

St. Michael's Church

High Street, Aveley.

RM 15 4AD



PROPOSAL FOR A NEW CHURCH HALL

Design and Access Statement





1.0 Introduction

- 1.1 Until 2001, The Congregation of the Church of St. Michael, Aveley, was able to use the old 'Reemer Hall' on Mill Road, Aveley, as their church hall. The Reemer Hall was built for the Church in 1957. It functioned well for over 50 years and provided versatile accommodation for both the congregation and the local community but, by 2010, had become increasingly difficult and expensive to maintain. Both the Reemer Hall and the Scout Hut were lined with asbestos panels.
- 1.2 The site of the old Church Hall, together with the adjoining former football pitch, is the subject of a planning proposal for the construction of affordable houses (reference 21/01497/FUL).
- 1.3 The Parochial Church Council of the Church of St. Michael has explored a number of options for a new church hall, including (a) extending the 'new wing (to the south of the existing kitchen and lavatories at the west end of the Church and (b) constructing a new community space to the south of the South Aisle. Details for these proposals are described and illustrated in this document. Neither proposal was acceptable due to their respective impacts upon the Grade I Listed Church.
- 1.4 This document has been prepared in support of a revised proposal for a new church hall to be constructed in the south-west corner of the Churchyard, in-keeping with the historic pattern of development around the site perimeter and sensitive of the historic views across the Churchyard.
- 1.5 Following the loss of their previous church hall, the Parochial Church Council prepared a brief for a much needed new facility that can accommodate the activities they provide in support of the community. This community is set to grow significantly over the coming years with the construction of two new large scale housing developments, and in parallel with the severe cuts to local government funding, the proposed Community Centre is needed more than ever.
- 1.6 The design and character of the proposed Church Hall was revised following negative comments from the Chelmsford Diocese, who found the previous interpretation of traditional vernacular architecture of Essex Barns erroneous. A more contemporary, less economical looking design was requested from the Architects.
- 1.7 Consultation with Historic England also revealed concerns regarding the proximity to a listed scheduled monument and the disruption of the interconnectivity between St Michael's Churchyard, the Moated Site scheduled monument to the east and the Grade II listed Aveley Hall along with its grounds to the south of the Churchyard.
- 1.8 The current proposal is the result of addressing the comments made by the public, Thurrock Council's Principal Design Officer and Conservation Officer, The Chelmsford Diocese, Historic England, St. Michael's Church Parochial Church Council and Thurrock's Highways Officer.



2.0 Foreword

2.1 This Statement relates to the Church of St. Michael, Aveley, Thurrock, Essex, RM15 4AD. The Statement has been prepared by MRDA Architects LLP with regard to the proposal to construct a new church hall within the existing Churchyard. The Design and Access Statement should be read in conjunction with The Statement of Need and the Statement of Heritage Significance.

3.0 Location & Access

3.1 The church of St. Michael lies within the historic settlement of Aveley, approximately eight hundred metres west of the M25 London Orbital motorway and half way between Central London and Southend-on-Sea. The location of the church is marked by a red dot on the regional map below.

3.2 The village is accessed by motorised vehicles from the south via Ship Lane; from the west via Purfleet Road; from the east and west via the B1335 that runs through the settlement at the northern end of the village; and from the north via Romford Road.

3.3 The A13 circumnavigates the southern end of the settlement and connects with the M25 at the large Mardyke Junction east of Aveley. The B1335 is known as Sandy Lane to the west of Aveley, Lance Corporal Nicky Mason Way within Aveley and Stifford Road to the east of Aveley.

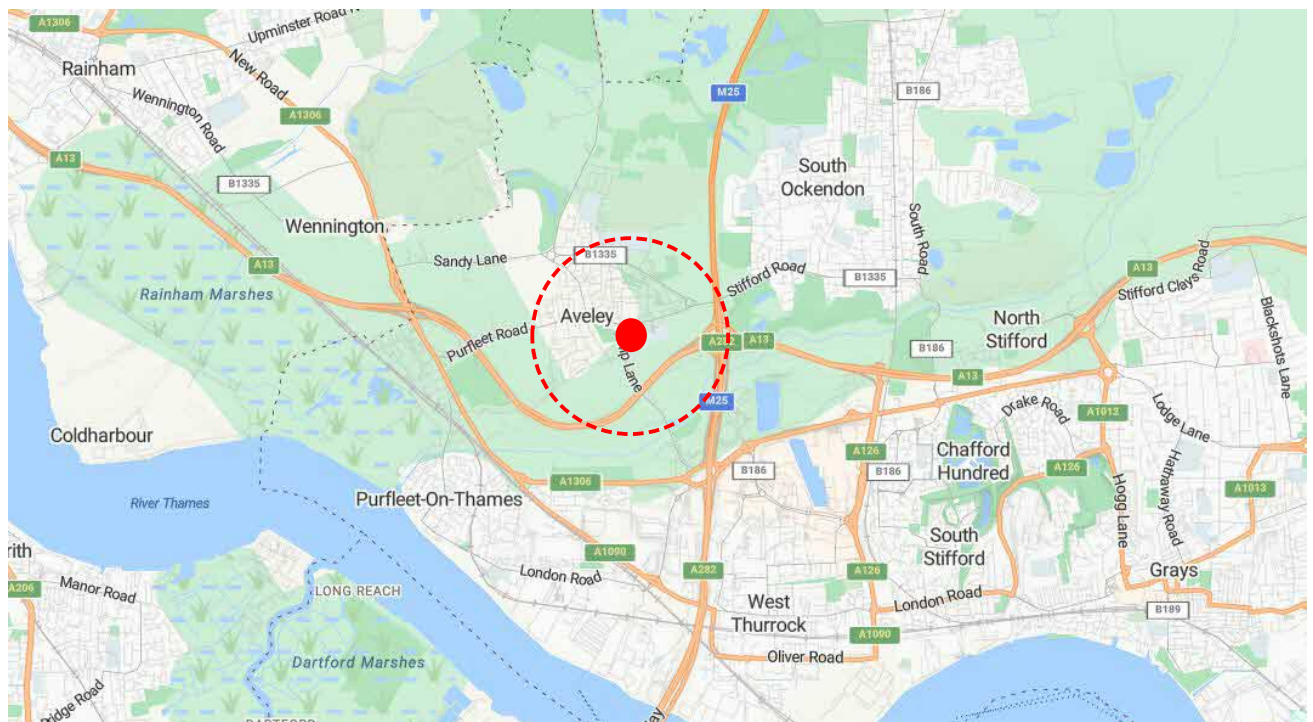


Figure 1. St. Michael's Regional Location Plan



Figure 2. St. Michael's Local Location Plan.

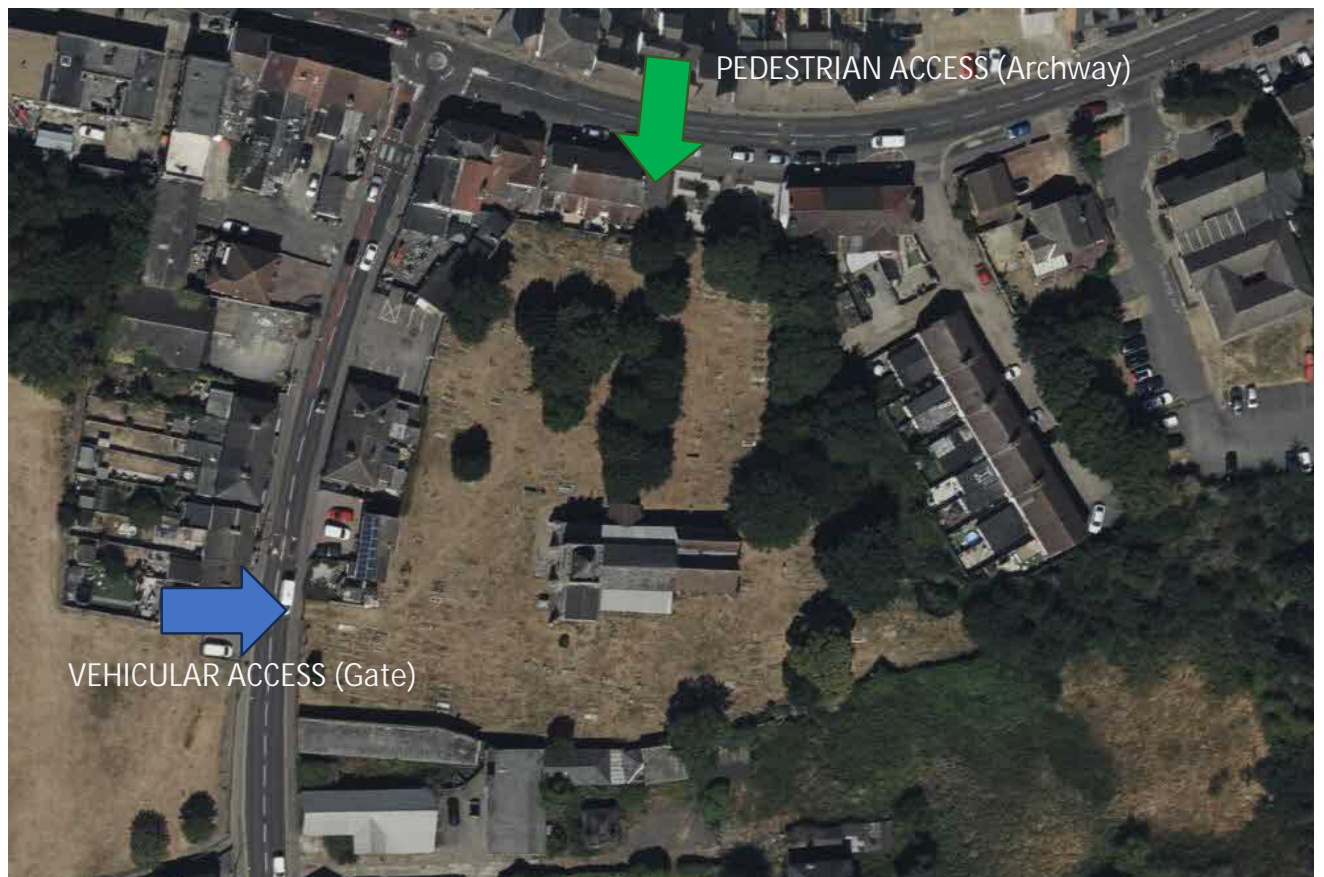


Figure 3. Satellite Image of St. Michael's Church and Grounds with Existing Access Points

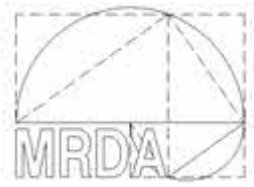


Figure 4. Arched Entranceway into Churchyard –Pedestrian Access (High Street)

- 3.4 Vehicular and emergency service access is from Ship Lane at the southern end of the western Churchyard boundary.



