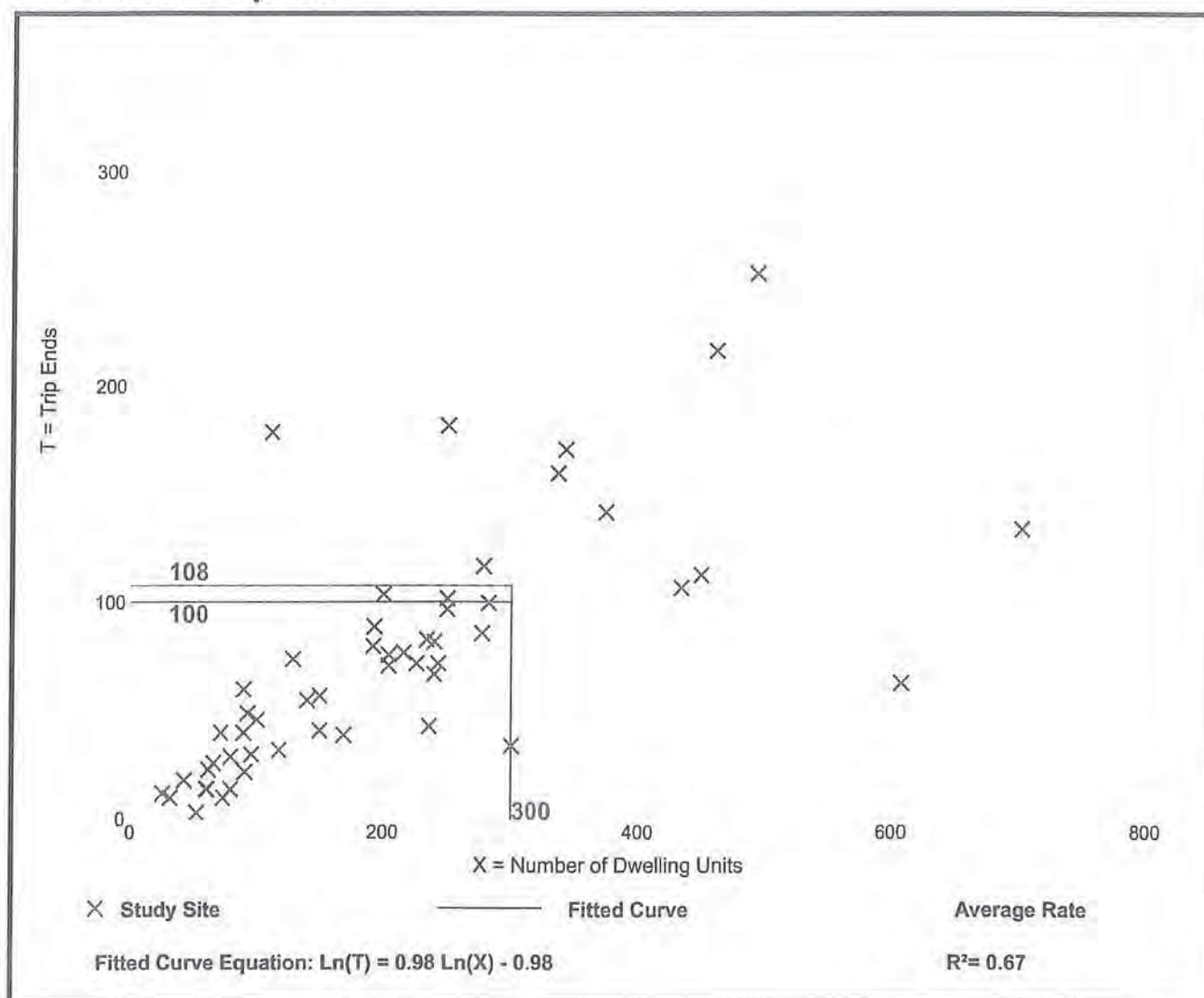
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Data Plot and Equation



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Vehicle Trip Ends vs: Dwelling Units

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Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 60

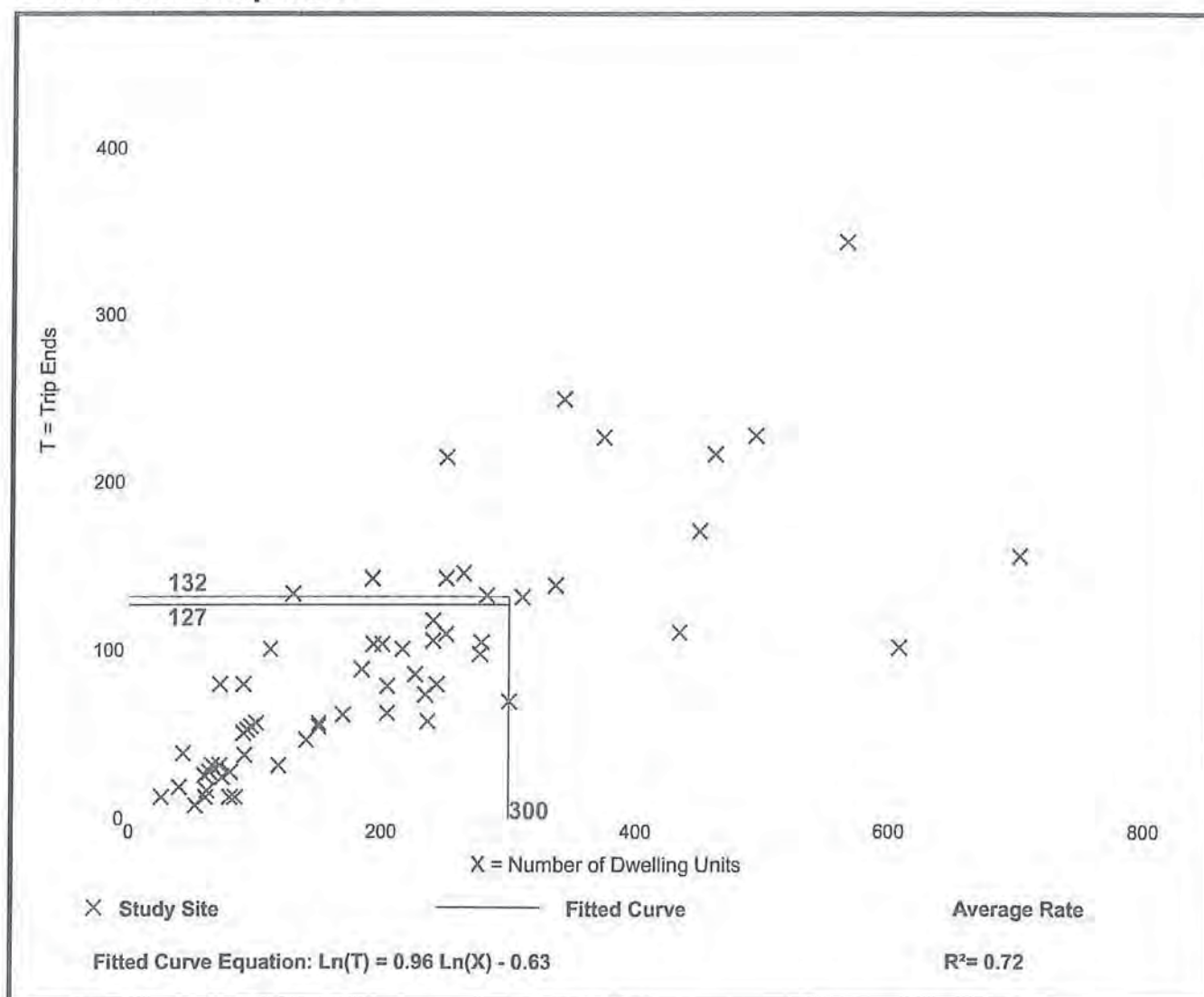
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Directional Distribution: 61% entering, 39% exiting

Vehicle Trip Generation per Dwelling Unit

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Data Plot and Equation



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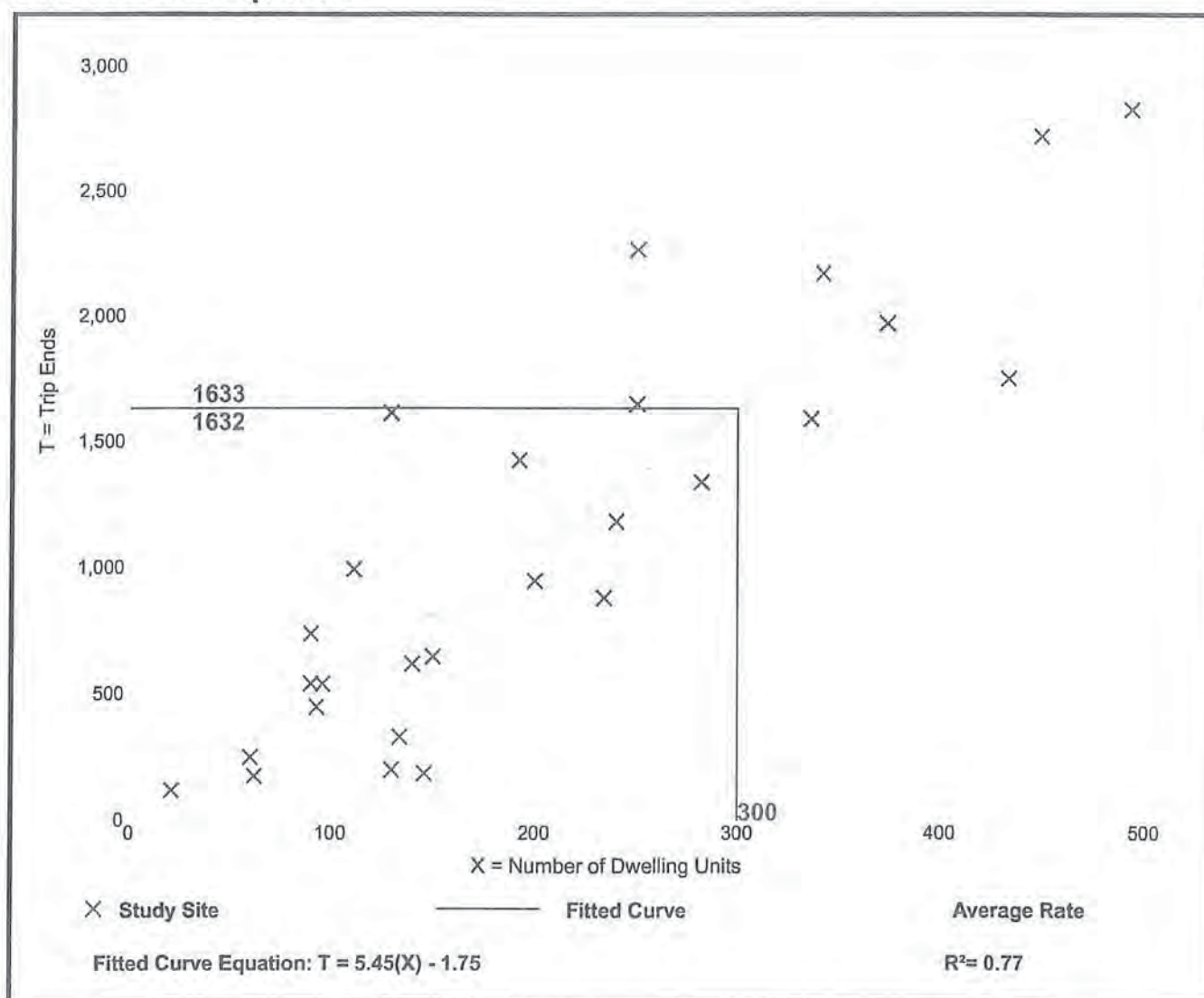
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Setting/Location: General Urban/Suburban
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Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

On a: Weekday,

**Peak Hour of Adjacent Street Traffic,
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Setting/Location: General Urban/Suburban

Number of Studies: 53

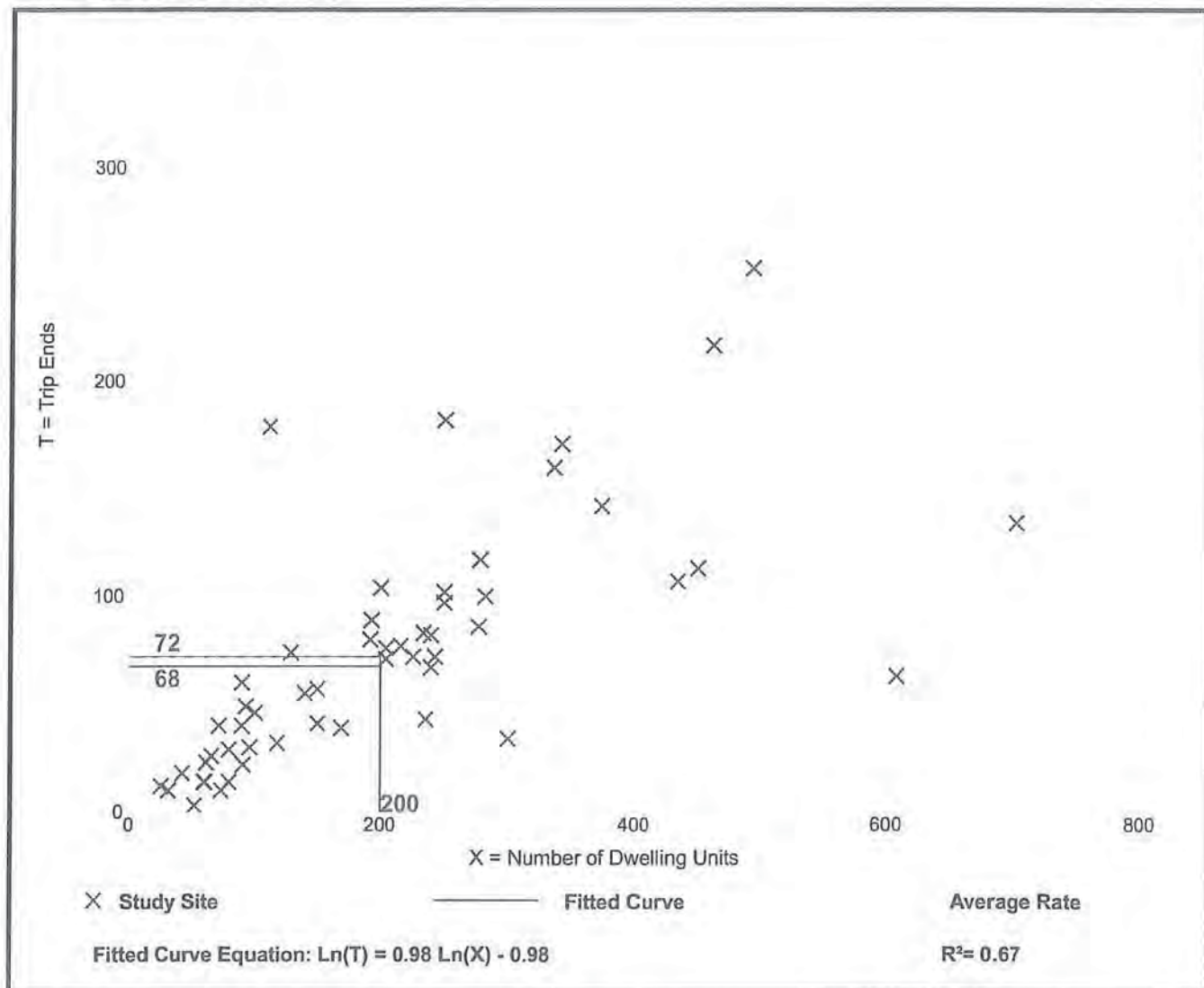
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Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units

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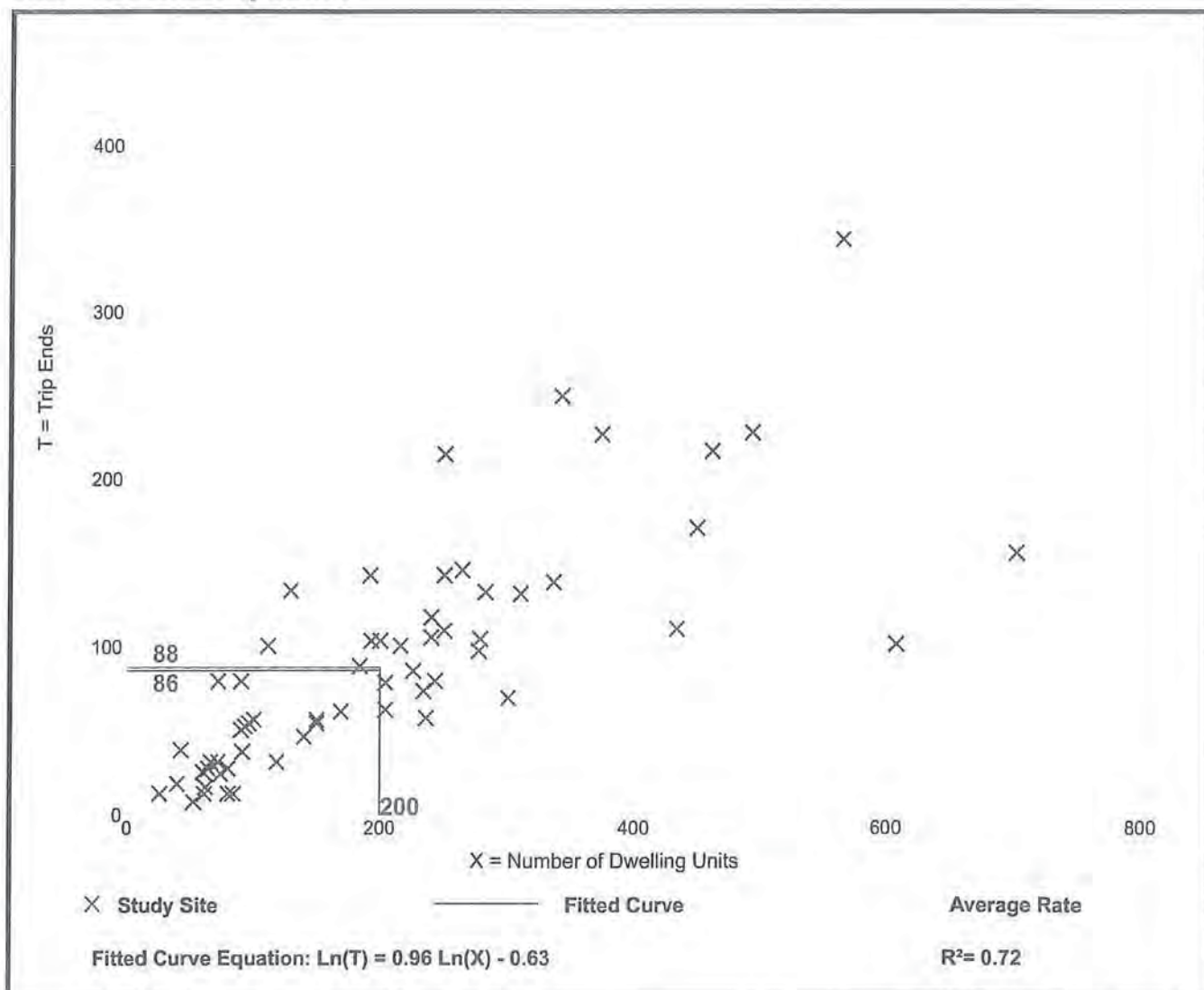
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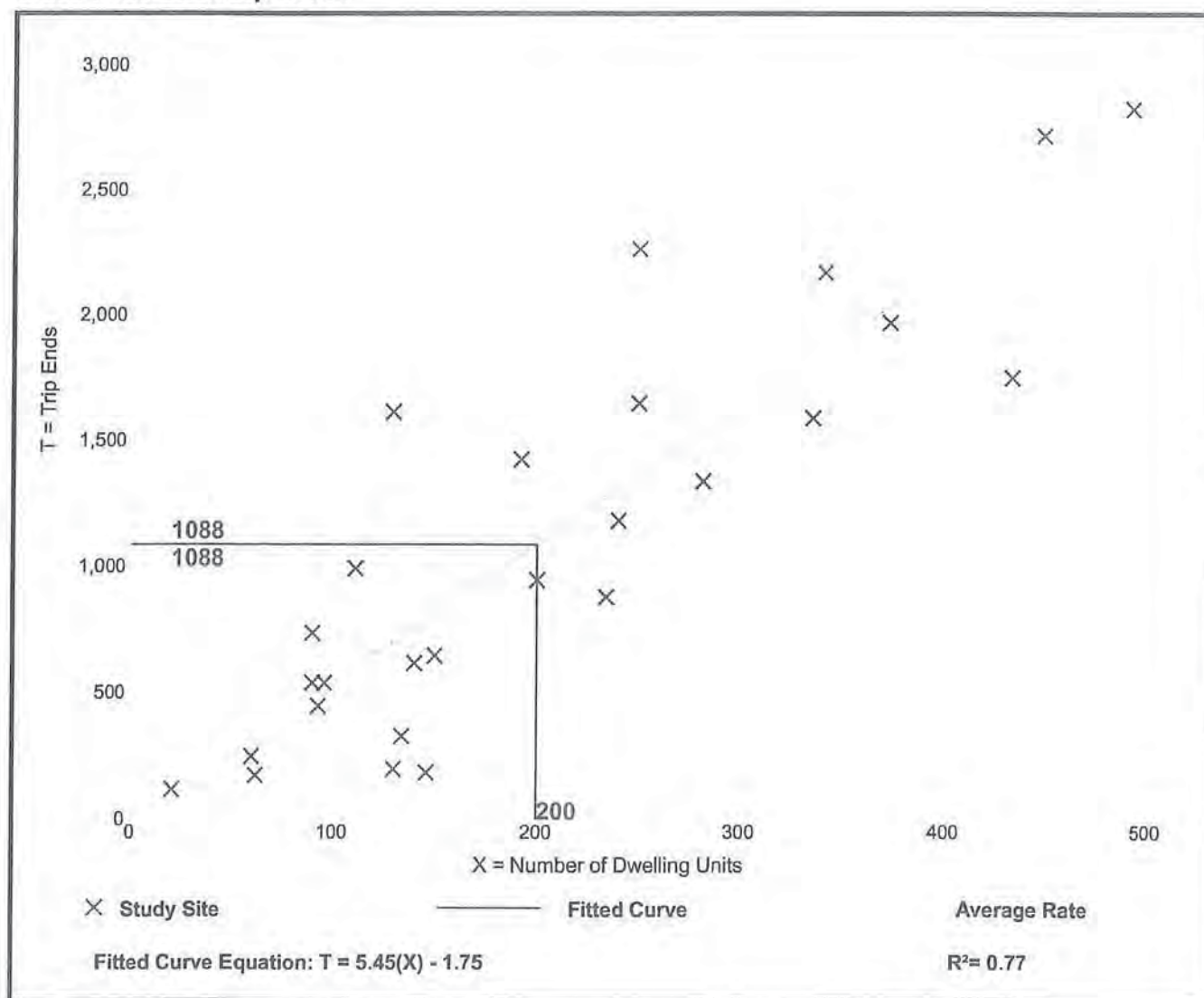
Vehicle Trip Ends vs: Dwelling Units
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Setting/Location: General Urban/Suburban
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Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

Data Plot and Equation



For non-project actions, the references in the checklist to the words "project", "applicant", and "property or site" should be read as "proposal", "proposer", and "affected geographic area", respectively.

A. BACKGROUND

1. Name of proposed project, if applicable:
☒ **Citadel Estates**
2. Name of applicant:
☒ **Jose Chavallo and Tammy Steele-Chavallo**
3. Address and phone number of applicant and contact person:
☒ **106 South Lincoln Street**
4. Date checklist prepared:
☒ **September 14, 2010**
5. Agency requesting checklist:
☒ **City of Kennewick Planning Department**
6. Proposed timing or schedule (including phasing, if applicable):
☒ **First Phase construction to begin spring 2011, Second Phase summer 2012**
7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.
☒ **Yes, lot 38 is being proposed as a commercial development site.**
8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.
☒ **Geotechnical Slope Stability Study within critical slope area will be completed with design of roads and grading plan**
9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.
☒ **Yes, rezoning of the area of lot 38**
10. List any government approvals or permits that will be needed for your proposal, if known.
☒ **City of Kennewick**

11. Give a brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page.

36 lot gated residential subdivision, and 1 commercial tract. Residential lots are approximately 1/3 acre.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

Extension of 25th Avenue and 26th Avenue, west of South Kellogg Street.

B. ENVIRONMENTAL ELEMENTS

1. Earth

- a. General description of the site (circle one): Flat, rolling, HILLY, steep slopes, mountainous, other
- b. What is the steepest slope on the site (approximate percent slope?)

30-40%, however the area to be developed is somewhere in the 20% range or flatter.

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any prime farmland.

**Warden Silt Loam (WdDO and Shano Silt Loam (ShF),
from USDA Soil Survey of Benton County 1971**

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

No

- e. Describe the purpose, type and approximate quantities of any filling or grading proposed. Indicate

Site grading will be required to build street and house pads.

- f. Could erosion occur as a result of clearing, construction or use? If so, generally describe?

Yes, wind and water erosion during construction.

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

✓ **22% roads and Buildings**

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

✓ **Exposed areas will be watered for dust control during construction. Soil erosion fencing will be used as needed. Disturbed areas will be hydro-seeded when construction is completed.**

2. Air

- a. What types of emissions to the air would result from the proposal (i.e., dust, automobile, odors, industrial wood smoke) during construction and when the project is completed? If any, generally describe and give approximate quantities if known.

✓ **Dust during construction.**

- b. Are there any off-site sources of emissions or odor that may effect your proposal? If so, generally describe.

✓ **No**

- c. Proposed measures to reduce or control emissions or other impacts to air, if any:

✓ **Exposed areas will be watered during construction. Disturbed areas will be hydro-seeded when construction is completed.**

3. Water

- a. Surface

- 1.) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

✓ **No**

- 2.) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

✓ **No**

- 3.) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

✓ **None**

4.) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

✓ **No**

5.) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

✓ **No**

6.) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

✓ **No**

b. Ground

1.) Will ground water be withdrawn, or will water be discharged to ground water? Give general description, purpose, and approximate quantities if known.

✓ **No**

2.) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals; agricultural; etc.) Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

✓ **Storm water maybe discharged to the groundwater with the use of dry wells and infiltration trenches.**

c. Water Runoff (including storm water)

1.) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

✓ **Storm water to be collected by catch basins and discharged to dry wells and infiltration trenches.**

2.) Could waste materials enter ground or surface waters? If so, generally describe.

✓ **Yes, through dry wells and infiltration trenches.**

d. Proposed measures to reduce or control surface, ground and runoff water impacts, if any:

✓ **Storm water collection system.**

4. Plants

a. Check or circle the types of vegetation found on the site:

deciduous tree: alder, maple, aspen, other

evergreen tree: fir, cedar, pine, other

✓ **SHRUBS**

grass

pasture

crop or grain

wet soil plants; cattail, buttercup, bulrush, skunk cabbage, other

water plants: water lily, eelgrass, milfoil, other

other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

✓ **All lots will be graded. General shrub grass will be removed within the development.**

c. List threatened or endangered species known to be on or near the site.

✓ **None**

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

✓ **Residential home landscaping.**

5. Animals

a. Circle any birds and animals which have been observed on or near the site or are known to be on or near the site:

✓ birds: hawk, heron, eagle, songbirds, other
mammals: deer, bear, elk, beaver, other
fish: bass, salmon, trout, herring, shellfish, other

NONE

b. List any threatened or endangered species known to be on or near the site.

✓ **None**

c. Is the site part of a migration route? Is so, explain.

✓ **The entire Columbia Basin is part of a mitigation route.**

d. Proposed measures to preserve or enhance wildlife, if any:

✓ **None**

6. Energy and Natural Resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

✓ **Residential homes. Electric heating and lighting. We are discussing with the Cascade Natural Gas to extend service to the area.**

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

✓ **No**

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

✓ **Energy efficient construction of the homes. We would like to investigate small individual home solar and wind energy projects.**

7. Environmental Health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill or hazardous waste, that could occur as a result of this proposal? If so, describe.

✓ **No**

- 1.) Describe special emergency services that might be required.

✓ **Standard City emergency services for residential homes.**

- 2.) Proposed measures to reduce or control environmental health hazards, if any:

✓ **None required.**

- b. Noise

- 1.) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

✓ **None**

- 2.) What types and levels of noise would be created by or associated with the project on short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

✓ **Construction noise for site and home construction.
Proposed work hours 7:00 a.m. to 8:00 p.m.**

3.) Proposed measures to reduce or control noise impacts, if any:

✓ **Short term during construction activities.**

*COMPLY WITH
CITY NOISE
ORDINANCE*

8. Land and Shoreline Use

a. What is the current use of the site and adjacent properties?

✓ **Residential and vacant land.**

b. Has the site been used for agriculture? If so, describe.

✓ **No**

c. Describe any structures on the site.

✓ **None**

d. Will any structures be demolished? If so, what?

✓ **No**

e. What is the current zoning classification of the site?

✓ **Residential Suburban**

f. What is the current comprehensive plan designation of the site?

Low Density Residential, LDR

*OPEN SPACE
AT TOP OF HILL*

g. If applicable, what is the current shoreline master program designation of the site?

✓ **N/A**

h. Has any part of the site been classified as an "environmentally sensitive" area? If so, specify.

✓ **Yes – Critical Slope**

i. Approximately how many people would reside or work in the completed project?

✓ **37 residential homes**

j. Approximately how many people would reside or work in the completed area?

✓ **120**

k. Proposed measures to avoid or reduce displacement impacts, if any:

✓ **N/A**

- i. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

✓ **Meets City Zoning Codes**

9. Housing

- a. Approximately how many units would be provided, if any? Indicate whether high, middle or low-income housing.

✓ **37 High-Income housing**

- b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

✓ **None**

- c. Proposed measures to reduce or control housing impacts, if any:

✓ **N/A**

10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Three story homes, most will be daylight first floor. Wood, stucco, brick, and rock exteriors.

- b. What views in the immediate vicinity would be altered or obstructed?

✓ **None**

- c. Proposed measures to reduce or control aesthetic impacts, if any:

✓ **N/A**

11. Light and Glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

✓ **None**

- b. Could light or glare from the finished project be a safety hazard or interfere with views?

✓ **No**

- c. What existing off-site sources of light or glare may affect your proposal?

✓ **None**

d. Proposed measures to reduce or control light and glare impacts, if any?

✓ None

12. Recreation

a. What designated and informal recreational opportunities are in the immediate vicinity?

✓ City parks and schools.

b. Would the proposed project displace any existing recreational use? If so, describe.

✓ No

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

✓ None

13. Historic and Cultural Preservation

a. Are there any places or objects listed on, or proposed for, national, state or local preservation registers known to be on or next to the site? If so, generally describe.

✓ No

b. Generally describe any landmarks or evidence of historic, archaeological, scientific, or cultural importance known to be on or next to the site.

✓ None

c. Proposed measures to reduce or control impacts, if any:

None

14. Transportation

a. Identify public streets and highways serving the site, and describe proposed access to the existing street system. Show on site plans, if any.

Extension of 25th Avenue and 26th Avenue, west of South Kellogg Street.
Subdivision will be a gated community with no public access.

b. Is site currently served by public transit? If not, what is the approximate distance to the nearest transit stop?

✓ No, approximately one mile.

- c. How many parking spaces would the completed project have? How many would the project eliminate?

✓ **None eliminated. Minimum 2 per lot, $37 \times 2 = 74$ residential parking.**

- d. Will the proposal require any new roads or streets, or improvements to existing roads or streets, not including driveways? If so, generally describe (indicate whether public or private).

**Extension of 25th Avenue and 26th Avenue, west of South Kellogg Street.
Subdivision will be a gated community with no public access.**

- e. Will the project use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

✓ **No**

- f. How many vehicular trips per day would be generated by the completed project? If known, indicate when peak volumes would occur.

✓ **4.5 per residential site. Approximately 170 trips.**

- g. Proposed measures to reduce or control transportation impacts, if any:

✓ **None**

15. Public Services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, health care, schools, other)? If so, generally describe.

✓ **City services will be required to meet the needs of the new 37 lot residential development.**

- b. Proposed measures to reduce or control direct impacts on public services, if any.

✓ **None**

16. Utilities

- a. Circle utilities currently available at the site: **electricity, natural gas, water, refuse service,**
✓ **telephone, sanitary sewer, septic system, other.**

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Electricity - Benton PUD
Water & Sanitary Sewer - City of Kennewick
Telephone - Verizon
Refuse - Waste Management
Gas - Cascade Natural Gas

C. SIGNATURE

The above answers are true and complete to the best of my knowledge. I understand that the City is relying on them to make its decisions.

Signature J. A. Duvall Telephone Number: (509) 539-1067

Date Submitted: 10/15/10

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

✓ **Increased impervious area will increase storm drainage discharge.**

Proposed measures to avoid or reduce such increases are:

✓ **On-site collection system with dry wells and infiltration trenches.**

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

✓ **None**

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

✓ **None**

3. How would the proposal be likely to deplete energy or natural resources?

✓ **Development of 37 new residential homes.**

Proposed measures to protect or conserve energy and natural resources are:

✓ **Homes to have energy efficient construction.**

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains or prime farmlands?

✓ **N/A**

Proposed measures to protect such resources or to avoid or reduce impacts are:

✓ **None**

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

✓ **N/A**

Proposed measures to avoid or reduce shoreline and land use impacts are:

/ **None**

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

/ **Development of 37 new residential homes.**

Proposed measures to reduce or respond to such demand(s) are:

/ **Construction of associated streets and utilities.**

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

/ **None**

ESA LISTED SALMONIDS CHECKLIST

The Listed Salmonids Checklist is provided in order that the City can identify a project's potential impacts (if any) on salmonids that have been listed as "threatened" or "endangered" under the Federal Endangered Species Act (ESA). A salmonid is any fish species that spends part of its life cycle in the ocean and returns to fresh water. Potential project impacts that may result in a "taking" of listed salmonids must be avoided, or mitigated to insignificant levels. Generally, under ESA, a "taking" is broadly defined as any action that causes the death of, or harm to, the listed species. Such actions include those that affect the environment in ways that interfere with or reduce the level of reproduction of the species.

If ESA listed species are present or ever were present in the watershed where your project will be located, your project has the potential for affecting them, and you need to comply with the ESA. The questions in this section will help determine if the ESA listing will impact your project. The Fish Program Manager at the appropriate Department of Fish and Wildlife (DFW) regional office can provide additional information. Please contact the Dept. of Fish and Wildlife at 1701 S. 24th, Yakima WA 98902-5720, Phone No. 509-575-2740.

1. Are ESA listed salmonids currently present in the watershed in which your project will be?

✓ YES X NO

Please Describe.

Columbia River Watershed

2. Has there ever been an ESA listed salmonid stock present in this watershed?

✓ YES X NO

Please Describe.

Columbia River Watershed

NOTE: Kennewick is located in the upper Mid-Columbia watershed. Salmonoids are present in the watershed - questions no. 1 and no. 2 already answered "yes". Questions A-1 and A-2 are also answered.

PROJECT SPECIFIC: The questions in this section are specific to the project and vicinity.

✓A1. Name of watershed: Upper Mid-Columbia

✓A2. Name of nearest waterbody: Columbia River

✓A3. What is the distance from this project to the nearest body of water? Four miles

Often a buffer between the project and a stream can reduce the chance of a negative impact to fish.

✓A4. What is the current land use between the project and the potentially affected water body (parking lots, farmland, etc.)

Residential and Commercial Developments

A5. What percentage of the project will be impervious surface (including pavement & roof area)?

22% roads and buildings

FISH MIGRATION: The following questions will help determine if this project could interfere with migration of adult and juvenile fish. Both increases and decreases in water flows can affect fish migration.

B1. Does the project require the withdrawal of

✓ a. Surface water? Yes ___ No X

Amount _____

Name of surface water body _____

✓ b. Ground water? Yes ___ No X

Amount _____

From Where _____

Depth of well _____

✓ B2. Will any water be rerouted? YES ___ NO X

If yes, will this require a channel change?

B3. Will there be retention ponds? YES ___ NO X

If yes, will this be an infiltration pond or a surface discharge to either a municipal storm water system or a surface water body?

*YES - INFILTRATION POND
WILL BE USED*

If to a surface water discharge, please give the name of the waterbody.

B4. Will this project require the building of new roads? (Increased road mileage may affect the timing of water reaching a stream and may, thus, impact fish habitat.)

Yes

✓ B5. Are culverts proposed as part of this project? Yes ___ No X

✓ B6. Are stormwater drywells proposed as part of this project? Yes X No ___

✓ B7. Will topography changes affect the duration/direction of runoff flows? Yes X No ___

If yes describe the changes.

Reduction of slopes will increase duration time, thus reduced runoff impact.

✓ B8. Will the project involve any reduction of a floodway or floodplain by filling or other partial blockage of flows? Yes ___ No X

If yes, how will the loss of flood storage be mitigated by your project?

WATER QUALITY: The following questions will help determine if this project could adversely impact water quality. Degraded water quality can affect listed species. Water quality can be made worse by runoff from impervious surfaces, altering water temperature, discharging contaminants, etc.

- C1. Will your project either reduce or increase shade along or over a waterbody?
☒ YES ☒ NO **X** (Removal of shading vegetation or the building of structures such as docks or floats often result in a change in shade.)
- C2. Will the project increase nutrient loading or have the potential to increase nutrient loading or contaminants (fertilizers, other waste discharges, or runoff) to the waterbody?
☒ YES ☒ NO **X**
- C3. Will turbidity (dissolved or partially dissolved sediment load) be increased because of construction of the project or during operation of the project? (In-water or near water work will often increase turbidity.)
☒ YES ☒ NO **X**
- C4. Will your project require long term maintenance, i.e., bridge cleaning, highway salting, chemical sprays for vegetation management, clearing of parking lots?
☒ YES ☒ NO **X**

Please Describe.

Vegetation: The following questions are designed to determine if the project will affect riparian vegetation, which can impact listed species.

- D1. Will the project involve the removal of any vegetation from the stream banks?
☒ YES ☒ NO **X**

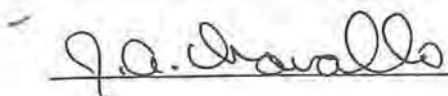
If yes, please describe the existing conditions and the amount and type of vegetation to be removed.

- D2. If any vegetation is removed, do you plan to re-plant? YES **X** NO ☒

If yes, what types of plants will you use? **Residential landscaping**

E. SIGNATURE

The above answers are true and complete to the best of my knowledge. I understand the City is relying on them to make its decision.



10/15/10
Date



March 21, 2011

Jose & Tammy Chavallo
102 N. Lincoln Street
Kennewick, WA 99336

Dear Mr. & Mrs. Chavallo:

Enclosed is a Mitigated Determination of Non-Significance #10-56 for PP #10-02/PDP 10-01 located at 6100 W. 25th Avenue, west of the Panoramic Heights neighborhood. This Determination means no Environmental Impact Statement is required in order for the City to continue the processing of your application.

Please notice that several changes have been made to your Environmental Checklist. Three conditions have been added. The City of Kennewick has determined that as mitigated, this proposal will not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed Environmental Checklist, and will be available to the public on request.

If you should have any questions, please feel free to contact me.

Sincerely,


Gregory McCormick
Planning Director

Enclosure:

cc: Dept of Ecology
Dept. of Fish & Wildlife – Perry Harvester, 1701 S. 24th Ave., Yakima, WA 98902
Dept. of Fish & Wildlife – Mark Teske, 201 N Pearl, Ellensburg, WA 98926
Yakima Nation, 815 Sanford Ave., Richland, WA 99352
CTUIR – Carey Miller, P O Box 638, Pendleton, OR 97801
SEPA File
PDP 10-01/PP 10-02 File

ED# 10-56

**CITY OF KENNEWICK
MITIGATED DETERMINATION OF NON-SIGNIFICANCE**

Description of Proposal: Planned Development Permit /Preliminary Plat for 38 lot gated community for a single family residential subdivision.

Proponent: Jose & Tammy Chavallo

Location of proposal, including street address, if any, 6100 W. 25th Avenue, extension of W. 25th Avenue and W. 26th Avenue, west of the Panoramic Heights neighborhood.

Lead Agency: City of Kennewick

Mitigation Required for Potentially Significant Adverse Impacts: According to KMC 18.42.040(1), the City may impose any condition necessary to protect the health, safety, and welfare or otherwise bring a proposed development into compliance with the purpose and intent of this Title.

