

DESIGN GUIDELINES FOR  
THE VILLAGE'S COMMERCIAL DISTRICTS



Village of East Hampton  
Design Review Board

# DESIGN GUIDELINES FOR THE VILLAGE'S COMMERCIAL DISTRICTS

These guidelines were written with close attention to the goals of the Comprehensive Plan and in particular the recommendation for Design Review on pages 143-144:

Objective: Continue to encourage design of commercial structures that are compatible with the Village's character and scale

Action C18 - Adopt simple and concise guidelines for commercial development that address the major aesthetic and functional concerns in each commercial district that impact Village character, without encouraging uniformity.

The Village should develop and adopt a set of simple and concise written design standards for the commercial districts. These should address the major aesthetic and functional concerns related to commercial development which have the potential to impact village character. The standards and guidelines should address the relevant issues in terms easily understood by the general public, including the following:

- Size, scale and character
- Color and materials
- Relationship to the surrounding residential community
- Pedestrian connections
- Parking
- Streetscape, landscaping and open space

Village of East Hampton  
Design Review Board

Adopted February 18, 2004

## TABLE OF CONTENTS

1	General Principles
2	Village Center Commercial District
11	Pantigo Commercial District
18	North Main Street Commercial District
26	Upper Newtown Lane Commercial District
33	Railroad Avenue Commercial District
40	Gingerbread Commercial District
47	Cove Hollow Commercial District
	<b>Additional guidelines for all properties</b>
53	Signs
54	Awnings
55	Mechanical Systems, Utilities, Trash Receptacles and Service Areas

These guidelines supplement Chapter 121 Design and Site Plan Review of the Code of the Village of East Hampton. Chapter 121 authorizes the Design Review Board to adopt guidelines for applying the policy, purposes and objectives of that Chapter.

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## GENERAL PRINCIPLES

1. **The unique features of each district which contribute to the desirable character and scale of the Village should be retained.**  
Today each commercial district has a different character and these differences should be retained. New development should contribute to the unique character of the Village rather than serve to make one commercial area the same as another or the same as found in other communities. For this reason these guidelines treat each commercial district separately.
2. **New development should maintain and enhance the desirable character of each district.**  
These guidelines describe the development of each commercial district and identify its desirable character, important qualities and best features. The guidelines identify what qualities and features should be retained in order to maintain that character. The guidelines indicate how new development can fit into and enhance the desirable character of each district.
3. **Design of commercial buildings should be restrained.**  
Commercial buildings in East Hampton have a restrained design and contribute to the overall character of a district. This is an important principle for new development and renovations. All elements of design should contribute to the harmony of the district as a whole and not draw attention to one building.
4. **Design of commercial buildings should respond to the best architectural traditions of each district.**  
Creating a new design that embodies the best principles of form, scale, proportions, materials, details, massing and rhythm in a district is encouraged. New work that imitates the historic styles of the existing buildings is discouraged.
5. **Allocation of open space.**  
Open space is critical to the rural character of the Village. In each district open space can be used to best advantage in different ways. The "Allocation of open space" guidelines establish the priorities for distributing open space in each district.

# VILLAGE CENTER COMMERCIAL DISTRICT

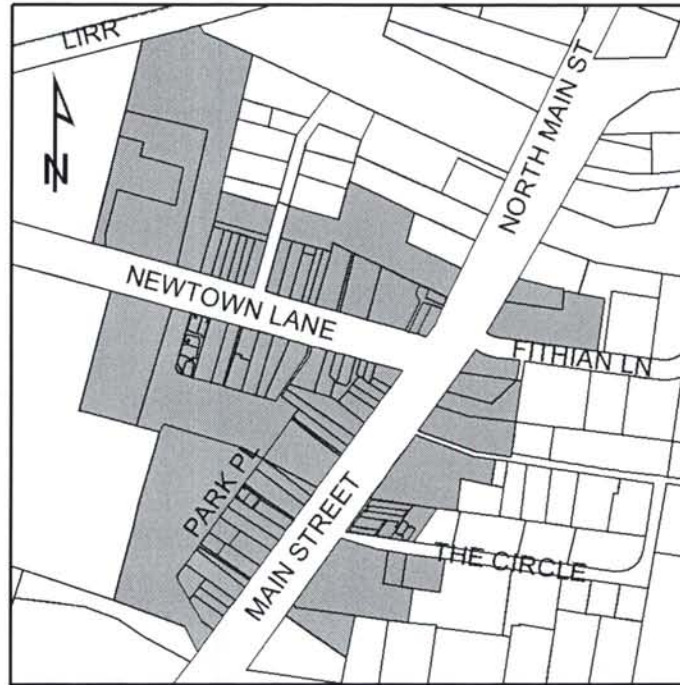


Fig. 1. 1927 Post Office, 46 Newtown Lane



Fig. 2. Odd Fellows Hall, 26 Newtown Lane



Fig. 3. Northrup Building and A.O. Jones Building, 47 and 51 Newtown Lane



Fig. 4. George Hand Building and Village Hall, 78 and 86 Main Street



Fig. 5. Parsons House and 1908 Post Office, 45 and 41 Main Street



Fig. 6. Osborne Trust Co. and Howard Building, 35 and 31 Main Street

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## VILLAGE CENTER COMMERCIAL DISTRICT

### Development

A small commercial district developed on the west side of Main Street, just south from Newtown Lane, at the same time that East Hampton's summer colony experienced a great building boom following arrival of the railroad in 1895. By the first decade of the twentieth century, this block had become a distinct village center with a row of thirteen closely-spaced, mostly two-story commercial buildings including a new post office and a new bank.

The commercial district expanded around the corner on the south side of Newtown Lane where by 1915 there was a row of eleven commercial buildings.

Following adoption of zoning in 1925, the north side of Newtown Lane and the east side of Main Street became more built up, particularly with construction of the Edwards Theatre on Main Street in 1926 and construction of the Post Office and Shops complex at 46-50 Newtown Lane in 1927.

### Existing Character

The Village Center is a collection of small-scale commercial buildings with a compact rhythm, a lively diversity and rich architectural details which create a successful pedestrian-oriented shopping district.

The large group of 44 commercial buildings which date from before 1930 are the cornerstone of the district and establish the essential scale, rhythm and architectural character. These older buildings are found in greater or lesser concentrations throughout the district. The character created by these buildings is dependent on the similarities of some features and the differences of others. Important shared features include: small scale; traditional storefronts; often upper stories with residential windows; a decorative cornice or parapet; and a Colonial Revival or Neoclassical style. Differences which create visual interest and a dynamic rhythm include contrasts in material between masonry and wood facades; contrasts in color often between unpainted brick and white-painted wood; a rhythm of differing heights between 2-story and 1 ½ story front facades; and a variety of different roofs and parapets.

Because of the way this commercial district began on the west side of Main Street, expanded around the corner onto Newtown Lane and then crossed to the opposite sides of these streets, each of these blocks has a slightly different character.

The **west side of Main Street** has the greatest concentration of early buildings and the most consistent character. Among the outstanding commercial buildings are: the brick 1917 Post Office at 1 Main Street with pedimented, arched doorways; the 1918 brick East Hampton National Bank at 27 Main Street with pilasters and marble columns; the 1907 frame G.A. Howard Building at 31 Main Street (Fig. 6) with an original Colonial Revival storefront; the 1916 brick Osborne Trust Co. at 35 Main

Street (Fig. 6); the 1908 brick Post Office at 41 Main Street (Fig. 5) designed by Joseph Greenleaf Thorp; and the 1916 brick Gilmartin Brothers Building at 63 Main Street with an original storefront and a bold, bracketed cornice.

