



Miranda Gold, President  
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Dave McKinney, Commissioner  
Patricia Nilsson, Commissioner

Date: September 10, 2025

To: Daphne Romani, Brio Architecture

Staff Contact: Sam Standal

Project Description: Juniper Sauna Co

Trip Generation: This development is estimated to generate less than 10 vehicle trip per hour in the PM peak hour, based on the Institute of Transportation Engineers Trip Generation Manual, 11<sup>th</sup> edition.

<b>Proposed Development Meets</b>	
All ACHD Policies	
Requires Revisions to meet ACHD Policies	X

<b>Traffic Impact Study</b>	
Yes	
No	X
If yes, is mitigation required	

<b>Area Roadway Level of Service</b>	
Do area roadways meet ACHD's LOS Planning Thresholds?	
Yes	X
No	
Area roads will meet ACHD's LOS Planning Thresholds in the future with planned improvements?	
Yes	X
No	

<b>ACHD Planned Improvements</b>	
FYP	X
CIP	

<b>Livable Street Performance Measures</b>	
Pedestrian	LTS2
Cyclist	LTS2

<b>Is Transit Available?</b>	
Yes	X
No	

Comments:

*connecting you to more*



**Project/File:** **Juniper Sauna Co / GC25-0009 / DSRFY2025-0010**

This design review application is for a 2,650-square-foot commercial building located on 0.344 acres.

**Lead Agency:** Garden City

**Site address:** 202 E 34<sup>th</sup> Street  
Garden City, ID 83714

**Staff Approval:** September 10, 2025

**Applicant:** Daphne Romani  
Brio Architecture  
3313 W Cherry Lane  
Meridian, ID 83642

**Representative:** Chris and Kyle Mutzel  
2312 N 20<sup>th</sup> Street  
Boise, ID 83702

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**Report Summary:**

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## ACHD Planned Improvements

### 1. Capital Improvements Plan (CIP)/ Five Year Plan (FYP):

- Clay Street is scheduled in the FYP for a pedestrian improvement project from 32<sup>nd</sup> Street to 34<sup>th</sup> Street in 2028 and from 34<sup>th</sup> Street to 37<sup>th</sup> Street at a future date.
- 35<sup>th</sup> Street is scheduled in the FYP for a pedestrian improvement project from Brown Street to its northwest terminus at a future date.

## Level of Service Planning Thresholds

### 1. Condition of Area Roadways

Traffic Count is based on Vehicles per hour (VPH)

Roadway	Frontage	Functional Classification
Clay Street	150-feet	Local Commercial
34 <sup>th</sup> Street	100-feet	Local Commercial

### 2. Average Daily Traffic Count (VDT)

*Average daily traffic counts are based on ACHD's most current traffic counts.*

- The average daily traffic count for Clay Street east of 37<sup>th</sup> Street was 1,636 on June 12, 2024.
- There are no current ACHD traffic counts available for 34<sup>th</sup> Street.

