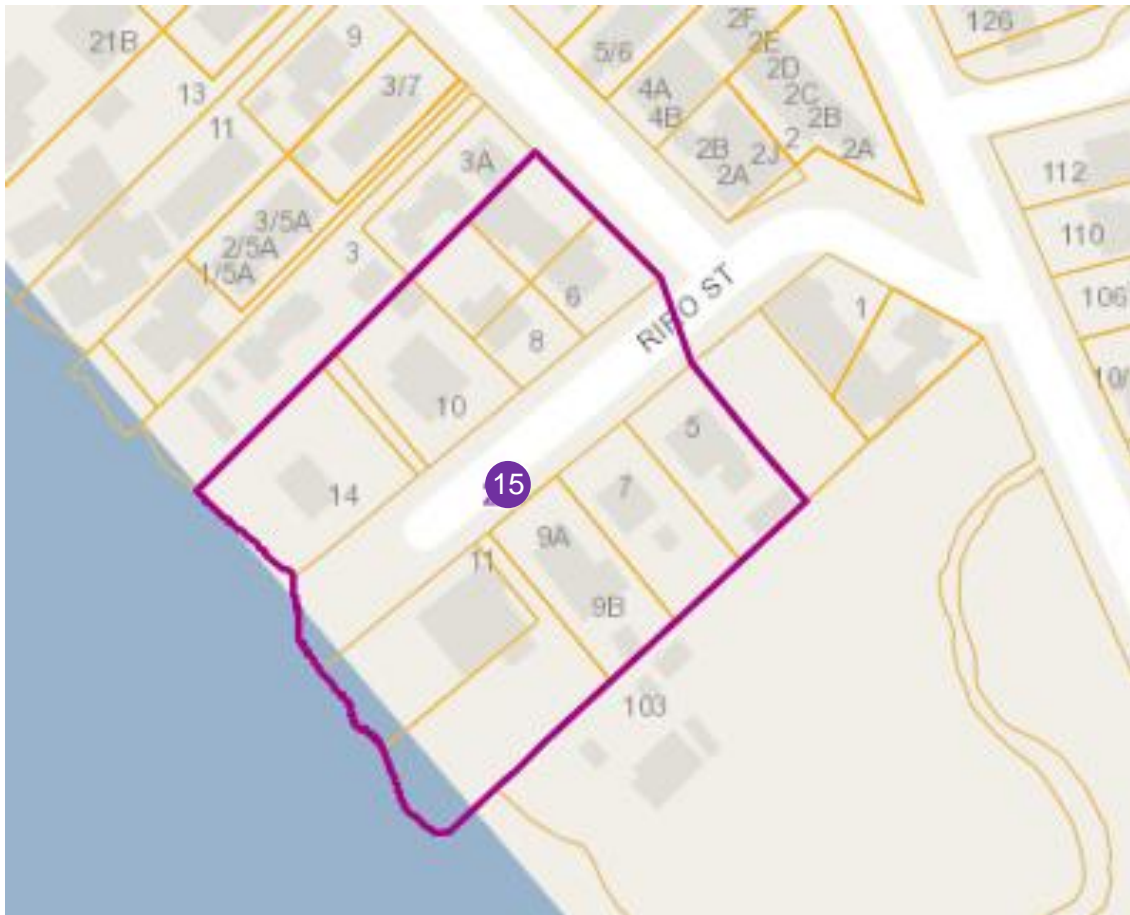


Riro Street HHA – Full Statement



Development Dates

- Shown survey of Township of Claudelands 1879, although many of the sites were subsequently further subdivided.
- Subsequent subdivisions were granted in Te Aroha Street soon after the turn of the 20th Century and in the second decade for Myrtle Street.
- The Record Map Pt.Hamilton Domain, 1935, shows subdivision patterns similar to the current time.

City Extension

- Within the 1st extension, October 1912

Background

Background (Historic, Cultural and Archaeological Qualities)

Prior to the 1864 invasion of the Waikato by colonial troops, Miropiko Pā, at River Road, in the north-west of Claudelands, was occupied by Ngāti Wairere, Ngāti Hānui and Ngāti Koura. However, following the invasion they moved to Gordonton and the land was confiscated and sold by the government.

Initially the land was allocated soldier settlers, but many of them sold their land to Francis Richard Claude, as an early wealthy settler from South America.

Overall Claude bought 400 ha (990 acres) and subdivided most of it in 1878.

Part of an area of existing kahikatea forest was cleared to create a racecourse, which was subsequently sold to the South Auckland Racing Club and then the Waikato A&P Association. The A&P Association had their first show on the 27th October 1892. Racing moved to the Te Rapa Racecourse in 1925.