For this proposal, PP No.10-02/PLN-2010-04492 & PDP No. 10-01/PLN-2010-04491, conditions include the mitigation fee for the impacts of a 38 lot subdivision on park zone 6W (Southridge) in the amount of \$20,691.72 in lieu of dedication of park land. Conditions also include traffic mitigation fees for a maximum amount of \$20,691.72 and are to be collected as a percentage of lots in each phase of development.

For this proposal, PP No. 10-02/PLN-2010-04492 & PDP No.10-01/PLN-2010-04491 conditions include mitigation for traffic impacts on the existing Panoramic Heights neighborhood in the amount of \$20,000 for traffic calming devices.

 X This Mitigated DNS is issued under 197-11-340(2). The City will not act on this proposal for fifteen (15) days from the date below. Comments must be submitted by 4/5/11.
After the review period has elapsed, all comments received will be evaluated and the DNS will be retained, modified, or withdrawn as required by SEPA regulations.

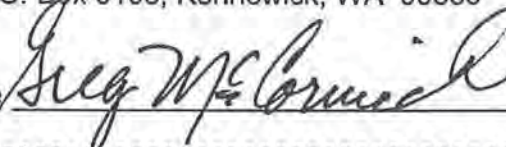
 X Changes, modifications and /or additions to the checklist have been made on the attached Environmental Checklist Review.

 X This MDNS is subject to the attached conditions.

Responsible Official: Gregory McCormick
Position/Title: Planning Director
Address: 210 West 6th Ave., P. O. Box 6108, Kennewick, WA 99336
Phone: (509) 585-4473

 3/21/11

Signature



.....
According to KMC 4.08.430, this determination may be appealed to:

Hearing Examiner
City of Kennewick
210 W 6th Ave., P.O. Box 6108

ED# 10-56

The time for appealing SEPA issues is twenty-one (21) days after notice (WAC 197-11-680(5)(a)). You should be prepared to make specific, written factual objections. Contact Gregory McCormick to read or request the procedures for SEPA appeals.

**CITY OF KENNEWICK
ENVIRONMENTAL CHECKLIST REVIEW**

E.D. File # 10 - 56**Review by: Wes Romine****Action: PP #10-02/PDP #10-01****Date: March 21, 2011**

The City of Kennewick has reviewed the checklist and has made changes on it.

Please note the following condition(s):

1.) Parks

For this proposal, PP 10-02/PLN-2010-04492 & PDP 10-01/PLN-2010-04491, conditions include the mitigation fees for impacts for the addition of 38 single-family dwelling units in Park Planning Zone 6W – Southridge. In lieu of land dedication fees are required to be paid to Park Planning Zone 6W – Southridge in the amount of \$20,691.72 as calculated per the City's Park Fee Determination Process form. This fee must be paid as a percentage of the total amount based on the number of units to be built in each phase of development.

2.) Traffic

For PP 10-02/PLN-2010-04492 & PDP 10-01/PLN-2010-04491, The developer will be required to provide a \$20,000 letter of credit to the City to be drawn upon by the HOA for their installation of up to 4 speed humps per city standard or a single traffic circle per city standard. These traffic calming measures will be constructed within the existing Panoramic Heights neighborhood specifically located to mitigate the impacts of increased traffic caused by the proposed development. Location and selection of traffic calming measures will be made by the Panoramic Heights Homeowner's Association (HOA) in accordance with KAC 13-40 and subject to the approval by the City. If said mitigation measures are not started within 5-years of final plat approval; the developer will be released of this obligation. These comments apply to the initial 38 lots proposed. Additional structures or development or any change of use that increases the traffic by more than 20 percent may require additional mitigation measures. The baseline for comparison is 364 daily trips or 38 PM peak hour trips. The measures are required as a result of long tangent sections (greater than 1000 feet) within the proposed development and between the development and the first collector/arterial access point.

3.) Water Quality

An NPDES Construction Stormwater General Permit from the Washington State Department of Ecology is required if there is a potential for stormwater discharge from a construction site with more than one acre of disturbed ground. This permit requires that the SEPA checklist fully disclose anticipated activities including building, road construction and utility placements. Obtaining a permit is a minimum of a 38 day

process and may take up to 60 days if the original SEPA does not disclose proposed activities.

Erosion control measure must be in place prior to any clearing, grading or construction. These control measures must be able to prevent soil from being carried into surface water (this includes storm drains) by stormwater runoff. A stormwater construction permit from Washington State Dept. of Ecology may be required if there is a potential for discharge from a **construction site larger than one acre**. A Stormwater Pollution Prevention Plan (Erosion Sediment Control Plan) is needed for all permitted construction sites. Please contact Ray Latham, at the Dept. of Ecology, (509) 575-2807, to determine if your site needs such a permit.

Water Resources

The water purveyor is responsible for ensuring that the proposed use(s) are within the limitations of its water rights. If the proposal's actions are different than the existing water right (source, purpose, the place of use, or period of use), then it is subject to approval from the Department of Ecology pursuant to Sections 90.03.380 RCW and 90.44.100 RCW.

If you have any questions concerning the Water Resources comments, please contact Breean Zimmerman at (509) 454-7647.

A. BACKGROUND**Evaluation for Agency Use Only****1. Name of proposed project, if applicable:**

2020 Comprehensive Plan Amendment
Amend Land Use Designation from Low Density Residential to
High Density Residential

2. Name of applicant:

Jose Chavallo and Tammy Steele-Chavallo

3. Address and phone number of applicant and contact person:

5927 W Quinault Avenue, Kennewick, WA
Jose Chavallo – 509-539-1067

4. Date checklist prepared:

The Environmental Checklist supplements, amends, clarifies and modifies previously submitted Environmental Checklists and materials filed herein. This checklist is filed at the request of City of Kennewick on December 30, 2020.

5. Agency requesting checklist:

City of Kennewick Planning Department

6. Proposed timing or schedule (including phasing, if applicable):

The proposed comprehensive plan amendment is a nonproject action. For purposes of land use and environmental review, this checklist includes a hypothetical possible project of an integrated boutique hotel (60 rooms) with a 500 unit condominium development. This checklist also includes calculations and information on comparative development buildout under the Low Density Residential and High Density Residential land use designations.

The Comprehensive Plan amendment is part of a phased review under WAC 197-11-060(5)(c). Future rezone and site-specific project application shall be combined in subsequent project review.

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Future plans include possible rezone and site-specific project. This checklist includes possible future development comprised of a 60-unit boutique hotel and 500-unit condominium/multi-family project. Applicant specifically conditions further activity on contemporaneous submission, review and processing of any rezone request with a project specific application.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

- Geotechnical Investigation for Panoramic Heights

May 2014

- August 21, 2007 (Attachment A)
- Critical Areas Report for Citadel Estates, July 1, 2009 (Attachment B)
- Addendum to Critical Areas Report for Citadel Estates, October 15, 2010 (Attachment C)
- Environmental Checklist – Citadel Estates, October 15, 2010 (Attachment D)
- Mitigated Determination of Non-Significance #10 – 56 for PP # 10-02/PDP 10-01 (Attachment E)
- Sunburst Engineering Traffic and Trip Generation analysis dated December 22, 2020. (Attachment F)
- Harms Engineering, Inc. Stormwater impact analysis dated December 14, 2020. (Attachment G)

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

Yes, Citadel Estates Preliminary Plat.

10. List any government approvals or permits that will be needed for your proposal, if known.

There is no current contemplated project. At time of project application, however, a project would require rezone application and project specific applications including land use permits (KMC 18.12.010 A.1 and KMC Ch. 18.42), Critical area review (KMC 18.58.130), critical area report – geologically hazardous area (KMC 18.58.150 and KMC Ch. 18.62), building permit and grading permits. (KMC 18.72.020).

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.

Applicant proposes an amendment to the Kennewick Comprehensive Plan from Low Density Residential (minimum 7500 square foot lots) to High Density Residential (up to 27 units per acre). The proposal includes four (4) parcels totaling 40.6 acres. Tax Parcel ID #1-0889-400-0004-000 is currently developed with a single-family residence. The uses considered as part of the environmental submission include single family residences, multi-family/high density build-out (up to 27 units per acre) and hotel. Also included is possible mixed-use project including 60-unit boutique hotel and 500 unit condominium project.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

Tax Parcel ID # 1-0889-400-0004-000
Tax Parcel ID # 1-0889-301-3081-0001
Citadel Estates Lots 37 & 38 of Preliminary Plat

Evaluation for Agency Use Only

May 2014

B. ENVIRONMENTAL ELEMENTS**Evaluation for Agency Use Only****1. Earth**

- a. General description of the site
(check one): ☐ Flat, ☐ rolling, ☐ hilly, ☐ steep slopes,
mountainous, ☒ other – Mixed Terrain

The site proposed for redesignation includes mixed terrain. Portions of the proposed redesignation area includes steep slopes (Lot 37 of Citadel Estates) while remaining portions include flat and rolling areas.

- b. What is the steepest slope on the site (approximate percent slope)?

A portion of Lot 37 of Citadel Estates includes slopes exceeding 40%.

- c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

Warden Silt Loam (WdDO) and Shano Silt Loam (ShF), from USDA Soil Survey of Benton County, 1971.

- d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.
No.

- e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

There is no current or proposed project for the site. Any area of filling, excavation and/or grading will be identified through a site plan application at time of rezone and project permit application and subject to environmental, critical area and grading review permit processes including critical area report (KMC 18.62.060), general and specific performance standards (KMC 18.62.080 and .090), grading review and approval process (KMC 18.72.020).

- f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

No erosion will occur as a result of the land use redesignation. At time of rezone and project application, a proposed project may call for erosion control measures to be in place prior to any clearing, grading or construction. Those control measures would be sufficient to prevent soil from becoming carried into surface water (including storm drains) by stormwater runoff. A stormwater construction permit from Washington State Department of Ecology may be required if there is a potential for discharge from a construction larger than one acre. A Stormwater Pollution Prevention Plan (Erosion Sediment Control Plan) may also be required for project development.

- g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

Harms Engineering calculated worst case impervious coverage for RS, RL and RH zoning districts. *Attachment G*. The estimated increase in impervious coverage potential by reason of the change in land use designation during a 25-year 24-hour storm is 21,900 cf or 17%. Stormwater facilities would be sized based on City of Kennewick stormwater design standards. Any use would be subject to Residential Site Development Standards (KMC 18.12.010 A.2). Any multi-family developments would be further subject to open space calculations set forth in KMC 18.12.010 A.2 fn.(3)(a). Similar standards apply to townhouse/row house development (KMC 18.12.010 A.2 fn.(6)). Geologically hazardous areas will have limited, if any, development potential under either Low Density Residential or High Density Residential land use designation. KMC 18.62.050.

- h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

At time of project development, exposed areas would be watered for dust control, erosion fencing placed on site, and disturbed areas hydro-seeded when construction is completed. Necessary stormwater construction permits from Washington State Department of Ecology, together with Stormwater Pollution Prevention Plan (Erosion Sediment Control Plan) shall be provided as required by applicable law or regulation. Stormwater will be retained on site with systems designed in accordance with City of Kennewick stormwater design standards.

2. Air

- a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

No project is currently planned for the project site. During project construction, dust emissions are possible. Compliant permits will be issued as necessary or required.

- b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

No.

- c. Proposed measures to reduce or control emissions or other impacts to air, if any:

No measures imposed for comprehensive plan amendment.

3. Water

- a. Surface Water:

- 1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

No.

Evaluation for Agency Use Only

- 2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

No.

- 3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

None.

- 4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No.

- 5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

No.

- 6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No.

b. Ground Water:

- 1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No.

- 2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

None.

y 2014

c. Water runoff (including stormwater):

- 1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

See attached report from Harms Engineering. (Attachment G). At time of rezone and project review, the specific project will include assessment of stormwater runoff calculations, mitigation and disposal standards. Stormwater runoff will be retained on-site and managed through the use of a comprehensive regional stormwater discharge system. No stormwater will leave the project site or flow into other waters. Project development performance standards shall also include compliance with specific hazards standards for erosion and the landslide areas (KMC 18.62.090(1)) and mitigation plan requirements (KMC 18.58.180).

The redesignation of the property from Low Density Residential to High Density Residential has the hypothetical potential to increase impervious coverage. Increased stormwater calculations for the comparative land use designations are set forth in Harms Engineering report.

- 2) Could waste materials enter ground or surface waters? If so, generally describe.

No.

- 3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

No.

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

No project is currently planned for the project site. A future development will require a comprehensive design of a stormwater collection and discharge system meeting state and local development standards.

4. Plants

a. Check the types of vegetation found on the site:

___ deciduous tree: alder, maple, aspen, other

___ evergreen tree: fir, cedar, pine, other

X shrubs

X grass

___ pasture

___ crop or grain

___ Orchards, vineyards or other permanent crops

___ wet soil plants: cattail, buttercup, bulrush, skunk,

Cabbage, other

___ water plants: water lily, eelgrass, milfoil, other

___ other types of vegetation

b. What kind and amount of ☐ vegetation will be removed or altered?

The kind and amount of vegetation removal will be reviewed at time of rezone and project application. The redesignation of the property from Low Density Residential to High Density Residential does not change the area subject to future development. either land use designation will result in the same potential development impacts to vegetation.

c. List threatened and endangered species known to be on or near the site.

None known.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

None.

e. List all noxious weeds and invasive species known to be on or near the site.

None.

5. Animals

a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site. Examples include:

birds: ☒ hawk ☐ heron ☐ eagle ☐ songbirds
☐ other:
mammals: ☐ deer ☐ bear ☐ elk beaver ☐
 other: ☐
fish: ☐ bass ☐ salmon ☐ trout ☐ herring ☐ shellfish
 other ☐

b. List any threatened and endangered species known to be on or near the site.

None known.

c. Is the site part of a migration route? If so, explain.

The entire Columbia Basin is part of the migration route.

May 2014

- d. Proposed measures to preserve or enhance wildlife, if any:

None

- e. List any invasive animal species known to be on or near the site.

Rattle Snakes

6. Energy and natural resources

- a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

Electric, natural gas and possibly solar at time of development.
Energy sources will be used to meet heating, air conditioning, lighting and other service needs.

- b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

Land use redesignation would not affect potential use of solar energy on adjacent properties.

- c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

Any development will consider all available energy conservation options including insulation, windows, and solar energy facilities.

7. Environmental health

- a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste that could occur as a result of this proposal? If so, describe.

No.

May 2014

- 1) Describe any known or possible contamination at the site from present or past uses.

None

- 2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

None

- 3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

None

- 4) Describe special emergency services that might be required.

None

- 5) Proposed measures to reduce or control environmental health hazards, if any:

None

b. Noise

- 1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

None

- 2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

At time of project construction, construction noise would be generated for site and home construction. Contemplated work hours would be 7:00 a.m. to 8:00 p.m. All land use would be subject to the maximum permissible environmental noise levels established by KMC 9.52.060. Noise impacts would be comparable for Low Density Residential and High Density Residential land use designations.

May 2014

- 3) Proposed measures to reduce or control noise impacts, if any:

None

8. Land and shoreline use

- a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

Residential and vacant land.

- b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or non-forest use?

No

- 1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

No

- c. Describe any structures on the site.

One single family residential home.

- d. Will any structures be demolished? If so, what?

No.

- e. What is the current zoning classification of the site?

Residential Low and Residential Suburban

May 2014

- f. What is the current comprehensive plan designation of the site?

Low Density Residential.

- g. If applicable, what is the current shoreline master program designation of the site?

N/A.

- h. Has any part of the site been classified as a critical area by the city or county? If so, specify.

Portions of Lot 37 have been identified as critical areas. In 2007, an on-site survey was made to determine the actual boundary of the slope area greater than 40%. This area totaled 6.1 acres within Lot 37 of Citadel Estates. Additional Information pertinent to such determinations can be found in the Critical Areas Report for Citadel Estates (July 1, 2009).

- i. Approximately how many people would reside or work in the completed project?

Unknown at this time.

- j. Approximately how many people would the completed project displace?

None.

- k. Proposed measures to avoid or reduce displacement impacts, if any:

N/A.

- l. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

Comply with City of Kennewick adopted zoning, development and Environmental regulations.

- m. Proposed measures to ensure the proposal is compatible with nearby agricultural and forest lands of long-term commercial significance, if any:

Amendment of Comprehensive Plan land use designation does not result in interfacing, conversion, or adverse impact to agricultural or forest lands of long-term commercial significance.

9. Housing

- a. Approximately how many units would be provided, if any?
Indicate whether high, middle, or low-income housing.

No project is planned at this time. Site development would be encumbered by site considerations such as topography, roads, parking, open spaces, utilities, development standards and other considerations. Land use designation as high density would increase potential site density based on zoning code. KMC 18.12.020 A.2. The maximum site density under Residential High land use designation would be 1096 dwelling units. Kennewick Comprehensive Plan indicates a high density housing shortage of 159 acres. *Kennewick Comprehensive Plan Table 17*. Setbacks and frontage minimums are the same for low and high density residential. *ID*. Trip generation for multi-family is lower than single family residential. ITE Trip Generation Manual (Land Use 210 and 220). The mix of units will depend on market demand, conditions and project specific development requirements.

- b. Approximately how many units, if any, would be eliminated?
Indicate whether high, middle, or low-income housing.

None.

- c. Proposed measures to reduce or control housing impacts, if any:
N/A.

10. Aesthetics

- a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Maximum height within RH zoning district is forty-five (45) feet. KMC 18.12.010 A2. Maximum height in RL zoning district is thirty-five (35) feet. Exterior building material will be determined at time of project application.

- b. What views in the immediate vicinity would be altered or obstructed? None.

- c. Proposed measures to reduce or control aesthetic impacts, if any:
N/A.

11. Light and glare

- a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

No project is currently planned for the project site. Light and glare from high density residential will be similar to single-family residential development. Light and glare will be generated by street lights, parking area lighting and residential land uses. All lighting will comply with KMC 18.39.030.

- b. Could light or glare from the finished project be a safety hazard or interfere with views?
No.

- c. What existing off-site sources of light or glare may affect your proposal?

None.

- d. Proposed measures to reduce or control light and glare impacts, if any:

Outdoor lighting will be subject to requirements set forth in KMC 18.39.030.

12. Recreation

- a. What designated and informal recreational opportunities are in the immediate vicinity?

City parks and schools.

- b. Would the proposed project displace any existing recreational uses? If so, describe.

No.

- c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

None.

13. Historic and cultural preservation

- a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers located on or near the site? If so, specifically describe.

No.

- b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

There is no known evidence of Indian or historic use or occupation of the subject property. Reports suggest that artifacts have been identified within one-half mile of the subject property. The change in land use designation from Low Density Residential to High Density Residential does change or result in an increased adverse environmental impact. The property is fully developable under current land use designation.

May 2014

- c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

A change in land use designation does not change, modify or revise archeological or cultural review in context of project specific review. Each and every project would be subject to guidelines set forth in Washington State standards for Cultural Resource Reporting and environmental review.

- d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

None.

14. Transportation

- a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

The site is served by West Hildebrand Road and South Sherman Street. It can be confirmed, however, that property access will not be provided through Citadel Estates. Citadel Estates is a gated community with private roads and will not be used for purposes of accessing the subject properties.

- b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

No, approximately one mile.

- c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

Off-street parking shall comply with dwelling, multifamily standards set forth in KMC 18.36.060.

- d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

At time of development, both on-site and off-site transportation improvements will be evaluated including consideration of existing roads, streets, pedestrian, bicycle and/or state transportation facilities. Road improvements would be required for any development whether it is Low Density Residential or High Density Residential.

- e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

No.

- f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and non-passenger vehicles). What data or transportation models were used to make these estimates?

See study prepared by Sunburst Engineering. (Attachment F). Trip generation modeling was based on *Institute of Transportation Engineers Trip Generation Manual (10th Edition)*. A redesignation of the property from Low Density Residential to High Density Residential would result in a hypothetical maximum increase of 4046 AFT; 245 AM Peak; and 281 PM Peak. The hypothetical development of a 60-unit hotel and 500 condominium units would result in trip generation increase of 1316 ADT; 58 AM Peak; and 55 PM Peak.

- g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

No.

- h. Proposed measures to reduce or control transportation impacts, if any:

Transportation impacts will be evaluated at time of project specific application. Any future development would be subject to potential impact fee assessment pursuant to KMC 13.16.050-.060.

15. Public services

- a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

No.

May 2014

- b. Proposed measures to reduce or control direct impacts on public services, if any.

Impacts and requirements associated with public services will be evaluated at time of project specific application. Public utilities (including water, sewer and roads) will be extended to and through the project site based upon proposed use and associated development considerations.

16. Utilities

- a. Check utilities currently available at the site:

Electricity, natural gas, water, refuse service, telephone,
Sanitary sewer, septic system, other

- b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Public utility services will be extended to and through project site at time of construction of specific land use proposal.

C. SIGNATURE

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature: _____

Name of signee Jose Chavallo

Position and Agency/Organization Applicant

Date Submitted: September 11, 2020

May 2014

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

(IT IS NOT NECESSARY to use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

Amendment of the land use designation from Low Density Residential to High Density Residential will not increase discharges to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise. Each of the identified consideration is subject to specific regulatory processes, standards and requirements that are applicable to both low density and high density residential development. Under neither scenario, would a proposed project be authorized that failed to comply with applicable and adopted development regulations.

Proposed measures to avoid or reduce such increases are:

Comply with applicable development regulations and environmental mitigation determined at time of project specific applications.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

The redesignation of the subject property from residential low density to residential high density will not affect plants, animals, fish or marine life. Development is authorized with similar development standards under either land use designation and subject to specific regulatory requirements related to critical areas, grading, stormwater and other development impacts.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

Project specific development would be subject to applicable development regulations including environmental review.

May 2014

3. How would the proposal be likely to deplete energy or natural resources?

None.

Proposed measures to protect or conserve energy and natural resources are:

None.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, floodplains, or prime farmlands?

Portion of area is within a critical slope area. Development would be subject to applicable development regulations including critical area and grading regulations.

Proposed measures to protect such resources or to avoid or reduce impacts are:

Area has had a geotechnical report written regarding the protection of the slope.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

Area is not within the vicinity of a shoreline.

May 2014

Proposed measures to avoid or reduce shoreline and land use impacts are:

None.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

It is uncertain whether redesignation of authorized land uses will result in an increased demand for transportation, public services for utilities. While the proposed amendment increases potential density, it also contains provisions for open space, critical area mitigation and related development requirements. Any increase in transportation, public service or utility demand would be insignificant and all facilities are available with adequate capacity for service to the property.

Proposed measures to reduce or respond to such demand(s) are:

All development will be subject to applicable development regulations including transportation impact analysis, assessment of impacts to public services and utilities, and other public related components for any site specific project.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

Future development will meet local, state, or federal laws or requirements for the protection of the environment.

May 2014

ESA LISTED SALMONIDS CHECKLIST

The Listed Salmonids Checklist is provided in order that the City can identify a project's potential impacts (if any) on salmonids that have been listed as "threatened" or "endangered" under the Federal Endangered Species Act (ESA). A salmonid is any fish species that spends part of its life cycle in the ocean and returns to fresh water. Potential project impacts that may result in a "taking" of listed salmonids must be avoided, or mitigated to insignificant levels. Generally, under ESA, a "taking" is broadly defined as any action that causes the death of, or harm to, the listed species. Such actions include those that affect the environment in ways that interfere with or reduce the level of reproduction of the species.

If ESA listed species are present or ever were present in the watershed where your project will be located, your project has the potential for affecting them, and you need to comply with the ESA. The questions in this section will help determine if the ESA listing will impact your project. The Fish Program Manager at the appropriate Department of Fish and Wildlife (DFW) regional office can provide additional information. Please contact the Dept. of Fish and Wildlife at 1701 S. 24th, Yakima WA 98902-5720, Phone No. 509-575-2740.

1. Are ESA listed salmonids currently present in the watershed in which your project will be?

Yes X No _____

Please Describe.

2. Has there ever been an ESA listed salmonid stock present in this watershed?

Yes X No _____

Please Describe.

NOTE: Kennewick is located in the upper Mid-Columbia watershed. Salmonids are present in the watershed - questions no. 1 and no. 2 already answered "yes". Questions A-1 and A-2 are also answered.

PROJECT SPECIFIC: The questions in this section are specific to the project and vicinity.

A1. Name of watershed: Upper Mid-Columbia

A2. Name of nearest waterbody: Columbia River

A3. What is the distance from this project to the nearest body of water?

Often a buffer between the project and a stream can reduce the chance of a negative impact to fish.

A4. What is the current land use between the project and the potentially affected water body (parking lots, farmland, etc.)

A5. What percentage of the project will be impervious surface (including pavement & roof area)?

FISH MIGRATION: The following questions will help determine if this project could interfere with migration of adult and juvenile fish. Both increases and decreases in water flows can affect fish migration.

B1. Does the project require the withdrawal of

a. Surface water? Yes _____ No _____
Amount
Name of surface water body

b. Ground water? Yes _____ No _____
Amount
From Where
Depth of well

B2. Will any water be rerouted? Yes _____ No _____
If yes, will this require a channel change?

B3. Will there be retention ponds? Yes _____ No _____
If yes, will this be an infiltration pond or a surface discharge to either a municipal storm water system or a surface water body?

If to a surface water discharge, please give the name of the waterbody.

B4. Will this project require the building of new roads? (Increased road mileage may affect the timing of water reaching a stream and may, thus, impact fish habitat.)

B5. Are culverts proposed as part of this project? Yes _____ No _____

B6. Are stormwater drywells proposed as part of this project? Yes _____ No _____

B7. Will topography changes affect the duration/direction of runoff flows? Yes _____ No _____

If yes describe the changes.

B8. Will the project involve any reduction of a floodway or floodplain by filling or other partial blockage of flows? Yes _____ No _____

If yes, how will the loss of flood storage be mitigated by your project?

WATER QUALITY: The following questions will help determine if this project could adversely impact water quality. Degraded water quality can affect listed species. Water quality can be made worse by runoff from impervious surfaces, altering water temperature, discharging contaminants, etc.

C1. Will your project either reduce or increase shade along or over a waterbody?
Yes _____ No _____ (Removal of shading vegetation or the building of structures such as docks or floats often result in a change in shade.)

C2. Will the project increase nutrient loading or have the potential to increase nutrient loading or contaminants (fertilizers, other waste discharges, or runoff) to the waterbody?
Yes _____ No _____

C3. Will turbidity (dissolved or partially dissolved sediment load) be increased because of construction of the project or during operation of the project? (In-water or near water work will often increase turbidity.)
Yes _____ No _____

C4. Will your project require long term maintenance, i.e., bridge cleaning, highway salting, chemical sprays for vegetation management, clearing of parking lots?
Yes _____ No _____

Please Describe.

Vegetation: The following questions are designed to determine if the project will affect riparian vegetation, which can impact listed species.

D1. Will the project involve the removal of any vegetation from the stream banks?

YES _____ NO _____

If yes, please describe the existing conditions and the amount and type of vegetation to be removed.

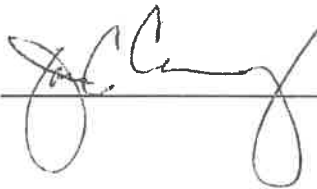
D2. If any vegetation is removed, do you plan to re-plant? YES _____ NO _____

If yes, what types of plants will you use?

E. SIGNATURE

The above answers are true and complete to the best of my knowledge. I understand the City is relying on them to make its decision.

Signature

A handwritten signature in black ink, appearing to be "J. C. [unclear]", written over a horizontal line.

Date 12/30/2020



**CITY OF KENNEWICK
COMMUNITY PLANNING DEPARTMENT**

DATE: February 23, 2021
FILE/PROJECT #: ED 20-14/PLN-2020-01014/CPA 20-06
APPLICANT: Jose A. Chavallo

Documents, Regulations and other Pertinent Information:

The Washington State Administrative Code (WAC 197-11-340 (2) (f)) requires the SEPA responsible official to consider timely comments that are provided in response to a SEPA threshold determination. The City has received a number of comments and as a result issuing a REVISED MDNS. This evaluation and environmental threshold determination are based on an analysis of information contained in the following documents, applicable local & state law and Growth Management Hearings Board cases:

1. Washington Administrative Code (WAC) 197.11;
2. Revised Code of Washington (RCW) 43.21C;
3. City of Kennewick, KMC Chapter 4.08, State Environmental Policy Act (SEPA);
4. City of Kennewick, KMC Title 18, Zoning;
5. City of Kennewick Comprehensive Plan;
6. Southridge Master Plan & Environmental Impact Statement
7. SEPA Environmental Checklist submitted under file number ED 20-14 dated 4/21/2020 and the updated SEPA checklist submitted 9/14/2020;
8. Revised SEPA checklist and supplemental information submitted on December 30, 2020;
9. Growth Management Hearings Board (GMHB) cases Spokane County v. Eastern Washington GMHB, 176 Wash. App. 555,579-580 (2013) and Heritage Baptist Church v. Central Puget Sound GMHB, 2 Wash. App. 737, 751 (2018).
10. Comments received regarding the Mitigated Determination of Non-significance issue February 4, 2021.

Comments Received:

The City has received a number of comments from citizens regarding the Mitigated Determination of Non-Significant issued for the above reference project number PLN-2020-01014. Comments regarding the following issue areas were provided:

1. Traffic/Ingress/Egress
2. Stormwater
3. Critical Areas (steep slopes)

4. Light and glare
5. Aesthetic Impacts
6. Underground Natural Gas Pipeline

Traffic

The issues raised related traffic are two-fold; first, a concern regarding ingress/egress to the subject property; and second, traffic generated by future development of the subject property. The Panoramic Heights Homeowners Association (PHHA) and others expressed a concern about traffic from a future development of the subject property accessing 25th and 26th Avenues through the area known as the Citadel Estates preliminary plat. The City of Kennewick Public Works Department, Traffic Division (Traffic Division) has reviewed the traffic study conducted by the applicant and has developed appropriate mitigation in response to that study, which includes prohibiting access through Citadel Estates on either 25th or 26th Avenues except for emergency vehicle access and no vehicular access to 27th Court.

Comments also suggest that any future traffic study be expanded “...to require evaluation of the comprehensive impacts of the new traffic from this development on all impacted roads, streets and intersections in the Southridge area.” The extent of traffic studies is the purview of the Traffic Division to determine the appropriate streets and intersections for this type of study as noted in mitigation measure #4 contained in the REVISED MDNS.

Critical Areas

Critical areas are well known to be on the subject property and are mapped on the City’s GIS mapping system. Areas of landslide hazard, extreme slope hazard (>40%) and steep slopes (>15%) are identified and mapped for reference. A mitigation condition regarding the extreme slope hazard has been added to the Revised Mitigated Determination of Non-Significance. There was also a comment regarding prohibiting development of slopes greater than 25% as identified in the Southridge Subarea Plan. That condition is not included as the subarea plan is a policy document and does not carry the force of law, the Kennewick Municipal Code would have to be amended to implement this policy.

Stormwater

Stormwater is another significant issue. The City’s standard for stormwater is a design that retains and disposes of a 25 year, 24 hour storm event on-site. Alternatively, an off-site facility may be developed to adequately handle the stormwater. In the Southridge area the City is encouraging the development of regional stormwater facilities where feasible. Detailed designs and plans must be developed and approved by the City at the time of a specific development proposal.

A comment regarding provisions in KMC 18.62.070 (Critical Areas – Geological Hazardous Areas) points out that in KMC 18.62.070 (1) requires “...a 100 year storm event must be considered per KMC 18.62.070(1) must be considered.” This provision of the KMC (18.62.070 (1.b.iv)) requires: “An estimate of the bluff retreat rate that recognizes and reflects potential catastrophic events such as seismic activity or a 100-year storm event.” Adherence to all applicable sections of KMC 18.62 is required.

Aesthetics

Building heights in the Residential, High zoning district are limited to 45 feet; under the current Residential, Low zoning building heights are limited to 35 feet. Development on the subject property will be required to meet applicable Design Standards contained in KMC 18.75 and/or 18.78 as applicable for the proposed development.

Light and Glare

Comments were received regarding the production of additional light and glare as a result of future development of the subject property. KMC Chapter 18.39 – Outdoor Light includes a number of requirements and restrictions regarding outdoor lighting related to excessive glare, light trespass and energy use; while balancing the need to provide for safety and security of residents and visitors.

Underground Pipeline

Comments received regarding an underground natural gas pipeline that runs along the east side of the subject property. This pipeline runs through several neighborhoods both north and south of the subject property with structures being built within five feet of the 50-foot wide pipeline easement. It is city staff's opinion that this feature does not rise to the level of any review under SEPA as encroachment by structures into the easement is prohibited. Mitigation measure(s) are not warranted.

Findings:

1. Location: The subject property is located at 2701 & 2711 South Sherman Road and lots 37 & 38 of the Citadel Estates Preliminary Plat;
2. Existing Comp Plan & Zoning Designation: The subject property is designated as Low Density Residential on the City's Comprehensive Plan Land Use Map. The City's official zoning map designates 2701 S. Sherman Street and Lots 37 & 38 of Citadel Estates Preliminary Plat as Residential, Suburban; 2711 S. Sherman Street is zoned Residential, Low.
3. Kennewick Municipal Code (KMC) Title 18, Zoning requires minimum lot sizes of 7,500 square feet for properties zoned Residential, Low and lot sizes of 10,500 square feet for properties zoned Residential, Suburban.
4. KMC Title 18, Zoning contains the following Critical Area chapters:
 - a. 18.58 – Critical Areas, General Provisions;
 - b. 18.59 – Critical Areas, Wetlands: None Identified;
 - c. 18.60 – Critical Areas, Critical Aquifer Recharge Areas: None Identified;
 - d. 18.61 – Critical Areas, Frequently Flooded Areas: None Identified;
 - e. 18.62 – Critical Areas, Geological Hazardous Areas: Several areas on the subject property that exceed 15% and areas that exceed 40% slopes;
 - f. 18.63 – Critical Areas, Fish and Wildlife Habitat Conservation Areas: Wildlife habitat areas identified on subject property.
5. The applicant is proposing a change to the Comprehensive Plan Land Use Map for the subject property from Low Density Residential to High Density Residential;
6. All properties surrounding the requested change are designated Low Density Residential. The nearest higher density areas are located along Hildebrand Avenue several hundred feet to the south of the subject property;
7. The implementing zoning for the High Density Residential Land Use Map designation is Residential, High (RH);

8. The Residential, High zoning district allows residential development up to 27 dwelling units per acre;
9. Additional permitted or conditionally permitted uses in the RH zoning district include, but are not limited to:
 - a. Group Living II;
 - b. Mini Storage;
 - c. Motels, Hotels & similar accommodations;
 - d. Nursing Homes;
 - e. Health Facilities; and
 - f. Hospitals
10. The applicant has indicated a “possible” future project if the change is approved, however, if approved any use listed, as either permitted or conditional use would be allowed;
11. The applicant submitted the required materials to be considered for inclusion in the City’s annual comprehensive plan update process;
12. As a part of the application materials, the applicant provided a State Environmental Policy Act (SEPA) environmental checklist;
13. After review of the SEPA checklist a Declaration of Non-significance was issued for the non-project action;
14. On August 20, 2020 the City received an appeal of the SEPA threshold determination filed by the Panoramic Heights Homeowners Association;
15. After review of the SEPA appeal and reconsideration of the environmental checklist the City withdrew the DNS for this proposed comprehensive plan amendment on August 28, 2020;
16. On August 28, 2020 the city sent a request for additional information to Jose Chavallo identifying a number of sections of the SEPA checklist where information was needed in order to issue a new threshold determination;
17. Based on two Washington State Growth Management Hearings Board decisions (Spokane County v. Eastern Washington Growth Management Hearings Bd., 176 Wash. App. 555, 5790-580 (2013) and Heritage Baptist Church v. Central Puget Sound Growth Management Hearings Bd., 2 Wash. App. 737, 751 (2018)) the applicant must consider likely development to occur if the request is granted and provide responses to the questions within the SEPA checklist accordingly;
18. On September 14, 2020 the applicant submitted a revised SEPA checklist with a number of attachments including the 2007 Geotechnical Report for Panoramic Heights 4 (August 21, 2007); Critical Areas Report for Citadel Estates (July 1, 2009); Addendum to Critical Areas Report for Citadel Estates (October 15, 2010); Mitigated Determination of Non-Significance #10-56 (March 21, 2011);
19. City staff reviewed the revised checklist and attachments and notes that the applicant has not addressed the key issue of assuming a likely development under the requested change and address potential impacts of the likely development;
20. Public access to the site is currently non-existent – the City requested the applicant provide a proposed means of accessing the subject property, which was not provided in the updated materials submitted on 9/14/2020.
21. The City issued a Determination of Significance (DS) and Scoping determination on October 7, 2020 citing the lack of information previously described in SEPA documents submitted. The comment period for this decision ended on October 22, 2020.
22. The applicant filed an appeal of the City’s DS threshold determination on October 23, 2020.
23. On December 30, 2020 a revised SEPA checklist was submitted by James Carmody on behalf of the applicant. The revised checklist included a number of attachments including the following:

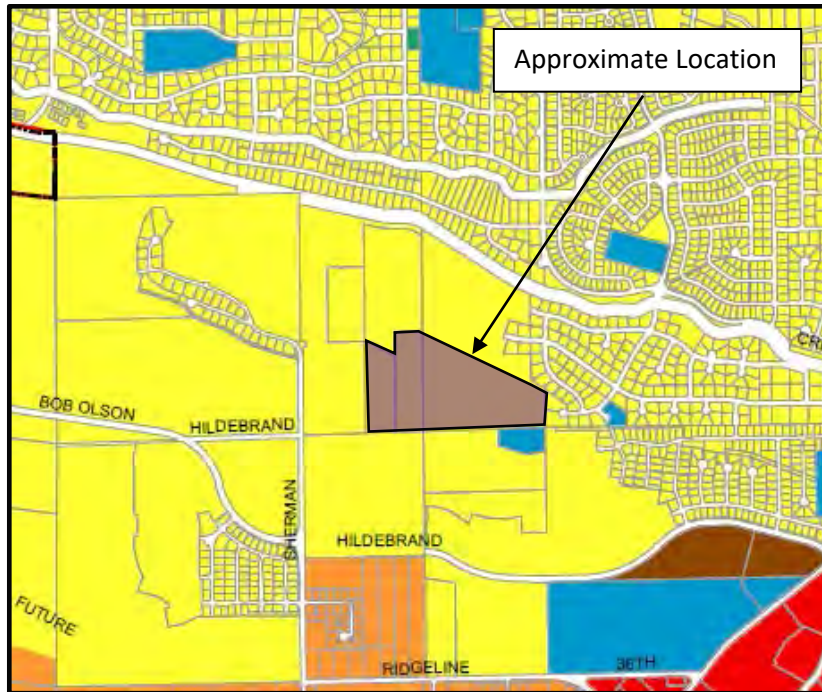
- a. Geotechnical Investigation for Panoramic Heights 4 dated August 21, 2007;
 - b. Critical Areas Report for Citadel Estates dated July 1, 2009;
 - c. Addendum to Critical Areas Report for Citadel Estates dated October 15, 2010;
 - d. SEPA checklist for Citadel Estates dated September 14, 2010;
 - e. Mitigated Determination of Nonsignificance ED 10-56 dated March 21, 2011;
 - f. Traffic Analysis dated December 22, 2020; and
 - g. Stormwater Impact Analysis dated December 14, 2020.
24. The Agency comment period for the revised SEPA checklist and attached materials ended on January 20, 2021.
 25. The City received a number of comments from internal City departments and outside agencies; the City received substantive agency comments from:
 - a. City of Kennewick Traffic Division
 - b. Consolidated Tribes of the Umatilla Indian Reservation
 - c. Washington State Department of Transportation
 - d. Washington State Department of Archeology and Historic Preservation
 26. Unlike previous versions of the SEPA checklist, the December 30, 2020 revision included identification of a “possible future development comprised of a 60-unit boutique hotel and 500-unit condominium/multi-family project.” (Page 2, question 7 of the 12/30/2020 SEPA checklist) This was a key aspect missing from previous SEPA checklists.
 27. As previously noted, the applicant included a number of attachments with the revised SEPA checklist. Of note, two attachments: Traffic Analysis dated December 22, 2020 and the Stormwater Impact Analysis dated December 14, 2020 assessed probable impacts from the “possible future development” identified in #26 above and as identified on Page 3, question 11 included the development of single family residences and multi-family/high density build out at up to 27 units per acre.
 28. On February 4, 2021, the City issued a Withdrawal of Declaration of Significance and Issuance of a Mitigated Determination of Non-significance. A 14-day comment period was provided, which ended on Friday February 19, 2021.
 29. City staff received a number of comments on the issuance of the MDNS noted above.
 30. After review and consideration of the comments provided, the City is issuing a REVISED MDNS for the proposed action. As afforded in WAC 197-11-340 a comment period is not provided.