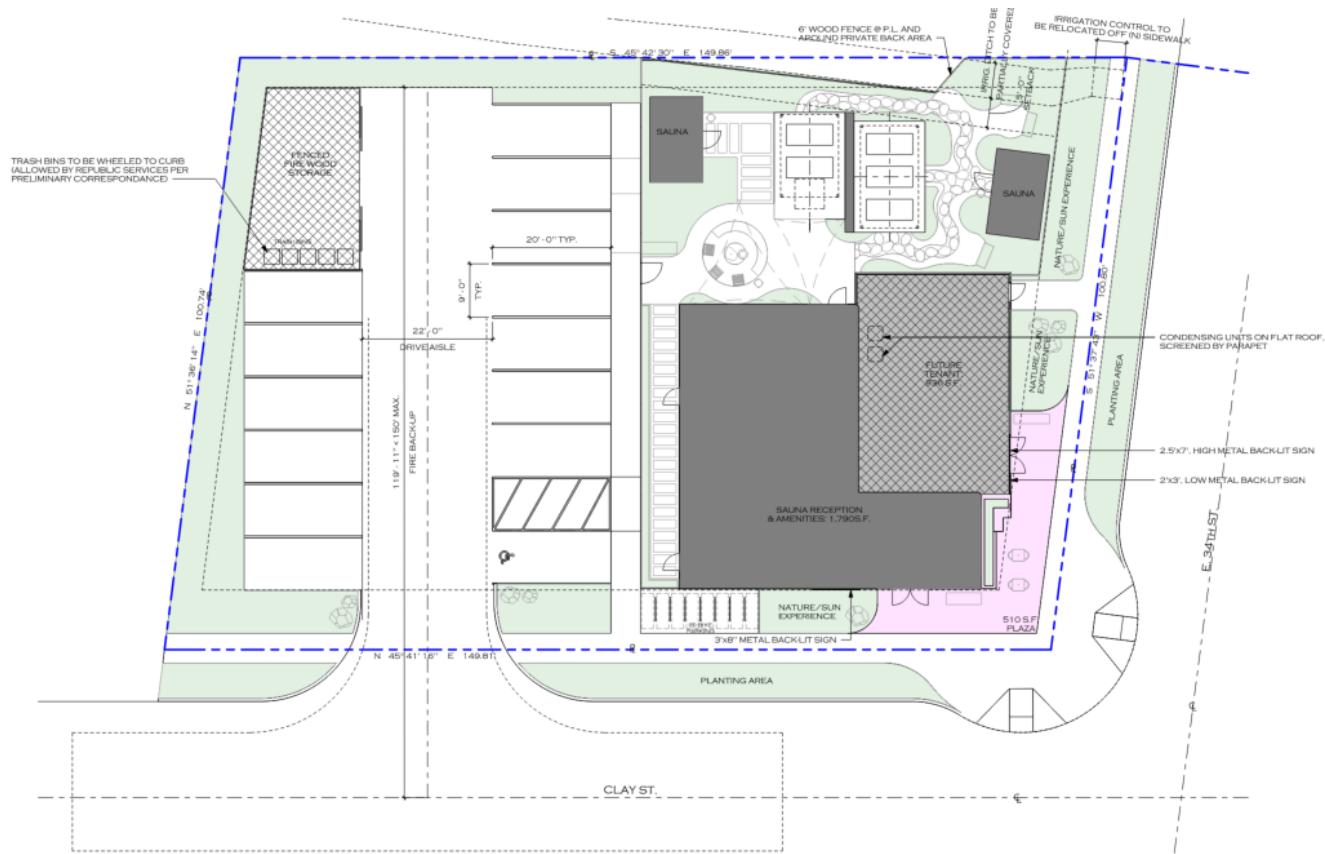
## **A. Site Specific Conditions of Approval**

1. Provide ACHD with a road trust deposit in the amount of \$27,600. ACHD will then use the road trust deposit money for pavement widening, vertical curb, gutter, and 5-foot wide concrete sidewalk abutting the site as part of the Clay Street community project.
2. Construct 34<sup>th</sup> Street as half of a 36-foot-wide local commercial street section with curb, gutter, and 5-foot-wide concrete sidewalk. If detached sidewalk is desired, then a minimum 8-foot-wide landscape barrier should be provided.
3. Dedicate right of way on 34<sup>th</sup> Street extending to 2-feet behind the back of sidewalk. For detached sidewalks, the applicant may dedicate right-of-way to extend 2-feet behind the back of curb and provide a right-of-way easement from the edge of right-of-way to 2-feet behind the back of sidewalk. Sidewalk should be located entirely within the right-of-way or entirely within the easement.
4. Construct a 20-foot-wide, paved driveway from the site onto Clay Street located 45-feet east of the site's west property line.
5. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
6. There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.
7. Comply with all Standard Conditions of Approval.

## **B. Vicinity Map**



## C. Site Plan



## **D. Findings for Consideration**

### **1. Clay Street**

- a. Existing Conditions:** Clay Street is improved with 2-travel lanes, 33 to 35-feet of pavement, and no curb, gutter or sidewalk abutting the site. There is 50-feet of right-of-way for Clay Street (27 to 29-feet from centerline).
- b. Applicant's Proposal:** The applicant is proposing to construct Clay Street abutting the site as half of a 33.5-foot-wide local street section with curb, gutter, 5-foot-wide detached concrete sidewalk, and 6-foot-wide landscape barrier. The applicant is also proposing to construct a 20-foot radius bulb-out at the intersection of Clay Street and 34<sup>th</sup> Street.
- c. Staff Comments/Recommendations:** The applicant's proposal exceeds District Policy and should not be approved, as proposed. Typically, the applicant would be required to construct Clay Street as half of a 36-foot-wide local commercial street section with curb, gutter, and 5-foot-wide concrete sidewalk. However, this segment of Clay Street is scheduled in ACHD's Five Year Plan for a pedestrian and community enhancement project. Therefore, in lieu of improving Clay Street with pavement widening, vertical curb, gutter, and sidewalk, the applicant should provide ACHD with a road trust deposit in the amount of \$27,600. ACHD will then use the road trust deposit money for pavement widening, vertical curb, gutter, and 5-foot-wide concrete sidewalk abutting the site as part of the Clay Street community project.

The Five Year Plan improvements to Clay Street also include the construction of a bulb-out at the intersection of Clay Street and 34<sup>th</sup> Street. Therefore, the applicant's proposal to construct a bulb-out located at the site's southeast corner should not be approved as proposed.