The Hamilton-Morrinsville railway opened on 1 October 1884; the railway station in Claudelands opened at the same time and remained open until 1991. This provided direct access to Claudelands from Auckland.

The Te Aroha Street and Myrtle Street area had been included in Claude's 1878 subdivision and is included on the 1879 Town of Claudelands plan. Subsequent subdivisions have been granted to create the lots seen across the area today, including in 1905 for Mr Atkinson to subdivide part of the land to the south of Te Aroha Street and in 1911 for J W Hardley to create Myrtle Street and the north side of Te Aroha Street from Myrtle Street to River Road.

The Record Map Pt. Hamilton Domain, 1935, shows subdivision patterns similar to the current time.

The street pattern created by the subdivisions is representative of the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period:

- Streets tend to meet at right angle
- Back to back lot pattern
- A relatively high-density built environment
- Retention of green open spaces (in the wider area, including the 'racecourse' and associated forest
- Single-storey detached villas and bungalows in an eclectic architectural style

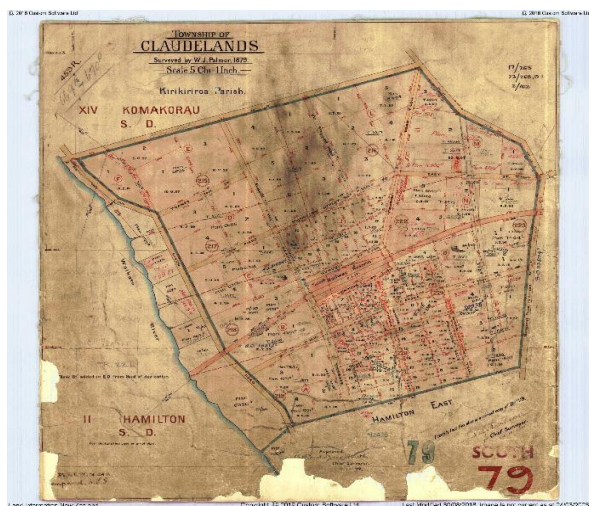


Figure 49 Plan of Township of Claudelands, 1879 (retrieved from premise.co.nz)



Figure 50 Hardley's 1911 plan for the subdivision of Myrtle Street (retrieved from premise.co.nz)

Buildings and Streetscape Elements (Architectural, Scientific and Technical Qualities)

As illustrated in by 1943 aerial photograph, the uptake of sections was almost complete by 1943. Unlike other parts of the Claudelands area, there has not been the redevelopment of a large number of lots with two storey flats in the 1960s and 1970s (although there are a limited number examples of this and other redevelopments). As a result, there is not significant variation in the architectural styles within the area; the 1910s to 1930s buildings are mainly single storey, and are California and English Bungalow style, with weatherboard

elevations, corrugated steel or tile gabled and hipped roofs, side hung casement windows and some ornamentation including on building gables. The layout of buildings within lots is relatively consistent, with buildings being placed reasonably central with in some cases equal depth front and rear yards. Most dwellings have had driveways added to the side of the building, with some garages in rear yards. However, there are also some garages built close to street frontages, these are generally small and some show on the 1942 aerial photo. These are significant as they show the emergence of the importance of the private car.

Front boundary treatments include low walls and fences, planting and some more dominant solid fences. Some of the taller more dominant fences take away from the consistency of the area but overall the low walls and fences are respectful of the boundaries which would have originally existed in the area. The continuation of low (less than 1.2m) fences or walls would not have a significant impact on the heritage values of the area.

Overall, the impression is that the buildings in the area represent the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period.



Figure 51 Cropped aerial photo, 1942 (retrieved from www.retrolens.nz)

The Myrtle Street and Te Aroha (West) area contributes to a clear understanding and appreciation of the development expected in the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period; it exhibits Outstanding heritage significance.

The area is an example of a very significant local developer, Francis Richard Claude, subdividing land beyond the boundaries of the Borough. The subsequent development of the area over time, guided by Claude's 1878 subdivision plan, with further subdivision from the early 20th Century onward, prior to the land being brought into the city, responded to the opening of the railway station in Claudelands in 1884, only 7 years after the railway arrived in Frankton, and the improved connectivity that this provided to Auckland and to the wider Waikato.