Two older buildings give this block a unique, rural East Hampton character. The 18<sup>th</sup>-century Parsons House at 45 Main Street (Fig. 5), a general store as early as 1840, and the late-19<sup>th</sup> century Old Barn at 57A Main Street, with its grass courtyard, provide distinctive breaks in this row of commercial buildings.

Within this block are scattered five newer stores built from the 1960s to the 1980s, which for the most part are harmonious in scale and other characteristics with the older buildings.

The **south side of Newtown Lane** has much the same character as the east side of Main Street. The 1917 Post Office at 1 Main Street anchors this row of shops and connects them with Main Street. Other important buildings include the 1917 brick firehouse for Maidstone Fire District # 1 at 17 Newtown Lane designed by John Custis Lawrence; the 1929 Scholtz Brothers Bakery at 39 Newtown Lane; the 1916 O.N. Northrup Building at 47 Newtown Lane (Fig. 3) with rockface concrete block and a signboard frieze of pattern block, the 1916 A.O. Jones Building at 51 Newtown Lane (Fig. 3) with a bracketed parapet and the 1904 East Hampton Hotel at 53 Newtown Lane with rock face concrete block.

The present Waldbaum's store and parking lot breaks the historic pattern of development on this side of Newtown Lane.

The **north side of Newtown Lane** has two important buildings that are focal points for this block: Joseph Greenleaf Thorp's 1897 Odd Fellows Hall (Fig. 2) and Robert Tappan's Colonial Revival style 1927 Post Office at 46 Newtown Lane (Fig. 1). Robert Tappan also designed the adjoining shops at 48 and 50 Newtown Lane, which used the elements of a traditional storefront (recessed center entrance, display windows with an apron, a signboard fascia and a cornice) in a new way demonstrating how a more modern, one-story building can fit into and enhance the overall character of this district (see *East Hampton's Heritage*, p. 217). This is an important model for this block where the greater number of one-story buildings create a different rhythm.

The 1980s 66 Newtown Lane introduces a large block into this row of small-scale buildings.

Like the north side of Newtown lane, the **east side of Main Street** has a lesser concentration of early buildings and a more diverse character. At the south end of this block the 1904 George Hand Building at 78 Main Street (Fig. 4), designed by John Custis Lawrence, makes a perfect transition from the commercial area to the residential area. George Hand lived in the Beecher House (Fig. 4), now Village Hall, and the relationship between these buildings recalls the early period when shops were interspersed with houses up and down Main Street. Attached to the building is the c.1890 George Hand carriage house. The Bank of New York with its lawn setting contributes to the more open and residential character of the south end of this block.

The north end of this block is anchored by the 1921 Neoclassical VFW building which stands alone at the corner with Fithian Lane. The 1927 Cleaves Hardware at 14 Main Street provides some continuity with the commercial buildings across the street. Like the north side of Newtown Lane, there is a long row of one-story buildings.

The collection of buildings fronting on **Park Place** is diverse. Some recent renovations have attempted storefronts that relate to the character of the Main Street buildings.

### **Goals for Future Development**

**The large group of pre-1930 traditional buildings establishes the desirable character of the Village Center District.** They give the streetscape its essential pedestrian scale, rhythm and architectural quality.

- To promote an overall cohesive district by guiding new work to fit into and enhance the scale, rhythm and architectural character established by this group of traditional buildings.
- To retain the architectural integrity of these traditional buildings that give the district its desirable character. In particular to retain the integrity of the buildings which are listed on the National Register of Historic Places: the entire block on the west side of Main Street; 78 Main Street; and Odd Fellows Hall.
- To protect the setting of Odd Fellows Hall, a local landmark, and to protect the setting of adjacent properties in the Hook Historic District and the Main Street Historic District.
- Although these guidelines recognize the group of traditional buildings as the cornerstone of the district it is not the intent for new work to replicate these buildings. A contemporary design that relates to the important features identified in these guidelines and that will enhance the desirable character of the district is encouraged.

## **GUIDELINES FOR BUILDINGS**

### **Alterations and additions**

- Buildings that make a positive contribution to the desirable character of the Village Center District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Village Center District as described in these guidelines.



### **New construction**

- New construction should be compatible with the desirable character of the Village Center District as described in these guidelines.

### **Orientation**

- The front facade, containing the primary entrance, should face the street.
- The front facade should be parallel to the street.
- Buildings on corner lots should also have a side facade that continues the material and details of the front wall.

### **Setback from the street**

- Although most buildings are set on the street the breaks from this similarity are important. Greater setbacks should be maintained where they contribute to the setting of the district.
- A setback on Park Place and the Schenck parking lot of 5' to 10' to allow for landscaping or planters is preferred.

### **Rhythm of buildings on the street**

- The compact rhythm of small scale buildings which vary in height and width within a prescribed range should be retained. The occasional break in spacing or setbacks can also be important to the setting of a particular building.

### **Size**

- Buildings with a maximum footprint of 5,000 s.f. and a maximum gross floor area of 8,000 s.f. enhance the desirable character of this district.

### **Height**

Two-story buildings establish the framework for this district, but one-story buildings are important in creating a lively rhythm. Two-story buildings vary in height with gable-fronts being the tallest. One-story buildings often have a front facade with the height of a 1 ½ or 2 story building.

- Two story buildings with a flat roof or flat parapet should be less than 25' in height. Two-story buildings with a pitched roof or pitched parapet (not less than 7:12) should be less than 30' in height fit.
- One-story buildings should have extra height by using high ceilings, a pitched roof or a parapet front facade.

**Width of the front facade**

- The width of a building or the width of a distinct storefront of a one-story building should not exceed 40'.

**Proportion of the front facade**

- The width of the front facade of a two-story building should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building, or the width of each distinct storefront of a one-story building, should not exceed 2 times the height.
- When a front facade must exceed these proportions, other elements of the design should be used to create components of a more vertical proportion.

**Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- A large building should have a two-story front block which fits into the context of the streetscape. A one-story mass can extend behind this front block.
- Buildings with a simple form enhance the desirable character of this district. Elements which may be inappropriate include: an upper story projecting over a lower story, even if supported by an arcade; a balcony; an exterior stairway; and a sunken courtyard.

**Roof form**

The predominant form is a flat roof with a parapet on the front facade. Other roof forms add to the diversity and rural character of this district.

- Flat roofs are appropriate provided there is a parapet on the front facade.
- Gable or hip roofs with a pitch not less than 7:12 are appropriate.

**Materials**

Acceptable siding materials:

- Painted wood shingles
- Painted wood clapboards
- Brick

- A combination of brick with cement stucco

Acceptable siding materials where wood is not allowed by the building code:

- Painted fiber-cement shingles.
- Painted fiber-cement clapboards.

Acceptable materials for pitched roofs:

- Asphalt shingles
- Wood shingles

Acceptable material for doors, windows and trim:

- Wood

### **Color**

Acceptable paint schemes:

- Natural brick and painted trim. Brick should not be painted.
- Painting siding and trim white or another very light neutral color.
- Painting siding one color and painting trim a second color.