Figure 5. Churchyard Entrance Gate - Vehicular Access (Ship Lane)



3.5 Aveley is serviced by the 10, 22, 32, 77, 77A, 372 and Z1 bus routes shown on the map below.

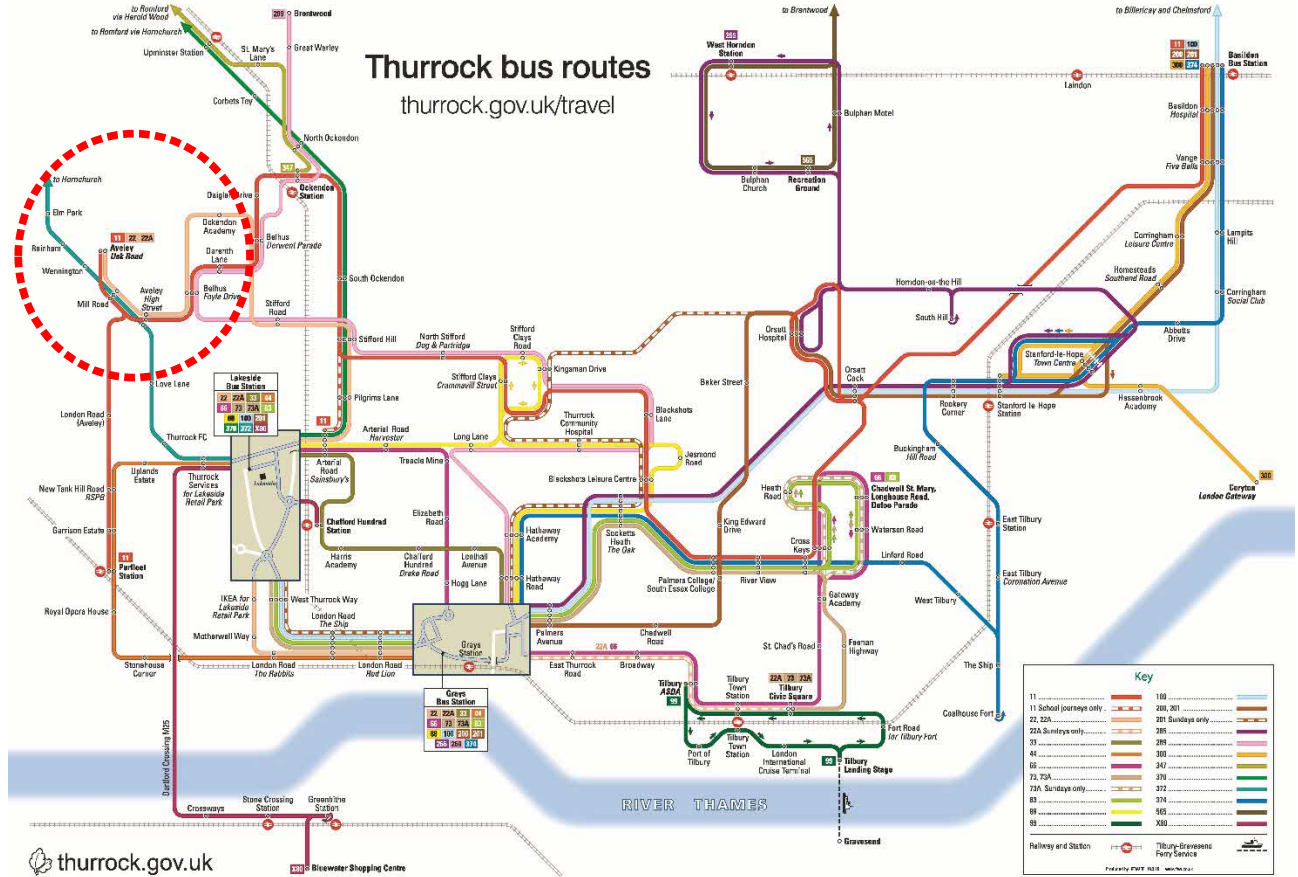


Figure 6. Local Bus Routes Map

3.6 The settlement's nearest train stations are those of Purfleet to the south, approximately 10 minute drive from the church by car, 18 minute cycle or an hour's walk; Chafford Hundred Station to the south-east of the Church, approximately 10 minute drive from the church by car, 18 minute cycle or an hour's walk. Ockenden to the north-east, approximately 8 minute drive by car, 15 minute cycle and also an hour's walk; and Rainham, an approximate 10 minute drive, 20 minute cycle or an hour and 20 minutes' walk.

3.7 Parking for the Church is currently on the highway in front of the church, in the forecourt of the garage across the road and in the recreation ground car park, also known locally as the 'Rec'. The Rec car park is widely used by the community on a daily basis for a multitude of errands and functions as a car park for Aveley High Street.

3.8 St. Michael's Church is accessed by pedestrians from the High Street to the north of the Churchyard through the historic arched entranceway, with secondary access via the gateway on Ship Lane.



4.0 The Application Site

- 4.1 The Church of St. Michael is situated to the south of Aveley High Street, to the west of Lennard Row and to the east of the Ship Lane. The entrance to the Church yard is marked by a stone arch located between the east end of a terrace of small houses and the World War II Memorial Garden, which is understood to have been built on land formerly occupied by small houses.
- 4.2 The Churchyard lies between the heart of the town and the surrounding countryside. The Dockland Development Corporation and other developers are currently considering proposals to enlarge the town.
- 4.3 The Churchyard occupies an area of approximately 5 hectares. It is in the shape of a large rectangle with an elongated spur at the south east corner and an extended entrance path on the north side. The Churchyard is approximately 800m from the north wall to the south boundary and averages 700m wide.
- 4.4 The land is comparatively level although the west margins of the churchyard are on slightly higher ground than on the east. With the exception of the spur of land to the southeast corner and a narrow margin alongside the south boundary, the Churchyard is well drained.
- 4.5 The short path from the formal stone entrance arch extends approximately 10m from the pavement of the High Street. It is bounded to the west by an irregular fence and to the east by a brick wall built in soft red bricks, assumed to be of local manufacture.
- 4.6 Aveley War Memorial occupies the site of former terraced house, to the east of the Church Entrance gate. The principal entrance to the church is on the north side via a traditional church porch. The porch is constructed of knapped flint, stone, brick with a timber roof frame and red clay tiles. A footpath leads down to it from Aveley's High Street as described above
- 4.7 A secondary subsidiary footpath leads away from this path at the front of the entrance porch towards the west. The path circumnavigates the church partially until it reaches an entrance into the tower wall on the west side of the church. The proposal aims to continue the circumnavigation until it reaches a new proposed path leading east to west, connecting the proposed church hall on the western side of the churchyard with the proposed car park on the eastern side of the church. Further details are laid out in the following sections.
- 4.8 In case of an emergency, emergency vehicles shall be able to pass through the main entrance of the proposed church hall on Ship Lane and thence through to the Churchyard, or into the Churchyard via the new proposed car park off Lennard Row.

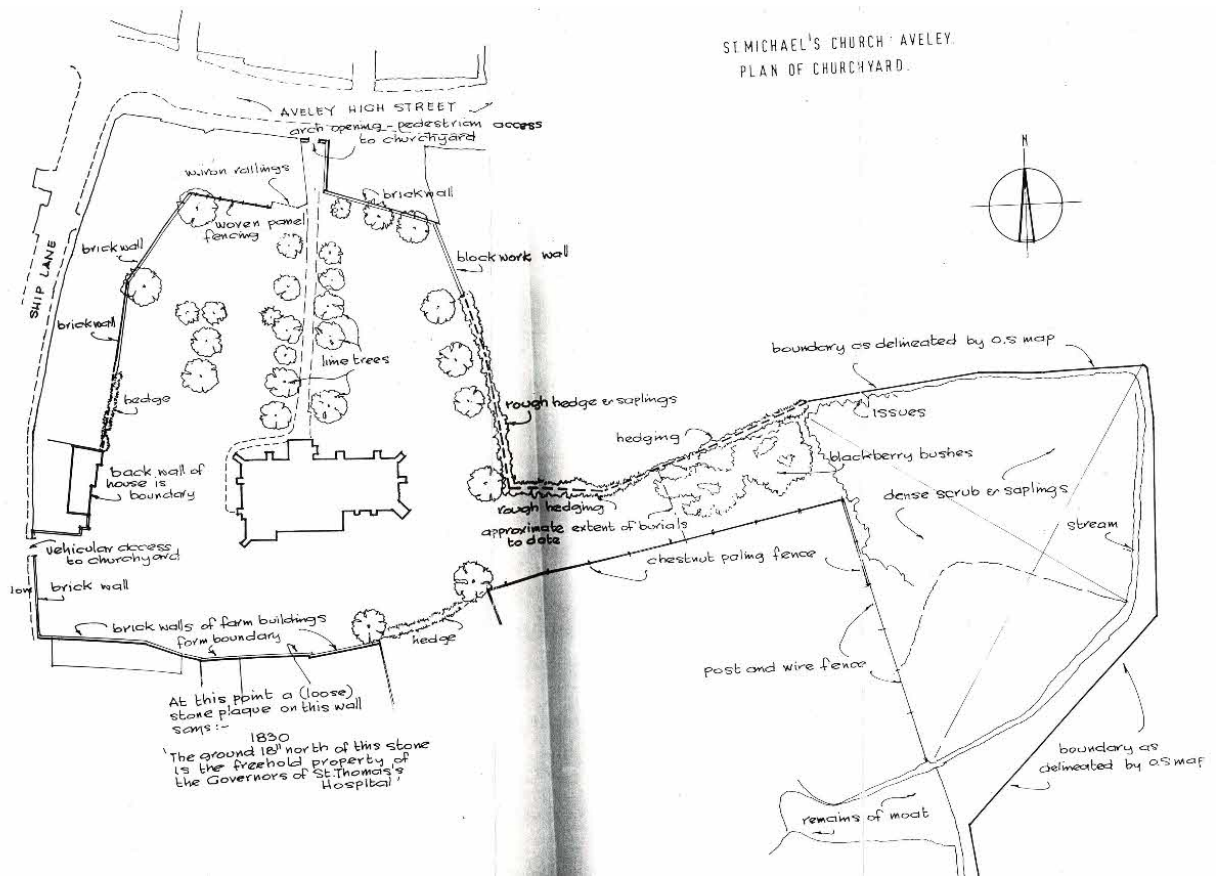


Figure 7. Keith Scarr's annotated plan of Churchyard and Scheduled Monument



Figure 8. Rear of end house on Ship Lane (top left), Garden wall end house No. 9 Ship Lane (top right), No. 1, 3, 5 & 7 Ship Lane; small terrace to the north of No. 9 Ship Lane (bottom left), South end of west boundary (bottom right)



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Figure 9. North end of east boundary



Figure 10. View to rear of Lennard's Row of houses



Figure 11. Derelict land to the east of Churchyard owned by the Church; site of proposed car park





Figure 12. Birds Eye View of St Michael's Churchyard



Figure 13. Overgrown land east of east end of Churchyard



5.0 Burials

5.1 The Church yard is no longer open for burials, other than for burials in family graves and the interment of ashes. There are burials dating from the seventeenth century through to the twentieth century. Recent burials include some maintained by the Commonwealth War Graves Commission. Many of the memorials have suffered damage in the past; those that survive are generally in a stable condition. Any crosses or stones which were thought to present a potential hazard have been detached from their bases and have been laid down. Ground clearance has revealed a number of grave marker stones which until recently had been completely overgrown. The trees, shrubs, bushes and grass are maintained by the Local Authority. The Chelmsford Diocese have stated that they do not oppose the relocation of grave markers and monuments within the churchyard.