Conclusions:

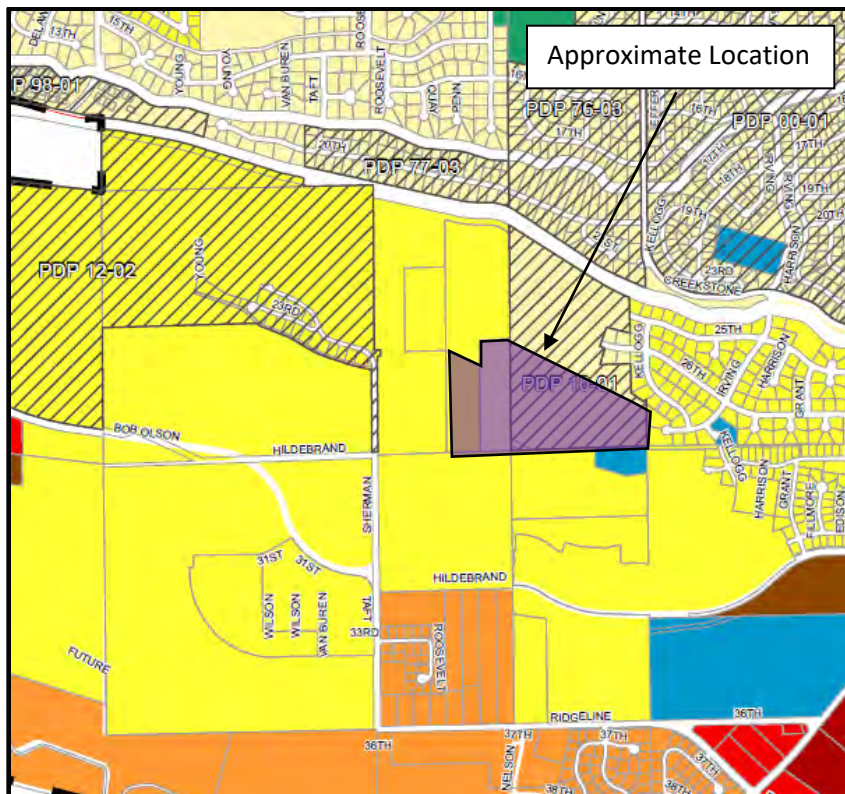
1. Through the resubmission of the revised SEPA checklist and attachments dated December 30, 2020, the applicant has provided the requested information as detailed in the August 28, 2020 letter requesting additional information;
2. The newly submitted materials include the identification of a future project that would be permitted under the revised comprehensive plan designation and the resulting zoning;
3. The subject property is completely isolated from any other higher density/intensity land uses and will create an island of high density/intensity zoning in an area completely enveloped in one or more critical areas according to the City’s critical areas information;
4. The applicant however, has provided the necessary information to allow for analysis of and consideration of future development, identify probable impacts and mitigation necessary to address the impacts.

Attachments

Comprehensive Plan Land Use Map



Kennewick Zoning Map





**CITY OF KENNEWICK
COMMUNITY PLANNING DEPARTMENT**

DATE: August 10, 2022

FILE/PROJECT #: ED 20-14/PLN-2020-01014

APPLICANT: José Chavallo

SEPA REVIEW FOR COMPREHENSIVE PLAN AMENDMENT, CPA-2022-0005

Staff determined that the proposed Comprehensive Plan Amendment, CPA-2022-0005, is less intense than Comprehensive Plan Amendment CPA 20-06/PLN-2020-01009. CPA-2022-0005 involves less acreage and less property amended to High Density Residential.

Review of the SEPA Documents submitted for CPA -2022-0005, show that not all questions were answered with enough details and that adopting the existing Mitigated Determination of Non-Significance for ED 20-14/PLN-2020-01014 will address probable impacts.

Staff is revising ED 20-14/PLN-2020-01014, by adding two additional mitigation measures that require conceptual plans for water, sewer and stormwater systems, along with a proposed street layout.



REVISION

Mitigated Determination of Non-Significance

August 10, 2022

Lead agency: City of Kennewick

Agency Contact: Anthony Muai - AICP, Community Planning Director

Agency File Number: ED 20-14/PLN-2020-01014 and CPA-2022-0005

Description of proposal: Request to amend the Comprehensive Plan Land Use Map for 38.24 ~~40.6~~ acres from Low Density Residential to High Density Residential (4.31 acres) and Medium Density Residential (33.93 acres). If approved, a rezone under a separate action will be required to implement the change to the Land Use Map.

Location of proposal: 2701 & 2711 S Sherman Street

Applicant/Proponent: Jose Chavallo, 5927 W. Quinault Avenue, Kennewick, WA

Discussion/Determination: The City of Kennewick is issuing a **REVISED** MDNS based on proposal change and comments received. The City of Kennewick has determined that this proposal will not have a probable significant adverse impact to the environment; however, mitigation measures are warranted. An Environmental Impact Statement (EIS) will not be required under RCW 43.21C.030(2)(c). This decision is made after review of a completed environmental checklist and other information on file with the City. This information is available to the public on request. Application for other required permits may require further review under SEPA procedures.

☐ There is no comment period for this DNS.

☐ This DNS is issued after using the optional DNS process in WAC 197-11-355. There is no further comment period on this DNS.

☒ This DNS is issued under 197-11-340(2); the City will not act on this proposal for fourteen days from the date below. Comments must be submitted by 4:30 p.m. on August 24, 2022. After the review period has elapsed, all comments received will be evaluated and the DNS will be retained, modified, or withdrawn as required by SEPA regulations.

This **REVISED** MDNS is subject to the conditions contained on page 2 of this document.

RESPONSIBLE OFFICIAL: Anthony Muai, AICP

POSITION/TITLE: Community Planning Director

ADDRESS: 210 W 6th Avenue, P.O. Box 6108, Kennewick, WA 99336

PHONE: (509) 585-4463

Signature

Date: August 10, 2022

Appeal: An appeal of this determination must be submitted to the Community Planning Department within fourteen (14) calendar days after the date issued. Appeals must be submitted no later than 4:30 p.m. on August 24, 2022. This appeal must be written and make specific factual objections to the City's threshold determination. Appeals shall be conducted in conformance with Section 4.12.090(9) of the Kennewick Municipal Code and the required fees pursuant to the City's adopted Fee Schedule shall be paid at time of appeal submittal.



Mitigation Conditions:

1. Prior to any earth disturbing activities on the subject property, a cultural and archeological survey conducted by a qualified professional must be completed.
2. No vehicular access will be allowed through Citadel Estates to 25th Avenue and 26th Avenue, provided that emergency vehicle access only may be allowed on the aforementioned streets as approved by the Kennewick Public Works Department.
3. No vehicular access allowed to 27th Court.
4. At the time of a specific project submittal, as determined by the City of Kennewick Transportation Manager, a traffic impact analysis shall be provided to assess site generated impacts at the following intersections:
 - a. Sherman Street and Bob Olson Parkway
 - b. Hildebrand Avenue and Southridge Boulevard
 - c. Roundabout at Bob Olson Parkway and 10th Avenue

NOTE: Contact the City of Kennewick Transportation Manager to discuss the full scope of the traffic analysis. The traffic analysis shall be coordinated with the Apple Valley traffic study
5. Future applications for rezoning of the subject property shall be considered only in combination with a site-specific project level land use application.
6. All development of the subject property must meet all of the requirements of Kennewick Municipal Code 18.62 as amended and in effect at the time of application.
7. All structures constructed on the subject property must meet the requirements of Kennewick Municipal Code chapter 18.75 and/or 18.78 as applicable to the development as amended and in effect at the time of application.
8. Exterior lighting associated with the development of the subject property must meet the requirements of Kennewick Municipal Code chapter 18.39 as amended and in effect at the time of application, which may include a photometric plan confirming compliance.
9. Development on slopes 40% or greater is prohibited. At the time of a specific development a critical areas report shall be prepared consistent with the applicable requirements of the Kennewick Municipal Code.
10. Submit a conceptual Utility Plan, including water, sewer and stormwater systems, to the Public Works Department for review and approval, two weeks prior to preparation of the CPA-2022-0005 Public Hearing Staff Report.
11. Submit a proposed street layout, including how secondary access will be accomplished, to the Public Works Department for review and approval, two weeks prior to preparation of the CPA-2022-0005 Public Hearing Staff Report.

Note: New conditions are underlined.

Copies of this **REVISED** MDNS Issuance Notice sent to:

1. Parties of Record
2. Department of Ecology, State of Washington
3. ED 20-14 File
4. Bonneville Power Administration

Attachments

1. Revised SEPA Checklist
2. Revised SEPA Attachments
3. Staff Analysis



**Development Services
Community Planning
210 W 6TH Ave
Kennewick, WA 99336**

ADOPTION OF EXSITING ENVIRONMENTAL DOCUMENT

Adoption for (check appropriate box): x DNS ☐ EIS ☐ Other

Description of current proposal: The applicant has proposed the following:

- To change the land use designation of 4.31 acres from Low Density Residential to High Density Residential and rezone the property from Residential, Suburban to Residential, High Density. The applicant intends to build a boutique hotel on the site.
- To change the land use designation of 33.93 acres from Low Density Residential to Medium Density Residential and rezone the property from Residential, Suburban and Residential, Low Density to Residential Medium Density. The applicant intends to builds condominiums on the site.

Proponent: José Chavallo
5927 W Quinault Avenue
Kennewick, WA 99336

Location of current proposal: 2701 and 2711 S Sherman Street

Title of document being adopted: ED 20-14/PLN-2020-01014 – Revised MDNS

Agency that prepared document being adopted: City of Kennewick, Community Planning

Date adopted documents was prepared: Originally on February 26, 2021, and revised on August 10, 2022.

Description of document being adopted: Request to amend the Comprehensive Land Use Map for 40.6 acres from Low Density Residential to High Density Residential. If approved, a rezone under a separate action will be required to implement the change to the Land Use Map.

If the document being adopted has been challenged (WAC 197-11-630) please describe:
The document has not been challenged.

The adopted document is available at: Attached

We have identified and adopted this document as being appropriate for this proposal after independent review. The document meets our environmental review needs for the current proposal and will accompany the proposal to the decision maker.

Name of agency adopting document: City of Kennewick

Responsible Official: Anthony Muai, AICP
Community Planning Director
210 W 6th Ave., P.O. Box 6108, Kennewick, WA 99336
(509) 585-4486

Signature _____

Date 8/31/2022

From: [Diane Katrin Vittone Baker](#)
To: [Steve Donovan](#); Thomas.Helgeson@ci.kennewick.wa.us; tina.gregory@ci.kennewick.wa.us; clark.stolle@ci.kennewick.wa.us; victor.morris@ci.kennewick.wa.us
Cc: veronica.griffith@ci.kennewick.wa.us; ken.short@ci.kennewick.wa.us; James.Hempstead@ci.kennewick.wa.us
Subject: CCP 2022-0005 Thompson Hill
Date: Thursday, July 21, 2022 6:28:33 PM

I am writing this because of my DEEP concern to change the density from Low to High. This would mean condos and apartments and increased traffic which would not match the area.

I am planning on attending the meeting on Aug 1 and 15.

Don't do it.

Diane K. Baker
1607 S. Jefferson Place
Kennewick, WA. 99338

206-214-6775

From: berges6@aol.com
To: [Steve Donovan](#)
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Friday, July 8, 2022 3:56:24 PM

July 3, 2022

Dear Kennewick City Council Members, Planning Commissioners and City Staff;
 Re: Comprehensive Plan Amendment (CPA) 2022-0005.

After reading the SEPA checklist and the guidance to fill out the SEPA Checklist, this application seems incomplete and deficient on providing required details. Mr. Chavallo presented this SEPA checklist as a Non-Project Action, and repeated that over and over again. Forget the fact that he has repeated over and over again that he wants this change in land designation because he would like to build a boutique resort on the top of Thompson Hill. If this is truly NPA, why change the land use designation?

As you know, a Non-Project and project proposal follow the same procedural requirements under SEPA. The process usually starts by completing an environmental checklist, unless the lead agency has already determined an Environmental Impact Statement (EIS) is needed or SEPA is complete.

"If the non-project action (NPA) deals with a land-use decision or a proposal to govern future development, the likely environmental impacts NEED to be considered. An early, detailed analysis can result in a less in-depth environmental review since the work has been done up front." (SEPA checklist general guidance for non-project proposals.)

A quick look at Mr. Chavallo's submitted form shows a lack of detail inconsistent with the guidance above. In the instructions, under the heading "Use of Checklist for Non-Project proposals", it is specifically noted to "...complete the applicable parts of Sections A and B plus the Supplemental sheet for Non-Project Actions (PART D). Mr. Chavallo says this is an NPA, but Section D is not filled out.

Also, the instructions specifically note that likely environmental impacts need to be considered. The following section of Part B, Environmental Elements, are not addressed—inconsistent with the SEPA instructions:

Section f: Deals with erosion. Mr. Chavallo answers NPA. The environmental impact needs to be considered consistent with the SEPA guidance.

Section g: Deals with covering raw land with impermeable material which causes additional runoff. Again, he answers NPA. This does nothing to address the environmental impact of increased runoff that this land use change would cause.

Section 3c: Dealing with water runoff. Once again Mr. Chavallo answers NPA. The issue of what controls will be used to control runoff caused by future construction activity and the roof and road covers which will increase the amount of runoff over what exists now.

Section 6a and c: Mr. Chavallo answer NPA. No description of the utilities that will be used in future construction so that it can be considered. No listed measures of how the additional load on power, internet, and/or gas will be mitigated with the new residences.

Section 14 d, f, and h: Has to deal with transportation through the Southridge area. Mr. Chavallo answered NPA to all of these sections. He has always refused to answer how people will get off the hill and through which neighborhoods. He does not address any potential transportation challenges that this change in land use will cause. NPA addresses nothing and as the NPA guidelines have stated, "if the non-project action deals with a land-use decision or a proposal to govern future development, the likely environmental impacts need to be considered." This land-use designation alteration will have a significant impact on the traffic environment in the Southridge area. Not to consider what improvements will be needed, how many vehicular trips per day generated, and how will these increases will be controlled is irresponsible to say the least. Mr. Chavallo has chosen not to deal with this situation.

It is clear from this application that it is incomplete, misleading, inaccurate, and not ready for review until filled out completely. He has stated that he wants to build a boutique resort on the top to Thompson Hill, how credible is this application when he continues to answer NPA to these vital questions.

Thank you for considering these points. I am concerned about the impact of this proposal on the families who have already invested their savings in the Southridge area. We need Kennewick city council members, planning commissioners, and city staff to evaluate this proposed action, it's impact, and

possible mitigation measures for CPA 2022-0005. I feel Mr. Chavallo is not giving all the information that is required by SEPA, and what is needed to evaluate this land-use designation.

Sincerely;

Dr. Gerald and Cathy Berges
5311 W. 25th Ave.
Kennewick, Washington 99338
Email: berges6@aol.com
Phone: 1-509-378-5370

From: agbooth.montana@gmail.com
To: [Bill Mckay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com
Subject: Thompson Hill Development
Date: Thursday, August 11, 2022 8:14:16 AM

As residents of Creekstone community, I am concerned with the proposal of building 556 multi-family housing units and a hotel in the Thompson Hill Development area. The environmental impact would be devastating to our area. The increased traffic, decrease in property value, steep slopes/run off, landscape safety, crime, noise and light pollution, and added school enrollment are only a few of the added problems if this proposal should pass.

Therefore, we are opposed to the proposal of the Thompson Hill Development.

Gordon and Anita Booth

5506 W.19th Ave.

Kennewick, WA 99338

From: [Bruce Boyum](#)
To: [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Bill McKay](#); [Melinda Didier](#); [Steve Donovan](#); [Terri Wright](#); [info@panoramichelghtshoa.com](#); [Loren Anderson](#); [Brad Beauchamp](#); [Gretl Crawford](#); [wtdixon3@gmail.com](#)
Subject: Proposed Comprehensive Plan Amendment 2022-0005
Date: Wednesday, August 24, 2022 12:14:23 PM

Dear Kennewick Planning Commission, City Council Members, and City Staff,

We live at 5908 W 26th Ave in Panoramic Heights, the neighborhood that borders this site on the east side of Thompson Hill. We have been residents in this neighborhood since November 1985. We will be significantly impacted by a high density residential development in this location. CPA 2022-0005 requests changing the Land Use Designation of about 40.6 acres of steep terrain on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multifamily housing units. High Density Residential would also allow a hotel on the top of Thompson Hill. Rather than 150 single family homes on 38.2 acres, future developers could build up to 556 multi family housing units.

We believe that the proposed change of density would dramatically affect the nature and character of our community. While we are concerned the change would affect our property values, we are more concerned that the place we call home will change for the worse. Specifically we oppose this proposed amendment for the following reasons:

- * The noise level could greatly affect our neighborhood. We experienced this when there was a wedding venue at the top of the Thompson Hill and could hear speakers and music into the night.
- * Building to heights in excess of 40 feet in a residential neighborhood and on an elevated hill above our community raises concerns of aesthetics.
- * There would be a significant increase in traffic flow through our community.
- * Fewer porous surfaces and steep terrains contribute to a concern with runoff.
- * Currently we are often challenged getting adequate irrigation water and this will exacerbate this situation.
- * This development would result in overloading local schools and impacting education.
- * Our Natural environment would affect both the animal habitat and native plants.

Please thoroughly address these serious concerns and potential impacts affecting our neighborhoods and community. We request that the City Staff and Planning Commission recommend that the proposal be denied. The City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhood and is not in the best interest for the current and future citizens of Kennewick.

In summary, we believe that this proposed amendment does not meet your approval criteria that it "bears a substantial relationship to the public health, safety, welfare, and protection of the environment." Nor have the following factors been considered thoroughly prior to approving this Comprehensive Plan Amendment:

- * The effect upon the physical environment;
- * The effect upon open space and natural features, including topography;
- * The compatibility with and impact on adjacent land uses and neighbors;
- * The adequacy of, and impact on community facilities, including utilities, roads, public

transportation, parks, recreation and school;

- * The current and projected project density in the area
- * The effect upon other aspects of the Comprehensive Plan.

Therefore, Comprehensive Plan Amendment 2022-0005 does not meet the requirements of Kennewick Municipal Code 4.12.110(7) and (8) and should be denied.

Thank you for considering our concerns and opposition to this Land Use Designation change and for serving the people of Kennewick. We must look to the future of our citizens and our environment. Once it is gone we can't bring it back.

Respectfully,

Janice and Bruce Boyum
5908 W 26th Ave, Kennewick
jmboyum@gmail.com
August 24, 2022

Sent from my iPad

Dear City Council Members, Planning Commissioners and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

We live at 5319 W 25th Ave Kennewick.

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multifamily housing units. High Density Residential would also allow a hotel.

We are opposed to CPA 2022-0005 for the following reasons, allowing a high density would add so much more traffic to our residential streets and increase the danger for our children when playing outdoors. The crime in the area would also go up drastically since the high density will put that many more vehicles to run through this area.

We request that the City Staff and Planning Commission recommend that this proposal be denied. Then, the City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhoods and is not in the best interest of the Citizens of Kennewick.

Thank you for your consideration of our concerns and our opposition to this Land Use Designation change. My wife and I are very concerned with this matter and we both want to thank you for taking the time to read and hear our concerns.

Jose Chavez

Jose.chavez727@outlook.com

7/31/22

From: [Darrel Duncan](#)
To: [Steve Donovan](#)
Cc: info@panoramicheightshoa.com
Subject: Subject: Thompson Hill proposal
Date: Monday, August 8, 2022 8:08:43 PM

Mr. Donovan:

I would like to express my disapproval for the proposed development on Thompson Hill. I believe that high density housing is appropriate for communities, but it should be designed to be affordable and close to arterials. This development will certainly not meet those criteria. It has been rejected before and should be again.

-Darrel Duncan
5403 W. 26th Avenue
Kennewick
Sent from [Mail](#) for Windows

From: [John Deskins](#)
To: [Steve Donovan](#)
Cc: "Bill Dixon"; john.deskins.home@gmail.com
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Friday, June 24, 2022 12:35:17 AM

Steve,

I've given some review to this new proposal. I can say that full development of the property, even as currently zoned will have significant impacts on our local streets. Specifically, it could be loading up 25th Avenue, 26th Avenue, 28th Avenue, Irving Street, and Kellogg Street south of Irving to traffic levels that exceed the typical volumes for local streets (1500 vehicles per day per the KMC 13.04.010(4)). We also have speeding problems now. Speeds are supposed to be typically 25 mph on Local Streets, but this is not attainable in general and certainly not with our wide, straight streets. Even the speed hump and radar sign near my home have little impact on the worst speeders. Though some are my neighbors for sure we already have a lot of cut-thru traffic due to the Middle School and the short cut route to the High School. So adding the homes allowed per current zoning will have a significant affect but adding many more units by increasing the zoning will have a much more significant affect yet. Since these homes would be forced to use our local street, without really having any "connection" to our neighborhood then we can only expect the problem to become much worse.

We've gone through the exercise before to calculate the potential traffic increase and though the proposal is somewhat reduced, this property has the potential to still generate a lot more trips. In fact it is the developers job to do this work of estimating trips and showing the access points. However, they again fail to do so. The developer ignores key questions in section 14 of the SEPA hiding behind the acronym "NPA" which I presume to mean Non Project Action. This is an "action" and it allows a much higher density and number of housing units and changing the zoning creates an expectation that it's OK to load up at the maximum possible density for the land and then not have to worry about adjacent neighborhood impacts. The developer makes no attempt to estimate the impacts. The developer doesn't clearly demonstrate a suitable access plan. Suggesting that the access points become the extension of 25th Avenue and 26th Avenue is unrealistic and forces every trip through the Panoramic Streets. There is also mention of a 27th Court, but this doesn't sound much better and at this time I am unable to find any such street on any map or diagram the developer has presented. There is a mention also of the use of Sherman Street for access, which is fine, but it is unclear how they might gain access to Sherman Street from the material presented. Assuming that they could do so it is still a circuitous, out of direction route that probably won't serve that many of the development trips considering that much of the attractions and destinations for these units would be to the north of the project.

I've been told that currently the property is zoned to accept about 159 single family units while the re-zone would allow up to 576 total units and some commercial. Since typical single family homes generate about 10 trips per day the current zoning would generate about 1500 ADT (or Average Daily Trips) which would get added on to our neighborhood local streets, several of which are probably operating at or above 1000 ADT today. Even though multifamily generate less trips, that is still going to be in the range of 5.5 to 7 trips per day meaning instead of the added 1500 trips, this

development could generate an additional 3000-4000 trips in residential alone.

It should be noted that Irving Street north of Creekstone Drive was planned and constructed like a Major Collector roadway with no homes directly fronting the street. Irving Street in Panoramic Heights was not designed this way meaning the impacts of this proposed development will have much more direct impacts on our homeowners. Any development will be impactful, but allowing a change that would dramatically increase trips on our local streets with no practical means to mitigate the impacts would be irresponsible and is not serving our citizens.

Sincerely,

John Deskins
5501 W 26th Avenue
Kennewick, WA 99338
509-308-4275

From: [Bill Dixon](#)
To: [Steve Donovan](#)
Cc: [Anthony Muai](#)
Subject: Re: CPA-2022-0005
Date: Tuesday, May 3, 2022 11:23:16 AM
Attachments: [image003.png](#)

Steve,

Thank you for sending this application to me.

As with CPA 2020-006, this application is not true, accurate and complete. The flaws include:

1. The map shows 33.93 acres are requested to be changed to Medium Density Residential Land Use Designation. The SEPA Checklist and CPA Supplemental Information say 35.45 acres, a discrepancy of 1.52 acres (SEPA Question A.12 and CPA Supplemental Information Question 1).
2. The map shows a section of land marked with dashed lines and crossmarks but never explains what this is about. This is a Critical Area for Steep Slopes that should not be developed.
3. Again the SEPA Checklist uses the deceptive response of "Non-Project Action" to avoid answering many of the critical questions on potential environmental impacts in sections A and B. Also the Applicant states "No" or "None" to many other obvious potential environmental impacts.
3. Further, the Applicant failed to provide any answers to the SEPA Checklist Section D.
5. The Applicant's responses to the CPA Supplemental Information questions are incomplete, confusing and misleading.

Therefore, the City Staff should advise the Planning Commission that it recommend to City Council that this Application be rejected, and not included in the CPA Amendment Package on the 2022 Docket.

Thank you for considering these concerns. I intend to express these concerns to the Planning Commission at their meeting on May 16.

Bill Dixon
 509.531.5913
wtdixon3@gmail.com

On Tue, May 3, 2022 at 9:06 AM Steve Donovan <Steve.Donovan@ci.kennewick.wa.us> wrote:

Mr. Dixon,

Attached is a comprehensive plan amendment and SEPA Checklist that was submitted for property up on Thompson Hill.

Let me know if you have any questions.

From: [Bill Dixon](#)
To: [Anthony Muai](#); [Steve Donovan](#); [Melinda Didier](#)
Subject: Comments on CPA-2022-0005
Date: Thursday, May 12, 2022 6:55:58 AM

Please transmit these comments to the Planning Commission before their meeting on May 16.

Dear Planning Commissioners:

I am representing the Panoramic Height Homeowners Association (PHHA). Here are our comments for your consideration at your meeting on May 16, 2022, Agenda item 6.a, regarding the proposed Comprehensive Plan Amendment CPA-2022-0005.

We request that the Planning Commission recommend that the City Council REJECT CPA-2022-0005 for processing. Here are our reasons per the City's Acceptance Criteria:

Is the timing of the requested amendment appropriate and will Council have sufficient information to make an informed decision?

No. The application's SEPA Checklist does not identify potential significant impacts of future development nor identify appropriate mitigation measures. Without this information the Council will not have sufficient information to make an informed decision.

Specifically, KMC 4.12.110(7): "Approval Criteria (a) The proposed amendment bears a substantial relationship to the public health, safety, welfare, and protection of the environment", and (8): "Additional Factors (a) The effect upon the physical environment... (c) The compatibility with and impact on adjacent land uses and surrounding neighborhoods...(d) The adequacy of, and impact on community facilities, including roads...(f) the current and projected project (population) density in the area".

Will the City be able to conduct sufficient analysis, develop policy and related development regulations?

No. This application is not accurate and complete. Therefore, the City's analysis will be insufficient. The flaws include:

- The area map shows 33.93 acres are requested to be changed to Residential Medium Density Land Use Designation. The SEPA Checklist and CPA Supplemental Information say 35.45 acres, a discrepancy of 1.52 acres (SEPA Question A.12 and CPA Supplemental Information Question 1).
- The map shows a section of land marked with dashed lines and crossmarks but never explains what this means. This appears to be a Critical Area for Steep Slopes (greater than 25%) that should not be developed.
- The SEPA Checklist uses the deceptive response of "Non-Project Action" to avoid answering many of the critical questions on potential significant impacts of future development in sections A and B. Also the Applicant states "No" or "None" to many other obvious potential impacts.
- Further, the Applicant failed to provide any answers to the SEPA Checklist Section D.
- The Applicant's responses to the CPA Supplemental Information questions are incomplete and confusing.

Will the amendment further implement the intent of the City's adopted Comprehensive Plan?

In part. As stated by City Staff, the proposal could help implement Housing Goal 1: "Support and develop a variety of housing types and densities to meet the diverse needs of the population."

However the proposal does not further implement the following aspects of the Comprehensive Plan.

- Geologically Hazardous Areas (pages 35 to 37): The site is in a designated Geologically Hazardous Area for Extreme Slope Hazards: "Severe erosion potential and high probability of slope failure & landslide occurrence, Slopes greater than 25%." Mitigation Sequencing states: "Proposed development should avoid impacting critical areas" and "Mitigation sequencing is listed in the order of preference. 1. Avoiding the impact by not taking a certain action;". These hazards are not addressed in the proposal.
- Land Use Goals and Policies: Goal 1, Policy 3 (page 47): "Require that multi-family structures be located near a collector street with transit, or a near arterial street, or near a neighborhood center." Multi-family structures that could be built throughout this site have no access except for residential streets, and no public transit. There is no neighborhood center nearby.

Goal 1, Policy 5 (page 47): "Encourage adequate pedestrian connections with nearby neighborhood and transit facilities in all residential site development." Due to the lack of roads, distances to these facilities, and steepness of the slopes, pedestrian connections will be difficult and worsened by higher population densities.

Goal 3, Policies 2, 3 and 4 (page 48):

"Residential Low Density: Place lands constrained by sensitive areas...or those appropriate for larger lot housing". This is the current Land Use Designation as appropriate for steep slopes less than 25% in a Critical Area.

"Residential Medium Density: Place areas that can support high-quality, compact, urban development with access to urban services, transit, and infrastructure". This proposed land use designation is not appropriate for a site with steep slopes in a Critical Area, not near urban services, without transit service, and lacking adequate access other than residential streets which are already overloaded.

"Residential High Density: Designate land for Residential High Density (HD) where access, topography, and adjacent land uses create conditions appropriate for a variety of unit types, or where there is existing multi-family development." This proposed land use designation is not appropriate for a site with steep slopes in a Critical Area, not near urban services, without transit service, lacking adequate access other than residential streets which are already overloaded, and not near existing multi-family developments.

Thank you for considering our comments in your deliberations and decision.

Bill Dixon
Representing the Panoramic Heights Homeowners Association
2500 S. Irving St., Kennewick
509.531.5913
wtdixon3@gmail.com

From: [Bill Dixon](#)
To: [Steve Donovan](#); [Anthony Muai](#); [Melinda Didier](#)
Cc: [GERALD BERGES](#); [Stephen Varner](#)
Subject: CPA 2022-0005 Documents
Date: Friday, June 10, 2022 3:29:23 PM

I request electronic copies of any new documents related to CPA 2022-0005. I have the application and SEPA checklist which you provided earlier.

Also, I am concerned about the lack of public notice as required by KMC 4.12.090. As of now, I only found one sign on S. Sherman St. Why weren't signs placed in all other adjacent neighborhoods?

There was no public notice yet published in the Tri-City Herald. I have not heard that homeowners within 300 feet of the property have yet received the required notification by direct mailing.

And to my knowledge, the Panoramic Heights Homeowners Association, a group with known interests, was not notified.

Bill Dixon
509.531.5913
wtdixon3@gmail.com

From: [Bill Dixon](#)
To: [Steve Donovan](#); [Anthony Muai](#); [Melinda Didier](#); [Bill Mckay](#); [Brad Beauchamp](#); [Chuck Torelli](#); [Gretl Crawford](#); [Jim Millbauer](#); [John Trumbo](#); [Loren Anderson](#); [Terri Wright](#)
Subject: Comprehensive Plan Amendment (CPA) 2022-0005: Request from Panoramic Heights Homeowners Association
Date: Thursday, June 23, 2022 7:00:18 AM
Attachments: [PHHA letter 1 on CPA 2022-0005 \(1\).pdf](#)
[PHHA Comments 1 on CPA 2022-0005.pdf](#)

Dear City Staff and City Council Members:

On behalf of the 159 families in the Panoramic Heights Homeowners Association (PHHA), we submit the attached comments on Comprehensive Plan Amendment (CPA) 2022-0005.

We find the Application is not complete and accurate. Specifically, it does not meet State Environmental Policy Act (SEPA) requirements and guidance to submit a completed SEPA Checklist that fully describes the proposal, the potential future impacts from developing this land as proposed, and possible mitigating measures.

We found that 37 Checklist questions on future uses and potential impacts were not answered substantively, mostly by simply stating it is a "Non-Project Action". Also, the seven questions specifically required for a "Non-Project Action" were not answered at all. This is not in accordance with SEPA Guidance which states in part:

“When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.”

Therefore, the City should require this Application to be revised (per WAC 197-11-100 “Information required of applicants”) to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for the City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation to City Council; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

City Staff, please share this response with the Planning Commission members.

Thank you for considering our comments and our request.

Bill Dixon, representing the Panoramic Heights Homeowners Association
2500 S. Irving Street

Kennewick, WA 99338
509.531.5913
wtdixon3@gmail.com

Panoramic Heights Homeowners Association
www.panoramicheightshoa.com

June 23, 2022

Dear Kennewick City Council Members, Planning Commissioners, and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

I am writing on behalf of the 159 families who are part of the Panoramic Heights Homeowners Association (PHHA). I have also delegated authority to Mr. Bill Dixon to represent PHHA in this matter.

PHHA has done a thorough review of the Application and its attachments for Comprehensive Plan Amendment (CPA) 2022-0005 dated April 21, 2022.

PHHA finds the CPA 2022-0005 Application to be incomplete, inaccurate, and in some cases false.

This Application is not in compliance with the City of Kennewick's requirements, as stated on the Application General Form. Further, this application does not meet the State law and implementing regulations under that State Environmental Policy Act (SEPA) to provide true, accurate and complete information about the proposed action, potential environmental impacts, and appropriate mitigating measures.

The City should require this Application to be revised (per WAC 197-11-100 "Information required of applicants") to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for the City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation to City Council; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

PHHA's specific comments and concerns about the inadequacies in this Application are discussed in detail in the attachment.

In summary, the Application does not meet the requirement for a completed SEPA Checklist as required in SEPA regulations (WAC 197-11) and Guidance. Specifically:

- The SEPA Checklist responses do not contain the information needed “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). There are no substantive responses about impacts and possible mitigations.
- The SEPA Checklist responses do not “apply to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The responses only cover proposed administrative changes to the Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.
- The Applicant simply answers most of the questions about impacts and mitigation as “Non-Project Action, NPA” or “None”. “SEPA Guidance for Non-Project Actions” states “When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

As submitted, there are at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially. All seven of the questions specifically required for Non-Project Actions (Section D) were not answered at all.

We note that in a prior Application (CPA 20-06) in 2020 for the same site, the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City responded at that time (in a letter from Steve Donovan to Jose’ Chavallo, “Request for Additional SEPA Checklist Information”, dated August 28, 2020):

“The above reference WAC (*Washington Administrative Code 197.11*) and GMHB (*Growth Management Hearings Board*) cases clearly state that answering questions in an environmental checklist for a non-project action with a reference to the fact that the proposal is a non-project action is not sufficient.

You must consider your request and the resulting zoning (if approved) and what is permitted within the new zoning district; responding to the questions more specifically and providing likely impacts such as traffic generated by a likely proposal and possible mitigation to address the likely impacts. The response

“NPP” is not acceptable and must be changed to reflect the previous comments.”

The City identified 40 checklist questions in that Application that needed additional details or clarification.

The same should be required this time.

Without this required information, the public has not been fully informed, the City Staff and Planning Commissioners can not make informed recommendations, and the City Council can not make an informed decision in the best interests of the citizens of Kennewick.

Thank you for considering our request to ensure a complete understanding of the proposed action, its impacts, and possible mitigation measures for CPA 2022-0005.

Sincerely,

Gerald Berges, PHHA President
5311W. 25th Avenue
Kennewick, WA 99338

Email: berges6@aol.com

Attachment:

Comments and Concerns About Proposed Comprehensive Plan Amendment
(CPA) 2022-0005 Completeness and Accuracy

PANORAMIC HEIGHTS HOMEOWNERS ASSOCIATION
COMMENTS AND CONCERNS ABOUT
PROPOSED COMPREHENSIVE PLAN AMENDMENT (CPA) 2022-0005
COMPLETENESS AND ACCURACY

June 23, 2022

The Panoramic Heights Homeowners Association (PHHA) has done a thorough review of the Application and its attachments for Comprehensive Plan Amendment (CPA) 2022-0005 dated April 21, 2022.

PHHA finds the CPA 2022-0005 Application to be incomplete, inaccurate, and in some cases false.

This Application is not in compliance with the City of Kennewick's requirements, as stated on the Application General Form. Further, this application does not meet the State law and implementing regulations under that State Environmental Policy Act (SEPA) to provide true, accurate and complete information about the proposed action, potential environmental impacts, and appropriate mitigating measures.

The City should require this Application to be revised (per WAC 197-11-100 "Information required of applicants") to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

PHHA's specific comments and concerns are discussed in detail below.

SEPA ENVIRONMENTAL CHECKLIST REQUIREMENTS (per WAC 197-11-960)

Purpose: The purpose of the checklist is:

“to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.” (emphasis added)

The CPA 2022-0005 checklist does not contain the information needed to make this determination. Detailed examples are below.

Instructions for applicants: These instructions state:

“The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time...” (emphasis added)

The Applicant states the only action requested is to “Amend Comprehensive Plan” and does not address the impacts of this action from the resulting possible future development of the land.

Instructions for Lead Agencies: These instructions state:

“Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.” (emphasis added)

The City should request and analyze the additional information needed (per WAC 197-11-100 “Information required of applicants”), and provide that information for public review and comment. Until the Application is complete, the City should NOT issue a threshold determination and thereby assume responsibility for a SEPA checklist that is clearly incomplete, inaccurate, and not in compliance with SEPA guidance.

Non-Project Action: The Applicant proposes “2022 Comprehensive Plan Amend (sic) from Low Density Residential to Medium and High Density Residential...Submitted as a Non-Project Action, NPA”.

Note that the SEPA Checklist Guidance, section D, Non-Project Actions states:

“Non-project actions are governmental actions involving decisions about policies, plans, or programs containing standards for controlling use or modifying the environment, or will govern a series of connected actions. Non-project action analysis provides an opportunity to evaluate planned actions before projects begin and permits applications are prepared. “

“If the non-project action is a land-use decision or similar proposal that will govern future project development, the probable impacts need to be considered of the future development that would be allowed. For example, environmental analysis of a zone designation should analyze the likely impacts of the development allowed within that zone.” (emphasis added)

And the Guidance for “Non-project actions: Comprehensive plans, future project development” specifically states:

“ When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

This Application is clearly inconsistent with this SEPA Guidance. The Applicant avoids answering most of the SEPA Checklist questions about subsequent actions, potential environmental impacts, and possible mitigations; mostly by simply responding “NPA” or “none”.

These omissions include (by checklist section number):

A.7: Plans for future additions, expansion, or further activity related to or connected with this proposal.

A.11: Complete description of the proposal, including proposed uses.

A.12: A vicinity map and a topographic map.

B.1.e: Filling, excavation and proposed grading.

B.1.f: Erosion potential.

B.1.g: Impervious surface area.

B.1.h: Erosion measures and controls.

B. 2.a: Air emissions.

B.2.c: Air emissions measures and controls.

B.3.c: Water runoff.

B.3.d: Water runoff measures and controls.

B.4.b: Vegetation removed or altered.

B.4.d: Measures to preserve or enhance vegetation.

B.5.d: Measures to preserve or enhance wildlife.

B.6.a: Kinds of energy to be used.

B.6.c: Energy conservation measures.

B.7.b: Noise and measures to control it.

B.8.a: Current use of nearby and adjacent properties and effect upon them.

B.8.i: Number of people who would work or reside on site.

B.8.l. Measures to ensure compatibility with existing and projected land uses and plans.

B.9.a: Number of housing units provided.

B.9.c: Measures to reduce or control housing impacts.

B.10.a: Tallest height of structures.

B.10.b: Views in immediate vicinity altered or obstructed.

B.10.c: Measures to control aesthetic impacts.

B.11.a: Light or glare produced.

B.11.b. Light safety hazards or interference with views.

B.11.d: Measures to reduce or control light and glare impacts.

B13.d: Measures to avoid, minimize or compensate for loss, changes to, and disturbance of cultural resources.

B.14.a: Proposed access to the existing street system, and shown on site plans.

B.14.c: Parking spaces.

B.14.c: New or improved roads, streets, pedestrian, bicycle, or state transportation facilities needed.

B.14.f: Vehicle trips per day generated and peak volumes.

B.14.h: Measures to reduce or control transportation impacts.

B.15.a: Increased need for public services.

B.15.b Measures to reduce or control direct impacts on public services.

B.16.b: Utilities proposed and construction needed.

Further, the Applicant failed to answer ANY of the questions in Section D. “Supplemental Sheet for Non-Project Actions”. This section is required to be completed for Non-Project Actions.

Instructions state:

“When answering these questions, be aware of the extent the proposal, or types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. (Emphasis added)

Questions about impacts that were not answered should have addressed:

1. Likely increases to water discharges, air emissions, and noise; and proposed measures to avoid or control them.
2. Likely effect on plants and animals; and measures to protect or conserve them.
3. Likely depletion of energy and natural resources; and measures to protect or conserve them.

