



Northern Pacific Railway Company.
Engineering Department Records.

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N. P. RY. CO.

OFFICE OF *Chief Engineer*FILE NO. *2381*

SUBJECT:

*Bridge Section - 1908**Bids =**See Contract files under**names of successful**Bidders -*

WLD*G

2381 +
St. Paul, May 30th, 1908.

Mr. H. E. Stevens,

Please note attached copy of letter from
Mr. Nutt under date of 25th instant, and my reply to him of
even date, relative to erection of White and Stuck River
Bridges.

W. L. Darling.

Encl.

2

WLD*G

St. Paul, May 30th, 1908.

Mr. H. C. Nutt,
General Manager,
Tacoma, Washington.

Dear Sir:-

Replying to your letter of the 25th instant
relative to erection of Hoquiam, Toutle and Cowlitz River
Bridges.

We will work on the basis of that letter, and
that you are to erect the Stuck and White River Bridges
also, and will therefore do nothing further about letting
them by contract.

Yours truly,

Copy to Mr. H. E. Stevens.

Chief Engineer.

Northern Pacific Railway Company

RECEIVED BY
CHIEF ENGINEER
MAY 30 1908
NOR. PAC. RY.
ST. PAUL, MINN.

Tacoma, May 25, 1908.

Mr. W. L. Darling,

Chief Engineer, Saint Paul.

Dear Sir:

I have your letter of May 20, in reference to the erection of Hoquiam, Toutle and Cowlitz River Bridges, and, inasmuch as we are going to get what tools and equipment are necessary for those three bridges, I think we might as well undertake to erect the Stuck River and White River bridges, as I am satisfied we can do it for considerably less than \$12.50 per ton.

Yours truly,

H. Rutt

Cy-BEP
ARC

WLD*G

St. Paul, May 20th, 1908.

Mr. H. C. Nutt,

General Manager,

Tacoma, Washington.

Dear Sir:-

I understand that you are willing to undertake the erection of Hoquiam, Toutle and Cowlitz River Bridges. Possibly you would also like to undertake the Stuck River and White River Bridges, for which we have been able to get bid of only \$12.50 per ton.

Will you please advise whether you care to undertake them or not on the basis of that price.

Yours truly,

Copy to H. E. Stevens.

Chief Engineer.

WLD*G

St. Paul, May 20th, 1908.

Mr. H. E. Stevens,
Bridge Engineer,

Dear Sir:-

Award has been made for bridges included
in your letter of the 18th instant as follows,-

To McCreary & Willard:

Lightning Creek, 187 tons, @ \$9.00 per ton.

Palouse River Bridge, 387 " @ 9.00 "

To Kelly-Atkinson Construction Company:

White Pine Hill Viaducts, 1,340 tons @ \$11.00 per ton.

Cowlitz, Toutle and Hoquiam River Bridges to be done by the
Operating Department. I also want to take up with the
Operating Department and see if they cannot do White River
and Stuck River as well.

Yours truly,

Chief Engineer.

Northern Pacific Railway Company.

St. Paul, May 18, 1908.

C

Mr. W. L. Darling,
Chief Engineer.

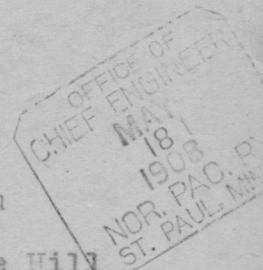
Dear Sir:-

Herewith proposals from the various erection contractors for erection of steel viaducts on White Pine Hill Line, Palouse River bridge, and various structures on the Pacific Division, also summary sheet of these bids.

You will note that McCreary & Willard are low bidders on all items, with the exception of Hoquiam draw span on which Kelly-Atkinson bid the same, that is, \$14.00. Based on their bid, the approximate cost of erection of the various bridges would be as follows:

Lightning Creek, 187 tons @ \$9.00,	\$ 1,683.00
White Pine Hill viaducts, 1,340 tons @ \$10.	\$13,400.00
Stuck River bridge, 468 tons @ \$10.	\$ 4,680.00
X Toutle River bridge, 225 tons @ \$10.	\$ 2,250.00
Y Cowlitz River bridge, 382 tons @ \$9.50.	\$ 3,629.00
7 ** Hoquiam Draw Span, 455 tons @ \$14.	\$ 6,370.00
White River bridge, 384 tons @ \$10.50	\$ 4,024.00
Palouse River bridge, 387 tons @ \$9.00	\$ 3,483.00