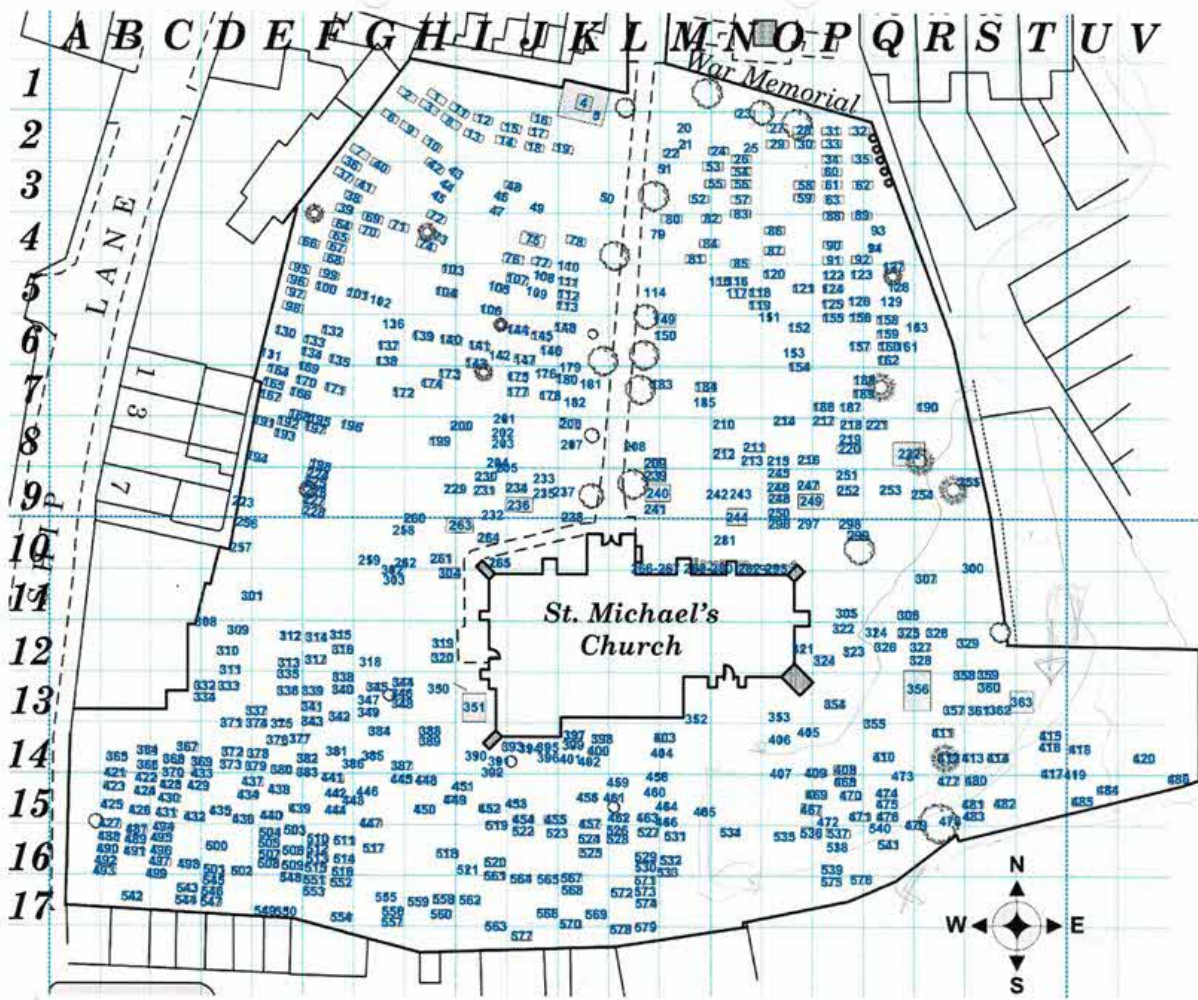


Figure 14. Source: Monument and Tombstone Inscriptions in the Parish Churchyard:

St. Michael's Church, Aveley. M.D. Bonfield. 2006



Figure 15. West boundary, south end



Figure 16. West boundary, north end

6.0 The Proposal

- 6.1 The proposal comprises the construction of a new two-storey church hall in the south-west corner of the churchyard along with a path leading to it from the church. A car park is proposed for the east side of the Churchyard within an overgrown plot of land enclosed by a small brick boundary wall. The enclosed plot of land lies within St. Michael's curtilage. Another path is proposed from this car park to the east of the Churchyard where it will join to existing footpaths within the Churchyard.
- 6.2 The paths are to match the existing hoggin path that partially circumnavigates the Church on the western side leading to a secondary entrance to the Church in the tower wall of the west elevation. The paths shall be of adequate structure to prevent pooling during rainfall, accommodate wheelchair users and allow universal access throughout the site without risk of sinking.
- 6.3 Following a review of all the site constraints and previous consultations, the western boundary in the south-east corner of the Churchyard was chosen for the location of the proposed church hall building as it follows the historic pattern of development around the Churchyard and will be the least imposing upon St. Michael's Church itself as described in the Heritage Statement.
- 6.4 Consideration was given to the same footprint rotated at ninety degrees, orienting the principal elevations north and south instead of east and west. This variation was proposed by some of the stakeholders and would have meant that the building would sit against the north face of the south boundary wall, which is also the north elevation of the west barn of Manor Farm.
- 6.5 This consideration was not pursued as it would introduce a random building line anomalous to the traditional and historic pattern of development of domestic and associated buildings alongside the boundary of the Churchyard. The alternative rotated footprint would result in a contravention of heritage conservation principles. Views of the Church are also preferable to views of the houses.

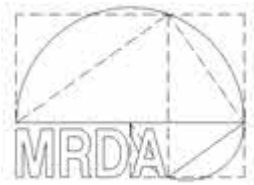


Figure 17. Church Hall Orientation Options

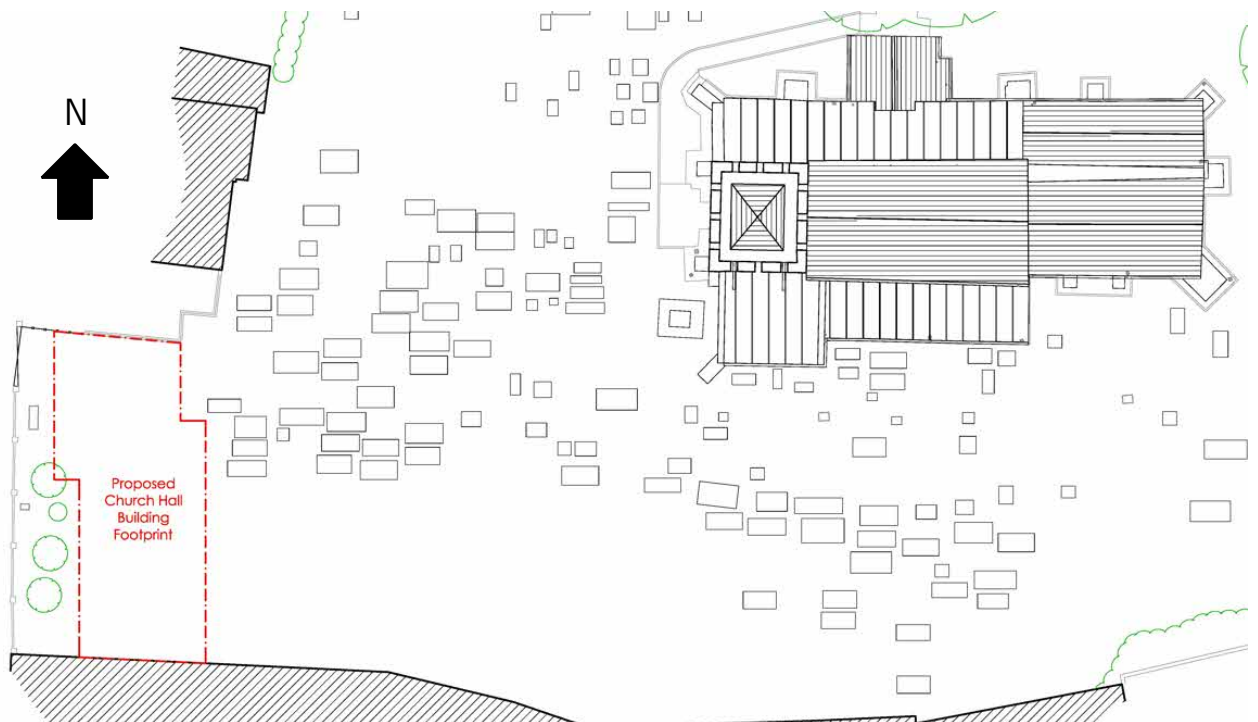


Figure 18. Location and Footprint of Proposed Church Hall



- 6.6 Construction against and in parallel to the west barn of Manor Farm would negate the potential benefits of the proposed orientation; giving west views of Ship Lane and east views of the Churchyard; good potential for solar capture and natural lighting; and importantly, potential for a totally beneficial relationship between the proposed church hall, the Churchyard and the Church.
- 6.7 Whilst we note that proposals for the redevelopment of Manor Farm are unlikely to be implemented for many years, we recognise that the north wall of the West Barn of Manor Farm adds some historic interest to the form and character of this part of Ship Lane. We anticipate that Historic England would seek to preserve and enhance such historic elements which are characteristic of the village of Aveley. Further details for the Manor Farm development can be found in the last section of this statement titled 'Development of Manor Court Farm and Aveley Hall'.
- 6.8 For visitors wishing to appreciate the historic relationship between the Church and the Scheduled Monument, the proposed orientation would be the most beneficial as they would enter the Churchyard facing eastwards towards the site of the Moated Manor House, the site of a building which is believed to have been lost approximately five hundred years ago.
- 6.9 As observed in the Heritage Statement, the Churchyard 'leaks' in the south-west corner. The proposed church hall would complete the framework within which the Grade I Listed Church of St. Michael survives.
- 6.10 The revised scheme also seeks to minimise the negative impact of the detached house and associated sheds to the north of the proposed site of the new church hall. The house's form and set-back location from Ship Lane is not sympathetic to the surrounding historic environment and has caused substantial harm to its setting.
- 6.11 Access to the proposed building - which can operate independently of the Church, is ideal. Users of the building can access from the east through the churchyard or from the west directly from Ship Lane. The west elevation will have a secondary entrance to allow deliveries directly into the kitchen.
- 6.12 Most of the local congregation walk to the church, however, a new car park to the east of the churchyard within St. Michael's curtilage is being proposed to accommodate a further twelve vehicles during daytime hours only. An electric barrier gate requiring an entrance code will regulate usage of this car park and will close at eight o'clock in the evening. This car park is to be accessed via Lennard Row, behind West & Co. funeral directors at No.28 on the High Street as shown by the red arrows in Figure 20. The Church have legal vehicular access to their curtilage from Lennard Row. The boundary for this plot of land shall be improved with remedial works for the brick wall where required and a new more robust and secure fence for the back of Lennard Row terrace gardens.