In considering color the Board will apply the following guidelines:

- Color should contribute to the harmony of the district as a whole and not draw undue attention to one building.
- White is an appropriate color for wood siding and trim.

### **Proportion and arrangement of windows**

- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.
- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.

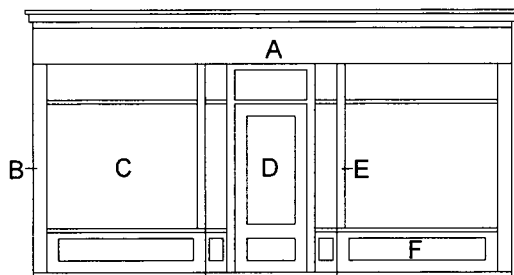
## Storefronts

Many of the traditional storefronts share the following features: an outer frame of pilasters carrying a lintel with a signboard and projecting cornice; a recessed center entrance with a transom above a glazed wood door; and flanking display windows with heavy wood sills and mullions often with transom windows above and paneled aprons below.

- These original storefronts make an important contribution to the desirable character of the district and should be retained.

The following guidelines apply for new storefronts or for renovations of storefronts that are not important features of the district.

- The relationship between the storefront and the building as a whole should be taken into account.
- As a general guide, the combination of display windows and entrance should extend across a minimum of 70% of the width of the front facade.
- The storefront should be framed at the sides by vertical elements such as piers, pilasters or mullions and at the top by a substantial horizontal element such as a signboard.
- These elements should be in proportion and the vertical elements should be of sufficient dimension to “carry” the signboard and the upper facade.
- The storefront should have depth achieved by projecting and recessed elements. Typically pilasters, signboards and cornices project; entrances are recessed; and display window glass is set in from the face of substantial mullions and sills.
- The width of a display window should not exceed 1 ½ times the height.
- Display windows should have an apron beneath providing a minimum distance of 18 inches from grade to the glass.
- Wood is an appropriate material for storefronts. Brick buildings usually have storefronts with both wood and brick elements.



ELEMENTS OF A TRADITIONAL STOREFRONT

- A. Lintel with signboard and cornice
- B. Pilaster
- C. Display window with transom
- D. Recessed center entrance
- E. Display window mullion
- F. Paneled apron

### **Building details**

- Important original building details such as storefront components, windows, doors, pilasters, cornices and parapets should be retained.
- Building facades should be designed and detailed to contribute to the streetscape as a whole and not to draw attention to one building.
- New work should reflect the tradition of quality in materials and workmanship found in the traditional buildings.
- With so many Colonial Revival and Neoclassical buildings, the proportions and relationships between the elements of classical architecture are important to this district.

## **GUIDELINES FOR THE SITE**

### **Allocation of open space**

Open space should be distributed to achieve the best balance between the following priorities:

- Open space that contributes to the setting of buildings in the district.
- Open space that enhances walkways to parking areas.
- A setback or courtyard to allow lawn, shrubs and trees to enhance the streetscape, especially on Park Place and on the Schenck parking lot.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district or limited office district.

### **Landscaping**

Trees and other landscaping are important in maintaining a sense of continuity between the Village Center District and the surrounding Village neighborhoods.

- The few trees that exist on private property should be retained.
- Installing trees and shrubs is especially important for properties fronting on Park Place or the Schenck parking lot and for walkways from parking areas to Main Street or Newtown Lane.

### **Parking and circulation**

- Any new parking should be to the rear of the buildings.
- When parking to the rear is not possible or desirable, parking should be set back from the street by at least 20'.

# PANTIGO COMMERCIAL DISTRICT

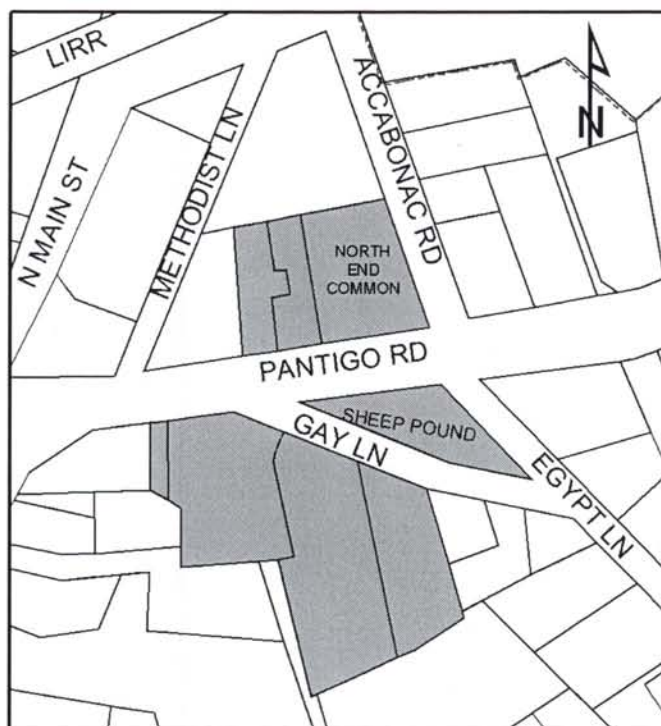


Fig. 7. Strong Agency and "Eelpot", 41, 43 and 43A Pantigo Road from North End Common



Fig. 8. Amaden-Gay Agencies, 11 Gay Lane from Sheep Pound

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## PANTIGO COMMERCIAL DISTRICT

### Development

The land within the Pantigo District was part of the Main Street common. This low land at the head of the Hook Pond dreen was not suitable for home lots and became part of the broad common which extended north from Town Pond. It was customary to make the commons available for commercial and manufacturing enterprises that would benefit the residents. This practice began in the 17<sup>th</sup> century when permission was granted for private companies to set windmills on the common. During the nineteenth century this north end common became the site of a number of businesses including the blacksmith shop and livery stable owned by the Young and Strong families, James E. Gay's carriage shop, F. H. Warner's bakery and William Lusty's cobbler shop.

When this area was designated a commercial zone in 1925 it was still the center of transportation-related businesses with two automobile garages, an automobile paint shop and the Gay carriage shop.

The Village's recognition of the historic value of this area as having been part of the former common resulted in eventual public ownership of the "Sheep Pound" and of the "North End Common".

The long history of transportation-related businesses came to an end within the past ten years when two automobile dealerships moved out of the district.

### Existing Character

Much of the setting for the Pantigo Commercial District is provided by the Hook Historic District, which is immediately to the north, east and west.

Three properties in the Pantigo District have small-scale buildings which are compatible with the character of the surrounding historic neighborhood. On the north side of Pantigo Road is a grouping of three small buildings in a relatively open setting at 41, 43 and 43A Pantigo Road (Fig. 7). Across the street at 34 Pantigo Road is a traditional commercial building with a narrow front facade.

The Amaden-Gay Agencies building at 11 Gay Lane (Fig. 8) was originally a small office built after a fire destroyed the J.E. Gay carriage shop in 1937. It has been added onto many times and has developed a complex additive massing of small-scale components which have traditional forms and materials. The resulting large building has an appearance that is largely compatible with the surrounding neighborhood.