** Kelly-Atkinson bid same on this item.



Mr. W. L. Darling-2-

The next lowest bidders, and the approximate cost of erection based on their bid for the various items, is as follows:

Lightning Creek---	Missouri Valley Bridge & Iron Co.	
187 tons @ \$9.25,		\$ 1,729.75
** White Pine Hill viaducts,--	Frankman Brothers.	
1,340 tons @ \$10.95,		\$14,673.00
Stuck River----	Frankman Brothers.	
468 tons, @ \$11.50-		\$ 5,382.00
Toutle River-----	Frankman Brothers.	
225 tons @ \$11.50,		\$ 2,587.50
Cowlitz River-----	Frankman Brothers.	
382 tons @ \$10.50,		\$ 4,011.00
Hoquiam Draw Span,--	Strobel Steel Construction Co.	
455 tons @ \$16.50		\$ 7,517.50
White River-----	Frankman Brothers.	
384 tons @ \$11.50,		\$ 4,408.00
Palouse River-----	Frankman Brothers.	
387 tons @ \$12.00		\$ 4,644.00

** Kelly-Atkinson Construction Company bid \$11.00 per ton on this item, that is , Five cents more per ton.

Superintendent Albee advises that the division forces can erect Hoquiam draw span, Cowlitz and Toutle River bridges for the following prices:

Hoquiam Draw Span, 455 tons @ \$9.00,	\$4,095.00
Cowlitz River, 382 tons @ \$8.00,	\$3,056.00
Toutle River, 225 tons, @ \$7.00	\$1,575.00

Yours truly,

H. E. Stevens
Bridge Engineer.

Item.	Approximate Tonnage.	McCreary & Willard.	Bernhisel Const. Company.	Kelly- Atkinson Cont. Co.	Missouri Valley R. & I. Co.	Stobbel Steel Cont. Co.	Ferro Const. Co.	Pittsburg Cont. Company.	Frankman Bros.	
Lightning Creek, 2-100' Through plate girders,	137	\$ 9.00 ✓	\$15.00	\$11.00	\$ 9.25	\$12.60	\$11.50	\$13.50	\$10.95	
White Pine Hill viaducts,	1,340	\$10.00 ^N	\$15.00	\$11.00 ✓	\$11.30	\$11.90	\$11.50	\$13.50	\$10.95	
Stuck River, 1-185' 6" double track T.P.S.,	468	\$10.00 ✓	No. bid.	\$12.50	\$12.50	\$14.60	No. bid.	No. bid.	\$11.50	
Toutle River, 1-185' through pin span,	225	\$10.00	" "	\$12.50	\$12.50	\$14.60	" "	" "	\$11.50	
Cowlitz River, 2-161' through pin spans on skew,	382	\$ 9.50 ✓	" "	\$12.50	\$12.50	\$14.60	" "	" "	\$10.50	
Hoquiam River, 1-317' T.P. draw span,	455	\$14.00	" "	\$14.00 ✓	\$19.50	\$16.50	" "	" "	\$18.00	
White River, 1-250' deck pin span,	384	\$10.50 ^N	" "	\$12.50	\$13.50	\$14.60	" "	" "	\$11.50	
Palouse River, 1-250' through pin span,	387	\$ 9.00 ✓	" "	\$12.50	\$13.50	\$14.60	" "	" "	\$12.00	

Tacoma, March 30th, 1908

Mr. C. M. Levey,

Third Vice-President

Dear Sir:-

I enclose, for your information, a letter from Mr. Palmer with one from Mr. Beamer, in reference to Mr. Darling's supposition that possibly the derailment of No. 5 at Cocolalla, February 18, was due to the fact that the tie plates had not been thoroughly embedded, but that they were placed on the ties with the intention of letting the traffic drive them down.

It seems from these letters that the tie plates were thoroughly embedded in the ties.

Yours truly,

H. C. Nutt

Encl.

*Referred to Mr. Darling's
information
Curt
Noted w/P
4/8
Return
to Brint 4/11*

At. Spokane, March 27th, 1908

Mr. H. C. Nutt,
General Manager,
Tacoma, Washington

Dear Sir:-

I invite your attention to Mr. Beamer's letter of explanation regarding Mr. Darling's letter of the 11th.

The steel at the point of this derailment has been laid for some time and tie plates have had ample time to become thoroughly embedded. I do not believe that the trouble was due to the cause mentioned by Mr. Darling. As a matter of fact as I recall the rules at the time this track was laid, we were not permitted to drive the tie plates by traffic but were obliged to drive the plates home with the spike maul. The real cause is still shrouded in some mystery that I am sorry we have not been able to clear up.