- 6.13 The car park proposal was initially submitted for planning in 2011 (Reference: 11/30064/PMIN) and found to be acceptable in principle by all stakeholders subject to conditions. It was acknowledged that the proposed car park could reduce pressure upon on street parking within the High Street. Highways engineers calculated the access width to be sufficient to allow for a bell mouth allowing two vehicles to pass. Further details of this application can be found in Appendix 10.
- 6.14 To accommodate those of low mobility, the proposed car park shall have two disabled parking spaces at the southernmost end of the car park with close proximity to the proposed footpath leading to the Church and onto the proposed church hall. Lighting for the car park shall be low level only and no security lighting shall project above the existing boundary treatment; the short brick wall that runs along the southern and eastern boundary can be seen in the images of Figure 18.
- 6.15 The Council's ecologist advised that no protected species are believed to populate the land, however, a ecological survey covering protected species should be submitted for proof. An ecological survey for the area has been submitted as part of this application.
- 6.16 The current invasive vegetation within the plot of land designated for proposed car park shall be removed, while all the trees lining the boundary shall remain. A non-compacting, porous surface shall be specified to ensure the trees remain healthy. None of the trees shall be affected as part of the development and precautions shall be implemented to ensure all trees are protected during the works as well as after with post development management.
- 6.17 The surfacing of the proposed car park shall be considerate of the close proximity to the Grade I Listed Building and the neighbouring heritage sites. Instead of Tarmac or Asphalt, hoggin self-binding gravel is to be used, providing continuity to the proposed hoggin footpaths through the Churchyard.



Figure 19. Boundary wall at the southern end of proposed car park plot with Lennard Row Terrace behind.

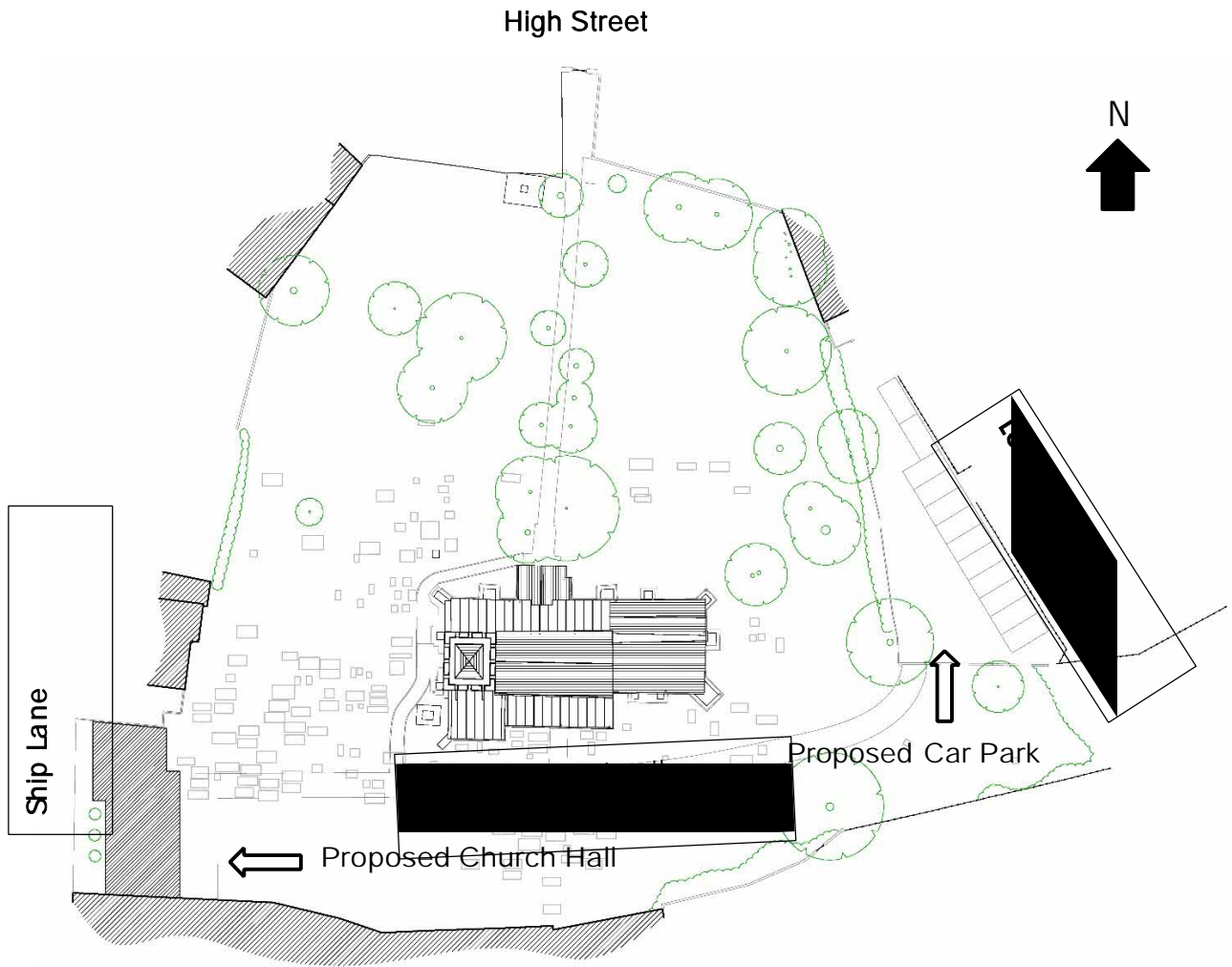


Figure 20. Site Plan showing proposed church hall, car park and footpaths

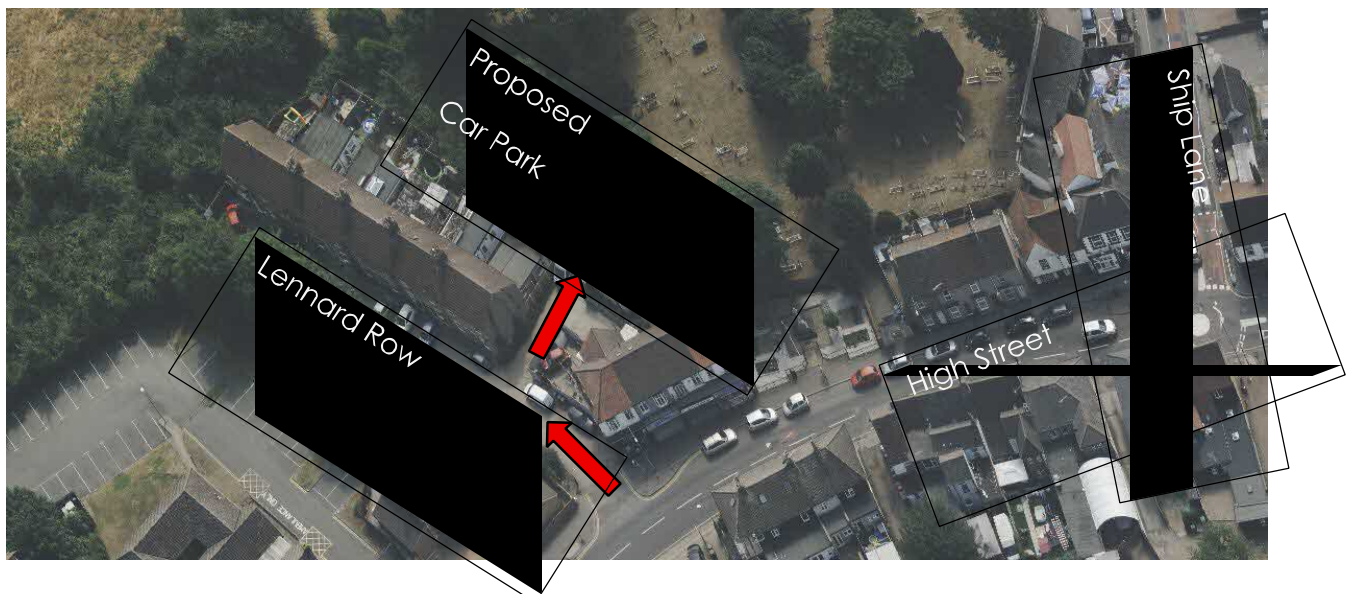


Figure 21. Proposed Car Park Access via Lennard Row



- 6.18 The Church experiences the highest amount of visitors during its Christmas and Spring Fairs, which see approximately three hundred visitors over the course of four hours. These events are attended by at least six marshals who are on-hand to guide visitors and provide help with site access. Visitors for the larger events at the Church arrive and leave sporadically, which means there is never any congestion. The new proposed church hall is not expected to attract more visitors than on these days, which do not experience any problems with parking.
- 6.19 Increased parking demand due to the development is not expected and it is anticipated that most of the users of the new facilities will originate within Aveley with very few, if any, outside visitors.
- 6.20 On the opposite side (west) of Ship Lane to site of the proposed church hall lies The Aveley Recreation Ground, known locally as the 'Rec'. The Rec contains a car park that is never full to capacity and is used by members of the congregation and visitors to the Church. The Rec car park is also widely use by the community when visiting the High Street. The Rec and other car parks currently provide adequate parking. Day to day activities with smaller groups and persons with mobility difficulties can use the new gated proposed car park within St. Michael's curtilage.
- 6.21 Furthermore, the local school can provide further overflow car parking in the unusual event of extreme attendance. There is also the possibility of the development of Manor Farm, which may come under the control of the church and will add further parking provision, however, this has not been confirmed with a formal agreement and cannot be relied upon. If the development were to go ahead, it would still take at least a few years for the plans to come to fruition.
- 6.22 To facilitate and improve the access from the Aveley Recreation Ground car park and Hub, the Church proposes that Thurrock Council open a new pedestrian access gate between numbers 16 and 18A Ship Lane as marked in Figure 22, directly opposite the proposed church hall site, along with a footpath leading to the car park supported by a pedestrian crossing on Ship Lane. This proposal would improve mobility between the Recreation Ground, Hub and the Church as well as improving access to the Recreation Ground for pedestrians from the south; the new footpath would link with existing footpath between the end of Church View and Ship Lane at the south-eastern end of the Recreation Ground, improving access for the South-East Aveley newly joined up community.
- 6.23 Another potential pedestrian access point into the Churchyard is from The Old Ship Inn public house car park on Ship Lane, which lies approximately forty metres to the north of the south-east corner of the Churchyard. An opening at this location would allow the Churchyard to be more accessible to the public and improve the Churchyard as a community amenity with greater connectivity with its surroundings.