4. Likely use of or effect on environmentally sensitive areas or areas designated for government protection (*such as Critical Areas for Steep Slopes*); and measures to protect, avoid or reduce them.
5. Likely effect on land use including incompatibility with existing plans; and proposed measures to reduce or avoid land use impacts.
6. Likely increased demands on transportation or public services and utilities; and proposed measures to reduce or respond to such demands.
7. Whether the proposal may conflict with local, state, or federal laws for protection of the environment.

Other SEPA Checklist Issues:

Section A.12 states that 35.45 acres is proposed for Medium Density Residential. Yet the referenced “Area Map” shows 33.93 acres, a difference of 1.52 acres. Which is it?

The Area Map also has a cross-hatched area in the land proposed for Medium Density Residential. What does this mean about proposed land use in that area?

The Applicant selected the answer “hilly” as the general description of the site rather than the correct selection of “steep slopes”. The Applicant also states that the steepest slope on the site is “30-40%”, however the area to be developed is somewhat in the range of 20% or flatter” when it is known that portions are greater than 40%, and much of the site is greater than 20% slope. (Checklist questions B.1.a and b)

The Application in SEPA Checklist Section 7.a.2 does not address the underground natural gas pipeline that is located on the site, even though this is well marked and known. Further, the application fails to discuss controls and restrictions to protect it from development.

COMPREHENSIVE PLAN AMENDMENT SUPPLEMENTAL INFORMATION:

The City also requires “Comprehensive Plan Amendment Supplemental Information”. This information is required for the Planning Commission and the City council to consider the request. Much of the requested information is missing.

Question 2 asks “What are the reasons for the requested amendment”. The Applicant just responded “Allow amending zoning area to Medium Density and High Density Residential” rather than stating the reasons for the amendment.

Question 3 asks “Which elements of the Comprehensive Plan will be affected and how”. The Applicant does not respond.

Question 3 goes on to ask for detailed information on the provision of utilities and how that corresponds to the City’s plans, and detailed information on the effects on public services. The Applicant simply states “Development of the areas will use the same public utilities and services as current Comprehensive and Zoning Areas.” This does not address the impact of changes to the types and level of utilities and services required.

Question 4 requests the Applicant to “Indicate how the requested amendment will implement the Comprehensive Plan and be in the best interest of the Kennewick area, reference specific Comprehensive Plan policies that will be implemented.” The Applicant responds “Hillside development areas require alternate design atandards (sic) to increase density and preserve more open areas within the development. A condensed development requires land usage for roads and utilities.” This response does not address the question.

SUMMARY OF SIGNIFICANT CONCERNS ABOUT THE APPLICATION:

Steep Slopes: Development on steep slopes would pose many hazards. The Application fails to identify which parts of the site are within the known Geologically Hazardous Area for Steep Slopes and to address the impacts of these steep slopes upon development.

The Comprehensive Plan says that “Erosion Hazards” can occur on “Slopes greater than 15%”, and defines “Extreme Slope Hazards” as “Severe erosion potential and high probability of slope failure & landslide occurrence, Slopes greater than 25%”. The Comprehensive Plan further states “Proposed development should avoid impacting critical areas.” (page 36)

Also, the Application fails to discuss the land with slopes greater than 40% which cannot be developed. The Applicant should be required to identify all areas of steep slopes on the site and to discuss any hazard controls, restrictions or prohibitions needed based on how steep the slopes are in each area.

Note that in a prior application for this same site (CPA 20-06) in 2020, the Applicant stated to the Planning Commision (in “Applicant’s Pre-Hearing Memorandum, April 19, 2021”) that:

“The north slope includes geologically hazardous critical areas and offers limited, if any, development opportunities.” (page 4)

“Lot 37...consists primarily of geologically hazardous slopes. The steep slope area extends along the northern perimeter of the amendment property...Lots 37 and 38 contain geologically hazardous critical areas which severely constrain development.” (page 7) *(Note that Lots 37 and 38 includes a large portion of the site proposed in CPA 2022-0005.)*

“A significant portion of the north face of the subject property will remain “open space” in perpetuity.”(page 8)

“The topography and presence of geologically hazardous critical areas on the north slope poses significant development restrictions to both Low Density Residential and High Density Residential project proposals.” (page 12)

“Significant portions of both Lot 37 and Lot 38 (which include the north slope) are undevelopable critical areas...In addition to absolute prohibitions on development, the remaining north slope presents significant development impediments for any residential development—single-family residential or multi-family residential.” (page 15)

Traffic impacts: High Density and Medium Density Residential Land Use Designations would allow developments which would cause significant traffic impacts in the area. The Applicant avoids addressing the significant traffic impacts from this proposal. For examples, there is no information provided to the following questions:

- “describe proposed access to the existing street system. Show on site plans” (Checklist question 14.a)
- “Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities...? (Checklist question 14.d)
- “How many vehicular trips per day will be generated by the completed project or proposal?...indicate peak volumes” (Checklist question 14.f)
- “Proposed measures to reduce or control transportation impacts” (Checklist question 14.h)
- “How would the proposal be likely to increase demands on transportation or public services....? (Checklist question D.6)

Note that in a prior application for this same site (CPA 20-06) in 2020, the Applicant stated to the Planning Commission (in “Applicant’s Pre-Hearing Memorandum, April 19, 2021”) that:

“No traffic from the subject property will pass through Panoramic Heights.”
(page 11)

This commitment resulted from a Mitigation Condition proposed by the City to avoid further traffic on overloaded neighborhood streets. This restriction is still needed and should be kept for this similar proposal on the same site.

Impacts on Surrounding Neighborhoods: The Application does not address any of the potential impacts that the resulting development would have on surrounding neighborhoods. Nor does it address the incompatibility of the proposed High and Medium Residential Land Use designations with the Residential Low Density and Residential Suburban zonings in the surrounding neighborhoods.

Hotel on Top: The Applicant has talked about building a “boutique hotel” on the top of Thompson Hill, but there is no mention of a hotel in this Application. Therefore, this Application, as written, does not address the impacts of a hotel. If the Applicant intends to build a hotel, the potential impacts and mitigations must be addressed.

CONCLUSION:

For all of the above reasons, the City of Kennewick should require this Application to be revised (per WAC 197-11-100 “Information required of applicants”) to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

From: [Bill Dixon](#)
To: [Steve Donovan](#)
Subject: CPA 2022-0005
Date: Sunday, July 3, 2022 9:45:05 AM

Steve,

Where would I find a street map (or maps) that shows the current and future planned streets around the proposed site?

I see the City Street Map (pdf) of March 2022 which shows some existing and planned streets, but I don't think it shows all planned streets in approved developments, such as Citadel Estates and all the Southridge area developments south of the site.

When I look at Google Maps, it shows 28th Avenue connected between Sherman and Creekstone. It also shows 28th Court (off Kellogg) connected to the AP lateral road, and to the unimproved roads going to the top of the hill and north to the 26th Avenue extension for Citadel Estates.

Are these correct? If so, that would provide several cut-through routes for northbound traffic from the site to Kellogg, Irving and 26th, all of which would severely impact Panoramic Heights.

Thank you for any information you can provide.

Bill Dixon
509.531.5913
wtdixon3@gmail.com

From: [Bill Dixon](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com; [Anthony Muai](#); [Melinda Didier](#)
Subject: CPA 2022-0005: Initial Results From Public Questionnaire
Date: Wednesday, July 6, 2022 11:30:12 AM
Attachments: [CPA_2002_0005_QUESTIONNAIRE_Jul_6_2022_for_City_\(1\).pdf](#)

Dear City Staff, Planning Commissioners and Council Members:

(Staff, please share with Planning Commissioners)

The Panoramic Heights Homeowners Association (PHHA) has started an open public questionnaire on Comprehensive Plan Amendment CPA 2022-0005 via its website (<https://panoramicheightshoa.com/>).

This CPA proposes to change the Land Use Designation on about 40 acres at and near the top of Thompson Hill from Low Density Residential to High and Medium Density Residential.

Here are the initial 26 responses received from residents around the site. These respondents agreed that their responses should be supplied to the City.

These 26 respondents are from numerous neighbors in the south Kennewick area. They are from:

- Panoramic Heights (8)
- Windsong (5)
- Creekstone (4)
- Apple Valley (4)
- Southridge Estates (3)
- Sagecrest (1)
- Highlands (1)

All 26 respondents have major concerns about the proposed Land Use Designation change. Their biggest concerns about this change were:

- Incompatible with existing neighborhoods (all 26)
- Increased traffic (all 26)
- Noise and light pollution (17)
- Steep slopes, runoff and landslides (16)
- Decrease in property value (13)
- Safety (13)
- Crime (13)

25 of the respondents explained in their own words why they oppose this development.

They request that their input be considered in your analysis, recommendations and decision about increasing the established land use density and allowing other than single family homes on this site.

I will provide further public input from this questionnaire in subsequent communications.

Thank you for considering these concerns.

Bill Dixon, PHHA Lead
509.531.5913
wtdixon3@gmail.com

Name	Address	Neighborhood	What are your biggest concerns about this development	In your owns words, please share why you oppose this development:
Bill Dixon	2500 S. Irving Street Kennewick, WA 99338 USA	Panoramic Heights	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	<p>This proposal is inconsistent with the City's Comprehensive Plan. It is similar to a prior proposal that both City Staff and the Planning Commission recommended denial, and that the City Council denied. High and Medium housing densities with multi-family housing units, and perhaps a hotel, would be incompatible with all surrounding Low density neighborhoods, including the approved Citadel Estates development.</p> <p>There are major hazards with the steep slopes that make much of the site unstable with erosion and runoff problems, and is largely unsuitable for safe development with higher densities.</p> <p>The additional traffic would have major impacts on surrounding neighborhoods.This has been and continues to be a bad idea for a major new higher density development on steep slopes, surrounded by single family homes, and with neighborhood streets that cannot withstand more direct and cut-through traffic.</p>
Gerald and Cathy Berges	5311 W. 25th Avenue Kennewick, Washington 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	<p>Traffic through our neighborhood. This issue has not been addressed. Also, an incomplete application, going against a process this city developed. I don't understand how the city can even consider this application until it is fill out completely to the standards they have established.</p>
Susan Dixon	2500 S. Irving Street Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	<p>I'm disappointed that this proposal is ignoring the City of Kennewick's Comprehensive Plan for our area. We purchased our home because we were attracted to low-density living and expected future development to respect that designation. Traffic has already increased in our area exponentially and this current proposal would increase that considerably. Kennewick's City Planners and City Council should first and foremost respect the nature and livability of existing neighborhoods before considering a land-use change to increase housing density. Would you vote to increase housing density adjacent to your neighborhood?</p>
Heather L Erhart	6037 W 16th Ave Kennewick , WA 99338 USA	Windsong	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	<p>It is completely incongruent with all of the surrounding neighborhoods. Keep it single family!</p>
Kelly Wetherell	6012 W 16th Ave Kennewick , WA 99338 USA	Windsong	Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution	

Exhibit A-19.13

John romines	5606 west 17th av Kennewick, Wa 99339 USA	Creekstone	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	We at creekstone already have too many random people just driving through the neighbord so sa to not have to drive further west to pick up kellog and drive southeast toward 395. I an EXTREAMLY OPPOSED to ANY high and medium density developments due to our already increased traffic and our crime has increased dramatically. I feel any such development would only add to our issues.
Jessica Holloway	3148 s Wilson pl Kennewick, WA 99338 USA	Apple Valley	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	This area is already growing and there isn't good traffic control and there is a huge increase in crime
Gene & Kerry St.Denis	3258 S. Van Buren St. Kennewick , Wa 99338 USA	Apple Valley	All the above!	Increased population, traffic, crime and will definitely over-crowd our wonderful quiet neighborhood.
Marla Holub	3327 S Lincoln Place Kennewick, WA 99338 USA	Southridge Estates	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety,Steep slopes/Runoff/Landslides	It is not consistent with the neighborhoods. Increased traffic through neighborhoods and on Hildebrand.
Sue Gano	5910 W 10th PL Kennewick , WA 99338 USA	Windsong	Incompatible with existing neighborhoods,Increased Traffic,A blot on one of the few remaining untouched areas.	It is not necessary to destroy every natural area bordering existing neighborhoods, for the benefit of a few and detriment to residents and natural viewscapes.
Ken Gano	5910 w 10th pl Kennewick , WA 99338 USA	Windsong	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety	Traffic on Kellogg is already to heavy and noisy. Substantially increasing the population in this area only make it worse. It will also destroy the esthetic of Thompson Hill. We donâ€™t need to cover every inch of every hill in the Tri Cities with houses just to make some developer rich.
Kevelene Marston	7030 W 29th Ave Kennewick , WA 99338 USA	Apple Valley	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety	We have been in our current home for two years now. When we purchased our home we knew there would be growth but that growth did not include condos or apartments. It was zoned for low density, nothing more!

Exhibit A-19.13

Eric Otheim	7044 W 33rd Place Kennewick , Washington 99338 USA	Apple Valley	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution	This is inconsistent with the the neighborhood's surrounding Thompson Hill. Most of us chose to live in this area because it was zoned for low to medium housing. Little did we know that it was possible to change the zoning so easily. This project would increase the noise and light pollution as well as increase congestion on what is already becoming an increasingly busy boulevard. Because it is at the top of the hill, the disruption from the light and noise would have a negative impact over a much larger area. I feel like it is similar to the situation with the state wanting to place a windmill farm in Kennewick's backyard. I hope our voice is listened to more than the state seems to be listening to our county's in regards to the windmill project.
Robert Langendorfer	5717 w 23RD AVENUE Kennewick, WA 99338 USA	Creekstone	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	Will this never end? One "modification" after the next it seems. The traffic increase does concern me. Our home is adjacent to Creekstone Blvd and at times, even with the existing traffic pattern, it can be extremely difficult and dangerous getting out of the Creekstone area. Being very familiar with police work in other very congested residential areas, with multiple crimes being committed almost daily. This in itself can put a huge impact on your law enforcement. Is Kennewick prepared to hire and train a larger number of officers to handle the daily calls which will certainly become a reality. As I had stated in previous comments made to the council, I have been there. Working cities like Los Angeles basin and San Diego, the number of reported crimes, increases dramatically in most high density neighborhoods. Car theft, Child abuse, Rape, Assault, even drive by shootings. I personally no longer wish to live in an area as I have seen in my career of 38 yrs as a Peace Officer. Please think of the surrounding neighborhoods, thousands of your law abiding citizens will or can be affected by such a development. Please try to think above your increase property tax income to the city and consider what this may or could, do to the good people in your community.

Exhibit A-19.13

Ivan Thomas	5216 W 26th Ave Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	This land was purchased with regards to it being designated as low density. The developer knew this and should be held to this designation. This is what he/ paid for and this is what he/she should get. Increasing the top 4.3 acres to high and the upper north and south slopes to medium is totally incompatible with the surrounding neighborhoods and the resulting traffic from this area would place a significant burden on the roads in surrounding neighborhoods. We've already seen an increase in the amount of traffic (and racing vehicles) passing through Panoramic Heights due to surrounding neighborhoods and the new school and this development would seriously exacerbate the problem. I'm also very concerned about the impact of building on the steep slopes of Thompson Hill and the real risk of run off and land slides. If this development is allowed, then the developer will be long gone once everything is built and the existing neighborhoods will be left to bear the adverse consequences of what it brings.
Dana Brunsdon	4609 W 4th Ct Kennewick, WA 99336 USA	Hi-lands area	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution	I've lived off of 4th and Union st. for 34 years and I believe it will have a huge impact on our area as well. To me it's about maintaining our quality of life. It makes no sense that you continue to let this group continue to push the hotel especially when we've said no over and over. Build a hotel in Vista Field they want you there.
Hulstrom Larry	5409, W 26th Ave Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic	This will ruin an iconic view that is seen throughout the TriCities, it is an attempt by the owner/developer to maximize his profit margin at the cost of everyone else living in the area
Patricia Wilson	5985 W 41st Ave Kennewick, Washington 99338 USA	Sagecrest	Crime,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	
John P. LaFemina	1406 South Kellogg Street Kennewick, WA 99338 USA	Windsong	Incompatible with existing neighborhoods	I could have checked all of the above for my concerns about this proposal, which by my accounting is the 4th or 5th time this developer has tried to get around the (to date) consistent rejections of his proposal. Unfortunately, with new council members, I fear that his persistence will be rewarded to the detriment of the wider community.
Bertha Garza	5117 W 32nd Ave Kennewick, WA 99338 USA	Southridge Estates	Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic	Area is still growing and this would really make it busier.
Richard Nelson	5718 W 25th Ave Kennewick, WA 99338 USA	Panoramic Heights	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	Kennewick Planning Committee studied the overall best use for this area several years ago. Most of us purchased property or made improvements based on this published study. There is no good reason to change the recommendations and current zoning of this area.

Exhibit A-19.13

William Reed	2107 S. Fillmore St. Kennewick, WA 99338 USA	Creekstone	Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	High-density housing does just not fit with what I had hoped for the future of Thompson hill. It would be ugly and result in increased traffic in an area that does not have much room for additional construction of new streets High-density housing (apartments) is also a source of increased crime as criminals tend to not purchase homes.
Glen Clark	2635 S. Kellogg St. Kennewick, Washington 99338 USA	Panoramic Heights	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	The increased traffic is a real concern. Also, this plan is not compatible with the single family homes in the neighborhood. Will my property value decrease? I'm afraid apartments built on the hillside would not be safe and would certainly be an eyesore.
David Long	2401 S Irving St Kennewick , WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Safety	Traffic has already become a problem without additional density
Beatte	1706 S. Fillmore St. Kennewick, Wash. 99338 USA	Creekstone	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	Kennewick is being over developed just for profit and no thought for the future. The type of development being proposed will take away the natural look of Thompson Hill. Also, it seems like there might be a future conflict of interest. Traffic is already a problem. With climate change it is uncertain how safe this land will be in 20 years for the neighborhoods below. The city of Kennewick is not keeping the promise they made to have Southridge be the impressive first view when driving into Kennewick. Don't destroy this natural area for our children and grandchildren. Once gone it is lost!

From: [Bill Dixon](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [Anthony Muai](#); [Melinda Didier](#)
Subject: CPA 2022-0005: Additional Concerns from Public Questionnaire
Date: Monday, July 25, 2022 2:05:41 PM
Attachments: [Questionnaire results 2.pdf](#)

Dear City Staff, Planning Commissioners and Council Members:

(Staff, please share with Planning Commissioners)

The Panoramic Heights Homeowners Association (PHHA) has an open public questionnaire on Comprehensive Plan Amendment CPA 2022-0005 via its website (<https://panoramicheightshoa.com/>).

This CPA proposes to change the Land Use Designation on about 40 acres at and near the top of Thompson Hill from Low Density Residential to High and Medium Density Residential.

On July 6, PHHA provided the first 26 responses received from residents around the site. Attached are 24 more responses. All respondents agreed that their responses should be supplied to the City.

These 50 responses are from numerous neighbors in the south Kennewick area. They are from:

- Panoramic Heights (20)
- South Cliffe (8)
- Windsong (5)
- Creekstone (5)
- Apple Valley (5)
- Southridge Estates (4)
- Sagecrest (2)
- Highlands (1)

All 50 responses expressed major concerns about the proposed Land Use Designation change. Their biggest concerns about this change are:

- Incompatible with existing neighborhoods (49)
- Increased traffic (49)
- Noise and light pollution (26)
- Decrease in property value (26)
- Safety (26)
- Steep slopes, runoff and landslides (24)
- Crime (23)

45 of the respondents further elaborated in their own words why they oppose this development.

They request that their input be considered in your analysis, recommendations and decision about increasing the established land use density and allowing other than single family homes on this site.

PHHA will provide further public input from this questionnaire as it becomes available.

Thank you for considering these concerns.

Bill Dixon, PHHA Lead
 509.531.5913
wtdixon3@gmail.com

Name:	Address:	Neighborhood:	What are your biggest concerns about this development:	In your own words, please share why you oppose this development:
Shane Van Den Hende	2511 S Fillmore Pl Kennewick, WA 99338 USA	Panoramic Heights	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Safety	Following the cut through of S. Kellogg and S. Irving, Panoramic Heights became a traffic nightmare; adding noise pollution, traffic safety issues and increased crime. The proposed medium and high density developments as well as a hotel, will exacerbate this issue and increase crime, traffic, and safety issues for our residents and especially our children. I am completely opposed to increased development in this area as Panoramic Heights and Creekstone will become the driveway for new residents. There must be a better way.
Darrel Duncan	5403 W 26th Street, West 26th Avenue Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods, Increased Traffic, Steep slopes/Runoff/Landslides	High density development should be placed close to arterial streets to minimize traffic concerns.
John Crosby	5300 W 25th Kennewick, WA 99348 USA	Panoramic Heights	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety	Do not like the new proposal due to the traffic. The new middle school already has given us more speeders. We have speed limit signs that show your speed and they still speed. So hopefully Gretl Crawford you still remember when I asked you your feelings about low density to high density when I ask why I should vote for you. Thanks city council John and Sheri Crosby
Shirley Griffin	2517 S Fillmore Pl Kennewick, Washington 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods, Increased Traffic, Safety	

Exhibit A-19.14

Robin Duncan	5806 W 25th Ave Kennewick, WA 99338 USA	Panoramic Heights	Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Steep slopes/Runoff/Landslides	The neighborhood streets cannot accomodate additional traffic flow. Surrounding neighborhoods are single family homes. The steep slope creates dangerous water run off and landslide possibilities.
jim and aj foster	6009 w26th ave kennewick, wa 99338 USA	Panoramic Heights	Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Steep slopes/Runoff/Landslides	10th ave east/west and Hildebrand east/west has all houses between hy 395 and Thompson hill . For traffic flow and safety low density will limit auto traffic somewhat but changes to med and hi den would increase many more autos within that rectangle and create more congestion.
Dominic Sansotta	6925 W 23rd Ct Kennewick, Washington 99338 USA	South Cliffe	Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety	I am very much concerned with the increased safety, light and noise pollution issues the hotel, and the medium and high-density housing will create. The road systems that connect to the subject land parcel are woefully inadequate to accommodate such a large increase in traffic the proposed development would generate.
Thomas Fillmore	7081, W23rd Ave Kennewick, WA 99338 USA	South Cliffe	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Safety	surrounding zoning was a large impact on why we purchased property and built our forever home where we did
Tim Fenske	6927 w 23rd ave Kennewick, Wa 99338 USA	South Cliffe	Crime, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety	Last attempt was not only not approved by city council, but was not recommended by city planning. If developer wanted to put in multi-family and businesses he should have purchased land zoned for it. Now that he believes he has a favorable City Council he is once again trying to push through.

Ann LaRiviere	7011 West 23rd Avenue Kennewick, WA 99338 USA	South Cliffe	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	Traffic and people density
Amber Morales	2269 S Belfair st Kennewick, Washington 99338 USA	South Cliffe	Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety	As a property owner I select my home based on location. Part of the location is seeing what is part of the neighborhood and how I can interact with my surroundings. With the current proposal, I will question my ability to have safe walks through the neighborhood based on increased traffic along with open up nonresident to the area. As a woman we have to constantly be aware of are surroundings and when we purchase our home it was surroundings did not include having a possible hotel or multi unit housing.
Kathy Sansotta	6925 W 23rd Ct Kennewick, Washington 99338 USA	South Cliffe	Crime,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety	When we purchased our home in 2017 we expected a certain quality of life would be maintained and preserved by the City by virtue of the existing zoning around our neighborhood. The proposed zoning changes would substantially decrease the quality of life in our neighborhood through increased traffic, congestion, noise and light pollution and crime.
john meehan	2331 S Young Ct Kennewick, WA 99338 USA	South Cliffe	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety	enough is enough building/homes/condos/hotels, etc there will be too much traffic.

Exhibit A-19.14

Arevalo	5509 w 25th ave Kennewick , Wa 993338 USA	Panoramic Heights	Apartments and home value. Increased crime	If the owner wants everyone to see the course. Build a golf course.
James Neary	5420, W 26th Ave Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	Once again we are faced with a proposed decimation of our neighborhood environment. The subtle changes being proposed don't mask the intended purpose of a massive increase in traffic that would be forced through our streets already crowded by single family housing and a new school and endangering the safety of our residents. Not opposed to adding single family homes but a hotel and the hundreds, possibly thousands of vehicles added to the mix is an absurd proposal.
Jobey Smith	2284 S Belfair St Kennewick, Wa 99338 USA	South Cliffe	Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic	
Frank Wentz	4908 W 27TH AVE KENNEWICK, Washington 993381921 USA	Panoramic Heights	Increased Traffic,Safety	How will the city manage the additional traffic? What is the plan?
Patty Wilson	5985 W 41st Ave Kennewick/Pasco (PSC), WA 99338 USA	Sagecrest	Incompatible with existing neighborhoods,Increased Traffic	
Carol Senn	3210 S Van Buren St, 3210 S Van Buren St Kennewick, WA 99338 USA	Apple Valley	Incompatible with existing neighborhoods,Increased Traffic	I feel like this is a bait and switch. Property owners purchase based on location and city planning. Not ok to change at will.

Bertha Garza	5117 W 32nd Ave Kennewick, WA 99338 USA	Southridge Estates	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution	Development in south ridge is already busy with all the homes being built in the area. With just the income based apartments in front of south ridge we have had lots of crime in our neighborhood as well.
JIM AND AJ FOSTER	6009 W 26TH AVE KENNEWICK, WA 99338 USA	Panoramic Heights	Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Steep slopes/Runoff/Landslides	THOMPSON HILL LOOKS LIKE A SPIDER WITH TOO MANY LEGS(ROADS) AND HI DENS WILL ADD TO THE PROBLEM (TOO MANY ROADS AND BLDGS CONCENTRATED IN FEW SPOTS).
Richard & Angela Weatherill	5831 W 28th Place Kennewick, WA 99338 USA	Panoramic Heights	Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution	Thompson Hill is completely inappropriate to be rezoned to medium or high density, even IF a boutique hotel is the developer's vision, rather than apartments/condos. The road system and traffic volume would be terrible for the residents on and at the base of the hill. When this matter was considered last year, residents expressed so many valid concerns that it is shocking this is still up for consideration. The only people who were in favor of changing the zoning were the developer, his lawyer, the real-estate agent that would have profited and Bill McKay who ignored the judgement of the city planning council. I pray that our elected representatives will do what is right for the residents that live on and around Thompson Hill, and not change the zoning to advance their own personal wealth and future land development leverage. Rick Weatherill

Haruko Ishii	5731 W 17th ave Kennewick, WA 99338 USA	Creekstone	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides	
Stephen Varner	5325 W 25th Ave Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods, Increased Traffic, Safety, Steep slopes/Runoff/Landslides	<p>#1 Through traffic is already a problem and would get much worse with high density zoning.</p> <p>#2 Medium to High density zoning would add a massive number of people living in a very tight area along with steep slopes - would not blend with existing low density developments.</p> <p>#3 The Developers proposal has many false statements that need to be corrected. I would not approve the proposed development until the SEPA Checklist was complete with no false statements. Example of many: Proposal states 100% buildable which is not possible with several acres well over 40% slopes per available documents. The areas not intended to be built on should be identified by location and total quantity.</p> <p>#4 Safety - Approval of the development as is would add a huge increase in through traffic. Local people would have more concerns with children and pet safety. We already have a huge issue with cars traveling Irvine Street and 25th Ave at over 35 mph in a 25mph. We have had speed tables and speed signs added to help curb the problem.</p> <p>#5 With first hand knowledge of storm drainage design to control steep slopes, it is especially difficult when there are no open areas to properly drain a steep sight. The proposed development does not specify any solutions or that there would be any issues at all. The developers plan for storm drainage should be included in the proposal.</p>

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To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [Anthony Muai](#); [Melinda Didier](#)
Subject: CPA 2022-0005: More Public Concerns About Proposed Thompson Hill Development
Date: Friday, August 12, 2022 4:35:29 PM
Attachments: [Questionnaire results 3.pdf](#)

Dear City Staff, Planning Commissioners and Council Members:

(Staff, please share with Planning Commissioners)

The Panoramic Heights Homeowners Association (PHHA) has an open public questionnaire on Comprehensive Plan Amendment CPA 2022-0005 via its website (<https://panoramicheightshoa.com/>).

This CPA proposes to change the Land Use Designation on about 40 acres at and near the top of Thompson Hill from Low Density Residential to High and Medium Density Residential.

Previously, we provided the first 50 responses received from residents around the site. Attached are 36 more responses. All respondents agreed that their responses should be sent to the City.

These 86 responses are from numerous neighbors in the south Kennewick area. They are from:

- Panoramic Heights (32)
- South Cliffe (11)
- Creekstone (11)
- Apple Valley (10)
- Southridge Estates (10)
- Windsong (5)
- Other neighborhoods (7)

All 86 responses expressed major concerns about the proposed Land Use Designation change. Their greatest concerns about this proposed change are:

- Increased traffic (97%)
- Incompatible with existing neighborhoods (86%)
- Noise and light pollution (60%)
- Decrease in property value (57%)
- Safety (57%)
- Steep slopes, runoff and landslides (53%)
- Crime (52%).

Most of the respondents further elaborated in their own words why they oppose this development. Please take a few minutes to read these personal, heartfelt concerns in the attachment.

They request that their input be considered in your analysis, recommendations and decision about increasing the established land use density and allowing other than single family homes on this site.

PHHA will provide further public input from this questionnaire as it becomes available.

Thank you for considering these concerns.

Bill Dixon, Panoramic Heights Lead
509.531.5913
wtdixon3@gmail.com

Exhibit A-19.15

Name:	Address:	Neighborhood:	What are your biggest concerns about this development:	In your own words, please share why you oppose this development:
Clint Whitney	6899 W 23rd Ave Kennewick , WA 99338 USA	South Cliffe	Incompatible with existing neighborhoods,Steep slopes/Runoff/Landslides	
Bruce Boyum	5908 W 26th Ave Kennewick, Wa 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	Due to its position high on the hill above our neighborhood, this development will be very visible to us and the entire TriCities. Having a high density development in this position is concerning from a visual standpoint as well as noise and light pollution and traffic flow.
Anthony Hausner	6150 W 33rd Avenue Kennewick, WA 99338 USA	Southridge Estates	Crime,Increased Traffic,Noise & Light Pollution	There is enough current development underway for the foreseeable future. High density housing will result in an increased traffic flow requiring traffic circles on Hildebrand to allow current residents reasonable access.
Judy Chambers	1501 S. Taft St. Kennewick , WA 99338 USA	25th and Taft	Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	The density of housing and the multi family housing is not compatible with the housing already there, a hotel in the middle of residential housing is not appropriate. Traffic would be a nightmare
Ron Mabry	2525 South Irving Street Kennewick , Wa 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods,Increased Traffic,Safety,Steep slopes/Runoff/Landslides	Proper preparations have not been taken for this development. Many short steps and side steps have been attempted to avoid compliance. Information has been withheld. Infrastructure will be strained including streets, gas lines, and schools. Cooperation with neighboring areas have been continuous without reason.
Melanie Schmitt	3203 S Wilson St Kennewick, WA 99338 USA	Apple Valley	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	
Jose Chavez	5319 W 25Th Ave US Kennewick, WA 99338 USA	Panoramic Heights	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	I love my neighborhood and have enjoyed living here for a few years now. I would be devastated for the increased traffic through our residential streets as we already have a lot of vehicular traffic traveling this area and it would be detrimental for all of us.

Exhibit A-19.15

Susan Sievers	2507 S Edison Place Kennewick, Washington 99338 USA	Panoramic Heights	Crime,Increased Traffic,Safety,Steep slopes/Runoff/Landslides	We do not want the safety of our children effected by extra traffic . We have noticed the speed has been a problem already with adjacent neighborhoods have shared.
Mary Weir	610 W. 26th Place Kennewick , WA 99337 USA	I live off Garfield	Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	Our area is growing rapidly and I realize we need new housing, but the extra traffic and eye pollution would be horrible. We are starting to look like California with the development of our hillsides and tops. The peacefulness of seeing open hillsides and crests is starting to be gone. This was a nice thing of our area. We pray in good faith that the city officials of whom many have construction backgrounds, understand the instability of soil mechanics on hillsides. I remember when this came up before at a hearing that the land had been bought so it could be developed even though it was not zoned for that. Iâ€™m assuming the builder had thought he could get it changed. That is the chance he decided to take. No guarantee. Hopefully the ones making this decision are reminded about it. We must protect our beautiful hillsides.
Scott Chambers	1501 South Taft St. Kennewick, WA 99338-1462 USA		Decrease in property value,Increased Traffic,Noise & Light Pollution,Steep slopes/Runoff/Landslides	CPA 2002-0005 strikes me as the epitome of development for the sake of the few, with negative impact for the many (who live downhill from Thompson Hill). Medium- or high-density development runs contrary to a commitment made by the city council some years ago to not urbanize Thompson Hill. The increase in traffic, decrease in property values, light an noise pollution, and runoff issues that are likely to accompany this development are going to be detrimental to everyone who lives in the shadow of Thompson Hill, and these perons far out number those who would benefit from the development.
Karen Allmann		Apple Valley	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	
Danielle Knudson	5503 W 11th Ave Kennewick, WA 99338 USA	Creekstone	Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Steep slopes/Runoff/Landslides	Overcrowding is a huge problem. We don't need to jam up traffic. That hill is not safe for more buildings or houses. It's going to destroy existing family's homes. It's really sad and preventable.

Exhibit A-19.15

Ann LaRiviere	7011 West 23rd Avenue Kennewick, WA 99338 USA	South Cliffe	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	
David Long	2401 S. Irving St Kennewick, Washington 99338 USA	Panoramic Heights	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety	Proposed development will radically impact neighborhoods and existing infrastructure. Traffic has already become a safety issue for residents.
Michael Strauss	3944 S Lincoln St Kennewick , Washington 99338 USA	Southridge Estates	Crime,Incompatible with existing neighborhoods,Increased Traffic,Safety	This type of development is incompatible with existing and future residential development in the area. A Hotel is actually laughable. Traffic on the hill will be extremely high.
Kimberlee Leonard	1810 S. Dawes Street Kennewick , WA 99338 USA	Union West	Crime,Decrease in property value,Increased Traffic,Noise & Light Pollution,Safety	My husband and I moved from Canyon Lakes in 2017 due to the increase of traffic along Canyon Lakes Dr. As of yesterday, I was informed over 7k vehicles drive through CL daily, via Ely to 395. This same senerio will occur through our neighborhood and along Creekstone Drive if these projects are allowed to continue. We vote NO. We're already seeing increases in crime and traffic. Do not add to these increases or put our Irrigation amounts in jeopardy by increasing the number of residents in our area.
Michelle Porter	5542 W 28th Ave Kennewick, WA 99338 USA	Southridge Estates	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety	Families have purchased homes in the above neighborhoods because of their desire to live in those specific communities. Adding hotels and multi-family dwellings would change them - they would change the feel of the area, dramatically increase traffic on neighborhood streets, increase crime (more people), and decrease property values.
Diane Steele	5510 W 18th Ave Kennewick, WA 99338 USA	Creekstone	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Safety,Steep slopes/Runoff/Landslides	It's shocking to me to see the desecration of the beautiful hills now increasingly filled up with homes and possibly high density housing. There's got to be a better location....

Exhibit A-19.15

Ashley Smith	5308 W 26th Ave Kennewick, WA 99338-1911 USA	Panoramic Heights	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety	We recently had a traffic study directly in front of my home, there are already drivers going 59mph on these small neighborhood streets. We have two small children and children across the street. This development and the unsafe amount of traffic that will be funneled into our neighborhood is criminal. Chinook Middle school students are being put in an unsafe environment walking home. The traffic MUST be funneled out towards Bob Olson parkway. What will it take? How many children need to be splattered across the street before anyone will LISTEN TO US RESIDENTS!!!
Jacque Fuller	7009 W. 23rd Court Kennewick, WA 99338 USA	South Cliffe	Crime, Decrease in property value, Increased Traffic, Noise & Light Pollution	The south side of the hill connecting via Sherman is just beginning to build out and already there are the beginning traffic issues at the corner of Sherman and Bob Olson Pkwy. Not only is this corner busy because of cross traffic but due to the curve in the road to the east, it is becoming a dangerous turn out. I can't even imagine a feasible way to handle this traffic if a portion of this hill was to transition to Med/High density. This is sure to be a situation looked back upon to wonder what officials had as their priority when approving a zoning change. Want to check it out? Spend some time watching in late afternoon, early evening.
Conrad Morrow	5328 W. 26th Ave. Kennewick, WA 99338 USA	Panoramic Heights	Decrease in property value, Increased Traffic, Noise & Light Pollution	The biggest reason why I am absolutely opposed to this development is because of the impact it will have on traffic in Panoramic Heights. I live on 26th Ave, where there is already significant traffic. I oppose any development on Thompson Hill that involves 26th Ave or 25th Ave as access routes. Any development on Thompson Hill should only be accessed from Bob Olson Parkway. My secondary concerns the negative impacts on property values and noise/light pollution.
Dean Kunigisky	6015 W. 20th Avenue Kennewick, Washington 99338 USA	Creekstone	Incompatible with existing neighborhoods, Steep slopes/Runoff/Landslides	The original geological survey made it clear that the ground in this area was not stable enough to support high density building. I do not feel that we need to amend the zoning just so some developer can make money. If the city needs more tax revenue, there are other places better suited to building. We don't need a "scenic" hotel.

Exhibit A-19.15

Kathy Otheim	7044 W. 33rd Place Kennewick , WA 99338 USA	Apple Valley	Crime,Decrease in property value,Incompatible with existing neighborhoods,Increased Traffic,Noise & Light Pollution,Safety,Steep slopes/Runoff/Landslides	Thompson Hill is one of the most visible parts of Kennewick. When entering Kennewick from Highway 82 visitors are greeted with the Badger Canyon apartment complex on Ridgeline Drive with row after row of apartment buildings. It is a huge unappealing eyesore on a hill. Now you want to bookend this by taking another highly visible prime location to be changed to high density zoning that would allow apartments to loom over Kennewick. I am not against much needed high density housing. I am strongly opposed to the location. The hills and river are the main points of beauty in our desert landscape. They should not be used for multiplexes. Furthermore, because of location on a hill the light and noise from high density living would have far reaching impact on the surrounding area where it is visible for miles and the sound will carry. There are also limited exit/entrance points that would greatly increase traffic through areas we as neighbors did not expect to have as much congestion. This development is a major concern for my family and one of the key factors on who we will vote for in upcoming elections.
Sara Elkington	5807 W. 25TH AVE KENNEWICK, WA 99338 USA	Panoramic Heights	Increased Traffic,Noise & Light Pollution	I'm Concerned over the amount of traffic this change would bring us. Already, last spring, our dog was hit by a school bus in front of our house.
Bill Fulwyler	5302 W 15th Ave Kennewick Wa Kennewick, WA 99338 USA	Creekstone	Crime,Decrease in property value,Increased Traffic,Noise & Light Pollution,Safety	because of the items I listed above
Kaye Gustafson	5732 w 17th Ave kennewick, WA 99338 USA	Creekstone	Crime,Increased Traffic,Noise & Light Pollution,Safety	

Exhibit A-19.15

Jamie Luce	5005 W 32nd Ave Kennewick, Washington 99338 USA	Southridge Estates	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety	<p>The peaceful neighborhood we moved into 10 years ago has begun to deteriorate. The traffic noise from cars with aftermarket mufflers and ambulance sirens are bothersome. The increased traffic on Hildebrand makes it difficult to get out of our neighborhood during specific times of the day. Our home and car were broken into recently with many possessions taken from us. KID service is already subpar and if the new development will be using KID, I can only imagine it will further decrease their performance/service.</p> <p>The high density plan being built in close proximity will only magnify the above concerns I already have with our once peaceful neighborhood.</p> <p>I am in HUGE support of the low density plan.</p>
Anita Booth	5506 W 19th Ave Kennewick, Washington 99338 USA	Creekstone	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides	All of the above
Richard & Kevelene Marston	5575 W 32nd Ave Kennewick, Wa 99338 USA	Southridge Estates	Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Steep slopes/Runoff/Landslides	How can a residential area be changed to include a hotel after homes are already well established. We weren't advised of this possibility when this home was purchased either. How are the slopes being secured? How will the increased traffic be handled? I've already witnessed a school bus being hit while it was stopped. The street racing is insane. Our home values, especially those with the view lots, will tank because who wants a hotel in your backyard let alone squeezed in houses.
Mike Greif	5410 W. 28th Ave. Kennewick, WASHINGTON 99338 USA	Southridge Estates	Increased Traffic, Noise & Light Pollution, Safety	I live on 28th and traffic is already increasing with each development. No speed bumps, and dangerous speeders.