The Post Office at 12 Gay Lane and the CVS building at 38 Pantigo Road constructed on adjacent properties in 1994 brought into this district a larger scale, new forms, a new architectural character and a different orientation to the street.

## Goals for Future Development

**The small-scale buildings or small-scale components of larger buildings with traditional forms and materials along with the open spaces establish the desirable character of the Pantigo District.** These features and qualities contribute to a compatibility with the Hook Historic District and with the scale and character of the Village.

- To guide future development in a way that will maintain or enhance the setting of the adjacent residential neighborhood and of the Hook Historic District.
- To encourage a pattern of small-scale buildings utilizing traditional forms and materials in an open setting. For larger buildings, encourage an additive massing of individual small-scale components.
- To encourage open space and landscaping that will complement and extend the character of the open greens of the “Sheep Pound” and the “North End Common.”

## GUIDELINES FOR BUILDINGS

### Alterations and additions

- Buildings that make a positive contribution to the desirable character of the Pantigo District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Pantigo District as described in these guidelines.

### New construction

- New construction should be compatible with the desirable character of the Pantigo District as described in these guidelines.

### Orientation

- The front facade, containing the primary entrance, should face the street.
- The front facade should be parallel to the street.



### **Setback from the street**

Setback from the street is an important component of the relationship with the open space of the “Sheep Pound” and “North End Common,” with the adjacent residential neighborhood and with the Hook Historic District.

- The setback from Pantigo Road and from Gay Lane should be 15’ to 20’.

### **Rhythm of buildings on the street**

- An open rhythm of small-scale buildings should be achieved by limiting the width of the building facade facing the street and by providing a significant space between buildings.

### **Size**

- Buildings with a footprint of 2,000 s.f. or less enhance the desirable character of this district. Breaking up the allowable building area for a parcel into individual buildings of this size is preferred.
- Larger buildings should have a maximum footprint of 5,000 s.f. and/or a maximum gross floor area of 8,000 s.f. and should have complex massing that diminishes the apparent size.

### **Height**

- Buildings should be one-and-one-half stories or two stories in height.

### **Width of the front facade**

- The width of the front facade or of each individual unit of the front facade of a building with complex massing should not be greater than 35’.

### **Proportion of the front facade**

- The width of the front facade of a two-story buildings should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building should not exceed 2 times the height.

### **Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- Buildings with a footprint greater than 2000 s.f. should have a complex massing, breaking the building down into separate small-scale components. A main block should be closest to the street and contain the primary entrance. Secondary wings can project from the sides or rear. Different units can have

different heights. The Strong Agency building at 43 Pantigo Road and the Amaden-Gay Agency building at 1 Gay Lane have additive massing that illustrates this principle.

### **Roof form**

- Gable roofs with a pitch not less than 7:12 are preferred.

### **Materials**

Acceptable siding materials:

- Wood shingles.
- Painted wood clapboards.

Acceptable substitute materials where wood is not allowed by the building code:

- Painted fiber-cement shingles.
- Painted fiber-cement clapboards.
- Cement stucco.

Acceptable materials for pitched roofs

- Asphalt shingles.
- Wood shingles.

Acceptable material for doors, windows and trim:

- Wood.

### **Color**

Acceptable paint schemes:

- Natural shingle siding and painted trim.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guidelines:

- Color should contribute to a harmonious relationship with the district as a whole and with the surrounding neighborhood and not draw undue attention to one building.

### **Proportion and arrangement of windows**

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Windows should be placed in side elevations which face a street.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.

### **Storefronts**

Although only one building in the district has a traditional storefront, 34 Pantigo Road, storefronts are appropriate here.

- Storefronts may have a simpler treatment but should have the features, relationships and proportions of the traditional storefronts found in the Village Center District (see page 9).

### **Building details**

The majority of the buildings in the Pantigo Commercial District have plain exteriors. The hallmark of the surrounding Hook Historic District is the plain, vernacular character of the buildings.

- The restrained use of decorative detail will promote the compatibility of a building with the character of the district and of the surrounding neighborhood.

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## GUIDELINES FOR THE SITE

### Allocation of open space

Open space should be distributed to achieve the best balance between the following priorities:

- A front setback from Pantigo Road or Gay Lane of 15' to 20'.
- Open space that will maintain the setting of properties in the Hook Historic District or of an adjacent residence.
- Open space that will complement and extend the character of the open greens of the "Sheep Pound" and the "North End Common."
- Separation between buildings to contribute to an open setting.
- Separation between buildings sufficient to allow wood exteriors by the requirements of the building code.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

### Landscaping

Landscaping should contribute to an open character that is compatible with the adjacent residential neighborhood and that enhances the open greens of the "Sheep Pound" and the "North End Common."

Appropriate treatments include:

- Lawns, shade trees, shrubs and planting beds at the natural grade.
- Privet hedges and low evergreen hedges set well back from the street to screen parking.

### Parking

- As a general principle, parking should be to the rear of the building.
- In some instances the Design Review Board may determine that placing all or some parking to the side may allow a building that better meets the guidelines for this district. In these instances, the parking should be set back 20' from the front property line.

# NORTH MAIN STREET COMMERCIAL DISTRICT

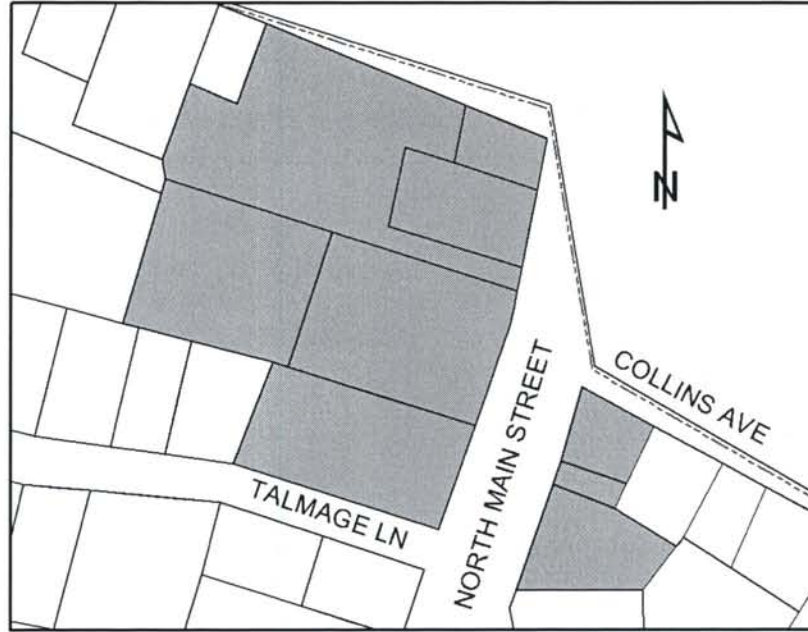


Fig. 9. Peter Fedi's Store, 79 North Main Street



Fig. 10. Talmage Building, 69 North Main Street



Fig. 11. Libert's Store, 84 North Main Street

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## NORTH MAIN STREET COMMERCIAL DISTRICT

### Development

The 17th-century Main Street common extended to Cedar Street with home lots laid out on the upland west side in the area of this commercial district. Construction of the railroad in 1895 cut off this area from the Village core.