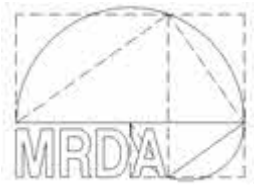


Figure 22. Public Footpath South-East end of Aveley Recreation Ground (Ship Lane)



Figure 23. The Old Ship Inn Car Park (Ship Lane)

- 6.24 To ensure emergency vehicles can access the Churchyard after construction of the proposed church hall, the main entrance on both the proposed east and west elevations shall be large enough to allow the passage of an ambulance through the main entrance hall. Fire tenders shall be able to extinguish fires in the Church and around the Churchyard with hoses passing through the proposed church hall. Alternatively, fire tenders can park in Manor Farm or in The Old Ship Inn car park and pass fire extinguishing hoses over the boundary wall.



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- 6.25 Emergency vehicles shall also be able to access the Churchyard through the proposed car park via Lennard Row. An opening in the small boundary wall at the southern end of the proposed car park shall be large enough to allow vehicles into the Churchyard. In extreme cases, helicopters can land in the Aveley Recreation Ground on the other side of Ship Lane.
- 6.26 The ground beam foundation system proposed for the new church hall lies just beneath the surface above the depth of burials, ensuring they remain undisturbed. The ground beam system is supported by concrete piles that avoid the burials. The foundation grid allows for the pile location to be moved if necessary. Advantages of such a system using pre-cast elements include reduced excavation, immediate follow-on construction, reduced lead times, bespoke geotechnical design and cost effectiveness. The modular foundation system is quick to off-load and install on-site; it is not weather dependant. Beam to beam connections reduces the number of piles required compared to other systems.

DESCRIPTION

RBeam precast foundations are one of the most versatile and highly efficient foundation solutions on the market. A value-engineered design system tailored for the specific projects ground conditions and requirements. Suitable for use in residential and commercial buildings up to four-storey buildings. The modular foundation system is designed, manufactured, and stored at RB's state-of-the-art manufacturing facility in South Derbyshire. Catering for all ground conditions including heave potential or both flat and sloping sites.

RBeam is certified under ISO 45001:2018, ISO 9001:2015, and ISO 14001:2015.

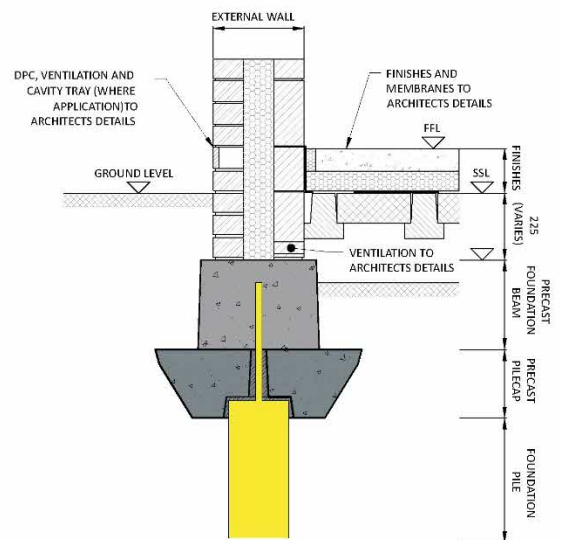


Figure 24. Example of Foundation System by Roger Bullivant

- 6.27 A number of grave markers/memorials will be moved and placed alongside the churchyard boundary wall as is common practice with churchyard developments. These proposals have been discussed and accepted by the Chelmsford Diocese. On-going discussions and on-site meetings with the Commonwealth War Graves Commission (CWGC) have indicated consent for moving three War Graves in the event of the proposed footpath having to pass through their location.
- 6.28 Numerous mature trees line the main path through the churchyard. There are three further mature trees located in the north-west quadrant and a grouping of further mature trees found on the north-eastern churchyard boundary. None of the trees shall be affected by the proposed development.



- 6.29 The proposed church hall is to be constructed from insulated concrete block walls with brick outer leaf. Curtain walling, a non-structural cladding system with an aluminium frame and glass infill panels is used to allow the penetration of light. The doors to the building are to be accommodated within the curtain walling. Individual windows are to have matching aluminium frames and can be opened to allow for passive ventilation. The roof and its overhang protect the building from the strongest mid-day summer solar gains, which are harnessed by the building's thermal mass when the sun is lower in the sky from autumn to spring. The brickwork is to be of London stock brick to match its surroundings.
- 6.30 The principal elevations are the east and west elevations, which contains openings with differing fenestration. The west elevation faces Ship Lane and contains two entrances: the main entrance, which is large enough to allow the passage of an ambulance and the smaller service door at the southern end of the elevation leading directly into the kitchen. The service door allows convenient delivery access. The curtain walling is less prominent than on the east elevation and is intended to shield users of the building from the traffic on Ship Lane and the afternoon summer sun. High level glazing allows for adequate natural lighting whilst maintaining privacy.

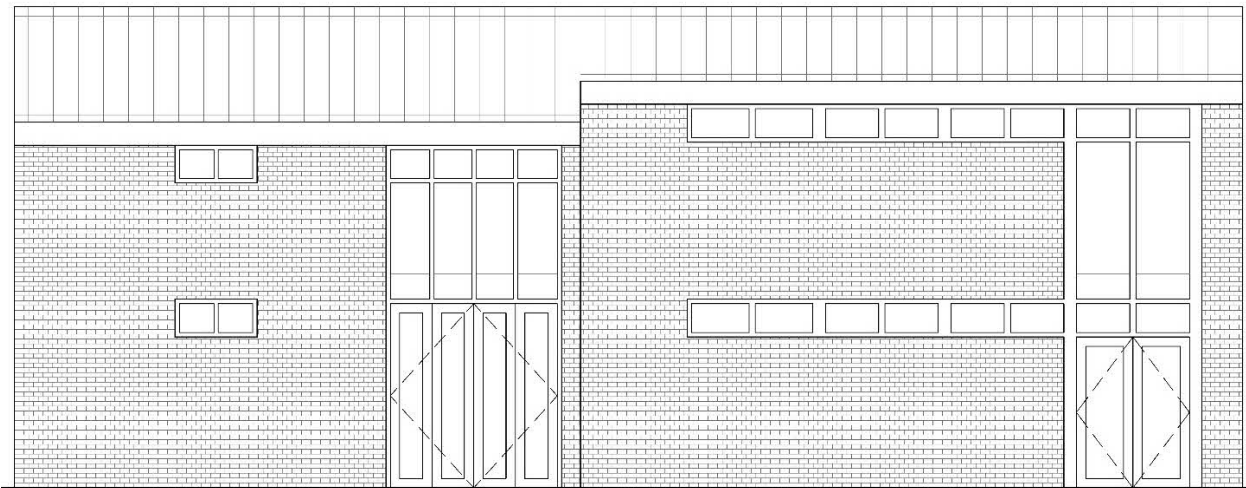


Figure 25. Proposed West Elevation

- 6.31 The east elevation contains a larger proportion of the curtain walling and faces the Churchyard. The elevation is designed to allow users to appreciate the historic views of the Church and views through to the scheduled monument. Doors on the ground floor allow the function room to open up when the weather is favourable and spill out onto a patio. A balcony on the first floor allows passage to the southern end of the first floor function room when the room is divided into two by the sliding walls. The northern part of the divided room can be accessed from the first floor hallway as illustrated on the floor plans below.

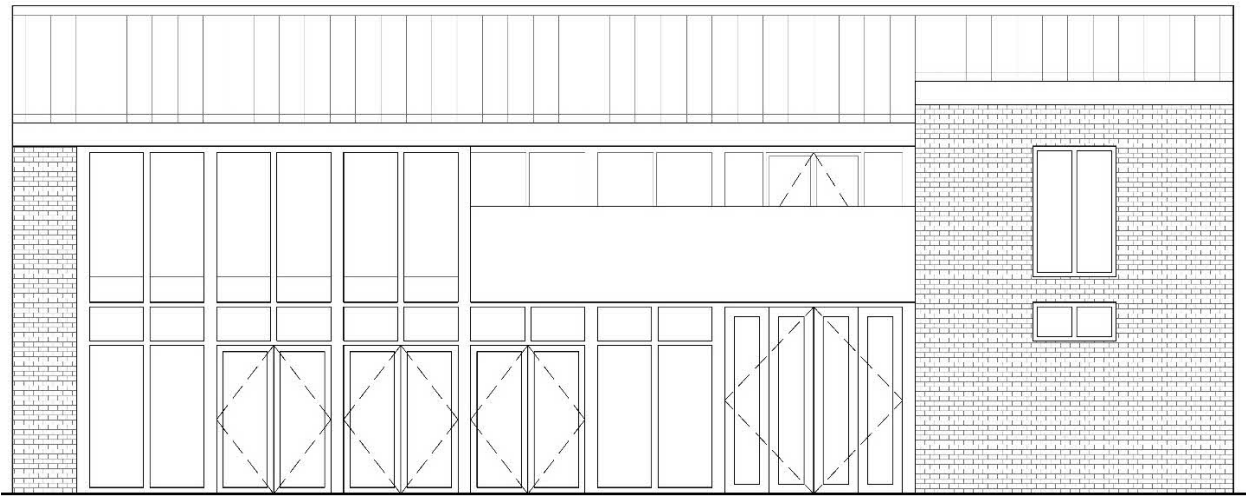


Figure 26. Proposed East Elevation

6.32 The east entrance door matches the west entrance door and has a clear, uninterrupted passage between them for vehicles to pass through. These door are composed of four leaves, of which the two inner leaves open for regular use. For emergency use, the two outer leaves onto which the inner leaves are hinged to, open ninety degrees inwards as shown on the Proposed Ground Floor Plan.