Exhibit A-19.15

Fran Handy	2513 South Harrison Place Kennewick, WA 99338 USA	Panoramic Heights	Incompatible with existing neighborhoods, Increased Traffic	Families who bought homes in this area considered the current zoning as a factor. Totally unethical to change zoning to something so opposite to what was in place at the time. Developer should choose an area that is already zoned to his specifications. How many times do we have to go through these procedures? The majority of homeowners has already spoken at least twice on this issue. Enough. The governing people should heed the wishes of their constituents.
Glenna Gale	6008 W 26th Ave Kennewick, WA 99338 USA	Panoramic Heights	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides, As a homeowner it is an earned privilege to enjoy some peace and quiet and privacy on my property. I love my neighbors but the absolute horror of apartments and too many homes ruins my peace of mind. Too many houses and apts. decrease the quality of life as a homeowner and value of my home!	See above
Don Gale	6008 w 26th Ave Kennewick , Wa 99338 USA	Panoramic Heights	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides	
Debbie Larson	7032 W 3rd Pl Kennewick, WA 99338 USA	Apple Valley	Crime, Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides	All of my concerns are checked above. Most of concern is the potential for increased crime with this amount of housing and therefore the safety of the existing neighborhoods.

Ellen Caristo
 7068 W 31st Place
 Kennewick, WA 99338 USA

Apple Valley

Crime, Decrease in property value, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides

We don't need more apartments. the traffic is already a joke. racing on Bobo Olsen is scary. What we need are a gas station, a Yokes, but no more housing. There is enough going on here. The slides are already happening and water issues. Some block walls have already fallen over. The last thing we need is a big apartment building. Besides the fact that there is more and more apartments being built on Ridgeline and Coldfelter area. Why do we need more? the fact that that guy just wants something to make himself some money is not good for the neighborhood. We don't need added traffic, added people, and danger due to slides and water issues. The good of the whole neighborhood should be what the board considers not what one guy wants. We all pay our taxes and try to keep our properties up, the more apartments, that have non owners brings trash and bad things and that is a fact. Please consider that this is an up-and-coming neighborhood, and we don't need more apartments at all. Please keep this community as a nice neighborhood without a lot of trouble in it. The traffic is bad enough and with the high school in session it gets worse. we do not need to add to the traffic and lights and roundabouts will not do it if we add 550 apartments and we all know that. Remember we are all the ones that vote and pay taxes and do what we can to keep our city nice and clean and safe. The safety of children and them being able to play outside is a consideration also. There are houses being built that are on hold now, so why would we allow apartments so they can sit empty or fill them instead of nice family homes. We don't need apartments and that guy up there need to get over it. If he is not happy sell and move on, leave our area alone we don't need a hotel, apartments or more traffic. the hill looks so beautiful now and it is a landmark for all of us. When my g come to town, seeing Thompson hill is there sign that they are close to NANA's house.

Dawn Thomas	5216 W. 26th Ave Kennewick, WA 99338 USA	Panoramic Heights	<p>Decrease in property value, Incompatible with existing neighborhoods, Increased Traffic, Noise & Light Pollution, Safety, Steep slopes/Runoff/Landslides, You cannot rezone an area where everyone currently living there bought homes BECAUSE of the existing zoning. This is the second time this has become an issue in a year, nothing has changed; except elected officials, This smacks of self promotion of self interest, & conflict of interest. Who benefits? Realtors & developers.. now Board members. The community who lives here, pays the taxes, votes... were in agreement & gave a resounding NO! when this was proposed last year. This is a corrupt use of taxes to revisit this again.</p>	<p>This makes me so frustrated. If you are positioned in a place of responsibility are you a part of the problem? You cannot rezone an area where everyone currently living there bought homes BECAUSE of the existing zoning, What then would be the value or purpose of these zones if they can be changed even if everyone there objects? This developer bought an existing property & land in a determined zone & IMMEDIATELY declared his intention to ignore the zoning, He began work that he knew was not legal; he pushed against rules & regulations, deliberately looked to destroy housing association covenants & now we are being told that by using these despicable bully tactics he will get his way?! This will not do! What is the point of housing associations, rules & regulations if those who don't want to follow them don't have to? This is lawlessness. What is the point or use of city officials; planning departments; local government etc if they do not attend to & protect the wishes & properties of the private individuals who pay their wages through local taxes? If they do not maintain the status quo of settled; established communities? If they do not follow through on existing regulations already in place? Corporations, companies, should not over ride the will of the people in a community to the extent that the individual is robbed of the ability to live peaceably in their homes and neighborhoods. We faced & argued & had an agreement just last year that this rezoning, high density development & commercial usage is not wanted, not appropriate & not considered safe for the infra structure around us. Nothing has changed; except elected officials who suspiciously stood for local government seats & who happen to be private developers & realtors. This smacks of self promotion of self interest. Elected representatives who promote & pass business that will benefit them is corruption.</p> <p>It is obvious that they are not working for the Southridge communities. This is all very questionable.</p>
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From: [Bill Dixon](#)
To: [Anthony Muai](#)
Cc: [Bill Mckay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com; [Melinda Didier](#); [GERALD BERGES](#)
Subject: Comprehensive Plan Amendment 2022-0005: Panoramic Heights Homeowners Association Request for Action on the Mitigated Determination of Non-Significance
Date: Tuesday, August 23, 2022 6:54:00 AM
Attachments: [PHHA letter on MDNS for CPA 2022-0005.pdf](#)
[PHHA letter 1 on CPA 2022-0005 \(2\).pdf](#)
[PHHA Comments 1 on CPA 2022-0005 \(1\).pdf](#)

Dear Mr. Muai,

I am representing the 159 households in the Panoramic Heights Homeowners Association (PHHA) on Comprehensive Plan Amendment (CPA) 2022-0005.

PHHA requests that you withdraw or modify the Mitigated Determination of Non-Significance (MDNS) on CPA 2022-0005. The State Environmental Policy Act (SEPA) Checklist provided by the Applicant for CPA 2022-0005 is incomplete for the following reasons:

- The SEPA Checklist responses do not contain the information needed “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). There are no substantive responses about impacts and possible mitigations.
- The SEPA Checklist responses do not “apply to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The responses only cover proposed administrative changes to the Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.
- The Applicant simply answers most of the questions about impacts and mitigation as “Non-Project Action, NPA” or “None”. “SEPA Guidance for Non-Project Actions” states:

“When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

There are at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially. All seven of the questions specifically required for Non-Project Actions (Section D) were not answered at all.

We note that in a prior Application (CPA 20-06) in 2020 for essentially the same site,

the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City required the Applicant to revise that Checklist to address its inadequacies before issuing an MDNS on CPA 20-06. The same action should be taken on CPA 2022-0005.

Otherwise, the scope of the MDNS is undefined. The Applicant never identifies future development that would be allowed under the proposed Land Use Designation change and the probable impacts. Although the City speculates that future development might involve a hotel and several hundred condominiums, in fact the Application and SEPA Checklist never mention either nor address the probable, significant adverse environmental impacts from such development.

Further, PHHA recommends Mitigation Conditions for these specific environmental impacts, as required by SEPA, which the Applicant failed to address:

- Relationship to existing land use plans and to estimated population,
- Vehicular traffic,
- Aesthetics,
- Noise, and
- Habitat for and numbers or diversity of species of wildlife and unique species.

Details are contained in the attached letter and our previous letter on this subject dated June 23, 2022.

Please share this request with the Planning Commissioners.

Thank you for considering our request to ensure that your final Environmental Determination is valid based on complete information.

Bill Dixon, Representing the Panoramic Heights Homeowners Association
2500 S. Irving Street
Kennewick, WA 99338
wtdixon3@gmail.com

**Comprehensive Plan Amendment (CPA) 2022-0005
Revised Mitigated Determination of Non-Significance (MDNS)
Dated August 10, 2022**

**Panoramic Heights Homeowners Association (PPHA)
Request for Action
August 23, 2022**

ACTION REQUESTED: Per Washington Administrative Code (WAC) 197-11-340(2)(f), the Panoramic Heights Homeowners Association (PPHA) requests that the City withdraw or modify the Revised Mitigated Determination of Non-Significance (MDNS) issued for public comment on August 10, 2022.

The requested actions are needed to clarify the scope of the MDNS and to add further Mitigation Conditions to address probable, significant, adverse environmental impacts.

SCOPE OF THE MDNS: The scope of the MDNS is undefined. This is because the Applicant has failed to identify the future development that would be allowed by this Land Use Designation change and the associated impacts. The State Environmental Policy Act (SEPA) regulations and Checklist guidance are very clear that the impacts of future development from a Comprehensive Plan Amendment must be considered.

As previously stated in detail in our letter of June 23, 2022 (attached), the Application and the SEPA Checklist submitted for CPA 2022-0005 do not meet SEPA requirements (WAC 197-11) and guidance. These require the Applicant to submit a completed SEPA Checklist that fully describes the proposal, the potential impacts from developing this land as proposed, and possible mitigating measures.

In summary:

- The SEPA Checklist responses do not contain the information needed “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). There are no substantive responses about impacts and possible mitigations.
- The SEPA Checklist responses do not “apply to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The responses only cover proposed administrative changes to the

Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.

- The Applicant simply answers most of the questions about impacts and mitigation as “Non-Project Action, NPA” or “None”. “SEPA Guidance for Non-Project Actions” states:

“When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

As submitted, there are at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially. All seven of the questions specifically required for Non-Project Actions (Section D) were not answered at all.

We note that in a prior Application (CPA 20-06) in 2020 for essentially the same site, the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City responded at that time (in a letter from Steve Donovan to Jose’ Chavallo, “Request for Additional SEPA Checklist Information”, dated August 28, 2020):

“The above reference WAC (*Washington Administrative Code 197.11*) and GMHB (*Growth Management Hearings Board*) cases clearly state that answering questions in an environmental checklist for a non-project action with a reference to the fact that the proposal is a non-project action is not sufficient.”

“You must consider your request and the resulting zoning (if approved) and what is permitted within the new zoning district; responding to the questions more specifically and providing likely impacts such as traffic generated by a likely proposal and possible mitigation to address the likely impacts. The response “NPP” is not acceptable and must be changed to reflect the previous comments.”

The City identified 40 checklist questions in that Application that needed additional details or clarification.

The same should be required this time.

Instead, the City has chosen to reference some of the documents from CPA 20-06 without requiring the Applicant to identify specifically the possible future uses of the site, as was done eventually for CPA 20-06.

Nowhere in the CPA 2022-0005 documents does the Applicant mention potential future uses, although the City seems to be assuming that this may involve a hotel and several hundred condominiums.

The City should require the Applicant to submit a completed SEPA Checklist that identifies potential future uses of the site and answers all relevant questions accordingly, including potential impacts. Only then could the City make a valid and complete Environmental Determination on CPA 2022-0005.

MITIGATION CONDITIONS: PHHA recommends that the following Mitigation Conditions be added to the final Environmental Determination. These Conditions are needed to address impacts to the following elements of the environment (per WAC 191-11-444) that are not adequately addressed. The relevant questions in the SEPA Checklist (in quotes) were not answered at all or substantively.

1. **“Relationship to existing land use plans and to estimated population”** per WAC 197-11-444(2)(b)(i).
 - “B.8.a. Will the proposal affect current land uses on nearby or adjacent properties?” Not answered.
 - “B.8.I. Proposed measures to ensure the proposal is compatible with existing and projected land use and plans?” Answer: “Amend City of Kennewick Comprehensive Zoning designation.” This does not ensure compatibility.
 - “B.9.a. “Approximately how many (*housing*) units will be provided?” Answer: “NPA (*Non-Project Action*).” This response avoids answering the question.

Note that in CPA 20-06 the City previously determined that High Density Residential was incompatible with all existing and planned Low Density Residential neighborhoods that border the site.

Also, the maximum number of allowable housing units would increase from about 153 single-family homes for Low Density Residential to 557 multi-family housing units for High and Low Density Residential. This is a factor of 3.6 in allowable housing density.

The City should impose a Mitigation Condition to ensure compatibility with surrounding neighborhoods.

2. **“Vehicular traffic”** per WAC 197-11-444(2)(c)(ii).

- “B.14.d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities?”
Answer: “NPA”. This response avoids answering the question.
- “B.14.f. How many vehicular trips per day would be generated by the completed proposal...?” Answer: “NPA”. This response avoids answering the question.
- “B.14.h. Proposed measures to reduce or control transportation impacts?”
Answer: “NPA”. This response avoids answering the question.
- “D.6. How would the proposal be likely to increase demands on transportation or public services and utilities?” No answer.

Note the number of maximum allowable housing units would more than triple, with corresponding increases in traffic. Yet the Applicant failed to provide any information in the CPA 2022-0005 Application on transportation and traffic impacts.

In the MDNS, the City proposes four Mitigation Conditions (conditions 2, 3, 4 and 11) to address transportation and traffic impacts. PHHA agrees with these Conditions.

However, PHHA recommends that Condition 11 be amended to require that the traffic impact analysis address the additional impact of traffic from CPA 2022-0005 in conjunction with the pertinent traffic data and impact analyses from all nearby neighborhoods and planned developments. This would provide a complete prediction of the future local traffic situation in this area for the public and decision-makers.

Further, PHHA notes that cut-through traffic in existing neighborhoods from other new Southridge Area developments (including schools) has become an increasingly worse problem. Therefore the CPA 2022-0005 traffic impact analysis should include the potential impacts of cut-through traffic in other neighborhoods. If needed, traffic calming measures should be required to avoid making these problems even worse.

3. **“Aesthetics”** per WAC 197-11-444(2)(b)(iv).

- “10.a. What is the tallest height of any proposed structure?” Answer: “NPA”. This response avoids answering the question.
- “10b. What views in the immediate vicinity would be altered or obstructed?” Answer: “None”. This response avoids addressing this obvious impact of higher density development on the top of Thompson Hill.
- “10.c. Proposed measures to reduce or control aesthetic impacts?” Answer “N/A”. This response avoids answering the question about obvious impacts.

The proposed changes in Land Use Designation would allow high and medium density development of large structures up to 45 feet tall. This would impact the views from surrounding neighborhoods. Also, it would change the iconic view of Thompson Hill from throughout the Tri-Cities area.

PHHA recommends a mitigation condition to limit the height of structures on the top of Thompson Hill to no more than 35 feet. Further, to preserve the iconic views of Thompson Hill, large structures should be prohibited on the ridgeline.

4. **“Noise”** per WAC 197-11-444(2)(a)(i).

- “7.b.2. What types and levels of noise would be created by or associated with the project on a short-term or long-term basis?” Answer: “NPA”. This response avoids answering the question.
- “7.b.3. Proposed measures to reduce or control noise impacts?” Answer: “None.” This answer avoids addressing a probable significant impact.

In the past, there have been many noise problems and complaints with prior activities on the top of Thompson Hill. A large complex of multi-family housing units with open spaces and outdoor recreation will create noise problems. Further, if a hotel is built, those noise problems could significantly worsen.

PHHA recommends that the City impose a Mitigation Condition for strict compliance with KMC 9.52 “Noise”.

5. **“Habitat for and numbers or diversity of species of wildlife and unique species”** per WAC 197-11-444(1)(d)(i and ii).

- “B.5.d. Proposed measures to preserve or enhance wildlife?” Answer “None”.

The City has identified the site as part of a Critical Area for wildlife habitat conservation per KMC 18.63. Therefore compliance with KMC 18.63 should be a Mitigation Condition.

NEW INFORMATION: Any new information provided by the Applicant or generated by the City should be provided for public review at least 14 days before the public hearing. This will allow the public time to read, understand and comment on any such new information.

Thank you.

Attachments: PHHA Letter on CPA 2022-0005 dated June 23, 2022, with its attachment.

Panoramic Heights Homeowners Association
www.panoramicheightshoa.com

June 23, 2022

Dear Kennewick City Council Members, Planning Commissioners, and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

I am writing on behalf of the 159 families who are part of the Panoramic Heights Homeowners Association (PHHA). I have also delegated authority to Mr. Bill Dixon to represent PHHA in this matter.

PHHA has done a thorough review of the Application and its attachments for Comprehensive Plan Amendment (CPA) 2022-0005 dated April 21, 2022.

PHHA finds the CPA 2022-0005 Application to be incomplete, inaccurate, and in some cases false.

This Application is not in compliance with the City of Kennewick's requirements, as stated on the Application General Form. Further, this application does not meet the State law and implementing regulations under that State Environmental Policy Act (SEPA) to provide true, accurate and complete information about the proposed action, potential environmental impacts, and appropriate mitigating measures.

The City should require this Application to be revised (per WAC 197-11-100 "Information required of applicants") to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for the City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation to City Council; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

PHHA's specific comments and concerns about the inadequacies in this Application are discussed in detail in the attachment.

In summary, the Application does not meet the requirement for a completed SEPA Checklist as required in SEPA regulations (WAC 197-11) and Guidance. Specifically:

- The SEPA Checklist responses do not contain the information needed “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). There are no substantive responses about impacts and possible mitigations.
- The SEPA Checklist responses do not “apply to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The responses only cover proposed administrative changes to the Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.
- The Applicant simply answers most of the questions about impacts and mitigation as “Non-Project Action, NPA” or “None”. “SEPA Guidance for Non-Project Actions” states “When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

As submitted, there are at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially. All seven of the questions specifically required for Non-Project Actions (Section D) were not answered at all.

We note that in a prior Application (CPA 20-06) in 2020 for the same site, the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City responded at that time (in a letter from Steve Donovan to Jose’ Chavallo, “Request for Additional SEPA Checklist Information”, dated August 28, 2020):

“The above reference WAC (*Washington Administrative Code 197.11*) and GMHB (*Growth Management Hearings Board*) cases clearly state that answering questions in an environmental checklist for a non-project action with a reference to the fact that the proposal is a non-project action is not sufficient.

You must consider your request and the resulting zoning (if approved) and what is permitted within the new zoning district; responding to the questions more specifically and providing likely impacts such as traffic generated by a likely proposal and possible mitigation to address the likely impacts. The response

“NPP” is not acceptable and must be changed to reflect the previous comments.”

The City identified 40 checklist questions in that Application that needed additional details or clarification.

The same should be required this time.

Without this required information, the public has not been fully informed, the City Staff and Planning Commissioners can not make informed recommendations, and the City Council can not make an informed decision in the best interests of the citizens of Kennewick.

Thank you for considering our request to ensure a complete understanding of the proposed action, its impacts, and possible mitigation measures for CPA 2022-0005.

Sincerely,

Gerald Berges, PHHA President
5311W. 25th Avenue
Kennewick, WA 99338

Email: berges6@aol.com

Attachment:

Comments and Concerns About Proposed Comprehensive Plan Amendment
(CPA) 2022-0005 Completeness and Accuracy

PANORAMIC HEIGHTS HOMEOWNERS ASSOCIATION
COMMENTS AND CONCERNS ABOUT
PROPOSED COMPREHENSIVE PLAN AMENDMENT (CPA) 2022-0005
COMPLETENESS AND ACCURACY

June 23, 2022

The Panoramic Heights Homeowners Association (PHHA) has done a thorough review of the Application and its attachments for Comprehensive Plan Amendment (CPA) 2022-0005 dated April 21, 2022.

PHHA finds the CPA 2022-0005 Application to be incomplete, inaccurate, and in some cases false.

This Application is not in compliance with the City of Kennewick's requirements, as stated on the Application General Form. Further, this application does not meet the State law and implementing regulations under that State Environmental Policy Act (SEPA) to provide true, accurate and complete information about the proposed action, potential environmental impacts, and appropriate mitigating measures.

The City should require this Application to be revised (per WAC 197-11-100 "Information required of applicants") to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

PHHA's specific comments and concerns are discussed in detail below.

SEPA ENVIRONMENTAL CHECKLIST REQUIREMENTS (per WAC 197-11-960)

Purpose: The purpose of the checklist is:

“to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts or if an environmental impact statement will be prepared to further analyze the proposal.” (emphasis added)

The CPA 2022-0005 checklist does not contain the information needed to make this determination. Detailed examples are below.

Instructions for applicants: These instructions state:

“The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time...” (emphasis added)

The Applicant states the only action requested is to “Amend Comprehensive Plan” and does not address the impacts of this action from the resulting possible future development of the land.

Instructions for Lead Agencies: These instructions state:

“Additional information may be necessary to evaluate the existing environment, all interrelated aspects of the proposal and an analysis of adverse impacts. The checklist is considered the first but not necessarily the only source of information needed to make an adequate threshold determination. Once a threshold determination is made, the lead agency is responsible for the completeness and accuracy of the checklist and other supporting documents.” (emphasis added)

The City should request and analyze the additional information needed (per WAC 197-11-100 “Information required of applicants”), and provide that information for public review and comment. Until the Application is complete, the City should NOT issue a threshold determination and thereby assume responsibility for a SEPA checklist that is clearly incomplete, inaccurate, and not in compliance with SEPA guidance.

Non-Project Action: The Applicant proposes “2022 Comprehensive Plan Amend (sic) from Low Density Residential to Medium and High Density Residential...Submitted as a Non-Project Action, NPA”.

Note that the SEPA Checklist Guidance, section D, Non-Project Actions states:

“Non-project actions are governmental actions involving decisions about policies, plans, or programs containing standards for controlling use or modifying the environment, or will govern a series of connected actions. Non-project action analysis provides an opportunity to evaluate planned actions before projects begin and permits applications are prepared. “

“If the non-project action is a land-use decision or similar proposal that will govern future project development, the probable impacts need to be considered of the future development that would be allowed. For example, environmental analysis of a zone designation should analyze the likely impacts of the development allowed within that zone.” (emphasis added)

And the Guidance for “Non-project actions: Comprehensive plans, future project development” specifically states:

“ When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

This Application is clearly inconsistent with this SEPA Guidance. The Applicant avoids answering most of the SEPA Checklist questions about subsequent actions, potential environmental impacts, and possible mitigations; mostly by simply responding “NPA” or “none”.

These omissions include (by checklist section number):

A.7: Plans for future additions, expansion, or further activity related to or connected with this proposal.

A.11: Complete description of the proposal, including proposed uses.

A.12: A vicinity map and a topographic map.

B.1.e: Filling, excavation and proposed grading.

B.1.f: Erosion potential.

B.1.g: Impervious surface area.

B.1.h: Erosion measures and controls.

B. 2.a: Air emissions.

B.2.c: Air emissions measures and controls.

B.3.c: Water runoff.

B.3.d: Water runoff measures and controls.

B.4.b: Vegetation removed or altered.

B.4.d: Measures to preserve or enhance vegetation.

B.5.d: Measures to preserve or enhance wildlife.

B.6.a: Kinds of energy to be used.

B.6.c: Energy conservation measures.

B.7.b: Noise and measures to control it.

B.8.a: Current use of nearby and adjacent properties and effect upon them.

B.8.i: Number of people who would work or reside on site.

B.8.l. Measures to ensure compatibility with existing and projected land uses and plans.

B.9.a: Number of housing units provided.

B.9.c: Measures to reduce or control housing impacts.

B.10.a: Tallest height of structures.

B.10.b: Views in immediate vicinity altered or obstructed.

B.10.c: Measures to control aesthetic impacts.

B.11.a: Light or glare produced.

B.11.b. Light safety hazards or interference with views.

B.11.d: Measures to reduce or control light and glare impacts.

B13.d: Measures to avoid, minimize or compensate for loss, changes to, and disturbance of cultural resources.

B.14.a: Proposed access to the existing street system, and shown on site plans.

B.14.c: Parking spaces.

B.14.c: New or improved roads, streets, pedestrian, bicycle, or state transportation facilities needed.

B.14.f: Vehicle trips per day generated and peak volumes.

B.14.h: Measures to reduce or control transportation impacts.

B.15.a: Increased need for public services.

B.15.b Measures to reduce or control direct impacts on public services.

B.16.b: Utilities proposed and construction needed.

Further, the Applicant failed to answer ANY of the questions in Section D. "Supplemental Sheet for Non-Project Actions". This section is required to be completed for Non-Project Actions.

Instructions state:

"When answering these questions, be aware of the extent the proposal, or types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. (Emphasis added)

Questions about impacts that were not answered should have addressed:

1. Likely increases to water discharges, air emissions, and noise; and proposed measures to avoid or control them.
2. Likely effect on plants and animals; and measures to protect or conserve them.
3. Likely depletion of energy and natural resources; and measures to protect or conserve them.

4. Likely use of or effect on environmentally sensitive areas or areas designated for government protection (*such as Critical Areas for Steep Slopes*); and measures to protect, avoid or reduce them.
5. Likely effect on land use including incompatibility with existing plans; and proposed measures to reduce or avoid land use impacts.
6. Likely increased demands on transportation or public services and utilities; and proposed measures to reduce or respond to such demands.
7. Whether the proposal may conflict with local, state, or federal laws for protection of the environment.

Other SEPA Checklist Issues:

Section A.12 states that 35.45 acres is proposed for Medium Density Residential. Yet the referenced “Area Map” shows 33.93 acres, a difference of 1.52 acres. Which is it?

The Area Map also has a cross-hatched area in the land proposed for Medium Density Residential. What does this mean about proposed land use in that area?

The Applicant selected the answer “hilly” as the general description of the site rather than the correct selection of “steep slopes”. The Applicant also states that the steepest slope on the site is “30-40%, however the area to be developed is somewhat in the range of 20% or flatter” when it is known that portions are greater than 40%, and much of the site is greater than 20% slope. (Checklist questions B.1.a and b)

The Application in SEPA Checklist Section 7.a.2 does not address the underground natural gas pipeline that is located on the site, even though this is well marked and known. Further, the application fails to discuss controls and restrictions to protect it from development.

COMPREHENSIVE PLAN AMENDMENT SUPPLEMENTAL INFORMATION:

The City also requires “Comprehensive Plan Amendment Supplemental Information”. This information is required for the Planning Commission and the City council to consider the request. Much of the requested information is missing.

Question 2 asks “What are the reasons for the requested amendment”. The Applicant just responded “Allow amending zoning area to Medium Density and High Density Residential” rather than stating the reasons for the amendment.

Question 3 asks “Which elements of the Comprehensive Plan will be affected and how”. The Applicant does not respond.

Question 3 goes on to ask for detailed information on the provision of utilities and how that corresponds to the City’s plans, and detailed information on the effects on public services. The Applicant simply states “Development of the areas will use the same public utilities and services as current Comprehensive and Zoning Areas.” This does not address the impact of changes to the types and level of utilities and services required.

Question 4 requests the Applicant to “Indicate how the requested amendment will implement the Comprehensive Plan and be in the best interest of the Kennewick area, reference specific Comprehensive Plan policies that will be implemented.” The Applicant responds “Hillside development areas require alternate design atandards (sic) to increase density and preserve more open areas within the development. A condensed development requires land usage for roads and utilities.” This response does not address the question.

SUMMARY OF SIGNIFICANT CONCERNS ABOUT THE APPLICATION:

Steep Slopes: Development on steep slopes would pose many hazards. The Application fails to identify which parts of the site are within the known Geologically Hazardous Area for Steep Slopes and to address the impacts of these steep slopes upon development.

The Comprehensive Plan says that “Erosion Hazards” can occur on “Slopes greater than 15%”, and defines “Extreme Slope Hazards” as “Severe erosion potential and high probability of slope failure & landslide occurrence, Slopes greater than 25%”. The Comprehensive Plan further states “Proposed development should avoid impacting critical areas.” (page 36)

Also, the Application fails to discuss the land with slopes greater than 40% which cannot be developed. The Applicant should be required to identify all areas of steep slopes on the site and to discuss any hazard controls, restrictions or prohibitions needed based on how steep the slopes are in each area.

Note that in a prior application for this same site (CPA 20-06) in 2020, the Applicant stated to the Planning Commision (in “Applicant’s Pre-Hearing Memorandum, April 19, 2021”) that:

“The north slope includes geologically hazardous critical areas and offers limited, if any, development opportunities.” (page 4)

“Lot 37...consists primarily of geologically hazardous slopes. The steep slope area extends along the northern perimeter of the amendment property...Lots 37 and 38 contain geologically hazardous critical areas which severely constrain development.” (page 7) *(Note that Lots 37 and 38 includes a large portion of the site proposed in CPA 2022-0005.)*

“A significant portion of the north face of the subject property will remain “open space” in perpetuity.”(page 8)

“The topography and presence of geologically hazardous critical areas on the north slope poses significant development restrictions to both Low Density Residential and High Density Residential project proposals.” (page 12)

“Significant portions of both Lot 37 and Lot 38 (which include the north slope) are undevelopable critical areas...In addition to absolute prohibitions on development, the remaining north slope presents significant development impediments for any residential development—single-family residential or multi-family residential.” (page 15)

Traffic impacts: High Density and Medium Density Residential Land Use Designations would allow developments which would cause significant traffic impacts in the area. The Applicant avoids addressing the significant traffic impacts from this proposal. For examples, there is no information provided to the following questions:

- “describe proposed access to the existing street system. Show on site plans” (Checklist question 14.a)
- “Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities...? (Checklist question 14.d)
- “How many vehicular trips per day will be generated by the completed project or proposal?...indicate peak volumes” (Checklist question 14.f)
- “Proposed measures to reduce or control transportation impacts” (Checklist question 14.h)
- “How would the proposal be likely to increase demands on transportation or public services....? (Checklist question D.6)

Note that in a prior application for this same site (CPA 20-06) in 2020, the Applicant stated to the Planning Commission (in “Applicant’s Pre-Hearing Memorandum, April 19, 2021”) that:

“No traffic from the subject property will pass through Panoramic Heights.”
(page 11)

This commitment resulted from a Mitigation Condition proposed by the City to avoid further traffic on overloaded neighborhood streets. This restriction is still needed and should be kept for this similar proposal on the same site.

Impacts on Surrounding Neighborhoods: The Application does not address any of the potential impacts that the resulting development would have on surrounding neighborhoods. Nor does it address the incompatibility of the proposed High and Medium Residential Land Use designations with the Residential Low Density and Residential Suburban zonings in the surrounding neighborhoods.

Hotel on Top: The Applicant has talked about building a “boutique hotel” on the top of Thompson Hill, but there is no mention of a hotel in this Application. Therefore, this Application, as written, does not address the impacts of a hotel. If the Applicant intends to build a hotel, the potential impacts and mitigations must be addressed.

CONCLUSION:

For all of the above reasons, the City of Kennewick should require this Application to be revised (per WAC 197-11-100 “Information required of applicants”) to include the information needed:

- for the public to understand the full scope of the proposal and its potential future impacts;
- for City staff to do a thorough analysis, make a threshold determination, propose needed mitigations for potential significant impacts, and make subsequent recommendations;
- for the Planning Commission to make an informed recommendation; and
- for the City Council to make an informed decision that is in the best interest of the citizens of Kennewick.

Dear City Staff and City Council Members,

Re: Proposed Comprehensive Plan Amendment 2022-0005

We are opposed to the proposed Comprehensive Plan Amendment 2022-0005, changing the land use designation for 4.3 acres on top of Thompson Hill from “Low Density Residential” to “High Density Residential” development and 35.5 acres on the upper north and south slopes from “Low Density Residential” to “Medium Density Residential”. This would allow up to 576 housing units including multi-family buildings on property that is currently allowed a maximum of about 159 single family homes. This poses significant impacts on current developments. We hope the City Planning Department and Planning Commission recommend this request be denied, and the City Council follows with denial of this request as they have previously done multiple times as recently as last year for a VERY similar request. The hill slope, surrounding land and neighborhoods have not changed in the last year.

Specifically, we oppose this proposed amendment for the following reasons:

1. Water runoff, landslide, and erosion issues; with large amounts of concrete added for high and medium density housing there is less permeable land/soil to absorb water runoff
2. Traffic access and flow through our community; our streets are not built to accommodate high or medium residential traffic nor does it allow alteration of existing streets due to the completed development of the existing surrounding neighborhoods.
3. It will create an island of high and medium density property surrounded by an ocean of low density residential housing; Municipal Code 4.12.110 (8 c & f). Per your own comprehension plan recommended process is a gradual increase of land use density on the top 4.3 acres. For example; low to medium, medium to high, high to commercial. If approved, there is no buffer between low and high density.

We believe this proposed amendment is, for all intents and purposes, the same proposal brought before the city two years ago and should be denied based on the city's own findings and recommendations regarding CPA 20-06. The Planning Commission's own determination last year stated;

Conclusions of Law

1. Pursuant to Chapter 4.08 of the Kennewick Municipal Code, the lead agency has determined that the proposed amendment does have a probable significant adverse impact on the environment.
2. The proposed amendment will change the land use designation for the subject sites from Low Density Residential to High Density Residential.
3. The proposed amendment is not consistent with the City of Kennewick Comprehensive Plan in regard to its compatibility with surrounding properties that are designated Low Density Residential. The proposed amendment will have a minimal impact on other aspects of the plan.
4. The proposed amendment will permit an increase to residential and commercial activities in the area.
5. Future development of the site has the potential to affect the park and traffic system. Future development of the site is subject to applicable Park Impact Fees, Traffic Impact Fees and improvements to the existing road network in the immediate area.

Further, Mr. Chavallo has once again submitted SEPA checklist that does not fully or accurately describe his proposal which does not follow SEPA Guidance. We hope you will look at **this** amendment proposal in conjunction with the city's Comprehensive plan and the city's comprehensive report on CPA 20-06.

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

Keith and Robin Duncan

5806 W 25th Ave
Kennewick

From: [Gail Everett](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#)
Subject: Citizen Comments: Thompson Hill - Comprehensive Plan Amendment
Date: Tuesday, August 23, 2022 6:30:52 AM

I would like to submit comments on the latest "Comprehensive Plan Amendment (CPA) 2022-0005". This proposal would increase the Land Use Designation on allowable housing density from Low Density Residential to High Density Residential on 4.3 acres, and from Low Density Residential to Medium Density Residential on 33.9 acres.

I am a life-long resident of Kennewick. I choose to live in what I feel is the best of the three (). My husband and I searched for "just the right home" for two years, 18 years ago. Panoramic Heights was our neighborhood of choice for the quality of life we found there. Peaceful, small, with a variety of house styles, close to hiking/walking areas, good schools and close to my parents.

I realize housing and development needs change thru the years, but why here? Why now?

There are a lot of other opportunities for development in the area, and this one is already booming to the south. Leave the front of the hillside alone as this WILL change the quality of life for all that live around this area, it will also change the visual aspect of the hillside, from a peaceful serene setting, to one of cluster/growth/overdevelopment and light pollution. That affects all who live, look and drive by.

Ask yourself, "Is this development necessary, here, now?" Does it improve quality of life or change it for the majority? Is it the right thing to do for this community? I really hope you think of that, with new council members who genuinely care about the community.

Thank you!

Gail Everett
2525 S. Harrison Place
Kennewick WA 99338
509-539-6453

From: cameojj@charter.net
To: [Steve Donovan](#)
Subject: [Possible Scam Fraud]thompson hill and new zoning.
Date: Thursday, June 23, 2022 8:56:34 PM

WARNING: Your email security system has determined the message below may be a potential threat.

The sender may trick victims into passing bad checks on their behalf.

If you do not know the sender or cannot verify the integrity of the message, please do not respond or click on links in the message. Depending on the security settings, clickable URLs may have been modified to provide additional security.

Steve, planning comm and city council-My complaint regards CPA 2022-0005 CHANGING THE LOW DENSITY (ESTABLISHED MANY YEARS AGO) RESIDENTIAL TO HI AND MED DENSITY (NEW PROPOSAL). WHEN LO RESIDENTIAL REQUIREMENTS ARE MET WE WILL HAVE LOTS OF TRAFFIC THRU AND AROUND THE NORTH AND SOUTH SIDES OF THE HILL AND THRU OUR NEIGHBORHOODS -THIS IS BAD ENOUGH. GOING TO HI AND MED DENS WILL PERMIT MULTI HOUSING AND ALLOW EVEN MORE TRAFFIC THAN BEFORE. LO DENS RESIDENTIAL WILL FIT IN WITH WHAT IS ALREADY HERE AND THE SURROUNDING NEIGHBORHOODS. I REALIZE THE MULTI PUTS MORE MONEY INTO CONTRACTOR WALLETS BUT IT MESSES UP THE RESIDENTIAL MIX. PLEASE DO NOT CHANGE THE ZONING. THANKS FOR YOUR TIME. JIM AND AJ FOSTER 6009 W 26TH AVE IN KENNEWICK

From: [LYNNE FREEMAN](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); jonh.trumbo@ci.kennewick.wa.us; [Steve Donovan](#)
Subject: Citadel South Estates: Comprehensive Plan Amendment 2022-0005
Date: Wednesday, August 24, 2022 4:24:43 PM

We live at 2013 S Kellogg St in the Windsong subdivision. We are very concerned about changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill from Low Density Residential to High Density Residential or Medium Density Residential. This would allow the building of a hotel and condos.

After seeing the Citadel South Estates Conceptual Design map, we are even more concerned and opposed to CPA 2022-0005. Thirteen huge buildings(with no indication on how tall these buildings will be) on the top and upper slopes of Thompson Hill will forever scar and ruin the aesthetic of Thompson Hill. This will have a long lasting negative impact on all the surrounding neighborhoods. Depreciation in our home values, increased traffic, noise and light pollution. A higher concentration of people in a more condensed location leads to an increase in crime. We have already seen huge spikes in crime in the Tri-Cities in the last couple of years. This area should have high priced single family homes on it to blend in with the surrounding neighborhoods, not thirteen; three or four story tall, ugly, rectangle buildings that will be visible for miles.

We agree with the Panoramic Heights Homeowners Association assessment that the CPA 2022-0005 and the SEPA Checklist are incomplete and all questions need to be answered. The Planning Commission, City staff and City Council should never move forward on any application from any developer without all the details to make an informed decision. That is really hard to do when developers answer questions with Non-Project Action, NPA or None.

We are very concerned about the negative effects on the wildlife of Thompson Hill. Just having Chavallo bring in dirt at the bottom of Citadel Estates the last two years has greatly reduced the number of quail, pheasants, cottontails and coyotes in the area. Unnecessary grading and moving of dirt is detrimental to wildlife habitat. Since the City has identified part of this site as a Critical Area for wildlife habitat conservation, true efforts need to be made to hold developers to a much higher standard on Thompson Hill.

We are also very concerned about the steep slopes on the property. Citadel Estates had many restrictions placed on it due to the makeup of the ground and the steepness of the slopes. Xeriscaping was recommended to cut down on watering and no pools are allowed in this development to help prevent erosion, rockslides, and flooding. But the plan for Citadel South Estates has a pool planned for the condos which seems reckless and could cause damage to anyone living downhill.

We request that the City Staff and Planning Commission recommend that this proposal be denied and that the City Council also vote to deny. Thank you for your time and consideration of our concerns,

Lynne Freeman
lynne.freeman13@yahoo.com
Cody Freeman
cmf8121@gmail.com

From: [Ken Gano](#)
To: [Steve Donovan](#)
Subject: CPA 2022-0005 Destruction of Thompson Hill
Date: Tuesday, June 14, 2022 8:50:05 AM

Please do not allow the change of zoning to high density on Thompson Hill. We don't need to destroy another hillside for the profits of a greedy developer. The traffic along Kellogg and adjoining neighborhoods is already too high.

From: [Ken Gano](#)
To: [Bill McKay](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Brad Beauchamp](#); [Gretl Crawford](#); [Jim Millbauer](#); [Loren Anderson](#)
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Tuesday, August 23, 2022 12:04:15 PM

Dear City Council Members, Planning Commissioners and City Staff:
Re: Comprehensive Plan Amendment (CPA) 2022-0005

We live at 5910 W 10th pl, Kennewick.

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi- family housing units. High Density Residential would also allow a hotel. We are opposed to CPA 2022-0005 because it will drastically increase traffic and noise in our neighborhood and along Kellogg. It will also destroy our view of Thompson hill and impact native habitat.

We request that the City Staff and Planning Commission recommend that this proposal be denied. Then, the City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhoods and is not in the best interest of the Citizens of Kennewick.

Thank you for your consideration of our concerns and our opposition to this Land Use Designation change.