Commercial development began here following a familiar pattern with a store or shop being built right on the street next to or in front of a dwelling. This is well illustrated by 69 North Main Street, a store built in 1888 by Daniel and DeWitt Talmage next to their houses. The 1929 Sanborn Map shows this pattern extending from the railroad underpass to Cedar Street with eight small shops built close to the street interspersed with houses set further back on the lots.

When the Village adopted zoning in 1925, this area was zoned for residential use. It was through a succession of petitions by property owners that commercial use was gradually granted by the Village Board. The most significant step was taken in 1929 with the rezoning of the Dominy home lot on the corner of North Main Street and Cedar Street. This change led to the demolition of the 1715 Dominy House and the loss of the clock shop and woodworking shop.

### Existing Character

This district retains the informal character which reflects its gradual development from a residential area to a commercial area. Three properties are stores attached to residences. The Talmage building at 69 North Main Street (Fig. 10) has a house attached in the back. The residence at 84 North Main Street (Fig. 11) had a storefront as early 1929 when Libert's Store was here. An early two-story house at 74 North Main Street has a more recent large storefront. Because of these buildings the district retains the spacing and some of the character of a residential area.

The brick building at 79 North Main Street (Fig. 9), which was built as Peter Fedi's Store in the 1930s, is a more conventional commercial building which has a residential scale and second-story residential windows.

The Village Emergency Services Building has a very different large-scale character, but it is set back from North Main Street.

Parking lots are prominent features of the streetscape of this district. The parking lot at the corner of North Main Street and Cedar Street and the parking lot next to 74 North Main Street are right on the sidewalk and have no screening. A strip of lawn and some street trees provide some screening for the large IGA parking lot.

Immediately south of the North Main Street Commercial District is the 1739 Hedges-Talmage House at 61 North Main Street. This is an important 18<sup>th</sup>-century house which also has a historic connection with the store across Talmage Lane at 69 North Main Street.

The commercial district in the Town of East Hampton which extends to the north also has an informal mixed character resulting from the combination of residences and storefronts. The Town district is also in the proximity of historic properties: the 1855 Samuel Hedges Miller House at the northwest corner with Cedar Street and the 1858 Sherrill House at 2 Springs Fireplace Road.

### **Goals for Future Development**

**The qualities of scale, open rhythm, traditional materials and informal character of the existing buildings, which include residences with attached storefronts and small-scale commercial buildings, establish the desirable character of the North Main Street District.**

- To allow this area to develop into a commercial district that has a pedestrian scale and some of the compactness of the Village Center District while maintaining a more open rhythm and informal character.
- To improve the contribution parking lots make to the character of the district by providing increased setbacks and additional landscaping.

## **GUIDELINES FOR BUILDINGS**

### **Alterations and additions**

- Buildings that make a positive contribution to the desirable character of the North Main Street District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the North Main Street District as described in these guidelines.

### **New construction**

- New construction should be compatible with the desirable character of the North Main Street District as described in these guidelines.

### **Orientation**

- The front facade, containing the primary entrance, should face North Main Street.
- The front facade should be parallel to the street.
- Buildings on corner lots should also have a side facade that continues the material and details of the front wall and relates to the character of the side street.

**Setback from the street**

- A setback of 5' to 10' to allow for landscaping or planters is preferred.

**Rhythm of buildings on the street**

- An open rhythm of small-scale buildings should be achieved by limiting the width of the building facade facing the street and by providing a significant space between buildings.

**Size**

- Buildings with a maximum footprint of 3,000 s.f. and a maximum gross floor area of 5,000 s.f. enhance the desirable character of this district.

**Height**

- All of the buildings in this district have two-story and one-story components which gives a lively rhythm. This mix of heights is a characteristic that should be retained.

**Width of the front facade**

- The width of the front facade or of each individual unit of the front facade of a building with complex massing should not be greater than 35'.

**Proportion of the front facade**

- The width of the front facade of a two-story building should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building, or the width of each distinct storefront of a one-story building, should not exceed 2 times the height.

**Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- Buildings with a footprint greater than 2000 s.f. should have a complex massing, breaking the building down into separate small-scale components. A main block should be closest to the street and contain the primary entrance. The prevalent form of complex massing in this district is additive massing with building components of different heights. 79 North Main Street is a good example of this principle.



**Roof form**

- Gable roofs with a pitch not less than 7:12 are preferred.
- A flat roof is acceptable for secondary components of a building or for buildings with a footprint not greater than 1,500 s.f. provided they have a parapet on the front facade.

**Materials**

Acceptable siding materials:

- Wood shingles.
- Painted wood clapboards.

Acceptable siding materials where wood is not allowed by the building code:

- Brick
- Painted fiber-cement shingles
- Painted fiber-cement clapboards

Acceptable materials for pitched roofs:

- Asphalt shingles
- Wood shingles

Acceptable material for doors, windows and trim:

- Wood.

**Color**

Acceptable paint schemes:

- Natural shingle siding and painted trim.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guidelines:

- Trim colors can be darker and more intense than siding colors.
- A small building can have greater latitude with color than a large building.

- The color of a large building should help to make it blend into the environment.
- Color should contribute to the streetscape and district as a whole and not draw undue attention to one building.

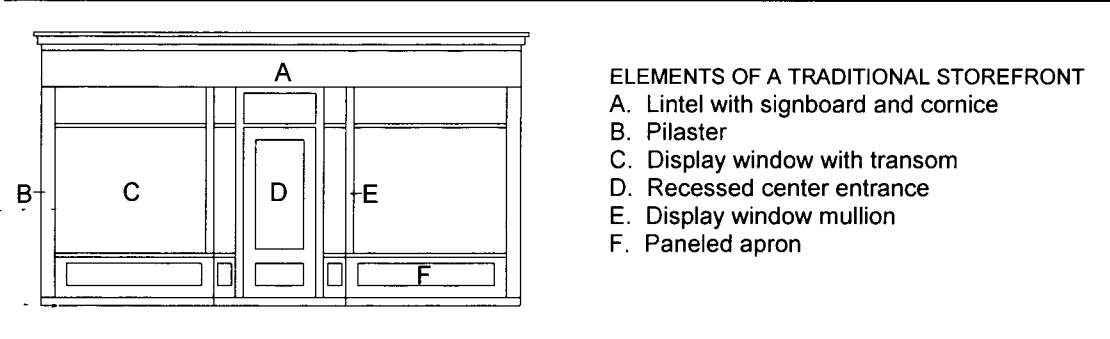
### **Proportion and arrangement of windows**

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Windows should be placed in side elevations which face a street.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.

### **Storefronts**

The goal of enhancing this district as a compact, pedestrian-oriented commercial area would be benefited from the type of architectural interest found in the Village Center District. Although storefronts here can have a simpler treatment than in the Village Center, many of the same guidelines apply.

- The relationship between the storefront and the building as a whole should be taken into account.
- As a general guide, the combination of display windows and entrance should extend across a minimum of 70% of the width of the front facade.
- The storefront should be framed at the sides by vertical elements such as piers, pilasters or mullions and at the top by a substantial horizontal element such as a signboard.
- These elements should be in proportion and the vertical elements should be of sufficient dimension to “carry” the signboard and the upper facade.
- The storefront should have depth achieved by projecting and recessed elements. Typically pilasters, signboards and cornices project; entrances are recessed; and display window glass is set in from the face of substantial mullions and sills.
- The width of a display window should not exceed 1 ½ times the height.
- Display windows should have an apron beneath providing a minimum distance of 18’ from grade to the glass.
- Wood is an appropriate material for storefronts. Brick buildings usually have storefronts with both wood and brick elements.