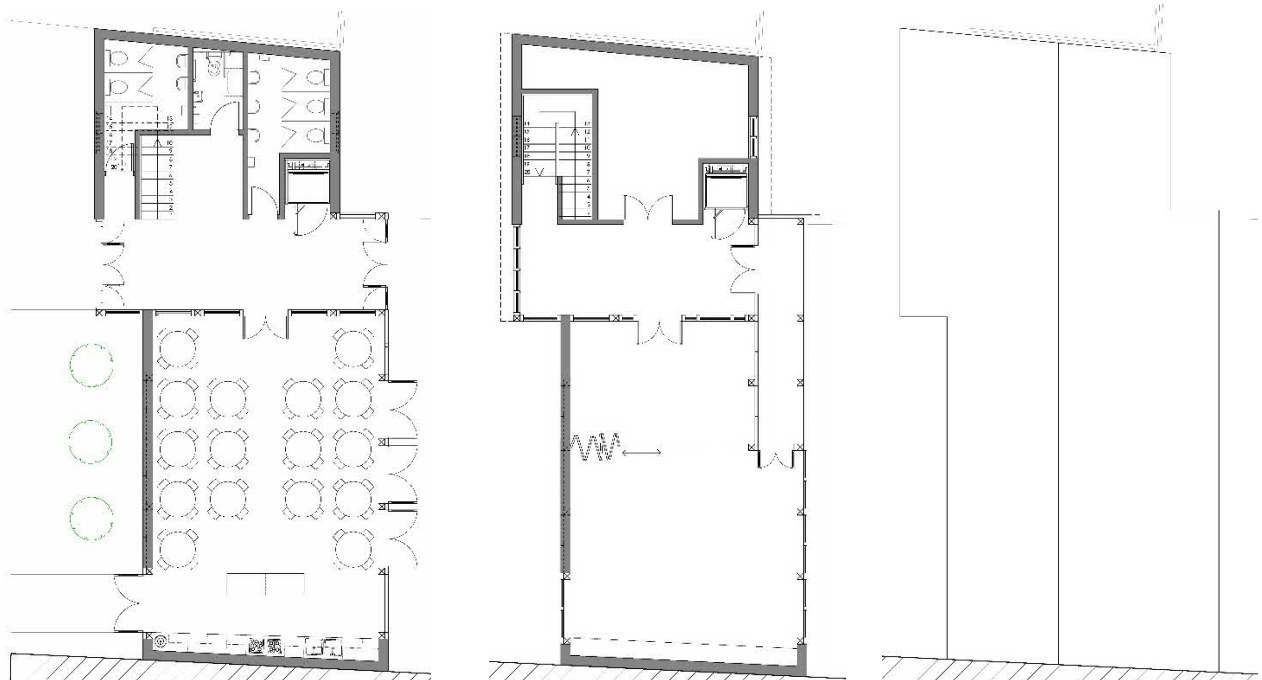


Figure 27. Ground Floor Plan, First Floor Plan and Roof Plan (left to right)



- 6.33 A lift will be installed to allow for universal access across the building. Lavatories for both men and women as well as a universal access lavatory are located on the ground floor for easy access directly off the main entrance hall. A quiet study room with greater privacy is to be found at the north end of the first floor with a storage area in the north-west corner. Further storage can be found under the staircase.
- 6.34 The roof covering material is to be of zinc with a standing seam to allow for expansion and contraction. This widely available and used material is pre-weathered, repels corrosion and combines durability with flexibility, making it easy to install. The material has been employed on other contemporary buildings in the area such as the Council's offices in Grays, which is appreciated by the local community. The roof has a single ridge to ensure the massing is considerate of its setting. The Church roofs have single ridges, as does the house to the north of the proposed church hall and the barns to the south. The Church intends to explore the viability of using photovoltaic panels on the roof for which there is an immediate precedent of the house immediately to the north.

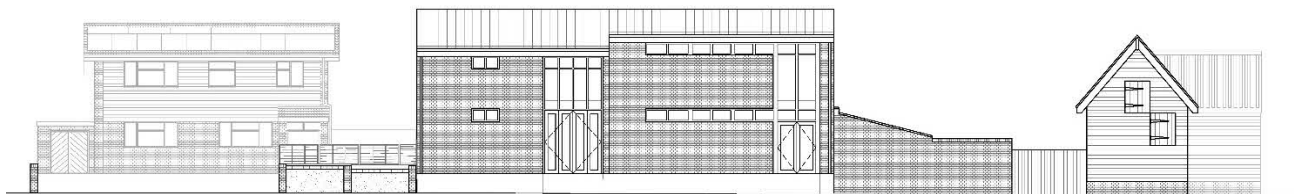


Figure 28. Ship Lane West Elevation

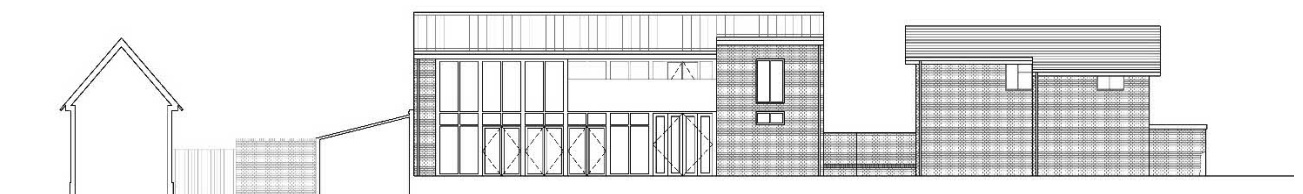


Figure 29. Churchyard East Elevation

- 6.35 Underfloor heating and an air source heat pump will work alongside the passive ventilation and thermal mass ensuring the building has adequate environmental performance. In addition to these technologies, there is potential to mount photovoltaic panels on the roof to harness the sun's energy. Triple glazed panels along with the insulated walls will ensure the building fabric has very little thermal conductivity and a low thermal transmittance rate.



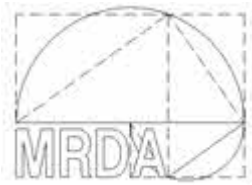
7.0 Previous Community Facility Proposals

- 7.1 Starting with the refurbishment of the original Reemer Hall, the church of St. Michael has explored numerous options for providing a space for community use.
- 7.2 The Reemer Hall refurbishment proposal was found to be unviable as the extent to which the building had to be refurbished was found to be less cost effective than building a new structure.



Figure 30. Reemer Hall Refurbishment Proposal 2003

- 7.3 The Reemer Hall was found to be unsafe and in a severe state of disrepair, which led to its demolition.
- 7.4 Following the unsuccessful attempt at refurbishing the original Reemer Hall, options were explored for extending the church to the south following a DAC visit on the 15th January 2010. MRDA met with the Archdeacon of Southend, two DAC Sub-Committee members, the Reverend Val Rockall and two churchwardens.



7.5 A further meeting was organised on the 12th of March 2010 with John Neale from English Heritage in attendance. The Archdeacon David W. Lowman made the following comments:

“The initial idea had been to glaze in the south aisle, but this would have produced a long and narrow space which would have been unsatisfactory. The suggestion that the whole of the nave could be used as the community hall as well as worship area is one that does not convince. The building itself is of exceptional historical merit, covering several centuries. It would be an enormous shame if work were to be done inside the church that would detract from that significance.

The architects have suggested that an extension the length of the south aisle with access to the church from a blocked up doorway would be the best option to consider. This could also mean that there would be level access for the disabled into the church itself as well as the hall.

The existing toilet and kitchen block which was added a few years ago could be incorporated into this new scheme and there would need to be a new staircase to the upper room which could be brought into use again. The Sub-committee has no fundamental objections to this scheme.

The architects outlined their ideas using some sketch drawings. The building may be of a timber frame construction with glass infill. It was noted that there are about 14 tombs on the site, 2 of which are war graves. The level of the church inside is much lower than the ground level outside. It should, however, be possible to raft over the graves. It is recognised that over the centuries the whole building has developed with a variety of materials and so could be enlarged once more. It would be important to ensure that the new will be worthy of this remarkable building, so will be a structure in its own right and a logical extension to the original.

The Sub-committee will await the views of English Heritage and will also report to the local Planning Authority. Subject to favourable responses from these organisations, the PCC would then be able to ask an architect to produce further sketches.

David W Lowman

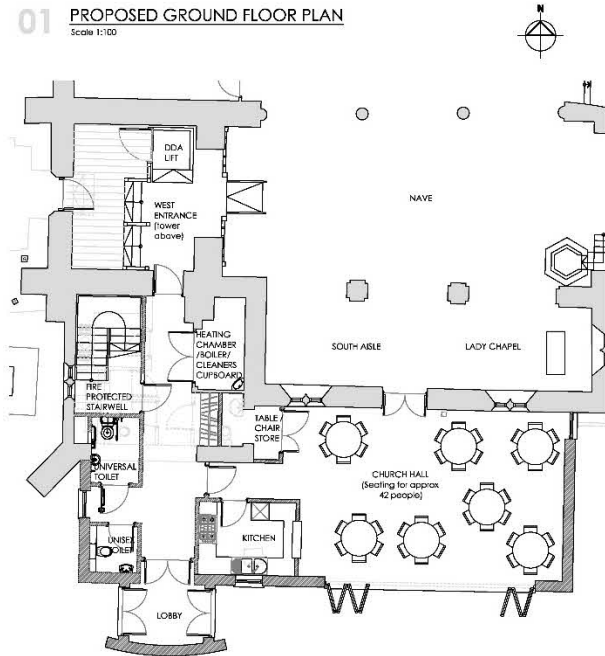
16th March 2010



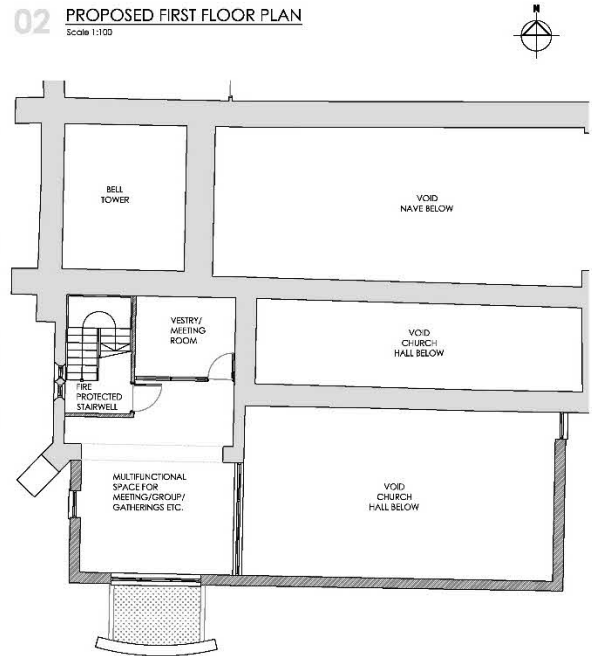
THE CHURCH OF ST. MICHAEL, AVELEY 1150-20 PROPOSAL FOR A NEW CHURCH HALL

DESIGN AND ACCESS STATEMENT

01 PROPOSED GROUND FLOOR PLAN
Scale 1:100



02 PROPOSED FIRST FLOOR PLAN
Scale 1:100

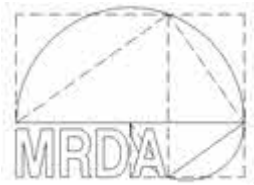


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1. All drawings remain the copyright of MRDA and cannot be reproduced without approval.	P1	Preliminary Issue	12.12.12	PRELIMINARY	PCC
2. This drawing shall not be scaled.	P2	Proposals shown at First floor level	18.12.12	Project	St. Michaels Church, Aveley, Essex
3. Documents/Drawings transferred by electronic mail may not be altered without the consent of MRDA.				Drawing title	Proposed sketch scheme - Floor Plans
4. Dimensions and/or accuracy only apply to the written notation.				Job No.	1150.05
5. The drawings/information issued by MRDA are to be cross referenced where necessary with other relevant Consultant information.				Dwg No.	SK.001
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				Dr:	CB Ch. MD
				Date:	12.12
				Revision:	P2

Architects and Conservation Consultants
3 King Street Coakley, Collier Walk,
Hammersmith, London, W6 0GJ
T: 020 8748 3755 F: 020 8748 3756
www.mrda.co.uk office@mrda.co.uk