Ken and Sue Gano
Ganokena@gmail.com
August 23, 2022

From: [Shirley Griffin](#)
To: [Steve Donovan](#)
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Wednesday, July 6, 2022 6:51:41 PM

Steve,

The Comprehensive Plan Amendment (CPA) 2022-0005, if approved, will have negative impacts on my neighborhood, Panoramic Heights.

We understand that there is a need for additional housing in the Tri-city area, but we don't support the plan amendment (CPA 2022-0005) currently under review. Ours is an established neighborhood of single family homes. We are surrounded by other neighborhoods of single family homes. The addition of buildings that will house multiple families and possibly a hotel on the top of Thompson Hill as well, will create unnecessary problems and totally disrupt the quality of life in this part of Kennewick.

I would ask the current Kennewick City Council members Bill McKay, Gretl Crawford, Loren Anderson, Brad Beauchamp, Jim Millbauer, Charles Torelli, and John Trumbo, to put themselves in our shoes and seriously think if they would vote to approve CPA 2022-0005 if the property in question was adjacent to their homes.

Please do not approve CPA 2022-0005!

Thank you,

Jeff and Shirley Griffin
2517 S Fillmore Pl.
Kennewick WA 99338

From: [Marie Mosley](#)
To: [Steve Donovan](#)
Subject: FW: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Wednesday, July 27, 2022 10:01:37 AM
Attachments: [image001.png](#)
[image002.png](#)
[image005.png](#)

FYI

Marie Mosley

City of Kennewick

City Manager

O: 509.585.4238 | C: 509.440.3994

marie.mosley@ci.kennewick.wa.us



From: Chuck Torelli <Chuck.Torelli@ci.kennewick.wa.us>
Sent: Wednesday, July 27, 2022 9:44 AM
To: Marie Mosley <Marie.Mosley@ci.kennewick.wa.us>
Subject: Fw: Comprehensive Plan Amendment (CPA) 2022-0005

FYI - (I didn't see city staff on distribution)

Chuck

From: Shirley Griffin <benlafitte@charter.net>
Sent: Tuesday, July 26, 2022 8:07 PM
To: Bill McKay; Gretl Crawford; Loren Anderson; Brad Beauchamp; Jim Millbauer; Chuck Torelli; John Trumbo
Cc: Bill Dixon
Subject: Comprehensive Plan Amendment (CPA) 2022-0005

To Kennewick City Council Members Gretl, Loren, Brad, Jim, Chuck, John, and Bill,

The Comprehensive Plan Amendment (CPA) 2022-0005, if approved, will have negative impacts on our neighborhood, Panoramic Heights.

We understand that there is a need for additional housing in the Tri-city area, but we don't support the plan amendment (CPA 2022-0005) currently under review. Ours is an established neighborhood of single family homes. We are surrounded by other neighborhoods of single family homes. The addition of buildings that will house multiple families and possibly a hotel on the top of Thompson Hill as well, will create unnecessary problems and totally disrupt the quality of life in this part of Kennewick.

We are asking you, our Kennewick City Council Members, to put yourselves in our shoes and seriously consider if you would vote to approve CPA 2022-0005 if the property in question was adjacent to your homes.

Please do not approve CPA 2022-0005!

Thank you,

Jeff and Shirley Griffin
2517 S Fillmore Pl.
Kennewick WA 99338

From: [Francine Handy](#)
To: [Steve Donovan](#)
Subject: CPA-2022-005
Date: Saturday, August 27, 2022 1:42:09 PM

Dear Mr. Donovan:

Please heed the stated (emphatically) wishes of your Kennewick constituents. We the residents of the Thompson Hill area most definitely do not want the zoning to be altered to allow multi-family housing and a possible resort/hotel type complex. We made that most clear in our response to the previous two attempts by Mr. Jose Chavallo to have the zoning changed to allow such development.

Our homes were purchased with the knowledge of the lawfully enacted zoning in place at that time. We feel we are being blindsided by this attempt to change our neighborhood environment solely for the benefit of one man's monetary benefit. Such a change will lead to endangerment of our children due to increased traffic, decreased home values, and the elimination of the tranquil neighborhood atmosphere that we cherish today.

Please consider these concerns.

Thank you.

Sincerely,

Francine Handy
Panoramic Heights Homeowner since 1978
2513 South Harrison Place
509-539-0767

Sent from Fran's iPhone

From: [Larry Hulstrom](#)
To: [Steve Donovan](#)
Subject: Opposed to Proposed Comprehensive Plan Amendment 2022-0005
Date: Wednesday, June 15, 2022 8:44:48 PM
Attachments: [Hulstrom letter to City Officials 06-2022.docx](#)

June 14, 2022

Dear Mr. Donovan,

Here we are again, not even two years since the last time this attempt was made. As a resident of the Panoramic Heights neighborhood in Kennewick I remain opposed to the proposed development of the portion of Thompson Hill addressed in this proposed CPA. The reasons this is a bad idea for the City were numerous and well outlined in opposition documents provided during the review of CPA 20-06. These have not changed. As with my previous email I ask that my opposition is noted and added to the count of numerous other citizens of Kennewick that will oppose this zoning change.

Please let me know that you are in receipt of this email. I am also sending the attached letter to members of the Kennewick City Council.

Thank you for your time,

Larry Hulstrom

From: Larry Hulstrom [mailto:rockhound132@charter.net]
Sent: Monday, July 27, 2020 2:00 PM
To: 'Steve Donovan' <Steve.Donovan@ci.kennewick.wa.us>
Subject: RE: Proposed rezoning on east end of Thompson Hill

July 27, 2020

Dear Mr. Donovan,

I am a dues-paying member of the Panoramic Heights Homeowners Association. Please make note that this email is notification of my opposition to the proposed change in zoning from low density to high density regarding Thompson Hill per the request of Jose Chavallo.

The long list of reasons why this change in zoning should not go forward has previously been provided to you by our Board. This email simply needs to count and be noted on the official record as one more vote in definite opposition to the zoning change.

Please reply to this email that you are in receipt and are counting my opposition vote to the zoning change to high density.

Thank you for your time.

Larry Hulstrom

June 14, 2022

Dear Kennewick Planning Commission and City Council Members:

Re: Proposed Comprehensive Plan Amendment (CPA) 2022-0005

Here we are not even two years after CPA 20-06 was rejected with a proposal from the same individual to do basically the same thing over again. The owner/developer wants to change the designation of his property on the top of Thompson Hill so that he can demolish the existing home and grounds and put a hotel in its place. This sounds like someone trying to increase his bottom line, not benefit the community in which he lives. I join with other members of the Panoramic Heights Homeowners Association who continue to oppose building any structures on the top of this iconic hill that is so much a part of Kennewick's image. When there is "land a plenty" on the south side of Thompson Hill that is currently being developed, and land along Bob Olsen Parkway for ongoing and future development why do we need to create an eyesore for the City that would exist for generations to come. There is no going back once this development occurs. There are many other areas within the Tri-Cities that are available for expansion. There is no need to develop the property identified within this proposed CPA area. The City would be better served to buy this property and turn it into a city park, one that all residents could enjoy. This does come with logistical and maintenance concerns that could be addressed, but ones that could be managed.

The impacts to the surrounding neighborhoods have already been identified and discussed during the public comment period for CPA 20-06. These have not changed and should not need to be restated. Look back at all of the letters that you received then that were in opposition to that proposal and listen to the individuals that spoke during the public hearing. Why must we continue to struggle over this every few years when the public has already, and continues, to voice strong opposition over this proposed development? The makeup of the City Council has changed since the last election but the voice of the people in the community they serve remains the same and requests that the Planning Commission recommend that this request be denied, and that the City Council deny this request.

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

Larry and Kay Hulstrom
5409 W. 26th Ave.
rockhound132@charter.net

Dear City Council Members, Planning Commissioners and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

I live at 5731 W 17th ave, Kennewick, WA, 99338.

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi- family housing units. High Density Residential would also allow a hotel.

I am opposed to CPA 2022-0005 for these reasons.

1. It is not incompatible with existing neighborhood.
2. It will increase the traffic in our area.
3. It will drastically decrease my property value.
4. The development of steep slope may create landslides.
5. The high and middle density area always create safety and crime issue.

I request that the City Staff and Planning Commission recommend that this proposal be denied. Then, the City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhoods and is not in the best interest of the Citizens of Kennewick.

Thank you for your consideration of our concerns and our opposition to this Land Use Designation change.

Haruko Ishii

(catharuko@hotmail.com)

7/23/2022

From: [Monte LaDow](#)
To: [Steve Donovan](#)
Subject: Thompson Hill Development
Date: Tuesday, June 14, 2022 8:24:15 AM

Well, Mr. Donovan, here we go once again. It seems the pressure you city folks are having to endure from both the developer AND the need to clutter Thompson Hill with high density housing/apartments/hotel in order to generate more tax revenue is upon you (and residents of the Thompson Hill area) again.

We residents in the area have spoken several times about this problem which you folks just don't seem to understand. We built our home in the (then/used-to-be) quiet neighborhood of Panoramic Heights #3, but now have to put up with SO MUCH additional traffic (including vehicles that DON'T stop at the stop sign at S. Irving and W. Kellogg streets AND the vehicles that have extremely loud muffler systems that the city police department won't do anything about. And, need to mention the HUGE amount of dust we are subject to from all the open terrain on the south side of Thompson Hill. The city is allowing the developers to open WAY MORE TERRAIN than they can possibly handle for dust control. I've now spent hundreds of dollars to pressure wash my home, driveway and clean inside my home... with NO END IN SIGHT. More development is only going to exacerbate these situations and frankly, WE ARE TRULY TIRED OF THIS!

PLEASE do not yield to the pressures of this developer. His attitude of thinking he can do whatever he wants takes NOTHING into consideration of the surrounding areas and the impact it has on our investments and quality of life.

Sincerely,
Monte LaDow
2643 S. Kellogg St.
Kennewick, WA 99338
509-521-6620

P.S. I'm not going to waste my time sending this to the City Council members as the developer has many of them wrapped around his little finger.

Sent from [Outlook](#)

From: [Monte LaDow](#)
To: [Steve Donovan](#)
Subject: Thompson Hill Zoning
Date: Friday, August 12, 2022 8:22:18 AM

Panoramic Heights has always been a nice upper middle-class neighborhood. Condos and a hotel atop Thompson Hill ARE NOT COMPATABLE with the type of neighborhood we home owners envisioned. We built homes against the backdrop of Thompson Hill to enjoy the VIEW... and the courts have found that a VIEW is 360 degrees... not just one direction. I do NOT want to look at a view that includes a hotel and hundreds of condos/apartment like out on Ridgline Dr. near I-82. It appears that the city folks haven't really done their homework as there will be a great deal more traffic, light and noise pollution, and DUST! I can't imagine what our neighborhood would look like if this proposed construction were approved. We currently live in a dust-bowl from all the construction on the southside of the hill. With more construction on the top of the hill, we are going to look like the "Sandbox-of-the-Tri-Cities!" Single family homes would be fine as there is greater "pride-of-ownership" and care. Whereas, condos etc. may start out looking good, but in a few years, they too lose their luster and become less-cared-for as the ownership changes. Hence, they don't really care how they look. Approval of this proposed plan is NOT a good idea! The Applicant seems to have this sense of entitlement and doesn't really consider the people who will be most impacted.

Please consider the impact to the existing neighborhood!

Monte LaDow
2643 S. Kellogg St.
Kennewick, WA 99338

Sent from [Mail](#) for Windows

From: [Tenbears Running](#)
To: [Steve Donovan](#); [Bill Mckay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#)
Subject: Thompson Hill Plan
Date: Tuesday, June 14, 2022 7:16:45 AM

Steve Donovan et all

Good Morning. I had opposed the plan for Thompson Hill last year, having been defeated by the city council. I had suggested at that time the increased traffic in the surrounding areas with a development such as being requested, as my primary objection.

I have seen in our area, over the last several months, a very significant amount of traffic on Creekstone Drive West of W 27th. Again, that is my primary concern. Even today, it is difficult at times to exit the Creekstone Subdivision onto Creekstone Drive.

As a retired Law Enforcement Officer of 38 years, please take into consideration the amount of crime that WILL be an issue, in a subdivision and commercial area as being suggested.

Is the City of Kennewick in a position to add several additional Officers to handle traffic incidents, car prowls and theft, Domestic issues and accidents that will occur with this increase of traffic on our/your city streets?

I do understand the tax advantage you might think will help with so many issues that will occur, but please take in consideration the neighborhoods that will be affected in the near and distant future.

Thank You

Robert Langendorfer

Creekstone area resident

The Application was accepted by the City on June 9. Initial public comments are requested by June 24. Send them to Steve Donovan, City of Kennewick at Steve.Donovan@ci.kennewick.wa.us. Also, send your comments to the City Council via email at these addresses: <https://www.go2kennewick.com/531/City-Council>. Reference "CPA 2022-0005".

From: mark.e.lucas@att.net
To: [Steve Donovan](#)
Cc: [Bill McKay](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); ["Bill Dixon"](#); Joan.L.lucas@att.net
Subject: Comprehensive Plan Amendment (CPA) 2022-0005 comments
Date: Sunday, June 19, 2022 7:33:54 AM

Mr. Donovan,

I am a resident of the Panoramic Heights subdivision and was recently made aware of a pending request for a zoning change adjacent to the area where I live. While I am not necessarily opposed to development, I am a firm believer in following the process to change zoning laws and allow open and candid discussion of the change such that those impacted by any resulting amendment to existing land use designations have a chance to review and comment on any proposed changes. When my wife and I moved into this subdivision 8 years ago, we were aware that Thompson Hill had been purchased and was likely to be developed. We also knew the current zoning was for residential housing similar to if not identical to the existing neighborhood. With that in mind we assumed that one day the area we lived in would grow to include new and similar lots with single family homes on them. We did not think the additional future growth would have a major impact to the existing neighborhood and I figured the undeveloped area to the west of us would likely support no more than 50 or so new homes on the north side of the hill due to the steep slopes. But with the proposed zoning change, it is clear this is not what the developer intends to do with the land around Thompson Hill.

With regard to CPA 2022-0005, the information recently provided to support the Comprehensive Plan Amendment is lacking in detail. Detail that is important to determine what exactly the changes are that the developer is proposing and detail that is required by regulatory agencies when considering a land use change. The information provided to support this request includes items that are vague and in some cases misleading and frankly impossible to use as a reference on what specifically this land use change will entail. Yes, it is clear that the amendment discusses adding a hotel and more compact housing. However the map does not show placement of these proposed structures or where new infrastructure (roads, utilities, etc.) would be added to support them. And in addition to the map not providing enough detail to fully understand the extent of the proposed development, the SEPA Checklist is far worse as a source of information. Information that is clearly identified and noted as being required in the guidance. See below for specific examples. The information directly below was copied directly from the following Department of Ecology website:

<https://ecology.wa.gov/Regulations-Permits/SEPA/Environmental-review/SEPA-guidance/SEPA-checklist-guidance>

SEPA checklist for non-project proposals

Non-project actions are governmental actions involving decisions:

- *About policies, plans, or programs containing standards for controlling the use or modifying the environment.*
- *That will govern a series of connected actions.*

Only government agencies can initiate non-project action reviews and requirements vary by

jurisdiction. Non-project action analysis gives agencies, communities, and public an opportunity to examine planned actions before a project begins and permit applications prepared. Early analysis of environmental impacts helps streamline the permitting process.

General guidance for non-project actions

Non-project and project proposals follow the same procedural requirements under SEPA. Environmental review starts as early in the process as possible when sufficient information is available to analyze probable environmental impacts.

The process usually starts by completing an environmental checklist. An exception is if the lead agency has already determined an Environmental Impact Statement (EIS) is needed or SEPA is complete. Proposals should be described as an alternative means of accomplishing an objective.

For example, a statewide plan to use chemicals to treat aquatic vegetation could be described as "a plan to control aquatic vegetation." This encourages a review of different treatment alternatives since the environmental review might evaluate biological or mechanical control methods, or a combination of various approaches.

If the non-project action deals with a land-use decision or a proposal to govern future development, the likely environmental impacts need to be considered. An early, detailed analysis can result in a less in-depth environmental review since the work has been done up front.

These are some (but not all) of the comments I have on the provided checklist itself:

A quick look at the submitted form shows a lack of detail inconsistent with the guidance from the Ecology website. For instance –

In the instructions, under the heading “Use of Checklist for Non-Project proposals” it is specifically noted to “**...complete the applicable parts of Sections A and B plus the SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS (part D).**”

And... In section A. 1. It is noted that this checklist is submitted as a Non-Project Action.

But... **Section D is not filled out.**

Also -

The instructions specifically note that likely environmental impacts need to be considered

The following sections of part B, Environmental Elements, are not addressed – inconsistent with the SEPA instructions:

f. – This section deals with erosion. The section is not answered but instead the response is “NPA”. This does not allow a reasonable person to consider the environmental impact of erosion this land use change would cause.

g. - This section deals with covering raw land with impermeable material which causes additional runoff. The section is not answered but instead the response is “NPA”. This does not allow a reasonable person to consider the environmental impact of increased runoff that this land use

change would cause.

h. - This section deals with how erosion will be controlled. The section is not answered but instead the response is "NPA". This does not allow a reasonable person to consider the environmental impact of increased erosion almost assuredly increase as a result of accumulating water on streets and roofs that would spill over onto raw land that this land use change would cause.

Moving on to the Air section. To shorten this e-mail I will just note that Sections a. and c. are both answered with NPA and no effort is made to address emissions from equipment during construction or any methods to reduce the emissions.

Moving on to section c. Water runoff, once again NPA is used to skirt the issue of what controls will be used to control runoff caused by future construction activities as well as the additional runoff that would occur due to impermeable roof structures and new road surfaces which will increase the amount of runoff over what exists now.

Also I am not sure what the legal definition of a steep slope is, but under section B. 1. b. the slope is first noted to be 30-40 % which I am pretty sure would be defined as "steep" but then noted that construction would be "in the 20% slope range" Not sure where that slope range exists on the upper slopes on the north side of the hill other than the top. Same note for section B.1.d.

Under 4. b. again NPA is used instead of describing the kind and amount of vegetation that is planned to be removed or altered. No attempt is made to describe the landscaping so that a reasonable person could consider the environmental impact of the removal of ground cover to the surrounding area.

Under 5. a. only rattlesnakes and rabbits are listed as animals in the area. This is incomplete. I am not aware of what all animals and birds call this area home, but I have seen racoons, mice, rats, squirrels, skunks, and even deer in the area. I have heard but not seen coyotes. There are hawks, owls, dove, quail, songbirds, geese, ducks, and other birds in the area that are not listed on this checklist. I am sure there are even rattlesnakes and rabbits. More detail is needed here.

Under 6. a. & c. Again NPA is used to answer these items. No description of the utilities that will be used in future construction so that it can be considered. No listed measures of how the additional load on power, internet, and/or gas will be mitigated with the new residences. Not possible to consider the environmental impact or impact to current residents without this information.

Previously I suspect the existing infrastructure was designed for the existing zoning density. If this is change will increase the density of housing then this is a critical element of the amendment and one that must be addressed satisfactorily.

Under 7. b. environmental health – Noise, again NPA is used.

It is clear from this application and specifically the SEPA checklist that it is: incomplete, misleading, inaccurate, and not ready for review until filled out properly and completely. Until the SEPA checklist is filled out completely and in accordance with the guidance on the Department of

Ecology's website, I think consideration of this amendment should be on hold pending the availability of complete and accurate information. Only then can this land action truly be evaluated as to the impact it would have when compared to the original zoning designated by the city for development of this area.

Mark and Joan Lucas
5808 W 26th Ave.
Kennewick WA, 99338
(509) 781 0815

From: [Marie Mosley](#)
To: [City Council](#)
Cc: [Department Heads](#); [Steve Donovan](#)
Subject: FW: Comprehensive Plan Amendment (CPA) 3022-0005
Date: Tuesday, August 2, 2022 8:20:19 AM
Attachments: [image001.png](#)
[image002.png](#)
[image005.png](#)

FYI

Marie Mosley

City of Kennewick

City Manager

O: 509.585.4238 | C: 509.440.3994

marie.mosley@ci.kennewick.wa.us



From: Jim Millbauer <Jim.Millbauer@ci.kennewick.wa.us>
Sent: Tuesday, August 2, 2022 6:22 AM
To: Marie Mosley <Marie.Mosley@ci.kennewick.wa.us>
Subject: Fwd: Comprehensive Plan Amendment (CPA) 3022-0005

Marie,

Please share this e-mail with the others on council as it was just addressed to me.

Thank you

Jim

Sent from my iPad

Begin forwarded message:

From: Kevie Marston <kevie1124@aol.com>
Date: July 31, 2022 at 9:58:24 AM PDT
To: Jim Millbauer <Jim.Millbauer@ci.kennewick.wa.us>
Subject: **Comprehensive Plan Amendment (CPA) 3022-0005**

Dear City Council Members, Planning Commissioners
and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

We live at 7030 W 29th Ave, Kennewick WA.

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi-family housing units. High Density Residential would also allow a hotel.

We are OPPOSED to CPA 2022-0005 for these reasons.

1. Increased traffic
2. Value of properties
3. More accidents
4. Noise level will increase

We request that the City Staff and Planning Commission recommend that this proposal be DENIED. That the City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhoods and is not in the best interest of the Citizens of Kennewick.

Thank you for your consideration of our concerns and our opposition to this Land Use Designation change.

Richard and Kevelene Marston

kevie1124@aol.com

07/31/2022

From: [Kevie Marston](#)
To: [Steve Donovan](#)
Subject: Fwd: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Sunday, July 31, 2022 9:52:29 AM

> Dear City Council Members, Planning Commissioners and City Staff:

>

> Re: Comprehensive Plan Amendment (CPA) 2022-0005

>

> We live at 7030 W 29th Ave, Kennewick WA.

>

> CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi- family housing units. High Density Residential would also allow a hotel.

>

> We are OPPOSED to CPA 2022-0005 for these reasons.

> 1. Increased traffic

> 2. Value of properties

> 3. More accidents

> 4. Noise level will increase

>

> We request that the City Staff and Planning Commission recommend that this proposal be DENIED. That the City Council should vote to deny it because this proposal would have negative impacts on the surrounding neighborhoods and is not in the best interest of the Citizens of Kennewick.

>

> Thank you for your consideration of our concerns and our opposition to this Land Use Designation change.

>

> Richard and Kevelene Marston

> kevie1124@aol.com

> 07/31/2022

>

>

From: [Amber Morales](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#)
Cc: info@panoramicheightshoa.com
Subject: Proposed Development on Thompson Hill
Date: Wednesday, August 3, 2022 2:58:02 PM

Hello City Council,

I am writing you to ensure as citizen of this community I have a voice at the table. It has come to my attention that there is another proposal on the table of high density development on Thompson Hill. It is concerning considering the amount of increased traffic, and non-residential individuals that could bring to the area. Like myself, many individuals utilize Thompson Hill to get outdoors and enjoy hiking the area. With the proposal it brings a lot of extra traffic, along with increased amounts of individuals who do not necessarily have the pride in taking care of the community, nor bring the trust/safety aspect of belonging to the community. It is also very concerning that the proposal has not met all the ecological aspects and approvals. For so many years we have ignored our impact on our sounding areas and need to start think what is better for the mass community and not just the few. In short i am asking to please consider stopping this proposal from gowing forward to ensure less of an impact on the current environment and individuals who call Thompson Hill home.

Amber Morales
Homeowner at 2269 S Belfair St. Kennewick WA. 99338

Dear City Council Members, Planning Commissioners, and City Staff:

Re: Proposed Comprehensive Plan Amendment 2022-0005

We are opposed to the proposed Comprehensive Plan Amendment 2022-0005, which would change the land use designation for approximately 40 acres on Thompson Hill from “Low Density Residential” to “High Density Residential” and “Medium Density Residential” development. We request that the Planning Commission recommend that this request be denied, and that the City Council deny this request.

We live at 5328 W. 26th Ave, in Panoramic Heights, the neighborhood that borders this site on the east side of Thompson Hill.

We oppose this proposed amendment for the following reasons:

- **The development will result in excessive vehicle traffic through Panoramic Heights. Currently, 25th, 28th, and especially 26th already experience high levels of traffic as drivers pass through the neighborhood to travel between the Creekstone area and the Southridge area. This is a safety concern and livability concern. No additional traffic burden should be placed upon these streets. Any future development on Thompson Hill should not be accessed through Panoramic Heights.**
- **The development is detrimental to the property values, livability, and quality of life in Panoramic Heights.**

Please thoroughly address these serious concerns and potential impacts before amending the Comprehensive Plan.

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

Conrad and Caroline Morrow
5328 W. 26th Ave
Kennewick, WA 99338
(509) 554-6040

August 14, 2022

Dear Kennewick Planning Commission and City Council Members:

Re: Proposed Comprehensive Plan Amendment (CPA) 2022-0005

My wife and I are opposed to the proposed Comprehensive Plan Amendment (CPA) 2022-0005. This amendment would change the land use designation for 38.2 acres on Thompson Hill from “low Density Residential” to “High Density Residential” development, from 150 single family homes to 556 multi-family housing units. My wife and I request that the Planning Commission recommend that this request be denied and that the City Council as well, deny this request.

We, James and Kathleen Neary, reside at 5420 W. 26th Ave, in Panoramic Heights, the neighborhood that borders this site on the east side of Thompson Hill. Our neighborhood will be most impacted by the proposed change. High density residential development would increase the housing density on the top and upper slopes of Thompson Hill by a factor of 3.7 when including apartments, condominiums, town houses, row houses and similar types of housing. This Conceptual Design also shows a **future site on the top of the hill for a hotel**. In the past, the Applicant stated this could have **about 100 rooms**. In addition the Conceptual Design includes **12 large buildings for multi-family housing units** from the ridgeline down the south side to the old irrigation canal and the existing City water tank. The Applicant has told the City Staff this would allow **for 360 condominiums**.

However, as required by the State Environmental Policy Act (SEPA), the **Applicant has failed to discuss this proposed future uses in the SEPA Checklist, nor identified the potential impacts and proposed mitigation measures**. Therefore, the **Application remains incomplete and should be rejected** outright by the City Staff, Planning Commission and City Council.

Specifically, my wife and I oppose this proposed amendment for the following reasons:

- Kennewick has been expanding to the Southwest for years and we have already experienced an expansion in the density of housing surrounding us as the city continues to grow. Recently a middle school was added to our neighborhood which has had a dramatic impact on the amount of traffic in our subdivision, adding more challenges as this increased traffic now amplifies the already large volume we have daily from Southridge High School. We are in favor of growth much like the new single family housing developments South of Panoramic Heights. Adding high density residential to our established neighborhood, already accepted as a much desired and established area of Kennewick, will destroy the ambience that our established neighborhood provides as a benchmark in how to maintain a premier subdivision and yet embrace growth. Adding undetermined multifamily housing

units will stress the natural environment and the features consistent with an older neighborhood. With a growing number of newer families moving into this area having small children, the intensified traffic increases the danger to that vulnerable population in addition to the infrastructure impact the multiples of vehicles will have on our neighborhood streets.

Please thoroughly address these serious concerns and potential impacts before amending the Comprehensive Plan.

In summary, my wife and I believe that this proposed amendment does not meet your approval criteria that it “bears a substantial relationship to the public health, safety, welfare, and protection of the environment.” Nor have the following factors been considered thoroughly prior to approving this Comprehensive Plan amendment:

- The effect upon the physical environment;
- The effect upon open space and natural features, including topography;
- The compatibility with and impact on adjacent land uses and neighborhoods;
- The adequacy of, and impact on community facilities, including utilities, roads, public transportation, parks, recreation and schools;
- The current and projected project density in the area; and
- The effect upon other aspects of the Comprehensive Plan.

Therefore, Comprehensive Plan Amendment 2022-0005 does not meet the requirements of Kennewick Municipal Code

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

James Neary

Kathleen Neary

5420 W. 26th Ave
Kennewick
jkneary@charter.net

From: [Rich Nelson](#)
To: [Steve Donovan](#); [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Jim Millbauer](#); chuck.tortelli@ci.kennewick.wa.us; [John Trumbo](#)
Cc: [Bill Dixon](#)
Subject: CPA 2022-0005
Date: Monday, June 20, 2022 9:54:51 AM

First of all, the City of Kennewick should install a statute of limitations on the number of times and frequency that a person or organization can file for a change to zoning plans. How many times do we have to tell the Chavallos that we do not want and will not tolerate a zoning change from low residential on Thompson Hill. This most recent filing nears the point of HARRASSMENT!.

Secondly, the Chavallos have demonstrated that they are not good neighbors. I have added pictures of the access to the Chavallo property that includes gates, fences, and no trespassing signs. They have the right to control their property but the way they did it is not neighborly and indicates disregard for the rest of us sharing the area.

Our experience with the applicants leads us to believe that they will not be easy to work with in the future. We anticipate they will push all City rules to the limit and beyond as we try to deal with future development on Thompson Hill. We recognize that Thompson Hill will eventually be developed—but not with commercial or high density zoning.

The applicant wants to put a hotel on the top of the hill and medium to high residential units on the rest. Based on their disregard for the rest of the community, I can envision them putting flashing neon signs on the top inviting travelers to use their hotel. That would not be acceptable to anyone in the Tri-Cities.

As we have pointed out before, development of the hill will require significant improvement to traffic flows. We cannot tolerate the additional traffic thru Panoramic Heights, a family residential community. Thus, the City should require that new development of the hill should include that the developer fund City work to establish new traffic access points.

We would encourage the City of Kennewick to follow the lead of Richland and West Richland in opening up strategic higher elevations such as Thompson Hill for hiking and resident access. We cite the heavy use of trails on Badger and Candy mountains. We would like to see any development plans to include hiking trails to fully utilize the benefits of Thompson Hill.

I encourage the Kennewick Planning Committee and the Council to once again REJECT this request for zoning change. I also request the City to establish some Statute of Limitations as to how many times a claimant can request zoning changes.

Richard Nelson
Richnelson1942@msn.com







From: [Shlomo Orr](#)
To: [Steve Donovan](#); [Melinda Didier](#)
Cc: [Anthony Muai](#)
Subject: Re Application for Zoning Code Amendment for building a Boutique hotel
Date: Thursday, March 3, 2022 2:21:24 PM

Dear Mr. Donovan, City Staff, and Planning Commission:

As residents of Panoramic Heights downhill from Thompson Hill, we are concerned with the application for Zoning Code Change submitted by Mr. Jose Chavallo. In particular, we are concerned with the inconsistency between the particular location he is interested in (the top of Thompson Hill) and the generality of the application. Mr. Chavallo's SEPA form is essentially devoid of answers to the important geotechnical and environmental concerns, conveniently claiming a "general application" that could be applied anywhere in the city.

Furthermore, in his application he lists residential zones as possible targets for building a boutique hotel (of 100 rooms). In other words, "eat the cake and have it too".

In our view:

1. There is no need to change a zoning code that has been thoroughly thought of by the Planning Commission.
2. In lieu of an EIA, a SEPA form should have been filled completely, specifically for The target location. SEPA implies a specific location for which basic environmental conditions must be addressed. No one should not have it both ways.
3. Residential zones should not be targeted for commercial developments, including a boutique hotel. We understand that this was the original intention of the Comprehensive City Plan, which makes sense.

We will appreciate your response to our concerns.

Respectfully,

Shlomo and Galya Orr

Shlomo and Galya Orr
5900 W. 25th Avenue
Kennewick, WA 99338
(509) 736-3111 (main)
(509) 591-8196 (cell)
shlomo.orr@gmail.com
galya.orr@gmail.com

From: [Shlomo Orr](#)
To: [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com; [Anthony Muai](#)
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Thursday, August 18, 2022 5:13:07 PM

Dear Council members and Planning Commission:

The proposed new development would be an environmental disaster and an economical ruin for all the surrounding neighborhoods. It is hard to comprehend how any responsible city council could allow one man with money to get that far, destroying several neighborhoods in the process, and risking the safety of so many people. How could that debacle be allowed?!

Respectfully,

Shlomo Orr
shlomo.orr@gmail.com
Panoramic Heights

From: [ERIC AND KATHY OTHEIM](#)
To: [Steve Donovan](#)
Cc: info@panoramicheightshoa.com
Subject: PROPOSED DEVELOPMENT ON THOMPSON HILL (CPA 2022-0005)
Date: Thursday, June 30, 2022 3:58:56 PM

Please say no to the Thompson Hill propose development CPA 2022-0005. This is inconsistent with the the neighborhood's surrounding Thompson Hill. Most of us chose to live in this area because it was zoned for low to medium housing. Little did we know that it was possible to change the zoning so easily. This project would increase the noise and light pollution as well as increase congestion on what is already becoming an increasingly busy boulevard. Because it is at the top of the hill, the disruption from the light and noise would have a negative impact over a much larger area. I feel like it is similar to the situation with the state wanting to place a windmill farm in Kennewick's backyard. I hope our voices are listened to more than the state seems to be listening to our county's in regards to the windmill project.

Thank you,

Eric Otheim
7044 W 33rd Place
Kennewick WA 99338

Sent from my iPad

From: [Marie Mosley](#)
To: [Steve Donovan](#)
Cc: [Anthony Muai](#)
Subject: FW: Amend 2022 Comprehensive Plan From Low Density Residential (LDR) to 35.45 Ac. Medium Density Residential (MDR), and 4.31 Ac. High Density Residential (HDR)
Date: Tuesday, June 14, 2022 5:44:14 PM
Attachments: [image001.png](#)
[image002.png](#)
[image005.png](#)

Below is one of the 3 communications sent to City Council I am aware of regarding comp plan 2022-0005.

Marie Mosley

City of Kennewick

City Manager

O: 509.585.4238 | C: 509.440.3994

marie.mosley@ci.kennewick.wa.us



From: Chuck Torelli <Chuck.Torelli@ci.kennewick.wa.us>

Sent: Sunday, June 12, 2022 4:56 PM

To: Marie Mosley <Marie.Mosley@ci.kennewick.wa.us>

Subject: Fwd: Amend 2022 Comprehensive Plan From Low Density Residential (LDR) to 35.45 Ac. Medium Density Residential (MDR), and 4.31 Ac. High Density Residential (HDR)

FYI...

Sent from my iPhone

Begin forwarded message:

From: Stephen Parent <stephenparent.parent@gmail.com>

Date: June 12, 2022 at 4:53:10 PM PDT

To: Bill McKay <Bill.Mckay@ci.kennewick.wa.us>, Gretl Crawford <Gretl.Crawford@ci.kennewick.wa.us>, Loren Anderson <Loren.Anderson@ci.kennewick.wa.us>, Brad Beauchamp <Brad.Beauchamp@ci.kennewick.wa.us>, Jim Millbauer <Jim.Millbauer@ci.kennewick.wa.us>, Chuck Torelli <Chuck.Torelli@ci.kennewick.wa.us>, John Trumbo <John.Trumbo@ci.kennewick.wa.us>, Bill Dixon <wtdixon3@gmail.com>

Cc: Stephen Parent <stephenparent.parent@gmail.com>

Subject: Amend 2022 Comprehensive Plan From Low Density Residential (LDR) to 35.45 Ac. Medium Density Residential (MDR), and 4.31 Ac. High Density Residential (HDR)

Proposed extension of 25th 26th and 27th streets is not acceptable. These streets are not designed for heavy traffic and will be a detriment to the existing home sites.

The plan needs to include an extension of S Kellogg Street to connect with S Sherman St. The traffic on S Irving street going up the hill from the roundabout from Creek Stone has already become a noise problem and nuisance in the morning and evening. This should be part of the cost of development paid by the applicant. I do not think the City council members would like to live on a thorough fair highway, and neither do we. Also, I did not see in the application where additional future costs will be incurred for police and emergency services that go along with high density zoning.

--

Stephen Parent
253-691-6433

From: glromano@charter.net
To: [Steve Donovan](#); [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); jim.millbauer@ci.kennwick.wa.us; [Chuck Torelli](#); john.trumbo@ci.kennewck.wa.us
Cc: ["Bill Dixon"](#)
Subject: CPA 2022-0005
Date: Wednesday, June 15, 2022 10:27:39 AM

I'm writing in opposition to Comprehensive Plan Amendment (CPA) 2022-0005. My concern is with the higher traffic counts these high- and medium-density changes will make to my neighborhood. The City of Kennewick failed to create appropriate thoroughfares from south of Thompson Hill to other parts of West Kennewick. Only two planned exits from this area exist: One is S. Union Street and the other is Bob Olson Parkway which leads to Steptoe Street. These exits are miles apart. As a result, people look for shortcuts. Those shortcuts send traffic through the Southridge and Panoramic Heights neighborhoods. The streets in these neighborhoods were designed for low-density residential traffic. Projects with medium- or high-density traffic are usually built near larger thoroughfares designed for this higher traffic. Such is not the case here. The proposed CPA would create a substantially higher traffic count through the existing residential neighborhoods previously described. People are active in these neighborhoods and use the sidewalks and streets for jogging, dog walking, biking, skateboarding, getting the mail and other activities. Adding a much higher traffic count through these neighborhoods increases the potential for serious auto/pedestrian accidents. I have seen estimates of the traffic impact for full buildout of the High Density Residential project to be about 8,500 vehicle trips per day. Add to that the additional traffic from the medium density project. The city should **never** allow that much traffic through low-density residential neighborhoods.

I believe it was well stated by a city councilman at the last time Mr. Chavallo appealed for a land use change. "Once a decision is made to change land use from low-density to high-density, the developer is free to build whatever he wants on the property." The developer is obviously trying to maximize his profits from his existing land holdings. There are already a number of low-density houses being built in this area. It is not the city's responsibility to increase profits for a developer. It is the city's responsibility to protect its residents.

I urge the city to deny the CPA 2022-0005 for the safety reasons stated above. I urge the pro-development members of the city council to put aside their personal beliefs on development and vote no for the safety of its existing citizens.

George
glromano@charter.net

June 15, 2022

Dear Mr. Donovan:

Re: Proposed Comprehensive Plan Amendment 2022-0005

We are submitting this letter to register our concerns with the proposed Comprehensive Plan Amendment 2022-0005. This amendment would change the land use designation for 39.76 acres on Thompson Hill from “Low Density Residential” to “High Density Residential” for 4.31 acres and “Medium Density Residential” for 35.45 acres.

We live at 6925 W. 23rd Ct. in the SouthCliffe development, a neighborhood in the near vicinity of the site. Our neighborhood stands to be substantially impacted by this land use change as one of the proposed access streets will be from S. Sherman St. Currently, S. Sherman St. is the sole access to SouthCliffe. High and Medium density residential development allows substantially more housing units per acre than the current Low-density designation. These could be apartments, condominiums, town houses, row houses or similar types of housing.

Due to the potential for a much higher housing density, we are concerned approval of CPA 2022-0005 at this time does not adequately take into consideration the following:

- The high volume of traffic and the related safety issues on access roads through adjacent neighborhoods and S. Sherman St. and the increased hazards at the intersection of Sherman St. and Hildebrand/Bob Olsen pkwy.
- It is incompatible with adjacent land uses and other neighborhoods on or near Thompson Hill.
- Adverse impact to the fragile environment of such a large multi-family development.
- Impacts from the additional noise and light pollution such a development would generate.

Furthermore, we believe the submitted SEPA Environmental Checklist fails to sufficiently identify the extent of all impacts such a land use change would create and therefore, should be rejected.

Therefore, we believe Comprehensive Plan Amendment 2022-0005 does not meet the requirements of Kennewick Municipal Code 4.12.110(7) and (8) and should be denied.

Thank you for the opportunity to comment on the proposed Comprehensive Plan Amendment.

Respectfully,

Dominic and Kathy Sansotta
6925 W. 23rd Ct.
Kennewick, WA 99338

Email: domkathy@hotmail.com

**Comprehensive Plan Amendment (CPA) 2022-0005
Revised Mitigated Determination of Non-Significance (MDNS)
Dated August 10, 2022**

ACTION REQUESTED: Per Washington Administrative Code (WAC) 197-11-340(2)(f), the below listed residents of SouthCliffe requests that the City withdraw or modify the Revised Mitigated Determination of Non-Significance (MDNS) issued for public comment on August 10, 2022:

SouthCliffe Residents:

Dominic & Kathy Sansotta
Jonathan & Christal Dickman
Tom Fillmore
Ann Lariver
Reed & Lisa McKinlay
Tim Fenske
Isaac Henry & Jessica Percifield
Bret & Amber Morales

The requested actions are needed to clarify the scope of the MDNS and to add further Mitigation Conditions to address probable, significant, adverse environmental impacts.

