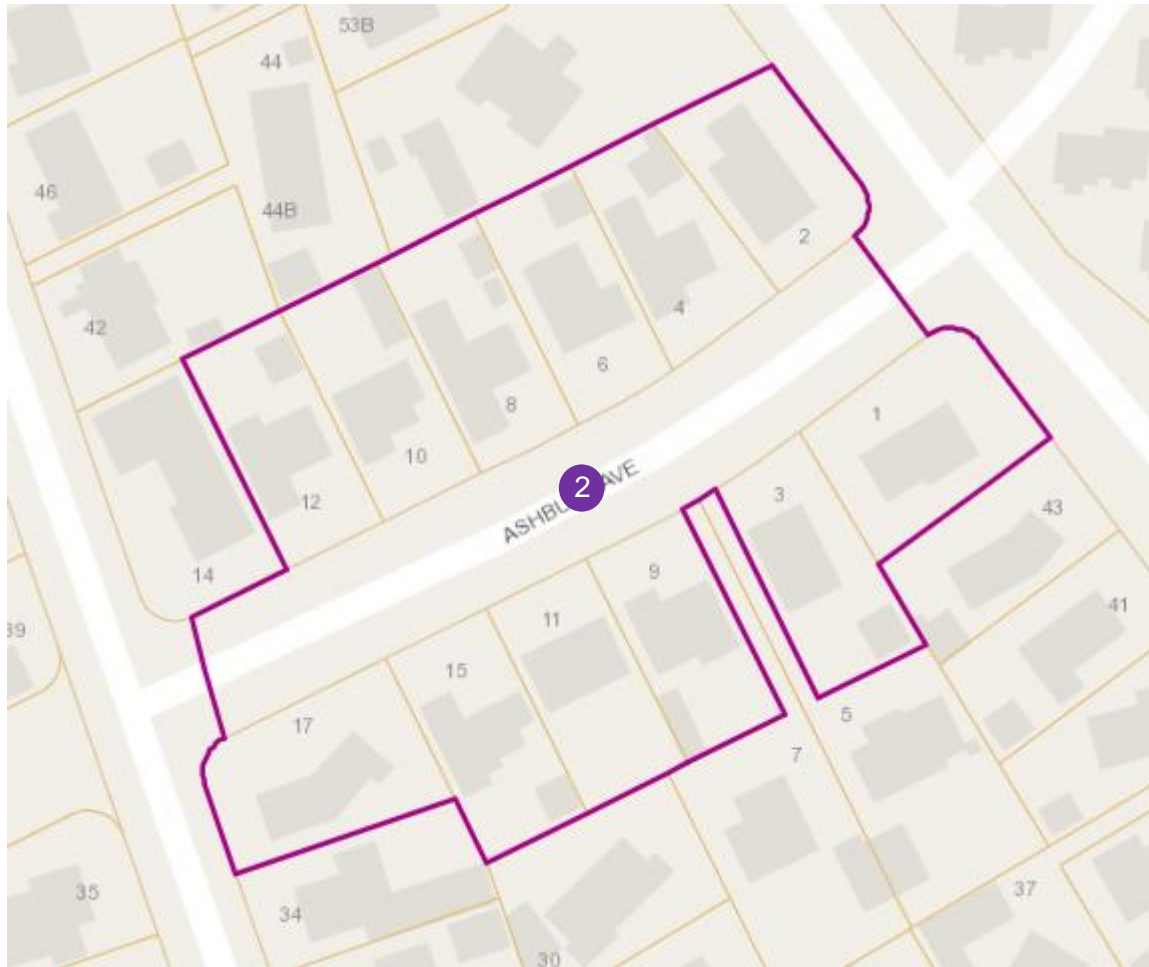


Ashbury Avenue HHA – Full Statement



Development Dates

- Subdivision Approved December 1965 with the first building permit granted in February 1965

City Extension

- Located within the 8th extension to the City; April 1962

Background

Background (Historic, Cultural and Archaeological Qualities)

Part of the land now forming Ashbury Avenue was surveyed for FC Lichfield in 1916, to create two lots facing Tramway Road; now Silverdale Road.