Figure 31. Southern Extension Proposals 2012



7.6 The southern extension proposals resumed in 2017 with a more detailed site plan developed as shown below.

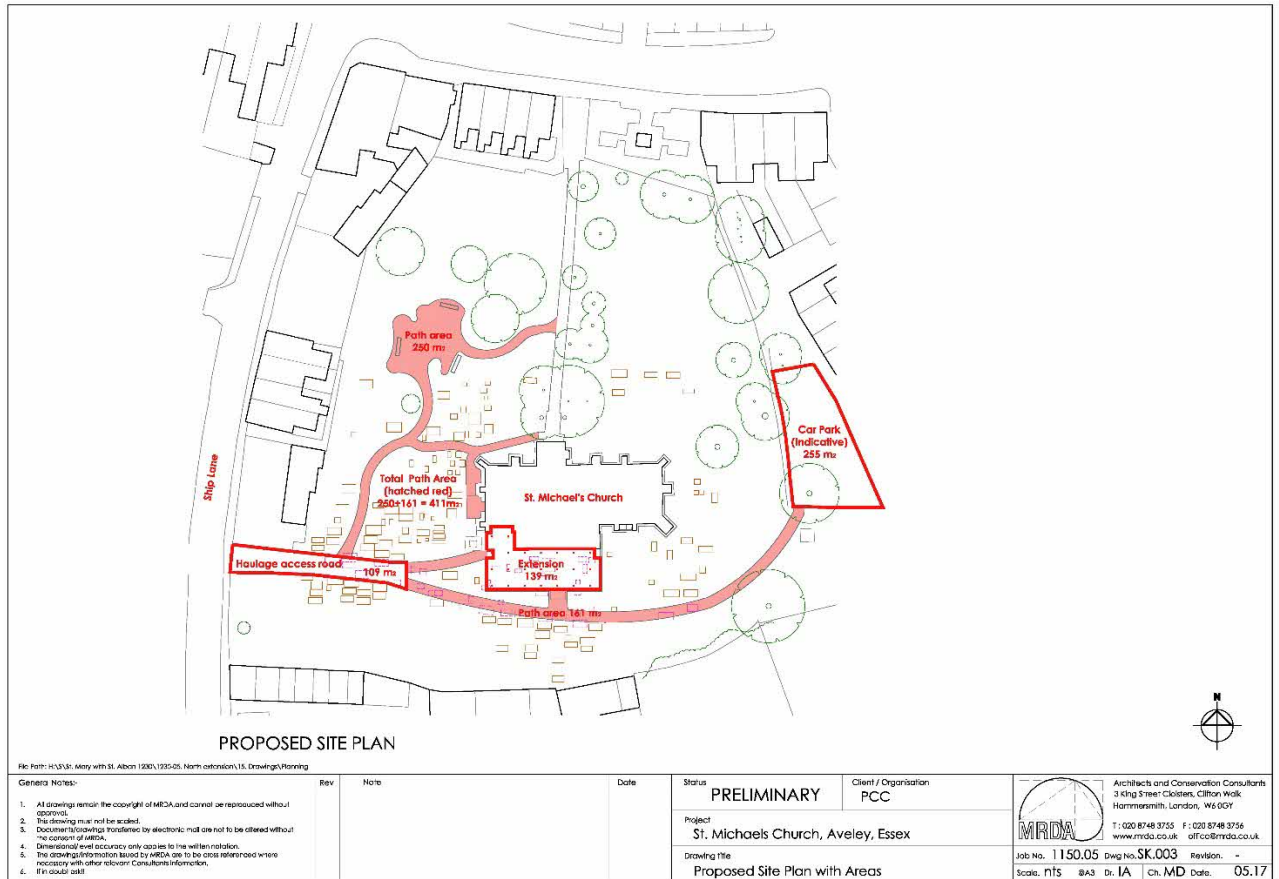


Figure 32. 2017 Proposed Site Plan

7.7 Extending the southern Aisle was found to be too intrusive of the Grade I listed medieval church with the extent of harm to the historic significance of the building too large. Linking a new structure across the entire South Aisle also meant that the environmental performance would be hard to manage; challenging to compartmentalise the new extension to ensure heating the space independently of the original church that has very poor thermal performance.

7.8 Following proposals for extending the church to the south, options that were explored for a church hall elsewhere in the churchyard in 2013 were revived. Three potential sites were proposed, two sites south of the church and one site south-east of the church as shown on the following markup of the existing site plan. The current scheme is a development of the free standing church hall proposal.

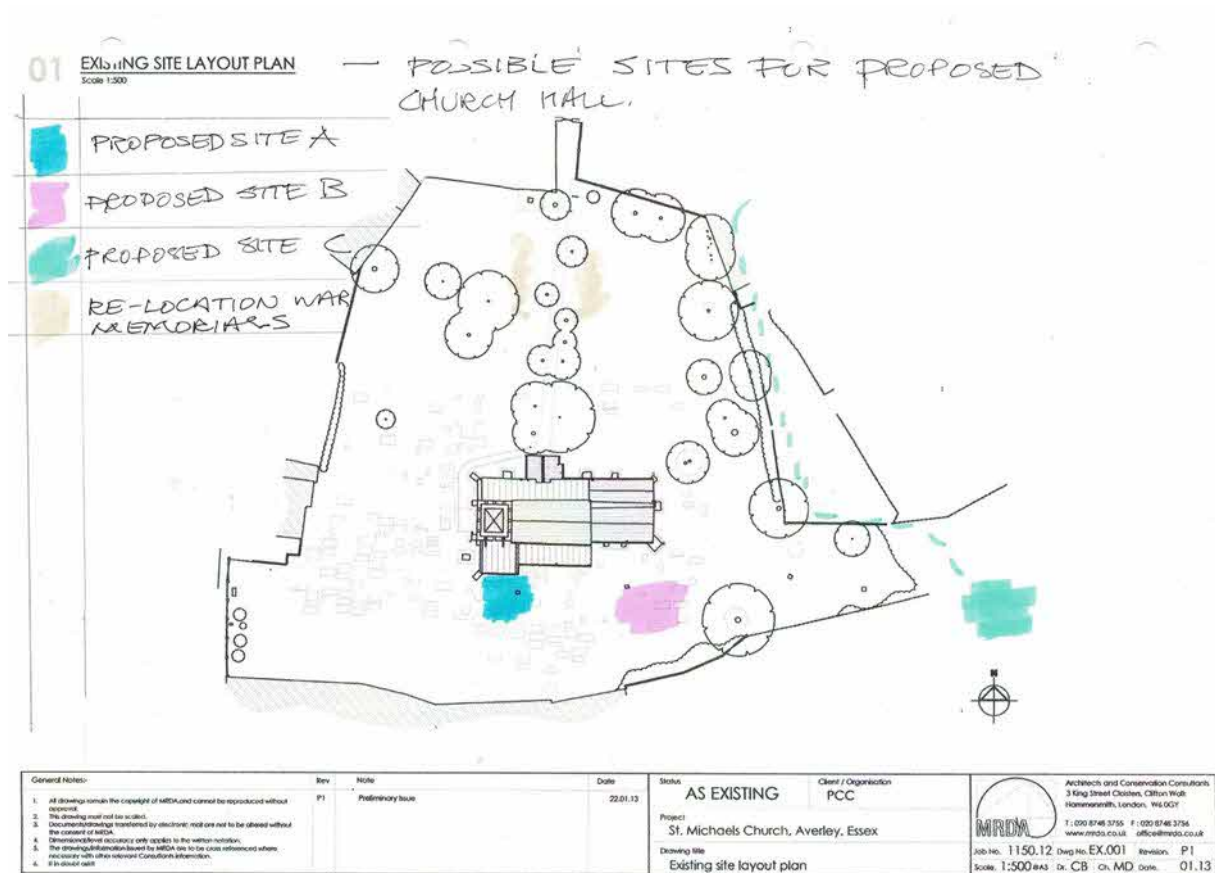


Figure 33. Three proposed site options for free standing church hall

8.0 Development of Manor Court Farm and Aveley Hall

8.1 The land directly to the south of St Michael's churchyard is subject to a wider development plan. The current owners of the land have expressed a strong interest in donating the land to the church in order to provide the community with a public hall and commercial kitchen, small offices for start up businesses, a hospice, a medical centre, substantial communal gardens and a new car park for the town centre. The farm development is subject to the approval of the wider housing expansion.

8.2 The redevelopment aims to restore Aveley Hall and its gardens as well as introducing new buildings that are to replace Manor Farm's derelict structures. Proposals have been explored to convert Aveley Hall into a hospice and the gardens into communal gardens, which can be used by the rest of the users of the larger site. The farm is to comprise a car park, offices for local start-ups, a public hall with commercial kitchen and a medical centre. These facilities will be in demand if the new large housing development to the south is completed.



Figure 34. Manor Farm Development Proposal



9.0 Appendix: Massing Studies

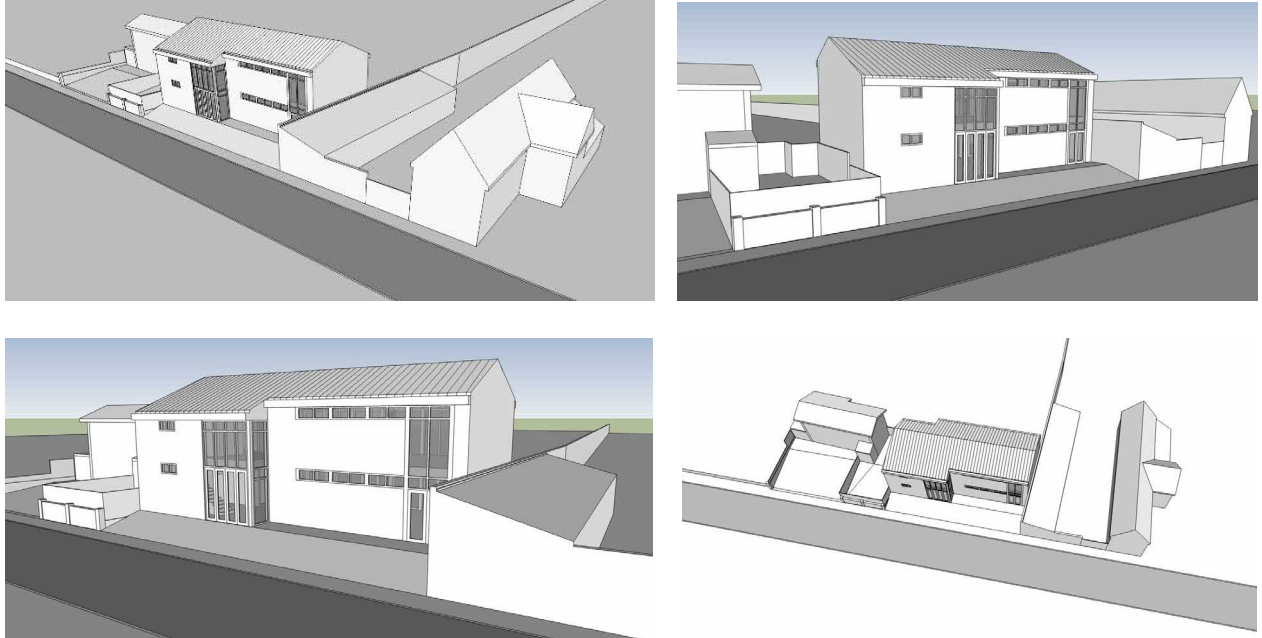


Figure 35. 3D Massing Studies (West Elevation)