The area maintains the high level structure illustrated in Claude's subdivision plan of 1878, and the finer grained subdivision of the area which occurred in the early 20th Century,

without significant change. Unlike other parts of the Claudelands area, there has not been the redevelopment of a large number of lots with two storey flats in the 1960s and 1970s (although there are a limited number of examples of this and other redevelopments). As a result, there is not significant variation in the architectural styles within the area; the 1910s to 1930s buildings are mainly single storey and are California and English Bungalow style. The overall layout of the area and buildings within it show significant integrity and have changed little since their original construction.

In order for the existing values of the HHA to be maintained, it is important that future development incorporate the following features:

- Discourage subdivision of existing sites, as the HHA currently shows a high degree of integrity of lot size and layout, with little further subdivision and development from its establishment.
- Development should respect existing building setbacks. Buildings can be placed parallel or perpendicular to the street.
- Alterations or new buildings should utilise designs and materials which match the original mainly California and English Bungalow style buildings in the area:
 - It is important that the integrity of these styles remain when viewed from the street.
 - Any alterations and extensions should respect the design of the principal building and site as existing.
 - New accessory buildings of greater than 20m² should respect the design of the principal building and site as existing.
 - Painted horizontal timber weatherboard with chimneys in painted roughcast plaster. Whilst there are some buildings with plaster elevations in the area, these are not a dominant feature; this should therefore not be replicated.
 - Roofs coverings should be corrugated steel or clay tiles (with gables or hipped forms).
 - Generally, windows should have timber frames with multi-pane side hung casements, bringing a vertical proportion within a large horizontal shape window.
- Driveways should remain single width. Large areas of parking should be not provided to the front of buildings, over and above the driveway which can widen to the rear of buildings.
- Whilst some historic garages are located forward of the original building, new garages should be detached and located to the rear of dwellings, with single doors, so that they do not become a dominant feature in the street.
- Other accessory buildings, regardless of size, should not be forward of the original building.
- Front boundaries should have low timber picket fences or very low concrete/plaster walls. Fences or walls taller than this are not sympathetic to the historic heritage values of the area.
- Planting within front yards is appropriate, particularly hedges along front boundaries.

- Care should be taken to ensure that species chosen will not grow so large that all views of the main dwelling on the site are lost; views of the dwellings contribute to the heritage values of the area.
- Existing street trees, other street planting and front berms should be retained/maintained as existing.

Heritage Assessment Criteria:

a. Historic Qualities

The place or area is directly associated with, or has a direct relationship to, an important person, group, institution, event or activity, or reflects important aspects of local, regional or national history, including development and settlement patterns, transportation routes and social or economic trends.

The area has direct association with Francis Richard Claude, as an early wealthy settler from South America. Overall Claude bought 400 ha (990 acres) and subdivided most of it in 1878.

It is a significant example of a developer subdividing land beyond the boundaries of the Borough. The evolution of the wider area over time, guided by Claude's subdivision plan of 1878 with further subdivision from the early 20th Century onward, prior to the land being brought into the city, responded to the population growth in the area following the opening of the railway station in Claudelands in 1884, only 7 years after the railway arrived in Frankton, and the improved connectivity that this provided to Auckland and to the wider Waikato.

The area forming Riro Street was originally part of Frank Claude's 400 hectare farm, which he had purchased from Colonel William Moule in 1860. The extension of the railway across the river divided Claude's land 1884.

A subdivision plan dated 1909, prepared for Dr A Brewis, shows the subdivision of the land along with land forming Opoia Road and as far north as the railway. At this time the Borough boundary was a little way to the south of the area.

The subdivision of the land illustrates the pressure for development during the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period, with land owners adjacent to the town boundaries pre-empting the town's expansion by subdividing their property into smaller parcels intended for residential purposes; this meant a large additional population existed with access to the town's amenities and jobs but not paying rates to Hamilton Borough Council. The land was brought into the Borough (which had been formed in 1860) in 1912, by way of the first extension.

Riro Steet is located on a flat area of land alongside the River. The road corridor connects through to the River, and it is likely that there were views of the River from the street before the vegetation along the bank grew to its current size. The current impression is that the land at the end of the formed street is within 14 Riro Street, although boundary plans indicate that this is not the case.

There is a direct link from the street to Parana Park/Memorial Park.

The place has **outstanding local** historic qualities.

b. Physical/Aesthetic/Architectural Qualities

The place or area is a notable or representative example of:

- (i) A significant development period or activity; and/or*
- (ii) Distinctive or special attributes of an aesthetic or functional nature; and/or*
- (iii) The work of a notable architect, designer, engineer or builder.*

The dwellings in the area are California and English Bungalow styles are representative of the development period, as is the overall simple layout of the street.