We have had a great deal of trouble with 72 pound angle bars from time to time, although this is the first case where a derailment was attributed to their being broken. From conversation with section foremen they say that the new 85 pound angle bars give no trouble at all but they very frequently find 72's broken. You will remember that not very long ago we were talking with the section foreman at Eltopia who corroborated this statement..

Yours respectfully,

B. E. Palmer

Spokane, March 21st, 1908

Mr. B . E. Palmer,
General Superintendent,
Tacoma, Washington

Dear Sir:-

Replying to yours of the 19th and returning
correspondence in regard to the derailment of No. 5 at Cocolalla
on February 18th.

I appreciate what might result from a condition like
that described by Mr. Darling, at the same time I positively
know that that condition did not obtain at this point and you
doubtless are aware of it also since the steel on this curve was
laid by you at the time you were Superintendent on the Rocky
Mountain Division, and while I do not know the method used for
applying the tie plates I do know that they were securely bedded
and gave no evidence whatever of movement at the time the
accident occurred because I examined the track personally.

Yours respectfully,

A. Beamer

Encl.

St.Paul, March 12th 1908.

Mr. H. C. Nutt,
Gen'l.Manager, Tacoma.

Dear Sir:-

Referring to your report of March 4th in regard to accident to No.5 on February 18th at Cocolalla; I attach copy of a letter from Mr.Darling stating what he thinks might have caused the accident.

There was certainly something wrong with this track.

Yours truly,

(Signed) C.M.Levey.

WLD-G.

St. Paul, Minn. Mar. 11th 1908.

Mr. C. M. Levey,

Third Vice-President.

Dear Sir:-

Referring to your notation on Mr. Nutt's report of the 4th instant relative to derailment of Train No. 5 on February 18th East at Cocolalla.

I believe that an investigation will show that the trouble was due to tie plates, -that the plates were placed on the ties and allowed to be driven down by the power, after which spiking was not followed up and kept close to the plates, the consequence being that the rail got a chance to spread and threw the whole force onto the angle bar. Of course this is merely supposition, but I have seen track, especially between here and Minneapolis where such an accident could occur: and neither present angle bar or any other bar that I know of could have withstood it, because either the bar would have to break or the bolts would have to go.

Yours truly,

Chief Engineer.

Encl.

Mr Elliott
Und
noted not give
W.L. Darling the information
about broken angle bar
Feb 10
HC

Tacoma, Washington.
March 4th 1908.

Mr Darling
to you
Cm se
3/10

Mr. C. M. Levey,
Third Vice-President,
St. Paul, Minn.
Dear Sir:-

At 3:50 a.m. February 18th passenger train No.5,
engine 2396, engineer Camp, conductor J.L.Brazelton, con-
sisting of seven cars, running about twenty-five miles per
hour, derailed engine tank and all of the following cars in
train, except rear trucks of the last sleeper, at a point
a quarter mile east of the telegraph office at Cocalalla.
Cars were jack-knifed and somewhat careened to the north.
Damage was as follows:

To Baggage car 359	25.00
Express car 252	25.00
Smoker 859	150.00
Chair Car 4432	200.00
Diner 1639	75.00
Cost of clearing wreck,	92.00
Total-----	\$567.00

Conductor Brazelton was just stepping out on to
platform of the smoker when derailment occurred, and was
somewhat bruised and cut about the face and head. Nine
passengers claimed to be slightly injured and were settled
with on the spot by Claim Agent Grover, the aggregate amount
of settlements being about \$200.00. The accident occurred
between the switches and trains were run over the east passing

CML-2.

track until the main line was cleared, about 2 A.M. the following day. The passengers were cared for at Cocoplalla and sent forward on No.1 about two hours after the accident.

Derailment was caused by angle bar on the outside of three degree curve breaking under the outward pressure of the engine drivers, allowing the rail to be forced over. Only one of the angle bars at the break could be found, and this showed a clean fracture, with no evidence of a flaw, but it is thought the other bar was cracked, leaving only one to withstand the strain. The track at the point of derailment is laid with 85# steel on tie plates. The north rail was forced out of position for about 300 feet, but south rail was not moved. Ties were not damaged and after cars were rerailed, the north rail was replaced on the tie plates.

There was no action taken.

Yours truly,

(Signed) H.C.Nutt.

2 3 5 2