### Building details

- Building facades should be designed and detailed to contribute to the streetscape as a whole and not to draw attention to one building.

### GUIDELINES FOR THE SITE

#### Allocation of open space

Open space should be distributed to achieve the best balance between the following priorities:

- A front setback from North Main Street of 5' to 10'.
- A setback from Talmage Lane of 10' to enhance the setting of that street.
- Separation between buildings to contribute to an open setting.
- Separation between buildings sufficient to allow wood exteriors by the requirements of the building code.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

**Landscaping**

Landscaping should contribute to an open character that is compatible with the adjacent residential neighborhood.

Appropriate treatments include:

- Lawns, shade trees, shrubs and planting beds at the natural grade.
- Privet hedges and low evergreen hedges set well back from the street to screen parking.

**Parking**

- As a general principle, parking should be to the rear of the building.
- In some instances the Design Review Board may determine that placing all or some parking to the side may allow a building that better meets the guidelines for this district. In these instances, the parking should be set back 15' to 20' from the street. Parking should be set back 10' from Talmage Lane.

# UPPER NEWTOWN LANE COMMERCIAL DISTRICT



Fig. 12. Louis Vetault & Son's Florists,  
91 Newtown Lane



Fig. 13. Mulford-Vetault House,  
95 Newtown Lane

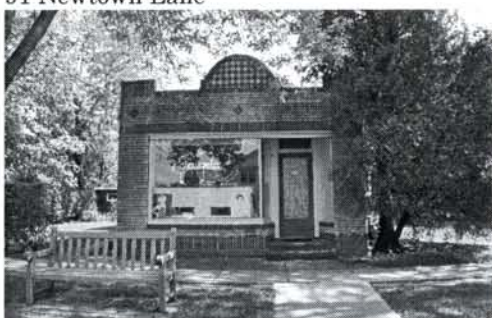


Fig. 14. Mosebach Tailor Shop, 100 Newtown Lane



Fig. 15. Filer House, 98 Newtown Lane



Fig. 16. Osborn House, 88 Newtown Lane



Fig. 17. Muchmore House, 83 Newtown Lane

## UPPER NEWTOWN LANE COMMERCIAL DISTRICT

### Development

This area of Newtown Lane, where there had been only a few farmhouses previously, began to develop as a residential community in the 1890s. By 1915 it was a built-up residential neighborhood with a regular rhythm of houses.

Because this area of Newtown Lane was between the Railroad Station and Main Street, some commercial development occurred here in the 1920s. Louis Vetault established a florist business with a greenhouse and retail store at 91 Newtown Lane in 1921. August Mosebach added a storefront to the house at 100 Newtown Lane in the 1920s for a tailor shop.

When the Village adopted zoning in 1925 all of Newtown Lane from the railroad to Main Street was designated commercial. After 1925 no new commercial buildings were constructed here until the 1980s when two residences were moved away and new commercial buildings constructed at 79 Newtown Lane and 87 Newtown Lane and a new commercial building was constructed at 94 Newtown Lane.

### Existing Character

The houses which remain in this district, some small-scale commercial buildings, the lawn setting of many buildings, the grass verge and the tree-lined sidewalk give this district a perceptively residential character and a pedestrian scale. On eight of the fourteen properties are buildings which were constructed as houses and which still retain a residential appearance. The 19<sup>th</sup>-century Isaac Scoy Osborn House at 88 Newtown Lane (Fig. 16), the 1895 Henry Chase Filer House at 98 Newtown Lane (Fig. 15), the 19<sup>th</sup>-century W.F. Muchmore House at 83 Newtown Lane (Fig. 17) and the 19<sup>th</sup>-century Mulford-Vetault House at 95 Newtown Lane (Fig. 13), which all have a lawn setting, make an especially important contribution to the desirable character of this district. The 1921 Louis Vetault & Son's Florists at 91 Newtown Lane (Fig. 12), designed by Joseph Greenleaf Thorp, and the 1920s Mosebach Tailor Shop at 100 Newtown Lane (Fig. 14) are small-scale commercial buildings in an open setting which complement the overall residential character of this district.

### Goals for Future Development

**The houses, small-scale commercial buildings, lawn settings and other qualities that contribute to its residential character establish the desirable character of the Upper Newtown Lane District.**

- To retain the architectural integrity of the buildings that give the district its desirable character and guide development of other properties to be compatible with that character.
- To retain the open space that contributes to the setting of the district.
- To encourage a rhythm of small-scale buildings separated by significant open space.

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## GUIDELINES FOR BUILDINGS

### Alterations and additions

- Buildings that make a positive contribution to the desirable character of the Upper Newtown Lane District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Upper Newtown Lane District as described in these guidelines.

### New construction

- New construction should be compatible with the desirable character of the Upper Newtown Lane District as described in these guidelines.

### Orientation

- The front facade, containing the primary entrance, should face Newtown Lane. For properties that do not abut Newtown Lane, the front facade should face Osborne Lane or Muchmore Lane.
- The front facade should be parallel to the street.
- Buildings on corner lots should also have a side facade that continues the material and details of the front wall.

### Setback from the street

- The setback from the street should be 15' to 20'.
- The Board may allow a lesser setback for very small buildings or for a small-scale component which contains the primary entrance of a larger building.

### Rhythm of buildings on the street

- An open rhythm of small-scale buildings should be achieved by limiting the size of buildings, limiting the width of the building facade facing the street and by providing a significant space between buildings.
- Side setbacks of 20' would best maintain the rhythm of this district.

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### **Size**

- Buildings with a footprint of 2,000 s.f. or less enhance the desirable character of this district. Breaking up the allowable building area for a parcel into individual buildings of this size is preferred.
- Larger buildings should have a maximum footprint of 3,000 s.f. and/or a maximum gross floor area of 5,000 s.f. and should have complex massing that diminishes the apparent size.

### **Height**

- Two-story buildings are preferred. A two-story building may have secondary one-story components.
- Small one-story buildings (with a footprint of 1,500 s.f. or less) are also appropriate and should have extra height by using higher ceilings, a pitched roof or a parapet front facade.

### **Width of the front facade**

- The width of the front facade or of each individual unit of the front facade of a building with complex massing should not be greater than 35'.

### **Proportion of the front facade**

- The width of the front facade of a two-story building should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building should not exceed 2 times the height.

### **Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- Buildings with a footprint greater than 2000 s.f. should have a complex massing, breaking the building down into separate small-scale components. A main block should be closest to the street and contain the primary entrance.
- A large building should be extended to the rear in order to minimize the width facing the street.

### **Roof form**

- Gable or hip roofs with a pitch not less than 7:12 are preferred.
- A flat roof is acceptable for small, secondary components of a building or for buildings with a footprint of 1,500 s.f. or less, provided they have a parapet on the front facade.