Overall the street is a good example of the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period, with orthogonal layouts and relatively high density development, which capitalises on its location close to the river to provide amenity to residents with single storey villas and bungalows in an eclectic architectural style.

The California and English Bungalows are:

- Generally single level, with one having first floor space within the roof.
- Gabled ridged roofs mainly of corrugated steel.
- Most have weatherboard elevations, although there is one brick building and one with shingle elevations.
- Side hung casement windows.
- Some ornamentation including on building gables.
- Front boundaries are generally low picket fences, open or planted; whilst these vary, they provide the street with a consistent appearance.
- Whilst lot sizes vary, becoming larger towards the river, the overall impression is that lot layout is reasonably consistent, with buildings arranged to provide a large private rear yard for the dwellings.
- The original levels/topography of the area are maintained.

The street includes wide berms and good-sized street trees along the south side of the street.

The buildings are typical of the period and so do not use unique or uncommon building materials, or demonstrate an innovative method of construction, or are an early example of the use of a particular building technique.

The area is associated with Francis Richard Claude, who is responsible for the original urban subdivision of the land, and who was very influential in the continuing growth of the area to the east of the River.

The place has **high local** physical/aesthetic/architectural qualities.

c. Context Qualities

The place or area is an important landmark or feature or contributes to or is associated with a wider historical theme, traditional, or cultural context, or physical setting.

The place has **unassessed** context qualities.

d. Technological Qualities

The place or area shows a high degree of creative or technical achievement at a particular time, is directly associated with scientific or technical innovations or achievements, or is associated with scientific “break-through”. The place uses unique or uncommon building

materials, or demonstrates an innovative method of construction, or is an early example of the use of a particular building technique.

The place has **no known** technological qualities.

e. Archaeological Qualities

The potential of the place or area to define or expand knowledge of earlier human occupation, activities or events through investigation using archaeological methods, or to provide evidence to address archaeological research questions. For example, but not limited to: The place or area is registered by Heritage New Zealand for its archaeological values, or is recorded by the New Zealand Archaeological Association Site Recording Scheme, or is an 'archaeological site' as defined by the Heritage New Zealand Pouhere Taonga Act 2014.

The area is shown on the 1879 survey of Township of Claudelands, although many of the sites were subsequently further subdivided, with Riro Street being subject to a further subdivision in 1909. There are known archaeological sites alongside the River and within close proximity to the area. Being a level area, immediately on the banks of the River, the potential for information regarding earlier human occupation is therefore high.

It is likely that the place could provide evidence to address archaeological research questions.

The place has **high local** archaeological qualities.

f. Cultural Qualities

The place or area is important or significant:

(i) As a focus of cultural sentiment; and/or

(ii) As a context for community identity or sense of place, and provides evidence of social, cultural or historical continuity; and/or

(iii) For having symbolic or commemorative significance to people who use or have used it, or to the descendants of such people. The place or area has a high degree of interpretative potential to increase understanding of past lifestyles or events.

The place has **no known** cultural qualities.

g. Scientific Qualities

The potential for the place or area to contribute scientific information about how the natural environment has influenced, events, phases or activities related to development.

The place has **no known** scientific qualities.

Summary Table of Heritage Values

The place is considered to have heritage significance in relation to the following criteria:

Heritage Criteria	Significance	Context
a) Historic Qualities	Outstanding	Local
b) Physical/Aesthetic/ Architectural Qualities	High	Local
c) Context Qualities	Not assessed	
d) Technological Qualities	None	
e) Archaeological Qualities	High	Local
f) Cultural Qualities	None	
g) Scientific Qualities	None	

Statement of Significance

Riro Street illustrates the pressure for development along the boundaries of the Borough during the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period, having been subdivided prior to the land being brought into the Borough through the first extension in 1912, and then further subdivided after.

Maintaining the existing open frontages or low picket fences is an important element in maintaining the historic heritage significance of the area.

Overall, the area contributes to a clear understanding and appreciation of the development expected in the Late Victorian and Edwardian and during and after inter-war growth (1890 to 1949) development period; it exhibits Outstanding heritage significance. The area maintains the high level structure illustrated in Claude's subdivision plan of 1878, and the finer grained subdivision of the area which occurred in the early 20th Century, without significant change. Unlike other parts of the local area (including Opoia Street), there has not been the redevelopment of a large number of lots with two storey flats in the 1960s and 1970s.. The orthogonal layouts and relatively high-density development, which capitalises on its location close to the river to provide amenity to residents, the single storey villas and bungalows, and the simple street pattern, are a clear representation of the development period and has undergone little change.