### **2. 34<sup>th</sup> Street**

- a. Existing Conditions:** 34<sup>th</sup> Street is improved with 2-travel lanes, 38-feet of pavement, and no curb, gutter, or sidewalk abutting the site. There is 49-feet of right-of-way for 34<sup>th</sup> Street (26-feet from centerline).
- b. Applicant Proposal:** The applicant is proposing to construct 34<sup>th</sup> Street as half of a 33.5-foot wide street section with curb, gutter, 5-foot-wide detached concrete sidewalk, and 6-foot-wide landscape barrier.

The applicant is not proposing any additional dedication of right-of-way.

- c. Staff Comments/Recommendations:** The applicant's proposal does not meet District Policy and should not be approved as proposed. To meet District Policy, the applicant should be required to construct 34<sup>th</sup> Street as half of a 36-foot-wide local commercial street section with curb, gutter, and 5-foot-wide concrete sidewalk. If detached sidewalk is desired, then a minimum 8-foot-wide landscape barrier should be provided.

The applicant should be required to dedicate right of way on 34<sup>th</sup> Street extending to 2-feet behind the back of sidewalk. For detached sidewalks, the applicant may dedicate right-of-way to extend 2-feet behind the back of curb and provide a right-of-way easement from the edge of right-of-way to 2-feet behind the back of sidewalk. Sidewalk should be located entirely within the right-of-way or entirely within the easement.

### **3. Driveways**

#### **3.1 Clay Street**

- a. Existing Conditions:** There are no existing driveways from the site onto Clay Street.
- b. Applicant's Proposal:** The applicant is proposing to construct a 20-foot-wide, paved driveway from the site onto Clay Street located 45-feet east of the site's west property line.

c. **Staff Comments/Recommendations:** The applicant's proposal meets District Policy and should be approved, as proposed.

## **E. Policy**

### **1. Federal Accessibility Design Guidelines and Standards**

District policy 7203.1.1 states that developers shall follow the current version of the U.S. Access Board's Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG), 36 CFR Part 1190, September 7, 2023; (Also see, <https://www.access-board.gov/prowag> and <https://www.access-board.gov/files/prowag/planning-and-design-for-alternatives.pdf> for additional information).

### **2. Minor Improvements**

**Minor Improvements Policy:** District Policy 7203.3 states that minor improvements to existing Highways adjacent to a proposed development may be required. These needed transportation facilities are to correct deficiencies or replace deteriorated facilities. Included are sidewalk and/or bike lane construction or replacement (with physical buffers if missing and needed); construction of transitional sidewalk segments; crosswalk construction or replacement; curb and gutter construction or replacement; repair, replacement or expansion of curb extensions; replacement of unused driveways with curb, gutter, sidewalk, repair or addition of traffic calming or speed mitigation features; installation or reconstruction of pedestrian ramps; pavement repairs; signs, motor vehicle, pedestrian and bicycle traffic control devices; and other similar items. The current version of PROWAG will determine the applicable accessibility requirements for alterations and elements added to existing streets. ACHD staff is responsible for identifying the minor improvements that would be proportionate to the size and complexity of the development.

### **3. Livable Street Performance Measures**

District policy 7203.8 states that ACHD has adopted performance measures (level of stress) for evaluating the experience of bicyclists and pedestrians. ACHD seeks to create a transportation network that serves all ages and abilities. Bike and pedestrian facilities built through development should achieve a bike and pedestrian level of traffic stress 1 or 2, with no new bike lane below a minimum of 5-feet.

### **4. Clay Street and 34<sup>th</sup> Street**

**Commercial Roadway Policy:** District Policy 7208.2.1 states that the developer is responsible for improving all commercial street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Street Section and Right-of-Way Policy:** District Policy 7208.5 states that right-of-way widths for new commercial streets shall typically be 50 and 70-feet wide and that the standard street section will vary depending on the need for a center turn lane, bike lanes, volumes, percentage of truck traffic, and/or on-street parking.

- A 36-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and on-street parking.
- A 40-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and a center turn lane.
- A 46-foot street section (back-of-curb to back-of-curb) will typically accommodate two travel lanes and a center turn lane and bike lanes.

**Off-Site Streets Policy:** District Policy 7208.2.3 states that if the proposed development is not served by a public street, the developer shall pave the street or widen the existing pavement to provide a 30-foot wide (minimum) paved street with 3-foot gravel shoulders from the proposed development to the public street specified by the District. Wider street widths may be required

depending on the magnitude of the development and other factors, including the potential for bicycle, bus and pedestrian traffic.

If the proposed development is served by a paved public road less than 30-feet wide, the developer shall widen the pavement to a minimum of 30-feet wide or add 3-feet of additional pavement plus 3-foot gravel shoulders to the existing road, whichever is greater. The road shall be widened from the site to the public street specified by the District. All utility relocation costs associated with the off-site street widening shall be borne by the developer.