SCOPE OF THE MDNS: The scope of the MDNS is undefined. This is because the Applicant has failed to identify the future development that would be allowed by this Land Use Designation change and the associated impacts. The State Environmental Policy Act (SEPA) regulations and Checklist guidance are clear that the impacts of future development from a Comprehensive Plan Amendment must be considered.

The Application and the SEPA Checklist submitted for CPA 2022-0005 do not meet SEPA requirements (WAC 197-11) and guidance. These require the Applicant to submit a completed SEPA Checklist that fully describes the proposal, the potential impacts from developing this land as proposed, and possible mitigating measures.

In summary:

- We believe the SEPA Checklist responses do not contain the information needed “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). There are no substantive responses about impacts and possible mitigations.

- The SEPA Checklist responses do not “apply to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The responses only cover proposed administrative changes to the Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.
- The Applicant answers most of the questions about impacts and mitigation as “Non-Project Action, NPA” or “None”. “SEPA Guidance for Non-Project Actions” states “When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

As submitted, there are at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially. All seven of the questions specifically required for Non-Project Actions (Section D) were not answered at all.

We note that in a prior Application (CPA 20-06) in 2020 for essentially the same site, the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City responded at that time (in a letter from Steve Donovan to Jose’ Chavallo, “Request for Additional SEPA Checklist Information”, dated August 28, 2020):

“The above reference WAC (*Washington Administrative Code 197.11*) and GMHB (*Growth Management Hearings Board*) cases clearly state that answering questions in an environmental checklist for a non-project action with a reference to the fact that the proposal is a non-project action is not sufficient.

You must consider your request and the resulting zoning (if approved) and what is permitted within the new zoning district; responding to the questions more specifically and providing likely impacts such as traffic generated by a likely proposal and possible mitigation to address the likely impacts. The response “NPP” is not acceptable and must be changed to reflect the previous comments.”

The City identified 40 checklist questions in that Application that needed additional details or clarification.

We believe the City should require the same of this application.

However, the City has chosen to reference some of the documents from CPA 20-06 without requiring the Applicant to identify specifically the possible future uses of the site, as was done eventually for CPA 20-06.

Nowhere in the CPA 2022-0005 documents does the Applicant mention potential future uses, although it appears the City is assuming that this may involve a 60-room hotel and five hundred condominium units.

The City should require the Applicant to submit a completed SEPA Checklist that identifies potential future uses of the site and answers all relevant questions accordingly, including potential impacts. Only then could the City make a valid and complete Environmental Determination on CPA 2022-0005.

MITIGATION CONDITIONS: We recommend that the following Mitigation Conditions be added to the final Environmental Determination. These Conditions are needed to address impacts to the following elements of the environment (per WAC 191-11-444) that are not adequately addressed. The relevant questions in the SEPA Checklist (in quotes) were not answered at all or substantively.

1. **“Relationship to existing land use plans and to estimated population”** per WAC 197-11-444(2)(b)(i).
 - “B.8.a. Will the proposal affect current land uses on nearby or adjacent properties?” *Not answered.*
 - “B.8.l. Proposed measures to ensure the proposal is compatible with existing and projected land use and plans?” Answer: “Amend City of Kennewick Comprehensive Zoning designation” *This does not ensure compatibility.*
 - “B.9.a. “Approximately how many (*housing*) units will be provided?” Answer: “NPA (*Non-Project Action*).” *This response avoids answering the question.*

Note that in CPA 20-06 the City previously determined that High Density Residential was incompatible with all existing and planned Low Density Residential neighborhoods that border the site.

Also, the maximum number of allowable housing units would increase from about 153 single-family homes for Low Density Residential to 557 multi-family housing units for High and Medium Density Residential. This is an increase of 3.6X in allowable housing density.

The City should impose a Mitigation Condition to ensure compatibility with surrounding neighborhoods.

2. **“Vehicular traffic”** per WAC 197-11-444(2)(c)(ii).
 - “B.14.d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities?” Answer: “NPA”. *This response avoids answering the question.*

- “B.14.f. How many vehicular trips per day would be generated by the completed proposal...?” Answer: “NPA”. *This response avoids answering the question.*
- “B.14.h. Proposed measures to reduce or control transportation impacts?” Answer: “NPA”. *This response avoids answering the question.*
- “D.6. How would the proposal be likely to increase demands on transportation or public services and utilities?” *No answer.*

Note the number of maximum allowable housing units would more than triple, with corresponding increases in traffic. Yet the Applicant failed to provide any information in the CPA 2022-0005 Application on transportation and traffic impacts.

In the MDNS, the City proposes four Mitigation Conditions (conditions 2, 3, 4 and 11) to address transportation and traffic impacts. We concur with conditions 2, 3 and 4. However, we recommend that Condition 11 be amended to require that the traffic impact analysis address the additional impact of traffic from CPA 2022-0005 in conjunction with the pertinent traffic data and impact analyses from all nearby neighborhoods and planned developments. This would provide a more comprehensive prediction of the future local traffic situation in this area for decision-makers.

Further, cut-through traffic in existing neighborhoods from other new Southridge Area developments (including schools) has become an increasing safety concern. Thus, the CPA 2022-0005 traffic impact analysis should include the potential impacts of cut-through traffic in other neighborhoods. If needed, traffic calming measures should be required to avoid making these problems even worse.

3. **“Aesthetics”** per WAC 197-11-444(2)(b)(iv).

- “10.a. What is the tallest height of any proposed structure?” Answer: “NPA”. *This response avoids answering the question.*
- “10b. What views in the immediate vicinity would be altered or obstructed?” Answer: “None”. *This response avoids addressing this obvious impact of higher density development on the top of Thompson Hill.*
- “10.c. Proposed measures to reduce or control aesthetic impacts?” Answer “N/A”. *This response avoids answering the question about obvious impacts.*

The proposed changes in Land Use Designation would allow high and medium density development of large structures up to 45 feet tall. This could have significant impact to the views from surrounding neighborhoods. Also, it would change the iconic view of Thompson Hill from throughout the Tri-Cities area.

We request a mitigation condition to limit the height of structures to no more than 35 feet. Further, to preserve the iconic views of Thompson Hill, large structures should be prohibited on the ridgeline.

4. **“Noise”** per WAC 197-11-444(2)(a)(i).
 - “7.b.2. What types and levels of noise would be created by or associated with the project on a short-term or long-term basis?” Answer: “NPA”. *This response avoids answering the question.*
 - “7.b.3. Proposed measures to reduce or control noise impacts?” Answer: “None.” *This answer avoids addressing a probable significant impact.*

In the past, there have been many noise problems and complaints with prior activities on the top of Thompson Hill. A large complex of multi-family housing units with open spaces and outdoor recreation will create noise issues. Further, if a hotel is built, those noise issues could become significantly worse.

We request that the City impose a Mitigation Condition for strict compliance with KMC 9.52 “Noise”.

5. **“Habitat for and numbers or diversity of species of wildlife and unique species”** per WAC 197-11-444(1)(d)(i and ii).
 - “B.5.d. Proposed measures to preserve or enhance wildlife?” Answer “None”.

The City has identified the site as part of a Critical Area for wildlife habitat conservation per KMC 18.63. Therefore, compliance with KMC 18.63 should be a Mitigation Condition.

NEW INFORMATION: Any new information provided by the Applicant or generated by the City should be provided for public review at least 14 days before the public hearing. This will allow the public time to read, understand and comment on any such new information.

Thank you.

From: [Susan Dixon](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com
Subject: CPA 2022-0005
Date: Wednesday, August 3, 2022 3:36:00 PM

Dear City Council Members, Planning Commissioners and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

I live at 2500 S. Irving Street, Kennewick.

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi-family housing units. High Density Residential would also allow a hotel.

I am opposed to CPA 2022-0005. I'm disappointed that this proposal is ignoring the City of Kennewick's Comprehensive Plan for our area. We purchased our home because we were attracted to low-density living and expected future development to respect that designation. Traffic has already increased in our area exponentially and this current proposal would increase that considerably. Kennewick's City Planners and City Council should first and foremost respect the nature and livability of existing neighborhoods before considering a land-use change to increase housing density. Would you vote to increase housing density adjacent to your neighborhood?

The City Council should vote to deny this proposal because it would have negative impacts on the surrounding neighborhoods, is not in the best interest of the Citizens of Kennewick, and ignores the thought-out City of Kennewick's Comprehensive Plan for our area.

Thank you for your consideration of my concerns and my opposition to this Land Use Designation change.

Susan Dixon

susan.levesque.dixon@gmail.com

To: Steve Donovan

Re: Proposed "Comprehensive Plan Amendment (CPA) 2022-0005

Greg & Vonda Smith are opposed to the proposed "Comprehensive Plan Amendment (CPA) 2022-0005. It would change the Land Use Designation from Low Density Residential to High Density Residential for the top 4.3 acres, and to Medium Density Residential for the 35.5 acres near the top on the North and South slopes. We request that the Planning Commission recommend that this request be denied, and that the City Council deny this request.

Numerous developments of this part of Thomson Hill have been proposed over the past 15 years. This proposal would have significant impacts on the surrounding neighborhoods. This development was proposed a couple of year ago and it was denied for the same reasons that it should not be approved at this time. If anything, the circumstances would be even more negative due to the increased traffic since that time due to the continued development of homes in Southridge that have already been built. There are acres of property on the Bob Olsen Parkway that could most definitely satisfy the below stated requests in the amendment.

- The City of Kennewick is needing additional areas for Medium and high-density housing.
- The City of Kennewick needs more variety in housing styles and types of construction.
- A more flexible design area will facilitate growth and attractive livability within the Southridge area. (This would create the exact opposite result in the Southridge area that has already been built and developed!)

We live at 2703 S Irving St. in Panoramic Heights, the neighborhood that borders this site on the east side of Thompson Hill. We will be most impacted by high density residential development. High density residential development allows up to 27 multi-family housing units per acre. These could be apartments, condominiums, town houses, row houses or similar types of housing. Up to 1,100 of these types of housing units could be packed into these 40.6 acres of steep terrain, with slopes over 40 percent, and no current vehicle access to most of the site.

Specifically, we oppose this proposed amendment for the following reasons:

- This area was not built to handle this amount of traffic. There are already challenges with the new housing developments and erosion from both rain and wind.
- Many of the families that have recently moved in this area have small children and the traffic that does not stop now endangers their well-being. Adding 4+ times that traffic is not acceptable! Currently it is a speedway on the street right below our house! It has become the main thoroughfare for traffic from Kellogg and Irving through the neighborhood to both Southridge High School as well as morning and evening work traffic connecting to Hildebrand. They have installed speed humps on the lower streets, so this is now the choice of travel. In addition, it is the route that is given on Google if someone is using their GPS. They typically do not even slow down much less stop at the STOP sign on Irving St.
- There needs to be better planning to keep the City of Kennewick specifically Southridge a place that people want to live instead of Richland or West Richland where they are clearly NOT placing this type of high density bascially commercial development as the center of a residential community. There is an adequate supply of land to expand this type of development as the

growth of the Southridge area continues, but the is NOT a good choice for wise development of the city. PLEASE consider the current residents and how you would feel if your home was going to be compromised by this variance. We all moved and built here because of the Comprehensive Plan that was in place at the time.

Please thoroughly address these serious concerns and potential impacts before amending the Comprehensive Plan.

In summary, (I or we) believe that this proposed amendment does not meet your approval criteria that it "bears a substantial relationship to the public health, safety, welfare, and protection of the environment." Nor have the following factors been considered thoroughly prior to approving this Comprehensive Plan Amendment:

- The effect upon the physical environment.
- The effect upon open space and natural features, including topography.
- The compatibility with and impact on adjacent land uses and neighborhoods.
- The adequacy of, and impact on community facilities, including utilities, roads, public transportation, parks, recreation, and schools.
- The current and projected project density in the area; and
- The effect upon other aspects of the Comprehensive Plan.

Therefore, "Comprehensive Plan Amendment (CPA) 2022-0005 does not meet the requirements of Kennewick Municipal Code 4.12.110(7) and (8) and should be denied.

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

Greg & Vonda Smith
2703 S Irving St.
Kennewick, WA 99338
vondagreg@aol.com

From: [VONDA SMITH](#)
To: [Steve Donovan](#)
Cc: info@panoramicheightshoa.com
Subject: Comprehensive Plan Amendment (CPA) 2022-0005
Date: Friday, August 19, 2022 1:56:00 PM

August 19, 2022

Dear City Council Members, Planning Commissioners and City Staff: Re: Comprehensive Plan Amendment (CPA) 2022-0005

We live at 2703 S Irving St. Kennewick, WA 99338

CPA 2022-0005 requests changing the Land Use Designation of about 40 acres on the top and upper slopes of Thompson Hill. About 4.3 acres would increase from Low Density Residential to High Density Residential, and about 35.5 acres would increase from Low Density Residential to Medium Density Residential. This would greatly increase the housing density and allow multi-family housing units. High Density Residential would also allow a hotel.

We are opposed to CPA 2022-0005 for these reasons.

• This area was not built to handle this amount of traffic. There are already challenges with the new housing developments and erosion from both rain and wind. We have water flowing under our fence from the current development on the hill and could potentially flood our crawlspace.

• Many of the families that have recently moved in this area have small children and the traffic that does not stop now endangers their well-being. Adding 4+ times that traffic is not acceptable! Currently it is a speedway on the street right below our house! It has become the main thoroughfare for traffic from Kellogg and Irving through the neighborhood to both Southridge High School as well as morning and evening work traffic connecting to Hildebrand. They have installed speed humps on the lower streets, so this is now the choice of travel. In addition, it is the route that is given on Google if someone is using their GPS. They typically do not even slow down much less stop at the STOP sign on Irving St.

• There needs to be better planning to keep the City of Kennewick specifically Southridge a place that people want to live instead of Richland or West Richland where they are clearly NOT placing this type of high density basically commercial development as the center of a residential community. There is an adequate supply of land to expand this type of development as the growth of the Southridge area continues, but this is NOT a good choice for wise development of the city. PLEASE consider the current residents and how you would feel if your

home was going to be compromised by this variance. We all moved and built here because of the Comprehensive Plan that was in place at the time.

Please thoroughly address these serious concerns and potential impacts before amending the Comprehensive Plan.

In summary, (I or we) believe that this proposed amendment does not meet your approval criteria that it “bears a substantial relationship to the public health, safety, welfare, and protection of the environment.” Nor have the following factors been considered thoroughly prior to approving this Comprehensive Plan Amendment:

- <!--[if !supportLists]-->● <!--[endif]-->The effect upon the physical environment.
- <!--[if !supportLists]-->● <!--[endif]-->The effect upon open space and natural features, including topography.
- <!--[if !supportLists]-->● <!--[endif]-->The compatibility with and impact on adjacent land uses and neighborhoods.
- <!--[if !supportLists]-->● <!--[endif]-->The adequacy of, and impact on community facilities, including utilities, roads, public transportation, parks, recreation, and schools.
- <!--[if !supportLists]-->● <!--[endif]-->The current and projected project density in the area; and
- <!--[if !supportLists]-->● <!--[endif]-->The effect upon other aspects of the Comprehensive Plan.

Therefore, "Comprehensive Plan Amendment (CPA) 2022-0005 does not meet the requirements of Kennewick Municipal Code 4.12.110(7) and (8) and should be denied.

Thank you for considering these concerns and for serving the people of Kennewick.

Respectfully,

vondagreg@aol.com

Greg & Vonda Smith
2703 S Irving St.
Kennewick, WA 99338

RE: The Proposed Redevelopment of Thompson Hill

Reference: CPA 2022-0005

Dear Sir,

This makes me so frustrated. If you are positioned in a place of responsibility are you a part of the problem?

You cannot rezone an area where everyone currently living there bought homes BECAUSE of the existing zoning,

What then would be the value or purpose of these zones if they can be changed even if everyone there objects?

This developer bought an existing property and land in a determined zone and IMMEDIATELY declared his intention to ignore the zoning, He began work that he knew was not legal; he pushed against rules and regulations, deliberately looked to destroy housing association covenants and now we are being told that by using these despicable bully tactics he will get his way?!

This will not do!

What is the point of housing associations, rules and regulations if those who don't want to follow them don't have to? This is lawlessness.

What is the point or use of city officials; planning departments; local government etc if they do not attend to and protect the wishes and properties of the private individuals who pay their wages through local taxes? If they do not maintain the status quo of settled; established communities? If they do not follow through on existing regulations already in place?

Corporations, companies, should not over ride the will of the people in a community to the extent that the individual is robbed of the ability to live peaceably in their homes and neighborhoods.

This is the second time this has become an issue, we faced and argued and had an agreement just last year that this rezoning, high density development and commercial usage is not wanted, not appropriate and not considered safe for the infra structure around us.

Nothing has changed; except elected officials who suspiciously stood for local government seats and who happen to be private developers and realtors. This smacks of self promotion of self interest. Elected representatives who promote and pass business that will benefit them is corruption.

It is obvious that they are working for the Southridge communities. This is all very questionable.

Dawn Thomas

5216 W. 26th Ave. Kennewick WA 99338

From: [Dawn Thomas](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com
Cc: ["Bill Dixon"](#); ["Ivan Thomas"](#)
Subject: RE: Proposed Thompson Hill Development.
Date: Thursday, August 11, 2022 2:33:51 PM
Attachments: [Thompson Hill Revisited.docx](#)

No, No. No. And again. NO.

My previous letter and our views still stand, what do you all not understand?

This man bought land accepting the current zoning as he signed.

He immediately began digging, moving earth and commenced plans to build non zone compliant facilities.

He is lawless and this arrogance and deliberate intention to ignore rules, regulations and existing communities to pursue his original intentions cannot be allowed to succeed.

It is noticeable that we have developers on our board now who may have personal and financial relationships with this developer and so find their opinions and decisions grossly affected; and others who possibly have worked with this developer for personal gain in the past.

Therefore they should possibly be disqualified from this long drawn out constant attempt to wear down the people of Panoramic Heights et al.

To reiterate the immutable facts:

This is a low density zone. Everyone who purchased a home here... including the developer, knew that prior to purchase. There are no businesses; it is for families. That is why we bought our homes here.

You cannot change a zone while everyone; except one who wants to make money by making everyone of the neighbors miserable; wants what they have, what they bought and signed to maintain.

You will grossly affect the value of our homes... not yours because you don't live here.

The level of noise, traffic, road wear, foot traffic will be greatly multiplied.

Strangers will be constantly running through our neighborhoods, putting our families... not your families... at possible risk.

You are elected to look after the existing neighborhoods, the safety, rights, laws are to be upheld, maintained and protected by you. You are not supposed to be moving and shaking with political positions to make yourselves and your friends more money. That's corrupt.

If this goes through it will be because someone is going to make a lot of money... a realtor? Developer? You?

You represent the Law you are NOT above it.

Do not accept ANY developing plans that involve rezoning.

Dawn Thomas

5216 W. 26th Ave
Kennewick, WA 99338

RE: The Proposed Redevelopment of Thompson Hill

Reference: CPA 2022-0005

Dear Sir/Madam,

This makes me so frustrated. If you are positioned in a place of responsibility are you a part of the problem?

You cannot rezone an area where everyone currently living there bought homes BECAUSE of the existing zoning,

What then would be the value or purpose of these zones if they can be changed even if everyone there objects?

This developer bought an existing property and land in a determined zone and IMMEDIATELY declared his intention to ignore the zoning, He began work that he knew was not legal; he pushed against rules and regulations, deliberately looked to destroy housing association covenants and now we are being told that by using these despicable bully tactics he will get his way?!

This will not do!

What is the point of housing associations, rules and regulations if those who don't want to follow them don't have to? This is lawlessness.

What is the point or use of city officials; planning departments; local government etc if they do not attend to and protect the wishes and properties of the private individuals who pay their wages through local taxes? If they do not maintain the status quo of settled; established communities? If they do not follow through on existing regulations already in place?

Corporations, companies, should not over ride the will of the people in a community to the extent that the individual is robbed of the ability to live peaceably in their homes and neighborhoods.

This is the second time this has become an issue, we faced and argued and had an agreement just last year that this rezoning, high density development and commercial usage is not wanted, not appropriate and not considered safe for the infra structure around us.

Nothing has changed; except elected officials who suspiciously stood for local government seats and who happen to be private developers and realtors. This smacks of self promotion of self interest. Elected representatives who promote and pass business that will benefit them is corruption.

It is obvious that they are not working for the Southridge communities. This is all very questionable.

Dawn Thomas

5216 W. 26th Ave. Kennewick WA 99338

Added to the public survey form I filled in 8/1/2022:

You cannot rezone an area where everyone currently living there bought homes BECAUSE of the existing zoning. This is the second time this has become an issue in a year, Nothing has changed; except elected officials, This smacks of self promotion of self interest, & conflict of interest. Who benefits? Realtors & developers.. now Board members.

Comments for Proposed Comprehensive Plan Amendment CPA 2022-0005

Steve Varner, Homeowner
5325 W. 25th Ave
Kennewick, WA
24 June 2022

Dear City Staff and City Council Members:

I find the Application is not complete or accurate for Comprehensive Plan Amendment (CPA) 2022-005 and therefore should be revised or rejected. As a design professional in the A/E industry for 40 years, I have a lot of experience with the process of this CPA represents. As is, I find this CPA unacceptable as submitted for this area of Kennewick. The following is my list of concerns:

Comment # 1:

The SEPA Checklist document submitted does not meet State Environmental Policy Act (SEPA) requirements and guidance. The potential future impacts from developing this land as proposed, and possible mitigating measures has many misleading, missing information, and false statements. Many answers were stated as, "Non Project Action (NPA)" and several were not even answered. Based on inserting a USGS Map into AutoCAD Civil 3D program, the proposed parcel of land has several acres that exceed 40% slope which should not be built on, or as a minimum, zoning restrictions. The SEPA states much less. If this is not corrected as the design goes forward the City will be open for litigation for not properly disclosing. Based on experience, the proposed property will have significant challenges with storm drainage and retention. No mention of these concerns are listed. Another missing SEPA information is that is if the current low density to high density, it would require significant demolition of an existing large house, secondary structures, and a large pool. Demolition is listed as none. When I was submitting SEPA's years ago, the missing, misleading, and incorrect information would have been rejected – I'm not sure why this one was not as well.

Comment # 2:

development has several negative impacts that should be highlighted related to existing topography: This CPA should identify buildable and non-buildable areas. As mentioned, the Topo plan I generated with AutoCAD Civil 3D indicates significant area that exceed 40% slopes. This could reduce zoning density in specific areas but is currently not identified. Based on experience, developments that exceed 20%, storm drainage & runoff issues are a major concern and a challenge during the design phase. These concerns are not identified in the SEPA. Currently, there are unresolved issues with running storm water diversion below existing KID irrigation canal.

Comment # 3:

Panoramic Heights area has fought the issue of pass-through traffic for years. We have had speed tables and speed signs installed over the last 3 years to mitigate the issue that still exists. This CPA does not identify traffic impact or how the proposed development would be accessed. This is number one issue for our area.

Comment # 4:

Concerns regarding Hillside Developments - The subdivision of hilly areas is a growing problem in Kennewick and Tri-Cities in general. Hills once bypassed as too costly to build on are now prime residential areas for subdividers for the very reason that they were bypassed and are closer to the metropolitan center than the nearest vacant flat land.

Unfortunately, hillsides are difficult and costly to subdivide. Developers — to help reduce costs — have appealed to planners to write subdivision controls that have same standards for hillside areas than flat land. But the problems peculiar to hillside subdivision often require controls that increase development costs. Experience shows, however, that if the controls written for flat lands are not modified for hillside use, subdividers will simply do the minimum requirements to maximize profits.

Unstable cuts and fills because of grading, erosion, streets, storm water drainage, sewage disposal, water supply, access for firefighting, and disposition of unusable land are problems of hillside areas that cities are attempting to solve. But there are so many problems that no single approach solves them all. City of Kennewick has made no attempts from zoning ordinances, as well as provisions from subdivision ordinances especially written for hillside areas.

Zoning problems are considered first in the report; then the changes needed in flat land subdivision controls to adapt them to hillside uses; third, the interrelationship of density and street standards; fourth, grading controls; and finally, other problems. Zoning and Density - Common sense points to low densities for all steep, hilly areas, although low densities do not necessarily mean large lots. Lot sizes should be quite different for houses built on a slope from what they are for those built on the flat top of hillside ridges or in valley flat lands. For instance, ridges or knobs of the hills, relatively flat land, offer ideal sites for single-family houses on small lots or for multi-family buildings. However, houses are frequently built on the actual slope. The "flat" site may accommodate a house on a quarter-acre, while the "slope" site may require much larger lot; and in either case some areas of a site may be completely unsuitable for construction. If all hillsides were uniform, area zoning would be a perfect solution, but such is not the case. Often a hillside is so steep and has so many ravines that one building site per two lots is necessary, yet nearby there may be benches or ridge-tops that are flat. A developer should be held to averages in lot size, with a more lenient minimum when justified, rather than to so-called averages which become minimum.

Thank you for considering my comments and concerns.

Steve Varner, Homeowner to Adjacent Proposed Development.
5325 W. 25th Avenue
Kennewick, WA 99338
509.551.4677

S1varner62@yahoo.com

From: [Frank Wentz](#)
To: [Steve Donovan](#)
Subject: CPA 2022-0005
Date: Sunday, June 12, 2022 7:06:19 AM

This proposal would significantly increase traffic on residential streets through Panoramic Heights. Those existing streets were not designed as through streets. The result would be dangerous and unsafe conditions for residents in Panoramic Heights.

Francis Wentz
4908 West 27th Ave.

From: [IEEE Gmail](#)
To: [Steve Donovan](#)
Subject: CPA-2022-0005
Date: Wednesday, June 22, 2022 4:01:22 AM

Please consider this email as me being opposed to the applicants request to change the land use designations at 2701 and 2711 S Sherman St from low density residential (LDR) to medium density residential (MDR) and high density residential (HDR). The request to rezone is inconsistent with nearby surrounding areas and will negatively impact existing residential traffic.

Clint Whitney
6899 W 23rd Ave
Kennewick, WA 99338

From: [Bill Dixon](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [Anthony Muai](#); [Melinda Didier](#)
Cc: info@panoramicheightshoa.com
Subject: Comprehensive Plan Amendment (CPA) 2022-0005: Opposition by Panoramic Heights Homeowners Association
Date: Wednesday, September 7, 2022 12:10:27 PM
Attachments: [PHHA Position on CPA 2022-0005 \(1\).pdf](#)

Dear City Council Members, Planning Commissioners, and City Staff:

On behalf of the Panoramic Heights Homeowners Association (PHHA), I respectfully submit the attached letter of opposition to Comprehensive Plan Amendment (CPA) 2022-0005. We request that the Planning Commission recommend that the City Council not adopt the Proposal. Then, we request that the City Council deny CPA 2022-0005.

This decision was not taken lightly. We spent nearly four months to review, evaluate, discuss and comment on all publicly available information. We also sought input from our individual members and people in surrounding neighborhoods. Ultimately, our Board of Directors decided to oppose this proposed large increase in housing density.

CPA 2022-0005 would allow unspecified developments with up to 557 multi-family housing units; as well as motels, hotels and similar accommodations on the top and upper north and south slopes of Thompson Hill. This change would increase the allowable housing density on this 38.24 acres by a factor of 3.7. Therefore, this Land Use Designation change from Low Density Residential to High and Medium Density Residential would result in many significant impacts on our neighborhood, other surrounding neighborhoods, and the entire Southridge area.

Our analysis shows that on balance this proposal is does not benefit public health, safety and welfare; and protection of the environment. Nor is it consistent with Kennewick's Comprehensive Plan. These are Approval Criteria for a Comprehensive Plan Amendment (per KMC 4.12.110(7)).

Further, we believe that the Applicant has not provided the required information to prove that CPA 2022-0005 will not adversely impact the following Additional Factors that you must consider in your decision (per KMC 4.12.110(8)):

- The current and projected property density in the area;
- The compatibility with and impact on adjacent land uses and surrounding neighborhoods;
- The adequacy of, and impact on community facilities, including roads;
- The effect on open space and natural features, including topography;
- The effect upon the physical environment; and
- The effect upon other aspects of the Comprehensive Plan.

Let us be clear that PHHA does not oppose reasonable and responsible developments on Thompson Hill. These developments should be compatible with the surrounding neighborhoods with low density, single family homes. They should be designed with access roads that do not force traffic through adjacent neighborhood streets. And, they should be built on slopes of less than 15%, such as Panoramic Heights and Citadel Estates.

In fact, we now view Citadel Estates as a model for the type of high-end single family homes on large, terraced view lots that could be built on the top and upper slopes of Thompson Hill.

We hope that you agree with our conclusion and deny CPA 2022-0005. Reasonable and responsible low density residential homes on large view lots can be built on Thompson Hill with the existing Low Density Residential land use designation and current residential zoning.

Bill Dixon, on behalf of the Panoramic Heights Homeowners Association

2500 S. Irving St.

Kennewick, WA 99338

509.531.5913

wtdixon3@gmail.com



Panoramic Heights Homeowners Association

www.panoramicheightshoa.com

September 7, 2022

Dear City Council Members, Planning Commissioners, and City Staff:

Re: Comprehensive Plan Amendment (CPA) 2022-0005

This letter is on behalf of the 159 households in the Panoramic Heights Homeowners Association (PHHA). I was appointed by the Board of Directors to represent PHHA.

PHHA has devoted nearly four months to review, evaluate, discuss and comment on all publicly available information related to CPA 2022-0005. This included submitting three prior comment letters to the City on May 12, June 23, and August 23, 2022.

Further, we have received personal input from about 100 households in neighborhoods throughout the Southridge area including: Panoramic Heights, South Cliffe, Creekstone, Apple Valley, Southridge Estates, Windsong and several others. These households have similar concerns about the impacts of CPA 2022-0005, and all but one expressed their opposition to this Proposal. Their top concerns are:

1. Increased traffic
2. Incompatibility with existing neighborhoods
3. Steep slopes, runoff and landslides
4. Decrease in property values
5. Noise and light pollution.

We have shared some of that public input with you previously in interim reports, and will provide a final report shortly.

Based on our extensive evaluation and the public input, **PHHA opposes CPA 2022-0005.**

Per Kennewick Municipal Code (KMC) 4.12.110 procedures, **we request that the Planning Commission recommend that the City Council not adopt the Proposal. Then we request that the City Council deny the Application.**

This Proposal would change the Land Use Designation from Low Density Residential to High and Medium Density Residential on 38.24 acres on the top, and north and south upper slopes of Thompson Hill. We conclude that CPA 2022-0005 does not meet the Approval Criteria and Additional Factors in Kennewick Municipal Code (KMC) 4.12.110.7 and 8. The Proposal would allow significant adverse impacts on the property, in surrounding neighborhoods, and throughout the Southridge area. These impacts and concerns are detailed as follows.

INCOMPLETE APPLICATION AND SEPA CHECKLIST

As stated in our previous letters, the Application with its State Environmental Policy Act (SEPA) Checklist are incomplete and inadequate. Most of the required information about environmental impacts is missing. Specifically:

- The SEPA Checklist requires responses “to determine if available avoidance, minimization or compensatory mitigation measures will address the probable significant impacts” (per “SEPA Checklist Purpose”). The Applicant provides no substantive responses about impacts and possible mitigations.
- The SEPA Checklist “applies to all parts of your proposal, even if you plan to do them over a period of time” (per “SEPA Checklist Instructions”). The Applicant’s responses only cover proposed administrative changes to the Comprehensive Plan Land Use Designation, and do not address the future impacts of those changes.
- The Applicant simply answers most of the Checklist questions about impacts and mitigation by stating “Non-Project Action, NPA” or “None”. The SEPA Guidance for Non-Project Actions states clearly:

“When a non-project action involves a comprehensive plan or similar proposal governing future project development, the probable environmental impacts that would be allowed for the future development need to be considered.” (emphasis added)

As submitted, there were at least 37 pertinent Checklist questions about future development and impacts that are not answered substantially or at all. Also, all seven of the questions specifically required for Non-Project Actions (Section D) were not answered.

In a prior Application (CPA 20-06) in 2020 for essentially the same site, the same Applicant failed to answer most key questions, claiming “Non-Project Proposal”, or “NPP”. The City responded at that time (in a letter from Steve Donovan to Jose’ Chavallo, “Request for Additional SEPA Checklist Information”, dated August 28, 2020):

“The above reference WAC (Washington Administrative Code 197.11) and GMHB (Growth Management Hearings Board) cases clearly state that answering questions in an environmental checklist for a non-project action with a reference to the fact that the proposal is a non-project action is not sufficient.”

“You must consider your request and the resulting zoning (if approved) and what is permitted within the new zoning district; responding to the questions more specifically and providing likely impacts such as traffic generated by a likely

proposal and possible mitigation to address the likely impacts. The response “NPP” is not acceptable and must be changed to reflect the previous comments.”

Therefore, since the Application and SEPA Checklist do not provide the required information, that is sufficient basis alone for the City to deny CPA 2022-0005.

ADDITIONAL FACTORS

Per KMC 4.12.110.8, the City must consider the following factors prior to approving Comprehensive Plan Amendments.

“The current and projected property density in the area”

Per KMC 18.12.010 A.2, this Land Use Designation change would allow future developers to build up to a maximum of 557 multi-family housing units (at 27 units per acre for High Density, and 13 units per acre for Medium Density) on lots as small as 1600 square feet. Currently, future developments are limited to a maximum of about 150 single family homes (about 4 per acre) on lots of at least 10,500 square feet. This change would allow the housing density to increase by a factor of 3.7.

This increased housing density will have significant impacts, including the increased traffic that would occur.

“The compatibility with and impact on adjacent land uses and surrounding neighborhoods”

High Density would also allow future development of motels, hotels and similar accommodations (per KMC 18.12.010 B.1), although the CPA 2022-0005 Application does not address any such development and its impacts.

In addition, High and Medium Density Land Use Designation would not be consistent with all of the neighborhoods bordering the site, including Citadel Estates. All of these neighborhoods consist of single family homes on medium to large residential lots larger than 7,500 or 10,500 square feet (depending on zoning).

Therefore, this Application is incompatible with adjacent land uses and surrounding neighborhoods.

Note that in 2021, the City determined that a High Density Residential Land Use Designation for this site was incompatible with all existing and planned Low Density residential neighborhoods that border this site.

“The adequacy of, and impact on community facilities, including...roads”

The Applicant provides no information on traffic impacts. This would be a significant impact from allowing higher density developments.

Based on prior traffic impact analyses, it appears that the Average Daily Traffic at full buildout of up to 557 multi-family housing units on site could be on the order of 3,000 vehicle trips per day, with peak hours being as much as 250 vehicles. If a small hotel were to be built (which is not mentioned in the CPA 2022-0005 Application), that could add about another 500 vehicle trips daily and 35 more vehicles in the peak hour.

Obviously, this large increase in traffic would impact existing local roads. It would also impact some neighborhood streets with cut-through traffic. These same neighborhoods are already suffering increased traffic from other Southridge area developments; including schools, a hospital and businesses.

Therefore, there is no information provided by the Applicant for the City to consider the effect of potential traffic impacts from future developments.

PHHA appreciates the three Mitigation Conditions imposed by the City on August 10, 2022 to prevent direct traffic from accessing the site throughout our neighborhood streets, except for emergency vehicles. However, we remained concerned about additional cut-through traffic due to this Proposal.

“The effect on open space and natural features including...topography”

The Applicant fails to address the nature, extent and impacts of steep slopes on the site, particularly on the north slope. In the SEPA Checklist, the Applicant states that the steepest slope on the site is “30-40%, however the area to be developed is somewhat in the range of 20% or flatter”. It is known that portions of the site have extreme steep slopes of 40% or greater, and much of the site has steep slopes greater than 15%. (Checklist questions B.1.a and b).

Development on steep slopes would pose many hazards. The Applicant fails to identify which parts of the site are within the known Geologically Hazardous Area for Steep Slopes and to address the impacts of these steep slopes upon development.

Also, the Applicant fails to discuss the land with slopes greater than 40% which cannot be developed.

Note that in a prior application for this same site (CPA 20-06) in 2020, the Applicant stated to the Planning Commission (in “Applicant’s Pre-Hearing Memorandum, April 19, 2021”) that:

“The north slope includes geologically hazardous critical areas and offers limited, if any, development opportunities.” (page 4)

“Lot 37...consists primarily of geologically hazardous slopes. The steep slope area extends along the northern perimeter of the amendment property...Lots 37 and 38 contain geologically hazardous critical areas which severely constrain

development.” (page 7) *(Note that Lots 37 and 38 are a large portion of the site proposed in CPA 2022-0005.)*

“A significant portion of the north face of the subject property will remain “open space” in perpetuity.”(page 8)

“The topography and presence of geologically hazardous critical areas on the north slope poses significant development restrictions to both Low Density Residential and High Density Residential project proposals.” (page 12)

“Significant portions of both Lot 37 and Lot 38 (which include the north slope) are undevelopable critical areas...In addition to absolute prohibitions on development, the remaining north slope presents significant development impediments for any residential development—single-family residential or multi-family residential.” (page 15)

Note that a topographic map of the site shows that nearly half of the site is on the north slope.

Therefore, the Applicant provided no substantive information for the City to fully understand and consider the effects of various steep slopes throughout this site. It is clear that portions of the site are unsuitable for higher density residential development.

Note that on August 10, 2022, the City imposed a Mitigation Condition to explicitly prohibit development on slopes of 40% or greater. Also, a Critical Areas Report must be prepared at the time of a specific development proposal.

“The effect upon the physical environment”

The Applicant’s SEPA Checklist for CPA 2022-0005 does not address many of the obvious impacts on the physical environment. Some key omissions include:

- Filling, excavation and grading
- Erosion
- Air emissions (including dust)
- Water
- Vegetation
- Wildlife
- Noise
- Height of structures, views and aesthetic impacts
- Light or glare
- Cultural resource protection
- Traffic impacts
- Public services and utilities.

Note that on August 26, 2022, a consultant for the Applicant did provide some information on stormwater, secondary access, and water service.

The City has identified the site as part of a Critical Area for wildlife habitat conservation per KMC 18.63. High and Medium Density Residential buildout across the site would essentially destroy this habitat. The Applicant failed to address this Critical Area.

Therefore, the Applicant provided no substantive information for the City to consider effects upon these various attributes of the physical environment.

“The effect, if any, upon other aspects of the Comprehensive Plan”

The Proposal is inconsistent with the following aspects of the Comprehensive Plan (*in italics*).

- Geologically Hazardous Areas (pages 35 to 37): The site is in a designated *“Geologically Hazardous Area for Extreme Slope Hazards: Severe erosion potential and high probability of slope failure & landslide occurrence, Slopes greater than 25%...Proposed development should avoid impacting critical areas...Mitigation sequencing is listed in the order of preference. 1. Avoiding the impact by not taking a certain action;”*. These hazards and mitigation are not addressed in the proposal.
- The Southridge Master Plan, which is part of the City’s Comprehensive Plan, states specifically that the City should “prohibit housing on slopes in excess of 25%.” (section 2.2.4, Goal 1.H)
- Land Use Goals and Policies: Goal 1, Policy 3 (page 47): *“Require that multi-family structures be located near a collector street with transit, or a near arterial street, or near a neighborhood center.”* Multi-family structures that could be built throughout this site would be without access to public transit and are not near an arterial street or neighborhood center.
- Goal 1, Policy 5 (page 47): *“Encourage adequate pedestrian connections with nearby neighborhood and transit facilities in all residential site development.”* Due to the lack of roads, distances to these facilities, and steepness of the slopes, pedestrian connections would be difficult.
- Goal 3, Policies 2, 3 and 4 (page 48):

“Residential Low Density: Place lands constrained by sensitive areas...or those appropriate for larger lot housing”. This is the current Land Use Designation and is appropriate for steep slopes less than 25% in a Critical Area.

"Residential Medium Density: Place areas that can support high-quality, compact, urban development with access to urban services, transit, and infrastructure".

This proposed land use designation is not appropriate for a site with steep slopes in a Critical Area, not near urban services, without transit service and lacking adequate access.

"Residential High Density: Designate land for Residential High Density (HD) where access, topography, and adjacent land uses create conditions appropriate for a variety of unit types, or where there is existing multi-family development."

This proposed land use designation is not appropriate for a site with steep slopes in a Critical Area, not near urban services, without transit service, lacking adequate access, and not near existing multi-family developments.

Therefore, CPA 2022-0005 is inconsistent with several other aspects of the Comprehensive Plan.