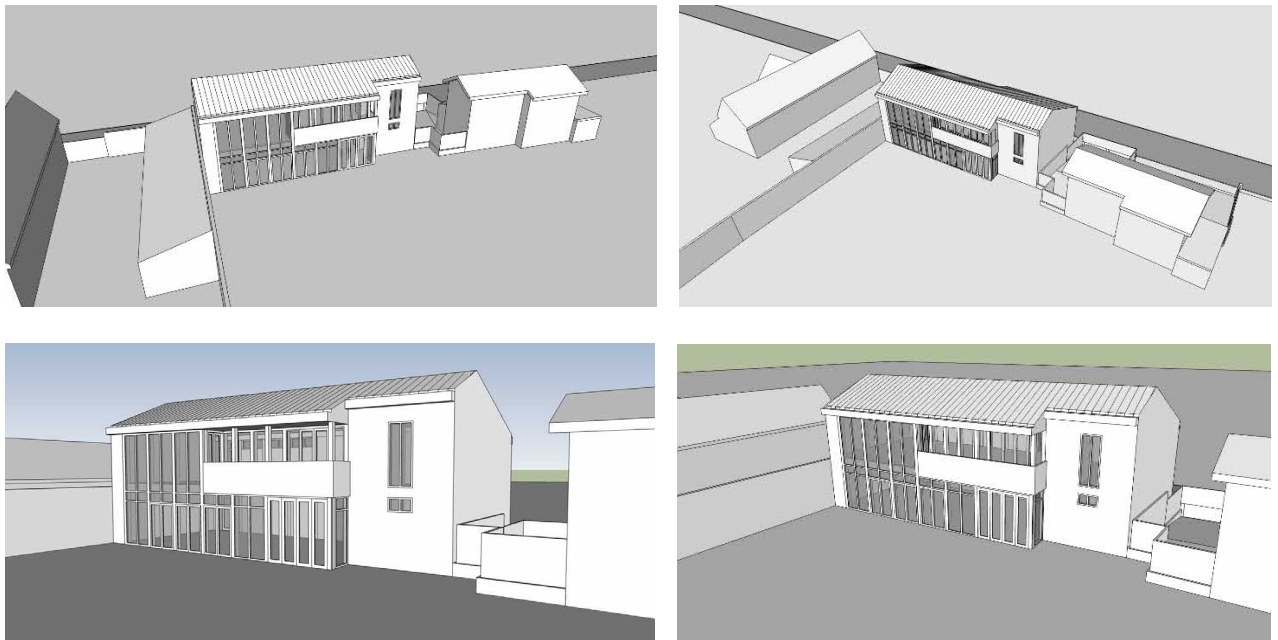
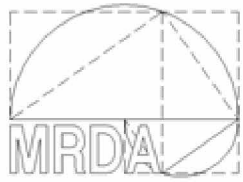
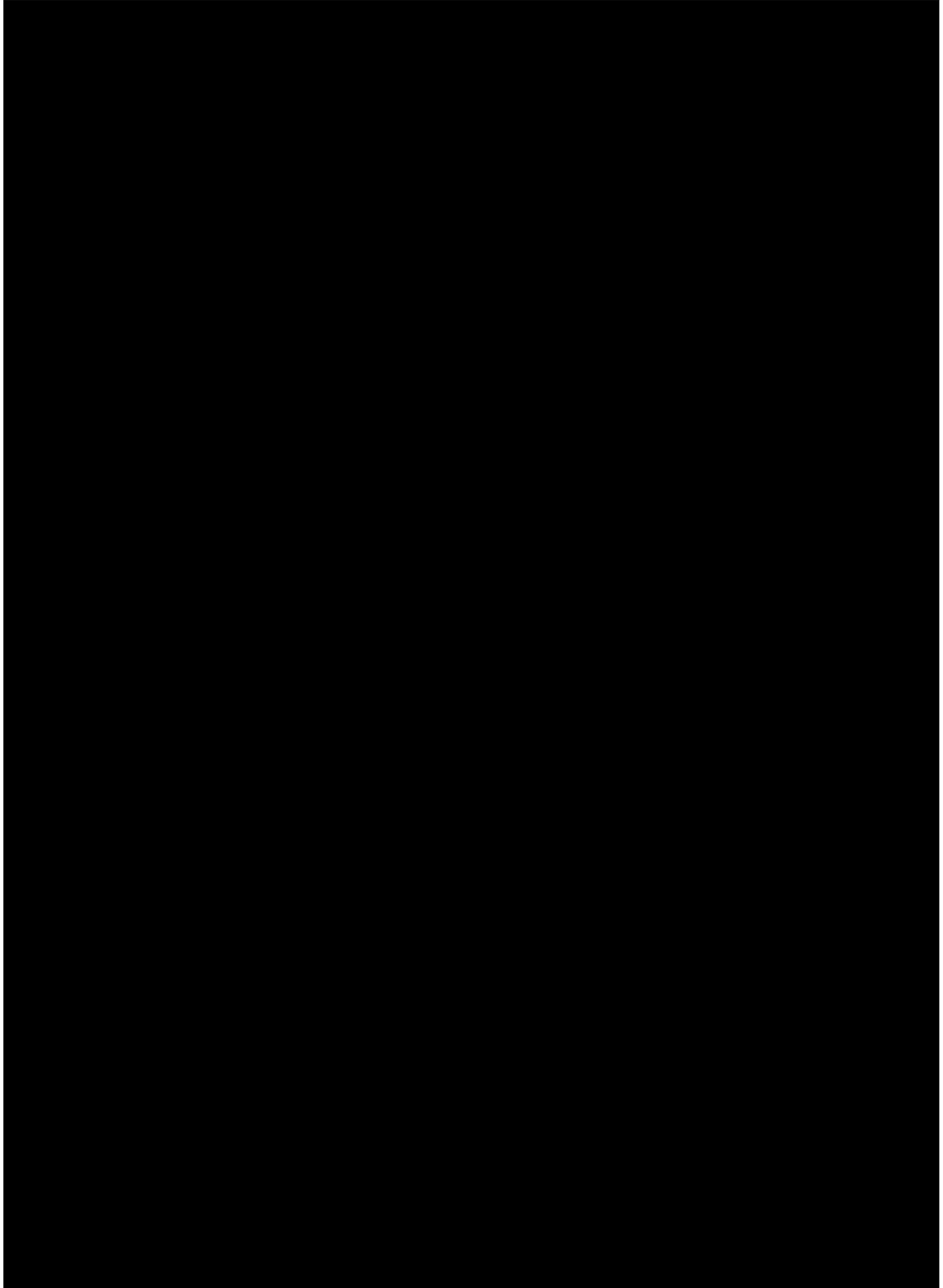
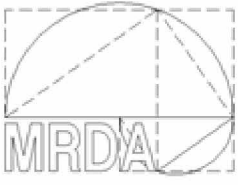


Figure 36. 3D Massing Studies (East Elevation)



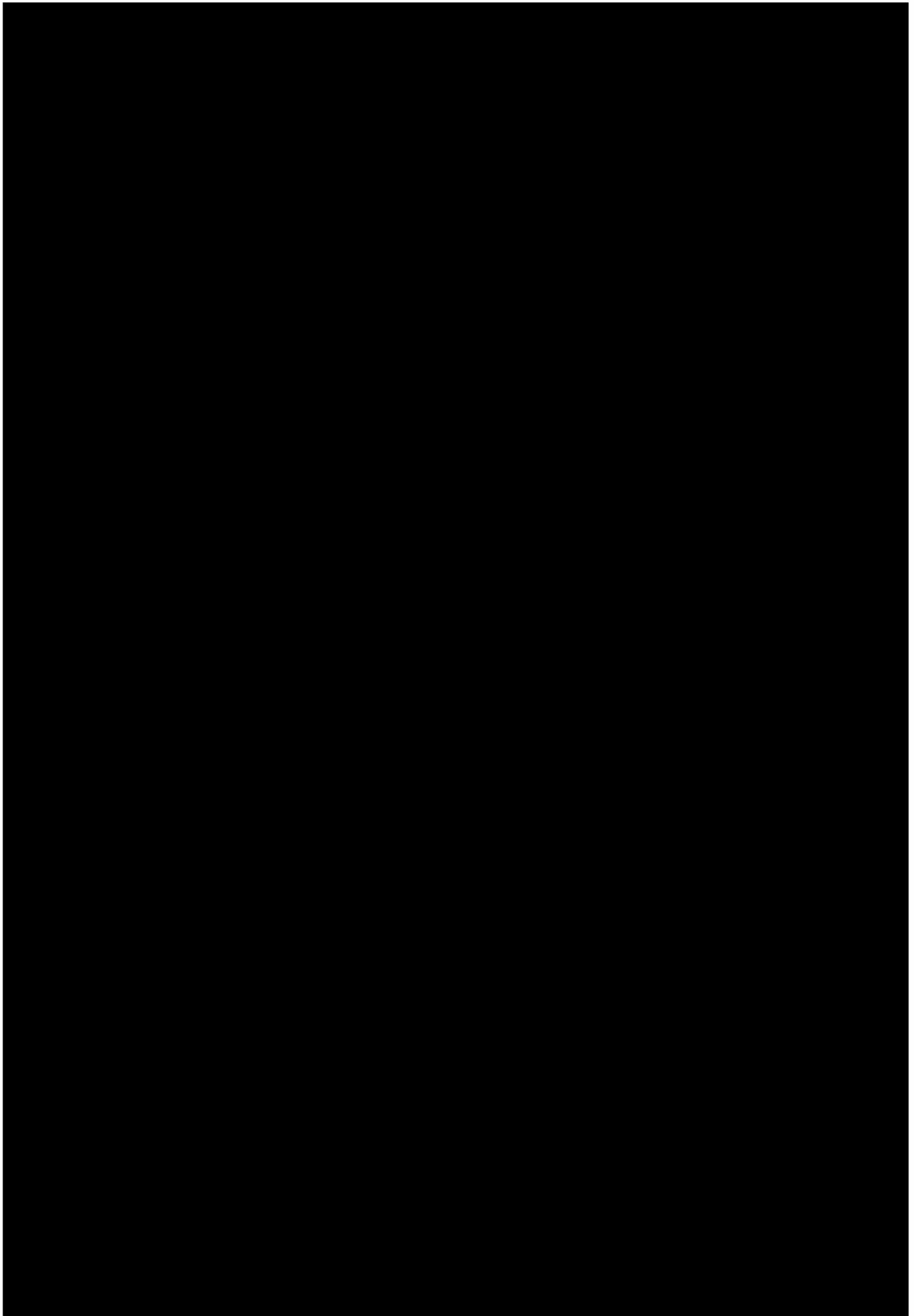
10.0 Appendix: Car Park Planning Letter 2011

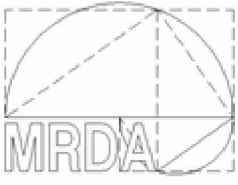




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