The land is located in the 8th Extension to the City; this was Hamilton's largest boundary extension which almost doubled the land area of Hamilton City.¹ Hamilton's population growth was occurring much faster than predicted, and there was insufficient land for the low-density suburban life that the growing population demanded.

¹ Alice Morris & Mark Caunter, Kirikiriroa – Hamilton's European Settler History, October 2021.

Previously the City's boundaries had been adjusted to respond to existing urban development, but the 8th extension planned for population growth, spatial development, and infrastructure.²

There was already the Ruakura Research Centre to the north and in 1960, a newly established Hamilton Teachers' College along with a branch of the University of Auckland opened a joint campus at Ruakura. In 1964, they moved to their new site around 1200m to the north of Ashbury Avenue, and the University of Waikato was established.

In December 1963, Chartwell Properties Limited were granted a subdivision of Lot 2, forming five lots facing Silverdale Road, and providing connection to Lot 6 to the rear.

Lot 6 was further surveyed in 1965 to create a series of lots facing Ashbury Avenue and Regent Street. The first building permit was granted in February 1965.

Ashbury Avenue was named in 1963 by Chartwell Properties owner Mr McLachlan, reportedly at the suggestion of one of the sales staff.³

Ashbury Avenue is the first of a series of linked culs-de-sac on west side of Silverdale Road, and provides the only link into the area. The street also provides direct access to Jansen Park; this park provides the west boundary to the residential area, and there are direct views westward along the straight alignment of the street into the park.

The layout of the wider street network, of which Ashbury Avenue is part, is typical of the Early Post War Expansions Development Period (1950 to 1980), comprising a series of linked culs-de-sac and irregular shaped roads.

Common with the development period, the subdivision layout includes a number of rear lots. These pairs of rear lots are accessed by wide shared driveways from the street, running between adjacent lots.

Overall, street-facing lots are generally of a similar size, shape and dimension (from around 650m² to 700m²) although corner lots are larger, as are the rear lots.

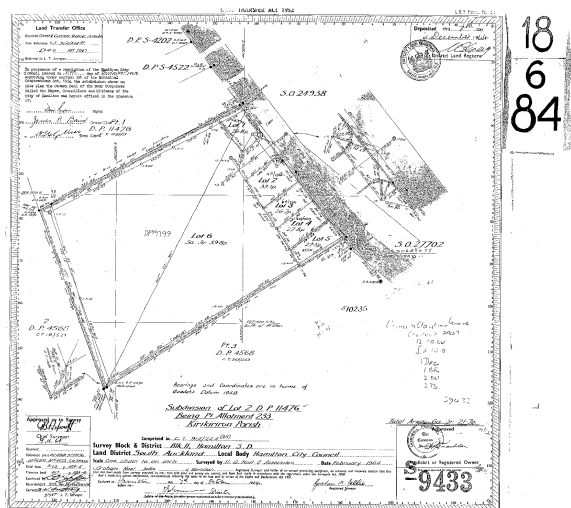


Figure 6 The 1916 subdivision plans for two lots facing Tramway Road (retrieved from premise.co.nz)

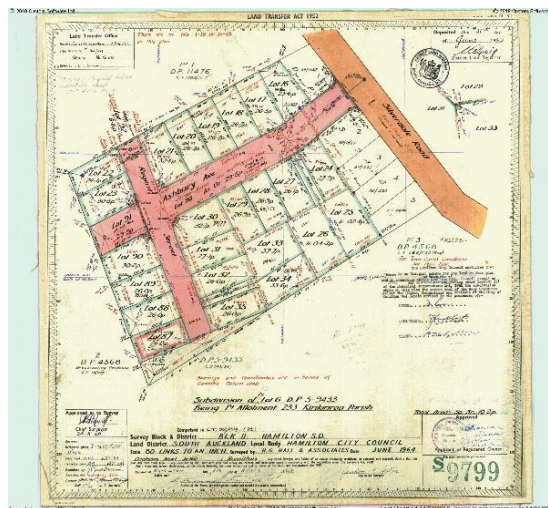


Figure 8 The original subdivision plan (retrieved from premise.co.nz)

² Morris & Caunter.