## **Materials**

Acceptable siding materials:

- Wood shingles
- Painted wood clapboards
- Brick (for buildings with a footprint of 2,000 s.f. or less)
- Cement stucco (for buildings with a footprint of 2,000 s.f. or less)

Acceptable siding materials where wood is not allowed by the building code:

- Painted fiber-cement shingles
- Painted fiber-cement clapboards

Acceptable materials for pitched roofs:

- Asphalt shingles
- Wood shingles

Acceptable material for doors, windows, trim and storefronts:

- Wood

## **Color**

Acceptable paint schemes:

- Natural shingle siding and painted trim.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guidelines:

- A small building can have greater latitude with color than a large building.
- The color of a large building should help to make it blend into the environment.
- Color should contribute to the streetscape and district as a whole and not draw undue attention to one building.

### **Proportion and arrangement of windows**

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Windows should be placed in side elevations which face a street.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.

### **Storefronts**

- Storefronts here may have a simpler treatment but should have the features, relationships and proportions of the traditional storefronts found in the Village Center District (see page 9).

### **Building details**

- Building facades should be designed and detailed to contribute to the streetscape as a whole and not to draw attention to one building.
- New work should reflect the tradition of quality in materials and workmanship found in the early commercial buildings and houses.

## **GUIDELINES FOR THE SITE**

### **Allocation of open space**

Open space should be distributed to achieve the best balance between the following priorities:

- A setback from Newtown Lane of 15' to 20'.
- A setback from Osborne Lane and Muchmore Lane of 15'.
- Open space that contributes to the setting of buildings in the district.
- Separation between buildings to contribute to an open setting.
- Separation between buildings sufficient to allow wood exteriors by the requirements of the building code.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

**Landscaping**

Landscaping should contribute to the open residential character of the district. Appropriate treatments include:

- Lawns, shade trees, shrubs and planting beds at the natural grade.
- Privet hedges and low evergreen hedges set well back from the street to screen parking.

**Parking**

- As a general principle, parking should be to the rear of the building.
- In some instances the Design Review Board may determine that placing all or some parking to the side may allow a building that better meets the guidelines for this district. In these instances, the parking should be set back 30' from the street.
- Parking areas should be set back 10' from Osborne Lane and Muchmore Lane.

# RAILROAD AVENUE COMMERCIAL DISTRICT

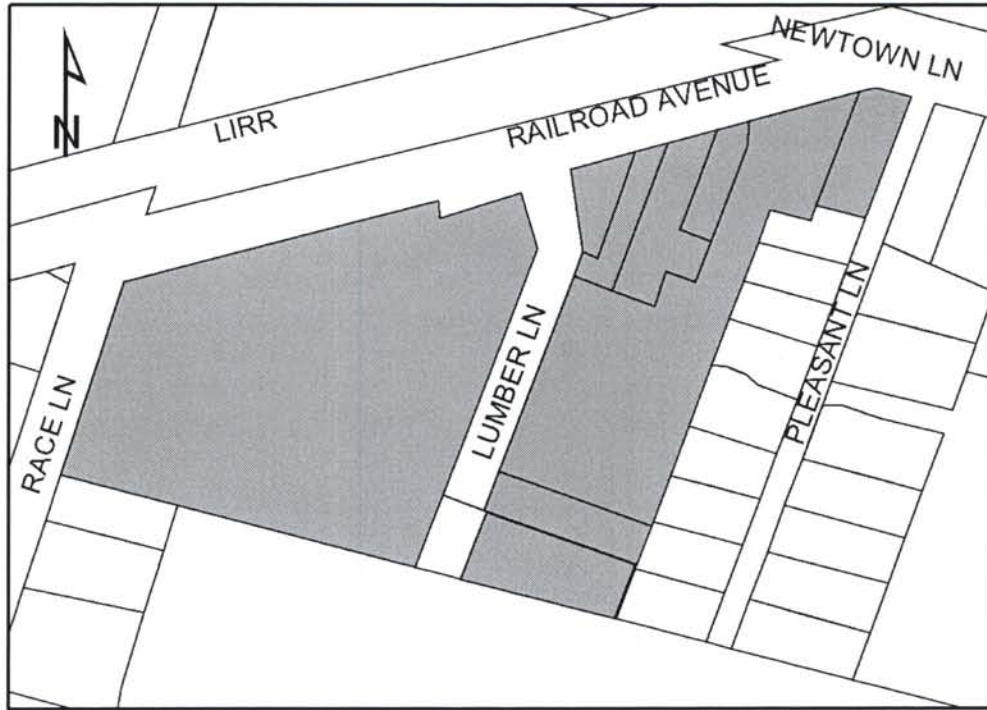


Fig. 18. Cavagnaro Building, 105 – 107 Newtown Lane



Fig. 19. Trunzo Building, 15 Railroad Avenue

## RAILROAD AVENUE COMMERCIAL DISTRICT

### Development

The land within this district was primarily undeveloped until the arrival of the railroad in 1895. The railroad station was built in that year along with a brick freight depot which stood next to the tracks between Race Lane and Fresno Place. Almost immediately the East Hampton Lumber and Coal Co. moved to the present site of Riverhead Building Supply. A railroad spur ran to this yard which became a major factor in the tremendous growth of the East Hampton summer colony from 1895 to 1905.

In 1923 the three-story, brick Cavagnaro Building was built on the corner of Railroad Avenue and Pleasant Lane containing a delicatessen, restaurant and apartments. Also in the 1920s three small automobile garages were built between the Cavagnaro Building and the lumber company.

### Existing Character

Railroad Avenue between Pleasant Lane and Race Lane has a two-part character. The Riverhead Building Supply yard dominates the west half. The east half has the beginnings of a dense commercial district. This trend was begun with the 1923 brick Cavagnaro Building (Fig. 18) but it was not until 1985 with construction of the two-story brick Trunzo Building (Fig. 19) that this character was established.

### Goals for Future Development

**The Railroad Station, the Cavagnaro Building, the Trunzo Building and other qualities that contribute to a compact, pedestrian-oriented commercial area establish the desirable character of the Railroad Avenue District.**

- To encourage development of a commercial block east of Lumber Lane with two-story buildings and storefronts following the pattern of development found on the west side of Main Street and following the character established by the Cavagnaro Building and the Trunzo Building. Should the Riverhead Building Supply yard be redeveloped for retail/office/apartment use, the goal would be to continue this same type of development west of Lumber Lane.

Although the Village Center District would be the general model, the goal would be for development on Railroad Avenue to have more open space, more enhancement from landscape plantings, wider landscaped walkways and parking lots with a smaller scale and more shade trees.

- To enhance the setting of the East Hampton Railroad Station, a local landmark, and to retain its position as the focus of this area and the primary visual and architectural landmark.
- Development fronting on Race Lane should relate to the small-scale buildings and more open setting of that street.

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## GUIDELINES FOR BUILDINGS

### Alterations and additions

- Buildings that make a positive contribution to the desirable character of the Railroad Avenue District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Railroad Avenue District as described in these guidelines.

### New construction

- New construction should be compatible with the desirable character of the Railroad Avenue District as described in these guidelines.

### Orientation

- The front facade, containing the primary entrance, should face Railroad Avenue. Buildings set back on Lumber Lane should face that street.
- The front facade should be parallel to the street.
- Buildings on corner lots should also have a side facade that continues the material and details of the front wall.

### Setback from the street

- The setback from Railroad Avenue should be 5' to 10' to allow for landscaping or planters.
- The setback from Lumber Lane should be 10' to allow for a landscaped walkway from the Village parking lot to Railroad Avenue.
- The setback from Race Lane should be 20' to protect the more rural character of that street.