**Continuation of Streets Policy:** District Policy 7208.2.4 states that an existing street, or a street in an approved preliminary plat, which ends at a boundary of a proposed development shall be extended in that development. The extension shall include provisions for continuation of storm drainage facilities. Benefits of connectivity include but are not limited to the following:

- Reduces vehicle miles traveled.
- Increases access for emergency services.
- Reduces need for additional access points to the arterial street system.
- Promotes the efficient delivery of services including trash, mail and deliveries, water and sewer.
- Promotes orderly development.

**Sidewalk Policy:** District Policy 7208.5.6 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all commercial streets. If a separated sidewalk is proposed, a parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Half Street Policy:** District Policy 7208.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-feet), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side.

**Cul-de-sac Streets Policy:** District policy 7208.5.7 states that the minimum radius permitted for a turnaround is 55-feet to back-of-curb.

**Landscape Medians Policy:** District policy 7208.5.15 states that landscape medians are permissible where adequate pavement width is provided on each side of the median to accommodate the travel lanes and where the following is provided:

- The median is platted as right-of-way owned by ACHD.
- The width of an island near an intersection is 12-feet maximum for a minimum distance of 150-feet. Beyond the 150-feet, the island may increase to a maximum width of 30-feet.
- At an intersection that is signalized or is to be signalized in the future, the median width shall be reduced to accommodate the necessary turn lane storage and tapers.
- The Developer or Homeowners Association shall apply for a license agreement if landscaping is to be placed within these medians.

- The license agreement shall contain the District's requirements of the developer including, but not limited to, a "hold harmless" clause; requirements for maintenance by the developer; liability insurance requirements; and restrictions.
- Vertical curbs are required around the perimeter of any raised median. Gutters shall slope away from the curb to prevent ponding.

## 5. Driveways

### 5.1 Clay Street

**Driveway Location Policy:** District policy 7208.4.1 requires driveways near intersections to be located a minimum of 75-feet (measured centerline-to-centerline) from the nearest local street intersection, and 150-feet from the nearest collector/arterial or arterial street intersection.

**Successive Driveways:** District Policy 7208.4.1 states that successive driveways away from an intersection shall have no minimum spacing requirements for access points along a local street, but the District does encourage shared access points where appropriate.

**Driveway Width Policy:** District policy 7208.4.3 restricts commercial driveways to a maximum width of 40-feet. Most commercial driveways will be constructed as curb-cut type facilities.

**Driveway Paving Policy:** Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7208.4.3, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway.

**Driveway Design Requirements:** District policy 7208.4.3 states if an access point is to be gated, the gate or keypad (whichever is closer) shall be located a minimum of 50-feet from the near edge of the intersection and a turnaround shall be provided.

## 6. Traffic Calming

**Speed Control and Traffic Calming Policy:** District policy 7208.3.7 states that the design of commercial street systems should discourage excessive speeds by using passive design elements. If the design or layout of a development is anticipated to necessitate future traffic calming implementation by the District, then the District will require changes to the layout and/or the addition of passive design elements such as horizontal curves, bulb-outs, chokers, etc. The District will also consider texture changes to the roadway surface (i.e. stamped concrete) as a passive design element. These alternative methods may require a maintenance and/or license agreement.

## 7. Tree Planters

**Tree Planter Policy:** Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

## 8. Landscaping

**Landscaping Policy:** A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

## 9. Pathway Crossings

United States Access Board R304.5.1.2 Shared Use Paths. In shared use paths, the width of curb ramps runs and blended transitions shall be equal to the width of the shared use path.

AASHTO's Guidelines for the Development of Bicycle Facilities 5.3.5 Other Intersection Treatments: The opening of a shared use path at the roadway should be at least the same width as the shared use path itself. If a curb ramp is provided, the ramp should be the full width of the path, not including any flared sides if utilized. . . . Detectable warnings should be placed across the full width of the ramp.

FHWA's "Designing Sidewalks and Trails for Access" (1999) reflected common ADA-related concepts: Chapter 6, Page 16-6: The width of the ramp should be at least as wide as the average width of the trail to improve safety for users who will be traveling at various speeds. In addition, the overall width of the trail should be increased, so the curb ramp can be slightly offset to the side. The increased width reduces conflict at the intersection by providing more space for users at the bottom of the ramp.

## **F. Standard Conditions of Approval**

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA), Public Right-of-Way Accessibility Guidelines (PROWAG), ISPWC, or ACHD requirements. The applicant's engineer should provide documentation of compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.

12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property, which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

## **G. Conclusions of Law**

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

### **Request for Appeal of Staff Decision**

To request an appeal of a staff level decision, see District policy 7101.6.7 at  
<https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>

### **Request for Reconsideration of Commission Action**

To request reconsideration of a Commission Action, see District policy 1006.11 at  
<https://www.achdidaho.org/home/showpublisheddocument/452/638243231708370000>