³ Hamilton City Libraries

Buildings and Streetscape Elements (Architectural, Scientific and Technical Qualities)

Buildings generally show a similar setback and are usually placed perpendicular to the street. They show designs and materials typical of the 1960s plan books, with large picture windows and varied roof shapes.

The majority of dwellings are single-storey with some two-storey.

Many front yards are open plan with some low retaining walls containing the original ground levels and some other low fences; the retaining walls are constructed in a range of materials although blockwork dominates. There is planting along the frontage of some front yard areas, along with some taller fences (both timber and ornate precast concrete).

Each lot has a fully formed driveway, leading to parking and garages. Many garages are detached and located within the rear yard; a typical arrangement for houses built earlier in the period. The two-storey buildings incorporate garages in their blockwork lower level.

The streets has berms with regularly spaced street trees on the north side. Overhead electricity lines on the south side of the street limit street trees.

Ashbury Avenue contributes to a clear understanding and appreciation of the development expected in the Early Post War Expansions (1950 to 1980) development period and exhibits High heritage significance as it retains significant integrity with no alterations from the original survey and formation of the street, with no subdivision or development from its establishment. The dwellings in the street are largely 1960s builds, dating from the original subdivision of the street, which also maintain their integrity as most appear to be unmodified. Additional heritage interest is brought by the fact that the subdivision is evidence of a commercial developer bringing forward a subdivision within an area recently added to the city, by way of the 8th extension, in part likely in response to the Ruakura Research Centre, new Teachers College and new University of Waikato campus all within easy distance.

Ashbury Avenue is one of a series of subdivisions by Chartwell Properties Limited, of land originally owned by FC Lichfield, who had also owned surrounding land.

In order for the existing values of the HHA to be maintained, it is important that future development incorporate the following features:

- Where an existing dwelling displays the features below, any alterations and extensions should respect the features. Where an existing principal building does not exhibit the features, any alterations and extensions should respect the design of the principal building and site as existing. All new buildings, including on rear sites, should incorporate these features. New accessory buildings of greater than 20m² on sites where the existing principal building exhibits the features should respect the design of the principal building and site as existing.
- Discourage subdivision of existing sites, as the HHA currently shows a high degree of integrity of lot size and layout from the original survey and formation of the street, with little further subdivision and development from its establishment.
- Dwellings should have L, T and shallow V plan shapes (overall building floorplan shape).
- Development should respect existing building setbacks, and buildings should remain generally parallel or perpendicular to the street, although recognising existing circumstances it is acceptable for buildings with a shallow V plan shape to be positioned at a narrow angle to the street.
- Alterations or new buildings should utilise materials which match the original buildings on the site, generally red, orange or light brown/buff coloured bricks,

- with some areas of lightweight panelling (timber or shallow profiled fibre cement), and blockwork (often painted) for ground floors areas of split level buildings.
- Buildings should incorporate plain, flat wall surfaces with rectangular picture windows. Generally, windows should have timber frames with opening top lights; however, aluminium windows with a similar profile and openings can be acceptable. Front doors should have large areas of glazing or glazed panels alongside.
 - Roofs should be low pitched, gabled or hipped with corrugated steel covering. Gable ends should be finished with a prominent but plain bargeboard and lightweight cladding on gables, such as fibre cement sheeting with a shallow profile.
 - Driveways should remain narrow, single or 1.5 vehicle width. Large areas of parking should not be provided to the front of houses, over and above the driveway which can widen to two cars wide to the front of garages.
 - Garages should be internal within two-storey dwellings or detached to the rear of single level dwellings, or perhaps as an attached open carport under the main roof of the building. They should not be forward of the original building and should use single doors.
 - Other accessory buildings, regardless of size, should not be forward of the original building.
 - Front boundaries should be open or have low retaining walls, without fencing above (even if the fencing is proposed to be an open design). There should be no gates. The low retaining walls should be constructed in stone or concrete block; the latter may be painted. Timber retaining walls are not sympathetic to the historic heritage values of the area.
 - Planting, including trees, within front yards is acceptable but care should be taken to ensure that the species chosen will not grow so large that all views of the main dwelling on the site are lost; views of the dwellings contribute to the heritage values of the area.
 - Existing street trees and wide front berms should be retained/maintained as existing.