MITIGATED DETERMINATION OF NON-SIGNIFICANCE

On August 10, 2022, the City issued a Mitigated Determination on Non-Significance with eleven Mitigation Conditions. These Conditions are to mitigate probable, significant environmental impacts that could result from the Proposal. The City acknowledged that: "Review of the SEPA Documents submitted for CPA-2022-0005, show that not all questions were answered with enough details..."

In our letter of August 23, 2022, PHHA expressed concern that the scope of the Determination is undefined. The Applicant never identified future development that would be allowed under the proposed Land Use Designation Change and the probable impacts. The City speculated that future development might involve a hotel and several hundred condominiums. In fact, the Application and SEPA Checklist responses never mentioned either, nor addressed the probable, significant environmental impacts from such development.

PHHA agreed with these eleven Mitigation Conditions.

However, we recommended a modification to one Condition to require a more comprehensive traffic impact analysis that includes pertinent traffic data and impact analyses from all nearby neighborhoods and planned developments. This would provide a complete prediction of the future local traffic situation in the area for the public and decision-makers. We also requested that the traffic impact analysis include the potential impacts of cut-through traffic in other neighborhoods. If needed, traffic calming measures should be required to avoid making these problems worse.

We also recommended Mitigation Conditions for these specific environmental impacts, as required by SEPA, which the Applicant failed to address:

- Compatibility with surrounding neighborhoods

- Views from surrounding neighborhoods and the iconic view of Thompson Hill from throughout the tri-Cities area
- Noise pollution
- Protection of the existing Critical Area for wildlife habitat conservation.

PHHA has not yet received a response from the City.

APPROVAL CRITERIA

For all of the above reasons, PHHA believes that the City cannot make the findings required by KMC 4.12.110(7) "Approval Criteria" that:

- **"The proposed amendment bears a substantial relationship to the public health, safety, welfare and protection of the environment" and**
- **The proposed amendment is consistent with...the portion of the City's adopted Comprehensive Plan not affected by the Amendment".**

Future developments allowed by this Land Use Designation change could result in:

- Increased housing density on Thompson Hill by a factor of up to 3.7 times,
- Construction of multi-family housing units on small lots in place of single family homes on large lots,
- Commercial activities, such as motels, hotels and similar accommodations,
- Incompatibility with adjacent land uses and surrounding neighborhoods,
- Significantly increased traffic on Southridge area roads and more cut-through traffic in neighborhoods,
- Housing and roads on steep slopes with associated increased risks, and
- Loss of existing wildlife and habitat.

REQUESTED ACTION

Therefore, the Planning Commission should recommend that the City Council not adopt the Proposal. Then, the City Council should deny the Application for CPA 2022-0005.

Thank you for considering our concerns.

Respectfully submitted,

Bill Dixon, on behalf of the Panoramic Heights Homeowners Association
2500 S. Irving St.
Kennewick, WA 99338

From: lcay3731@charter.net
To: [Steve Donovan](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Bill Mckay](#); "info@panoramicheightshoa.com"
Subject: Proposed Thompson Hill Land Use Designation Change
Date: Sunday, September 11, 2022 8:07:07 PM

Hello:

As a long-time resident of Panoramic heights, I am speaking in opposition to the proposed change to the proposed Thompson Hill Land Use Designation Change.

What has changed? Similar proposals have have been rejected over the past few years.

The developer continues to gloss over properly filling out the proper land use and environmental impact forms. He tries to assure us that everything will be OK but there is no proper proof given

The same problems mentioned before still exist - namely the huge increase in traffic congestion in the streets leaving the site, potential drainage problems from the steep hillside, installation of smaller housing that will potentially lower values for most of us in the area, etc. Have reviews been made as to the effect on policing, schools, access to parks, ?

Than you for your thoughtful consideration and, hopefully, once again rejecting the proposed change.

Lawrence E. Clay
5322 W. 26th Avenue
Kennewick, WA 99336

From: [Melinda Didier](#)
To: [Steve Donovan](#)
Subject: FW: Thompson Hill Project _ Request to pass on to the Planning Commission
Date: Monday, September 12, 2022 8:53:07 AM

fyi

From: Tenbears Running <tenbearsrunning@gmail.com>
Sent: Saturday, September 10, 2022 9:12 AM
To: Melinda Didier <Melinda.Didier@ci.kennewick.wa.us>
Subject: Thompson Hill Project _ Request to pass on to the Planning Commission

Good Morning, Ms. Didier

Not being a public [conversationalist](#), I hesitate to consider discussing the Thompson Hill Project being proposed, again and again, on a public forum. I have submitted my concerns to the commission as well as each of the City Commission in the past, last year and again on a forum provided online.

Being a member of the Creekstone Community, I do object to the proposed project for several reasons. My point in writing the short note this morning is ask, the Planning committee to review the land use and do consider, the extensive report submitted early last year, reference the possible dangers to the adjacent subdivisions primarily to the terrain on which this proposed project is to be built. I no longer have a copy of the report in my email and I was hoping this project had been abandoned.

And, reading some of the documents, it does appear, the project goes beyond what had been initially requested by the owner(s). I only ask, the Planning Committee read and study the land use and the report on what I recall, concern for the unstable terrain, should it occur (Las Vegas and Death Valley two prime examples) (Weather changes)

Far over my head as a layman, but I ask they review the very extensive report.

Thank You for your time
Robert Langendorfer
Retired LEO

From: [Bill Dixon](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [Anthony Muai](#); [Melinda Didier](#)
Cc: info@panoramicheightshoa.com
Subject: Panoramic Heights Homeowners Association Position on Comprehensive Plan Amendment 2022-0005
Date: Tuesday, September 13, 2022 12:26:55 PM

Panoramic Heights Homeowners Association

POSITION ON COMPREHENSIVE PLAN AMENDMENT 2022-0005 September 13, 2022

The Panoramic Heights Homeowners Association (PHHA) has devoted four months to review, evaluate, discuss and comment on all publicly available information related to the proposed Comprehensive Plan Amendment (CPA) 2022-0005. This included submitting four prior comment letters with details to the City on May 12, June 23, August 23 and September 7, 2022.

This Proposal would change the Land Use Designation from Low Density Residential to High and Medium Density Residential on 38.24 acres on the top, and north and south upper slopes of Thompson Hill.

We found that future developments allowed by this Land Use Designation change could result in:

- Up to 557 multi-family housing units on the proposed 38.24 acres on the top and upper north and south slopes of Thompson Hill.
- Commercial activities; such as motels, hotels and similar accommodations on the top of the Hill.
- Increased allowable housing density on Thompson Hill by a factor of up to 3.7 times.
- Construction of multi-family housing units on small lots in place of single family homes on large lots.
- Incompatibility with adjacent land uses and surrounding neighborhoods, which are all low density residential developments.

- Significantly increased traffic on Southridge area roads and more cut-through traffic in surrounding neighborhoods.
- More housing and roads on steep slopes with increased risks of erosion, runoff and landslides.
- Loss of existing wildlife and habitat.

We have received personal input from about 100 households in neighborhoods throughout the Southridge area including: Panoramic Heights, South Cliffe, Creekstone, Apple Valley, Southridge Estates, Windsong and several others. These households have similar concerns about the impacts of CPA 2022-0005, and all but one household expressed their opposition to this Proposal.

Their top concerns are:

1. Increased traffic,
2. Incompatibility with existing neighborhoods,
3. Steep slopes, runoff and landslides,
4. Decrease in property values, and
5. Noise and light pollution.

We have also reviewed all of the written public comments submitted. They expressed similar concerns, and all of the commenters opposed CPA 2022-0005.

We conclude that CPA 2022-0005 does not meet the Approval Criteria and Additional Factors in Kennewick Municipal Code (KMC) 4.12.110.7 and 8.

The Proposal would allow significant adverse impacts on the property, in surrounding neighborhoods, and throughout the Southridge area.

Based on our extensive evaluation and the public input, **PHHA opposes CPA 2022-0005.**

We request that the Planning Commission recommend that the City Council deny the Proposal. Then we request that the City Council deny CPA 2022-0005.

PHHA understands the need for more multi-family housing units throughout Kennewick, especially affordable ones. However, there are more suitable locations to construct them than on the steep slopes of an isolated hilltop surrounded by single family homes. They should be built in areas that will have close access to arterial roads, public transportation, commercial activities and public services.

PHHA does not oppose reasonable and responsible developments on Thompson Hill. These developments should be compatible with surrounding neighborhoods, which have low density, single-family homes. New developments should be designed with access roads that do not force traffic through adjacent neighborhood streets. And, housing should be built on slopes of less than 15% to avoid geological hazards, similar to Panoramic Heights and Citadel Estates.

PHHA views Citadel Estates as a model for the type of high-end single family homes on large, terraced lots that could be built on the top and upper slopes of Thompson Hill. And, we believe the future residents of Citadel Estates would share our concerns about CPA 2022-0005.

Reasonable and responsible low density residential homes on large view lots can be built on the less steep slopes and top of Thompson Hill within the existing Low Density Residential land use designation and current residential zoning.

Submitted by,
Bill Dixon, on behalf of the Panoramic Heights Homeowners Association
2500 S. Irving St, Kennewick

From: [Bill Dixon](#)
To: [Steve Donovan](#); [Melinda Didier](#)
Subject: Fwd: Proposed Thompson Hill Development: Last Opportunities to Comment
Date: Tuesday, September 13, 2022 4:21:07 PM

----- Forwarded message -----

From: **Gordon Brastad** <gordon1945@yahoo.com>

Date: Tue, Sep 13, 2022 at 3:41 PM

Subject: Re: Proposed Thompson Hill Development: Last Opportunities to Comment

I am one of the first homeowners in Panoramic Heights 2. I purchased my home in 1977. I selected my first home in the Tricities there because of the peaceful and quiet out of the way neighborhood. I have enjoyed that piece and quiet for 43 years.

I am now retired, as are many others in Panoramic Heights 2. I need that piece and quiet even more now. That piece and quiet is on the verge of being destroyed by the proposed high-density residential development for Panoramic Heights. The proposed apartments to be set on the side of Thompson Hill would not only be a hideous sight from below for the Tricities but, because there are only 2 streets for hundreds of cars to leave and return the apartment area, instead of 10 cars passing our homes each day, there will be hundreds. That, of course, will certainly destroy my long enjoyment of peace and quite, but will also greatly diminish my property value that I worked 30 years to maintain.

Thanks , Gordon Brastad

[Sent from Yahoo Mail on Android](#)

From: [Glen A. Clark](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); info@panoramicheightshoa.com
Cc: wtdixon3@gmail.com
Subject: Proposed Comprehensive Plan Amendment (CPA) 2022-0005
Date: Tuesday, September 13, 2022 7:40:10 PM

Dear City Council members and others,

I am writing this email to express my concerns with the latest proposal set forth for rezoning property on Thompson Hill. I do not oppose Mr. Chavallo's plan to develop his property, which I believe is his right to do. What I do oppose is the subject proposal to have his property rezoned from low density residential to medium and high density. Mr. Chavallo has tried to have his property rezoned for many years now. Last year's survey of the surrounding neighborhoods clearly showed that the overwhelming number of neighborhood residents were opposed to having this property rezoned. The roads leading to this property on the top of Thompson Hill are on city streets that pass through several neighborhoods. Allowing a hotel and several multi-family housing units to be placed on the top of Thompson Hill would result in a very significant increase in traffic and noise in these neighborhoods, probably leading to increased risks of traffic accidents occurring. Also of concern would be the decrease in the surrounding neighborhood homeowners' property values, the negative impact to the environment, and the increased crime rates that could be expected. When Mr. Chavallo purchased his property at the top of Thompson Hill he knew that it was zoned low density residential. The same can be said for the homeowners in the neighborhoods surrounding his property. I don't think it's fair to all of the neighborhood residents to have his property rezoned. If Mr. Chavallo wants to build a hotel and apartment complexes surely he can find some property that is already zoned for such projects.

I trust that you will listen to and act upon the concerns of the residents in the surrounding neighborhoods.

Sincerely,

Glen A. Clark
2635 S. Kellogg St.
Panoramic Heights neighborhood

From: [Paula Long](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [info@panoramicheightshoa.com](#)
Cc: [Dave Long](#)
Subject: CPA 2022-005
Date: Wednesday, September 14, 2022 8:36:22 AM

To:

[bill.mckay@ci.kennewick.wa.us](#), [gretl.crawford@ci.kennewick.wa.us](#),
[loren.anderson@ci.kennewick.wa.us](#), [brad.beauchamp@ci.kennewick.wa.us](#),
[jim.millbauer@ci.kennewick.wa.us](#), [chuck.torelli@ci.kennewick.wa.us](#),
[john.trumbo@ci.kennewick.wa.us](#), [Steve.Donovan@ci.kennewick.wa.us](#),
[clerkinfo@ci.kennewick.wa.us](#); [info@panoramicheightshoa.com](#)

From:

David & Paula Long
2401 S. Irving Street
Kennewick, WA 99338

To: Kennewick City Council Members
Re: CPA 2022-0005

We are writing to express our agreement with the position of the Panoramic Heights Home Owners Association, objecting to the proposed development being considered by the Council under CPA 2022-0005.

Changes in the Southridge area have already impacted our previously quiet neighborhood, disturbing wildlife, increasing traffic and noise, and altering the underlying safety and security of our neighborhood. We believe that the drastic nature of the proposed development will irrevocably and adversely impact our property and the surrounding neighborhoods.

The CPA has notably attempted to mitigate some of the obvious problems with such a radical departure from the existing land use of the area, but cannot get past the undeniable impacts of the massive and radical departure from the type of land use that has existed in our area since before we purchased our home in the 1990's.

Consideration should be given to preserving the existing residential character of the area and in focusing on preserving existing commercial areas in our City, rather than promoting urban "sprawl" in Kennewick. Every effort should be made to promote the custom homes and neighborhoods that have been established in south Kennewick and not creating urban/suburban blight. We believe that the current proposal is short-sighted and will adversely affect not only our neighborhood but Kennewick as a whole.

/S/ Dave & Paula Long

From: [Sondra Rader](#)
To: [Steve Donovan](#); greti.crawford@ci.kennewick.wa.us; [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Bill McKay](#)
Cc: [Bill Dixon](#)
Subject: Comments regarding Thompson Hill Amendment
Date: Thursday, September 15, 2022 10:03:24 AM

Dear City of Kennewick Council Members:

My husband and I bought our Panoramic Heights home in 1986, 36 years ago. We chose Panoramic Heights because of the neighborhood friendliness, families with children, appearance and upkeep of the development and the sense of privacy and security. Over the years we have worked hard to maintain our home and we take pride in the appearance of our home as well as the other homes in our neighborhood. Panoramic Heights is now an older neighborhood but still viewed as a desired place to live.

The development proposal that is AGAIN being considered by the city would change all of that. Access to 10th Avenue and Creekstone would be directly through our neighborhood destroying the peace and quiet that we all enjoy and increasing the daily traffic count and safety to the residents. A large majority of residents love walking our beautiful neighborhood daily. Their safety would become an issue. The opening of the middle school on 27th Avenue has already affected the daily traffic count. The Thompson Hill traffic would definitely turn Panoramic Heights into a major thoroughfare as it would be the only convenient access to anything north of Thompson Hill.

I have other concerns.

1. In 2011-2012 the city approved 30+ lots at the west end of 27th and 26th Avenue. This approval was granted to the same developer that is now requesting approval to develop the top of Thompson Hill. Our home is located on the corner of Kellogg and 26th. To date, ten years later, nothing has been done to improve, prepare or develop the property that was approved for development in 2011-2012. Excuse me, except to allow the city to dump the mountains of dirt that were removed from the new water treatment plant on Irving and 10th. I am sure the developer's motive in allowing the city to use his property was to gain the city's favor for his upcoming proposal for the top of Thompson Hill.
2. Is the developer financially able to pursue the proposed development on Thompson Hill? I question this as he has made no improvements to the property at the end of 26th and 27th Avenues. It seems that if he was financially unable and with the need for homes in the Tri-Cities he would have been focused on developing and the income from selling these lots.
3. I have had several encounters with the developer. He cannot be trusted! He will say to your face whatever he feels you want to hear. I have heard his "dream" speech many times. I believe his "dream" is for self-gratification and not for the enhancements to our community as he claims.

4. And finally, why are we revisiting this proposal with minor changes once again. The city did not approve his original proposal two years ago. Our neighborhood spoke out loudly at that time. We were unified and expressed our concerns loudly. Thankfully the city listened and his request was denied. Here we go again! Is he figuring he can beat us down, is he relying on the support of new council members that he has in his pocket? The homeowners that pay property & city taxes and have lived in the neighborhood for many years have already expressed their concerns.

I request and, I am hoping, that both the Planning Department and the City Council will thoroughly review this land usage proposal and deny it. Please take into consideration the quality-of-life issues that will affect one of the largest, oldest and nicest neighborhoods in Kennewick.

Thank you.

Sondra & Mike Rader

2503 S. Kellogg Street

From: jon7116@gmail.com
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#)
Cc: info@panoramicheightshoa.com; ["Jon Dickman"](#); [Christal Dickman](#)
Subject: Public Hearing on CPA 2022-0005
Date: Thursday, September 15, 2022 1:06:01 PM

To our City Council & Planning Commission,

Regarding Public Hearing on CPA 2022-0005, a proposed change to the "Land Use Designation" in Kennewick's Comprehensive Plan from "Low Density Residential" to "High and Medium Density Residential", we are strongly opposed to proposed change to increase the housing density on Thompson Hill and to allow multi-family housing units and hotels. Our concerns about this proposed change are for the following reasons:

- We reside in Southcliffe. The purchase of our home in 2020 was based on the master plan of the community and future development of the surrounding property's. As homeowners, we base our decisions to protect our investments by ensuring we make sound personal and professional judgements on the current environment and future planning of the surrounding properties. We were confident in our choice to invest in Southcliffe as we received copies of the approved platting of the Southcliffe development. Our real estate agent was instrumental in assuring us that future development on the "hill" would be single family homes. Changing the land use from low density housing to higher density, multi-family and commercial (hotels) is not compatible nor does it fit in a low-density neighborhood. More importantly a change in land use is a complete betrayal of all homeowners in the surrounding neighborhoods trust in the planning process, planning commission and city council. I am sure that you would agree with us, that as citizens, we need assurances that our leaders will support our neighborhoods.
- Higher density multi-family housing units and commercial operations are not compatible with our neighborhood because this change would make our neighborhood streets unsafe by creating more direct traffic to and from the higher density development and cut-through traffic in our neighborhoods. These concerns are not without facts. Consider that the current, and future construction, of approximately 400+ homes below Thompson Hill will have access to W. 28th street connecting to S. Sherman St, driving increased traffic to Bob Olsen, or to S. Kellogg St, cutting through Panoramic Heights to S. Irving St and then to Creekstone Drive.
- Adding more concern to increased unsafe traffic and cut-through/short-cutting is the future completion of W 23rd Street through Southcliffe connecting with Bob Olsen. South Belair is expected to be extended in the future over the backside of Thompson hill connecting with W 20th Ave or W 21st Ave, creating an unintentional shortcut/cut-through from Bob Olsen to W 10th Avenue. These streets are single family residential neighborhoods that are not designed, nor designated as major arterials. To allow a change to high density in our neighborhoods is putting families and children at unnecessary risk to hundreds of cars daily that should not be on these city streets.
- Consider that changing from low density to high density has unintentional long-term effects as neighborhoods age. Overtime, as we have seen in other communities, increasing density provides opportunities for investors, speculators, and absentee owners to tear down existing homes and replace them with new denser housing types. Adding zoning density encourages

existing neighborhoods to be torn down one house at a time.

- Adding zoning density to residential neighborhoods encourages absentee owners to replace homeowners, which destabilizes neighborhoods.
- Increased density increases transience and turnover of residents in the neighborhood. Neighbors know a lesser percentage of people in the neighborhood. Long-term homeowners are more protective of their neighborhood and the homes around them than are short-term tenants.
- An increase in a neighborhood's density decreases neighborhood involvement. Isn't one of the goals of "urbanism" is to have more neighborhood interaction. Density decreases neighborhood personal interaction and involvement.
- The increased density of multi-story buildings blocks breezes that flow through the established older neighborhoods. The detrimental effects of eliminating the cooling breezes drives people out of their backyards and back porches and inside to their air-conditioned homes. This increases the air conditioning usage and cost for each home, along with having a negative impact on the environment.
- Increased density contributes to global warming. Rather than trees providing canopies over roofs shading houses, increased density removes the trees and adds more rooftops and more concrete to absorb heat and radiate back into the neighborhood and atmosphere. Again, more air conditioning is needed for every home when trees are not absorbing the heat and shading the homes. A leafy canopy over a roof reduces the amount of air conditioning a home needs. Extra concrete and rooftops absorbing and radiating heat increases the amount of air conditioning each home needs.
- Studies have shown that overbuilt and dense neighborhoods are much more vulnerable to the deadly small West Nile mosquito. The large and relatively harmless field mosquito is more prevalent in less dense neighborhoods while the West Nile mosquito is more prevalent in dense neighborhoods.
- Our neighborhoods were not built for density. We rely on the yard's natural absorption, or water runoff to protect our environment. High density lot surfaces do not absorb water, creating more runoff that floods streets and creeks. The proposal of the developer to utilize the abandoned AP lateral or the catch basins from another developer is unacceptable. The excess water is just being shifted from one property owner to become the community's responsibility.
- Adding zoning density to neighborhoods makes them less attractive to both homeowners and even renters. Less attractive neighborhoods attract less desirable tenants. This creates a downward neighborhood spiral. Less attractive tenants make the neighborhood less attractive for homeowners.
- Neighborhoods are fragile and need to be nourished. Neighborhoods either get stronger or they get weaker and decay. In the neighborhoods the city wants to protect and make stronger, the city should protect our low-density designation. Instead of changing to high density the city should create more greenspace, parks & trees. This puts us more in sync with our city and state's vision working towards a greener tomorrow.

Again, as first stated, we moved into our neighborhood because it and all surrounding neighborhoods had low-density single-family homes. Any development on Thompson Hill was supposed to be the same. We are strongly recommending that the City Council deny this proposal.

Furthermore, we are asking that our city leaders stand strong with us and support our neighborhoods today and in the future.

Respectfully,

Jonathan & Christal Dickman
2272 S. Belfair St.
Kennewick, Wa 99338

From: [Mabry, Ronnie](#)
To: [Steve Donovan](#)
Cc: carlvr509@yahoo.com
Subject: CPA 2022-0005
Date: Thursday, September 15, 2022 3:19:27 PM

Exhibit A-19.63

Steve,

This is let you know that I am opposed to changing the area in question from low density to high density. I have stated my reasons in the past and they remain the same; mostly the lack of infrastructure improvements (roads, sewer, water drainage) plus the additional strain on the schools.

Let me know if you have questions.

Ron Mabry
2525 South Irving St. Kennewick
509-308-2698

From: [Trisha Chase](#)
To: [Bill McKay](#); [Gretl Crawford](#); [Loren Anderson](#); [Brad Beauchamp](#); [Jim Millbauer](#); [Chuck Torelli](#); [John Trumbo](#); [Steve Donovan](#); [Terri Wright](#); [info@panoramicheightshoa.com](#)
Subject: Concerns with Thompson Hill Development
Date: Saturday, September 17, 2022 11:01:10 PM

Hello there,

I know that it is too late for my comments to be formally considered prior to the hearing regarding the proposed Thompson Hill development, but I still wanted to share my thoughts. I live in a home off of 20th Ave right up against Thompson Hill. We have enjoyed the views of the hillside, the natural geography, the peace, and the safety that comes from living by the undeveloped hillside. We also have enjoyed being able to take our kids for bike rides in the surrounding neighborhoods such as Creekstone and Panoramic Heights because of the relatively low levels of traffic. We often cross Kellogg street for these rides. We are concerned that changing the zoning of Thompson Hill will make it unsafe to enjoy outdoor recreation as a family in the neighborhood. We are also concerned about the loss of privacy and property value that will come with these zoning changes.

I would like to see the city of Kennewick, the planning commission, and the city council engage in more thoughtful development. It sometimes seems that the approach is to throw up as many new housing developments as possible on any open land. This may help solve the problems of housing shortages in the short-term, but it ultimately makes Kennewick a less livable place. Please make zoning decisions and plans that increase the beauty, livability, and uniqueness of Kennewick. Through the master plan, zoning decisions, and other local options, let's make Kennewick a place that people want to visit, move, and stay!

Thanks,

--

Trisha Chase, PhD
trisha.m.chase@gmail.com

From: pakjwong@charter.net
To: [Steve Donovan](#)
Cc: info@panoramicheightshoa.com
Subject: FW: Proposed Thompson Hill Development:
Date: Thursday, September 15, 2022 10:57:06 PM
Attachments: [City-Council-Commissioners-Clerk-Contact-Updated-6.13.22 \(3\).pdf](#)
[PHHA Position on CPA 2022-0005 \(Summary\) \(2\).pdf](#)

Hi, our names are Pak Keung and Joana Wong (Southridge Estates)

These are the reasons we disagree and don't agree with the development of this project.

1. SAFTY: We have 4 schools within our area and with that said, we are concern for the safety of our children who walk to school and the school bus drivers who transport our children with the increase in traffic.

2. NOISE: We already have the noise of ambulances, fire trucks and helicopters flying over our community to transport patients to the hospital that has increased our noise pollution tremendously.

3. EVENT TRAFFIC: As you know, we house the Kennewick Southridge Event Center. There are concern on our part of the increase in traffic from the Thompson Hill Development since it won't be a single home development rather a commercial development.

4. A lot of us who lives in Southridge Estate are already experiencing safety issues as we enter onto Hildabrand because of the increased of Traffic as Southridge Estate continue to grow with more single-family homes. We ourselves are experiencing traffic blind spots because of the City of Kennewick's landscape blocking the view of oncoming traffic. So, more increase of traffic could increase a critical hazardous saturation. So, you see WE DON'T NEED COMMERCIAL DEVELOPMENTS within our single-family home community.

5. We already are experiencing city water pressure. What would happen if this commercial development occurred?

To conclude, the Thompson Hill Development just don't belong here within our community since we have been told when we purchased our lot and built our home that this whole area is a single-family home community.

Thank you
Pak Keung and Joana Wong

From: "Bill Dixon"
To: "Adrian McCall", "aimeem_marshall@hotmail.com", "Amy Porter", "Anita Booth", "Ann LaRiviere", "Annette Freier", "Arvid and Debbie Larson", "Ashley Smith", "b_grz@icloud.com", "Barb Marchese", "Bart and Sue Sievers (bart_sievers@yahoo.com)", "Beatte", "Bill Fulwyler", "Bob Langendorfer", "Booth", "Brad Brannon", "Brendon Sillito", "Bret & Amber Morales", "Carol and Charles Bartell (ryanhill4ever@gmail.com)", "Carol Senn", "Carol Wondrack", "Chad and Katherine Pettijohn (pettijohn5@gmail.com)", "Charles

May", "childisabel@yahoo.com", "Chris Barnes", "Chris Sorensen", "christa clay", "Christal Dickman", "Christy Watts", "Chuck Rogers", "Cindy and David Landis", "Cindy Kellie", "cjalley16@gmail.com", "Clint Whitney", "Conrad and Caroline Morrow", "craig taylor", "crosby.john56@gmail.com", "daanniem632@gmail.com", "Dana Brunsdon", "Dana Klepper Mitzel", "Daniel & Sherry Dengate", "Danielle Knudson", "Darrel Duncan", "Dave and Marla Brown", "David Long", "Dawn Thomas (office@westernreclamation.com)", "deankunigisky@gmail.com", "Dennis and Traci Bradshaw", "Diane Steele", "Dominic Sansotta (domkathy@hotmail.com)", "Doug and Kathy Williams", "Dr. Noel Ybarra", "dream16maker@gmail.com", "Eileen and John Romines", "Ellen and Nick Caristo", "Eric and Kathy Otheim", "Eric Bertrand", "Erin Sheeran", "Fernando Arevalo", "Fran Handy (francinehandy@gmail.com)", "Frank Wentz", "Gail and Brian Everett (geverett82@msn.com)", "Gary & Jeanette Hutchcraft", "Gary & Keryl Bosley", "Gary Schenck", "Gene & Kerry St. Denis", "Gene and Marie Kernan", "George Romano", "GERALD BERGES", "Gina Dallas", "Glen Clark", "Glenn & Rene' Konzek", "Glenna and Don Gale", "Gordon Bradstad", "Greg and Vonda Smith (vondagreg@aol.com)", "Haruko Ishii", "hayleymichellecollen@yahoo.com", "Heather Boynton", "Heather Erhart", "Heather Ybarra", "Heidi Nixon", "ICE Susan Dixon", "Ivan Thomas (ramsfanusa@msn.com)", "James Neary", "jamie@kidexpert.org", "Jane and Nathan Cathey", "Janice and Bruce Boyum (bmb49@aol.com)", "Jason Mercier", "Jason Smith", "Jeff Adamson", "Jeff and Shirley Griffin (benlafitte@charter.net)", "jeff griffin", "Jeff Hylden", "Jeff Moody", "Jenna Higley", "jennifer keller", "jennifer nevills", "Jennifer Smith", "jennifer.luce7@gmail.com", "jeremy swanson", "Jessica Holloway & Keith Haytcher", "jessica percifield henry", "Jill York", "Jim & Sharon Angel", "Jim and AJ Foster (cameojj@charter.net)", "Joan Lucas", "Joann Pringle (jojohp7@hotmail.com)", "Jobey Smith", "Joe Porter", "John and Barbara Stone", "John and Lynn Hanson", "John and Sheri Crosby", "John and Sheri Crosby", "John Deskins (john.deskins.home@gmail.com)", "john kuhn", "John LaFemina", "John Meehan", "johnlaustin@charter.net", "Jon and Jacque Fuller", "Jonathan and BryeAnne Stewart", "Jonathan and Christal Dickman", "jose chavez", "Joseph Sparks", "Judith Smith (jasmith212@charter.net)", "Juli Troxel", "juliepaul30@outlook.com", "Katherine Gardiner", "Kathryn John Heather Kalunian", "KatieS.RDH@gmail.com", "Kay Gustafson", "Keith and Annette Freier", "Keith and Robin Duncan", "Kelley Bruss", "Ken & Helen Daus", "Ken and Sue Gano", "Ken Gano", "Kenneth and Glenda Hahn", "Kevie Marston", "kevinmelschmitt@gmail.com", "Kimberlee & William Leonard", "kliphardt@yahoo.com", "Kostodgk", "Kyle Towne", "Larry and Mary Thomas", "Larry Hulstrom", "Larry Powers", "Lawrence Clay", "Linda McCullough", "Lindsay Steele", "Luzy and Isaac King", "Lynne and Cody Freeman", "Mark Lucas", "Mark Wilson", "Marla Holub", "marlando jordan", "Mary Weir", "McEntire789@gmail.com", "mdhopkins1@gmail.com", "Mekenzie Tarver", "merhart@charter.net", "michaelrstrauss@hotmail.com", "Michelle Porter", "Mike Erhart", "Mike Greif", "mike@evm-productions.com", "Molly Hamaker-Teals", "Monte and Lola LaDow", "mtlaurel@comcast.net", "Nathan and Jacqueline Pope", "Nelson & Kristine Rueda", "Nick and Judy Doyle", "Noreen Mack", "Pak Keung & Joana Wong", "Pam Mewes", "Patrick and Jamie Luce", "Patty Wilson", "Paula Torrey", "Rebecca Etheridge", "Reed and Lisa McKinlay", "Reg Unterseher", "rich144@live.com", "Richard & Angela", "Richard Nelson (richnelson1942@msn.com)", "Ron Mabry (carlv509@yahoo.com)", "Ron Weyer", "Ronald Butler", "Ronald Kuklinski", "Sandi Smith", "Sanjay Shinde", "Sara Elkington", "Sean Gossett", "Sean Mathews", "Sergey Nersesyan", "Seth Elkington", "shanevdh@gmail.com", "Sharon Decker", "Shawn Middleton (smjmidlton@yahoo.com)", "shawnfost3@gmail.com", "Sheila Dunlop", "Shlomo Orr", "Sondra Rader", "Stephanie de los Santos", "Stephen and Sandra Parent (stephenparent.parent@gmail.com)", "Steve & Mickee Madden", "Steve Varner", "Steven and Sherri Erhart", "Steven Grimshaw", "Sue Sievers",

"Terri Hash", "thomas fillmore", "Tim Fenske", "Tony Hausner", "Traci Bradshaw", "Trevor Jones", "Trisha Markle", "vonda@zplacesalonspa.com", "William Dixon", "William Reed"

Cc:

Sent: Tuesday September 13 2022 1:16:18PM

Subject: Proposed Thompson Hill Development: Last Opportunities to Comment

Dear Southridge Area Neighbors:

This is a reminder for those who haven't done so yet, that **written comments must be submitted by Thursday** for the Planning Commission to read them before the Public Hearing next Monday. City contact information is attached below.

Also if you want to **speak online at the Public Hearing, you must register before 4:30 PM on Monday** at:

<https://www.go2kennewick.com/598/Planning-Commission>.

If you want to **speak in person, you should sign in at the City Council Chambers before the meeting starts at 6:30 PM.**

If you haven't done so, **let me know if you plan to speak.** I will send you some "Information and Guidance" on speaking at the hearing.

For your information, also attached in the "Panoramic Heights Homeowners Association Position on Comprehensive Plan Amendment 2022-0005."

Thanks for your involvement in this important decision about the future of the Southridge area.

Bill Dixon, Panoramic Heights Lead

509.531.5913

wtdixon3@gmail.com

WHO SHOULD WE SEND COMMENTS TO?

All comments should be sent to all City Council members, Steve Donovan, Senior Planner, Community Planning Department: Steve.Donovan@ci.kennewick.wa.us and cc info@panoramicheightshoa.com.

To send group email to City Council, Planning Commission City Liaison, Senior Planner & City Clerk, please copy and paste:

bill.mckay@ci.kennewick.wa.us, gretl.crawford@ci.kennewick.wa.us, loren.anderson@ci.kennewick.wa.us, brad.beauchamp@ci.kennewick.wa.us, jim.millbauer@ci.kennewick.wa.us, chuck.torelli@ci.kennewick.wa.us, john.trumbo@ci.kennewick.wa.us, Steve.Donovan@ci.kennewick.wa.us, clerkinfo@ci.kennewick.wa.us; info@panoramicheightshoa.com

Or if you choose to email and or call each individually -

City Council:

gretl.crawford@ci.kennewick.wa.us 509-531-0454
loren.anderson@ci.kennewick.wa.us 509-586-5532
brad.beauchamp@ci.kennewick.wa.us 509-308-6556
jim.millbauer@ci.kennewick.wa.us 509-531-4134
Chuck.Torelli@ci.kennewick.wa.us 509-405-2156
John.Trumbo@ci.kennewick.wa.us 509-366-2241
Bill.McKay@ci.kennewick.wa.us 509-585-4238

Steve Donovan, Senior Planner, Community Planning Department:
Steve.Donovan@ci.kennewick.wa.us

Panoramic Heights Action Team Point of Contact:

Bill Dixon: wtdixon3@gmail.com
info@panoramicheightshoa.com

City of Kennewick Addresses -

Planning Commission and City Council Members
210 W. 6th Avenue - P.O. Box 6108
Kennewick, WA 99336-0108



Panoramic Heights Homeowners Association
www.panoramicheightshoa.com

POSITION ON COMPREHENSIVE PLAN AMENDMENT 2022-0005
September 13, 2022

The Panoramic Heights Homeowners Association (PHHA) has devoted four months to review, evaluate, discuss and comment on all publicly available information related to the proposed Comprehensive Plan Amendment (CPA) 2022-0005. This included submitting four prior comment letters with details to the City on May 12, June 23, August 23 and September 7, 2022.

This Proposal would change the Land Use Designation from Low Density Residential to High and Medium Density Residential on 38.24 acres on the top, and north and south upper slopes of Thompson Hill.

We found that future developments allowed by this Land Use Designation change could result in:

- Up to 557 multi-family housing units on the proposed 38.24 acres on the top and upper north and south slopes of Thompson Hill.
- Commercial activities; such as motels, hotels and similar accommodations on the top of the Hill.
- Increased allowable housing density on Thompson Hill by a factor of up to 3.7 times.
- Construction of multi-family housing units on small lots in place of single family homes on large lots.
- Incompatibility with adjacent land uses and surrounding neighborhoods, which are all low density residential developments.
- Significantly increased traffic on Southridge area roads and more cut-through traffic in surrounding neighborhoods.
- More housing and roads on steep slopes with increased risks of erosion, runoff and landslides.
- Loss of existing wildlife and habitat.

We have received personal input from about 100 households in neighborhoods throughout the Southridge area including: Panoramic Heights, South Cliffe, Creekstone, Apple Valley, Southridge Estates, Windsong and several others. These households have similar concerns about the impacts of CPA 2022-0005, and all but one household expressed their opposition to this Proposal.

Their top concerns are:

1. Increased traffic,
2. Incompatibility with existing neighborhoods,
3. Steep slopes, runoff and landslides,
4. Decrease in property values, and
5. Noise and light pollution.

We have also reviewed all of the written public comments submitted. They expressed similar concerns, and all of the commenters opposed CPA 2022-0005.

We conclude that CPA 2022-0005 does not meet the Approval Criteria and Additional Factors in Kennewick Municipal Code (KMC) 4.12.110.7 and 8. The Proposal would allow significant adverse impacts on the property, in surrounding neighborhoods, and throughout the Southridge area.

Based on our extensive evaluation and the public input, **PHHA opposes CPA 2022-0005.**

We request that the Planning Commission recommend that the City Council deny the Proposal. Then we request that the City Council deny CPA 2022-0005.

PHHA understands the need for more multi-family housing units throughout Kennewick, especially affordable ones. However, there are more suitable locations to construct them than on the steep slopes of an isolated hilltop surrounded by single family homes. They should be built in areas that will have close access to arterial roads, public transportation, commercial activities and public services.

PHHA does not oppose reasonable and responsible developments on Thompson Hill. These developments should be compatible with surrounding neighborhoods, which have low density, single-family homes. New developments should be designed with access roads that do not force traffic through adjacent neighborhood streets. And, housing should be built on slopes of less than 15% to avoid geological hazards, similar to Panoramic Heights and Citadel Estates.

PHHA views Citadel Estates as a model for the type of high-end single family homes on large, terraced lots that could be built on the top and upper slopes of Thompson Hill. And, we believe the future residents of Citadel Estates would share our concerns about CPA 2022-0005.

Reasonable and responsible low density residential homes on large view lots can be built on the less steep slopes and top of Thompson Hill within the existing Low Density Residential land use designation and current residential zoning.



City Council Meeting Schedule November 2022

The City broadcasts City Council meetings on the City's website
<https://www.go2kennewick.com/CouncilMeetingBroadcasts>.

November 1, 2022

Tuesday, 5:30 p.m.

WORKSHOP MEETING

1. 2023/2024 Biennial Budget Presentation

Tuesday, 6:30 p.m.

REGULAR COUNCIL MEETING

November 8, 2022

Tuesday, 6:30 p.m.

WORKSHOP MEETING

1. 2023 Property Tax Levy

2. Council Legislative Priorities

3. Stormwater Code Amendments

November 15, 2022

Tuesday, 6:30 p.m.

REGULAR COUNCIL MEETING

November 22, 2022

Tuesday, 6:30 p.m.

WORKSHOP MEETING

November 29, 2022

Tuesday, 6:30 p.m.

NO MEETING SCHEDULED

To assure disabled persons the opportunity to participate in or benefit from City services, please provide twenty-four (24) hour advance notice for additional arrangements to reasonably accommodate special needs.

Please be advised that all Kennewick City Council Meetings are Audio and Video Taped

November 2022
Updated 09/29/22



City Council Meeting Schedule December 2022

The City broadcasts City Council meetings on the City's website
<https://www.go2kennewick.com/CouncilMeetingBroadcasts>.

December 6, 2022
Tuesday, 6:30 p.m.

REGULAR COUNCIL MEETING

December 13, 2022
Tuesday, 6:30 p.m.

WORKSHOP MEETING
1. Animal Control Update
2. City Manager Goals & Accomplishments

December 20, 2022
Tuesday, 6:30 p.m.

REGULAR COUNCIL MEETING

December 27, 2022
Tuesday, 6:30 p.m.

WORKSHOP MEETING

To assure disabled persons the opportunity to participate in or benefit from City services, please provide twenty-four (24) hour advance notice for additional arrangements to reasonably accommodate special needs.

Please be advised that all Kennewick City Council Meetings are Audio and Video Taped

December 2022
Updated 10/06/22