Heritage Assessment Criteria:

a. Historic Qualities

The place or area is directly associated with, or has a direct relationship to, an important person, group, institution, event or activity, or reflects important aspects of local, regional or national history, including development and settlement patterns, transportation routes and social or economic trends.

The subdivision of Ashbury Avenue is evidence of a commercial developer bringing forward a subdivision within an area recently added to the city, by way of the 8th extension, in part likely in response to the Ruakura Research Centre, new Teachers College and new University of Waikato campus all within easy distance (both of which have regional significance). This relationship adds to the overall historic qualities of the area.

The land is located in the 8th Extension to the City; this was Hamilton's largest boundary extension which almost doubled the land area of Hamilton City. Hamilton's population growth was occurring much faster than predicted, and there was insufficient land for the low-density suburban life that the growing population demanded.

In December 1963 Chartwell Properties Limited were granted a subdivision of Lot 2, forming five lots facing Silverdale Road, and providing connection to Lot 6 to the rear. Lot 6 was further surveyed in 1965 to create a series of lots facing Ashbury Avenue and Regent Street. The first building permit was granted in February 1965.

Ashbury Avenue is the first of a series of linked culs- de-sac on west side of Silverdale Road, and provides the only link into the area. The street also provides direct access to Jansen Park; this park provides the west boundary to the residential area, and there are direct views westward along the straight alignment of the street into the park.

Common with the development period, the subdivision layout includes a number of rear lots. These pairs of rear lots are accessed by wide shared driveways from the street, running between adjacent lots.

The layout of the wider street network, of which Ashbury Avenue is part, is typical of the Early Post War Expansions Development Period (1950 to 1980), comprising a series of linked culs-de-sac and irregular shaped roads.

The place has **high regional and local** historic qualities

b. Physical/Aesthetic/Architectural Qualities

The place or area is a notable or representative example of:

- (i) A significant development period or activity; and/or*
- (ii) Distinctive or special attributes of an aesthetic or functional nature; and/or*
- (iii) The work of a notable architect, designer, engineer or builder.*

The dwellings in the street are largely 1960s builds, dating from the original subdivision of the street, and most appear to be unmodified. Together, these dwellings form a cohesive, yet varied, collection of 1960s buildings which are characteristic of the Early Post War Expansions (1950 to 1980) development period.

The layout of the wider street network, of which Ashbury Avenue is part, is typical of the Early Post War Expansions Development Period (1950 to 1980), comprising a series of linked culs-de-sac and irregular shaped roads.

The design of the buildings is typical of that expected in the Development Period, and designs seen in plan books. They are largely unaltered.

The incorporate the following typical architectural elements:

- Plan forms incorporate L, T and shallow V shapes.
- They have brick elevations, with some split block and artificial stone, fibre cement cladding to gables and some blockwork plinths on other buildings.
- A mixture of gable and hipped roofs with mainly corrugated steel coverings, although there are some tiled roofs.
- Large areas of horizontal proportion picture windows.

The layout of the wider street network, of which Ashbury Avenue is part, is typical of the Early Post War Expansions Development Period (1950 to 1980), comprising a series of linked culs-de-sac and irregular shaped roads.

Common with the development period, the subdivision layout includes a number of rear lots. These pairs of rear lots are accessed by wide shared driveways from the street, running between adjacent lots.

The buildings are typical of the period and so do not use unique or uncommon building materials, or demonstrate an innovative method of construction, or are an early example of the use of a particular building technique.

The buildings are of interest in so much as they are typical of the period, rather than being designed by a particular known practitioner.

The place has **moderate local** physical/aesthetic/architectural qualities.

c. Context Qualities

The place or area is an important landmark or feature or contributes to or is associated with a wider historical theme, traditional, or cultural context, or physical setting.

The place has **unassessed** context qualities.

d. Technological Qualities

The place or area shows a high degree of creative or technical achievement at a particular time, is directly associated with scientific or technical innovations or achievements, or is associated with scientific "break-through". The place uses unique or uncommon building materials, or demonstrates an innovative method of construction, or is an early example of the use of a particular building technique.

The place has **no known** technological qualities.

e. Archaeological Qualities

The potential of the place or area to define or expand knowledge of earlier human occupation, activities or events through investigation using archaeological methods, or to provide evidence to address archaeological research questions. For example, but not limited to: The place or area is registered by Heritage New Zealand for its archaeological values, or is recorded by the New Zealand Archaeological Association Site Recording Scheme, or is an 'archaeological site' as defined by the Heritage New Zealand Pouhere Taonga Act 2014.

Lots within what is now Ashbury Avenue were first surveyed in 1916, with further subdivisions granted in 1965. There are no records regarding the proposed HHA or local area.

The place has **low local** archaeological qualities.

f. Cultural Qualities

The place or area is important or significant:

(i) As a focus of cultural sentiment; and/or

(ii) As a context for community identity or sense of place, and provides evidence of social, cultural or historical continuity; and/or

(iii) For having symbolic or commemorative significance to people who use or have used it, or to the descendants of such people. The place or area has a high degree of interpretative potential to increase understanding of past lifestyles or events.

The place has **no known** cultural qualities.

g. Scientific Qualities

The potential for the place or area to contribute scientific information about how the natural environment has influenced, events, phases or activities related to development.

The area has **no known** scientific qualities.

Summary Table of Heritage Values

The place is considered to have heritage significance in relation to the following criteria:

Heritage Criteria	Significance	Context
a) Historic Qualities	High	Local and regional
b) Physical/Aesthetic/ Architectural Qualities	Moderate	Local
c) Context Qualities	Not assessed	
d) Technological Qualities	None	
e) Archaeological Qualities	Low	Local
f) Cultural Qualities	None	
g) Scientific Qualities	None	

Statement of Significance

Ashbury Avenue is one of a series of subdivisions by the Chartwell Properties Limited, of land originally owned by FC Lichfield, who had also owned surrounding land.

The subdivision, and dwellings brought forward on the land, are typical of the Early Post War Expansions (1950 to 1980) development period, including linked roads and cul-de-sacs and building plan forms which incorporate L, T and shallow V shapes.

The street shows a high degree of integrity of lot size and layout from the original survey and formation of the street, with no subdivision or development from its establishment. The dwellings in the street are largely 1960s builds, dating from the original subdivision of the street, and most appear to be unmodified. Together, these dwellings form a cohesive, yet varied, collection of 1960s buildings which are characteristic of the Early Post War Expansions (1950 to 1980) development period.

Maintaining existing open (unfenced) frontages is an important element in maintaining the historic heritage significance of the area.

Overall, the area contributes to a clear understanding and appreciation of the development expected in the Early Post War Expansions (1950 to 1980) development period and exhibits High heritage significance as it retains significant integrity with no alterations from the original survey and formation of the street, with no subdivision or development from its establishment. The dwellings in the street are largely 1960s builds, dating from the original subdivision of the street, which also maintain their integrity as most appear to be unmodified. Additional heritage interest is brought by the fact that the subdivision is evidence of a commercial developer bringing forward a subdivision within an area recently added to the city, by way of the 8th extension, in part likely in response to the Ruakura Research Centre, new Teachers College and new University of Waikato campus all within easy distance.