### Rhythm of buildings on the street

- A dense rhythm of buildings characteristic of the Village Center should be achieved by limiting the width of the building facade facing the street and setting buildings close together.

**Size**

- Buildings with a maximum footprint of 4,000 s.f. and a maximum gross floor area of 6,500 s.f. enhance the desirable character of this district.

**Height**

- Two-story buildings are preferred. A mixing of one-story and two-story buildings is characteristic of the Village Center District and is also appropriate here. One-story buildings should have extra height by using higher ceilings, a pitched roof or a parapet front facade.
- A building on the corner with Race Lane or fronting on Race Lane should be one story.

**Width of front facade**

- The width of a building or the width of a distinct storefront of a one-story building should not exceed 40'.

**Proportion of the front facade**

- The width of the front facade of a two-story buildings should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building, or the width of each distinct storefront of a one-story building, should not exceed 2 times the height.
- When a front facade must exceed these proportions, other elements of the design should be used to create components of a more vertical proportion.

**Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- A narrow, deep building is preferred over a shallow building with a long facade on Railroad Avenue.
- A large building should have a two-story front block which fits into the context of the streetscape. A one-story mass can extend behind this front block.

**Roof form**

- Flat roofs are appropriate provided there is a parapet on the front facade.
- Gable or hip roofs with a pitch not less than 7:12 are appropriate.

**Materials**

Acceptable siding materials:

- Brick
- Wood shingles.
- Painted wood clapboards.
- Cement stucco

Acceptable siding materials where wood is not allowed by the building code:

- Painted fiber-cement shingles.
- Painted fiber-cement clapboards.

Acceptable materials for pitched roofs:

- Asphalt shingles.
- Wood shingles.

Acceptable material for doors, windows and trim:

- Wood.

**Color**

Acceptable paint schemes:

- Natural brick or shingle siding and painted trim. Brick should not be painted.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guideline:

- Color should contribute to the streetscape and district as a whole and not draw undue attention to one building.



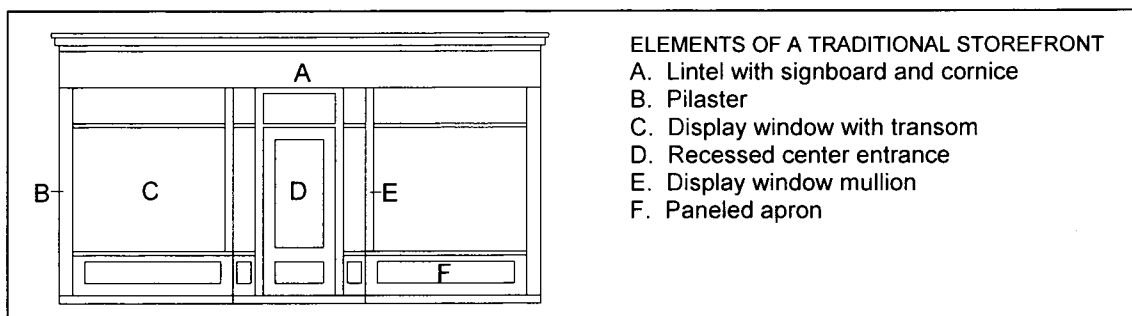
## Proportion and arrangement of windows

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.
- Windows should be placed in side elevations which face a street.

## Storefronts

Storefronts may incorporate the traditional elements found on many of the buildings in the Village Center District and therefore many of the guidelines for those storefronts are included below. In the Railroad Avenue District it would also be appropriate for storefronts to have a more contemporary character.

- The relationship between the storefront and the building as a whole should be taken into account.
- As a general guide, the combination of display windows and entrance should extend across a minimum of 70% of the width of the front facade.
- The storefront should be framed at the sides by vertical elements such as piers, pilasters or mullions and at the top by a substantial horizontal element such as a signboard.
- These elements should be in proportion and the vertical elements should be of sufficient dimension to “carry” the signboard and the upper facade.
- The storefront should have depth achieved by projecting and recessed elements. Typically pilasters, signboards and cornices project; entrances are recessed; and display window glass is set in from the face of substantial mullions and sills.
- The width of a display window should not exceed 1 ½ times the height.
- Display windows should have an apron beneath providing a minimum distance of 18 inches from grade to the glass.
- Wood is an appropriate material for storefronts. Brick buildings usually have storefronts with both wood and brick elements.



### ELEMENTS OF A TRADITIONAL STOREFRONT

- A. Lintel with signboard and cornice
- B. Pilaster
- C. Display window with transom
- D. Recessed center entrance
- E. Display window mullion
- F. Paneled apron

**Building details**

- Architectural details should be used with restraint. Buildings should defer to the East Hampton Railroad Station as being the focal building of this district.
- Building facades should be designed and detailed to contribute to the streetscape as a whole and not to draw undue attention to one building.

**GUIDELINES FOR THE SITE****Allocation of open space**

Open space should be distributed to achieve the best balance between the following priorities:

- A setback from Railroad Avenue of 5' to 10' to allow for landscaping or planters.
- A setback from Lumber Lane of 10' to allow for a landscaped walkway from the Village parking lot to Railroad Avenue.
- A setback from Race Lane of 20' to protect the more rural character of that street.
- Providing generous walkways from any parking area behind the buildings to Railroad Avenue.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

**Landscaping**

- Landscaping, flower beds and planters in front of the buildings on Railroad Avenue would contribute to the setting of the district and to the setting of the Railroad Station.
- Trees, shrubs and plant beds would enhance future sidewalks on Lumber Lane and Race Lane and walkways from parking areas to Railroad Avenue.

**Parking**

- Parking should be to the rear of the buildings on Railroad Avenue.
- Parking should be set back 20' from Race Lane and 15' from Lumber Lane.

## GINGERBREAD COMMERCIAL DISTRICT

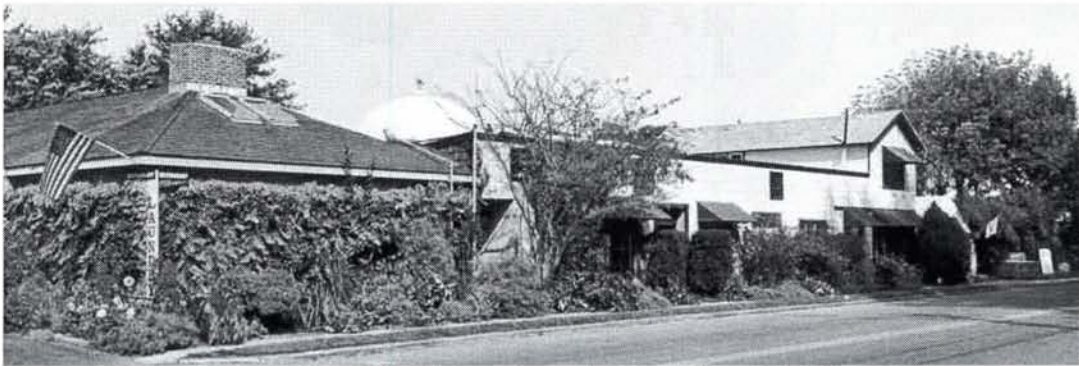


Fig. 20. East Hampton Steam Laundry complex, 29-31 Race Lane



Fig. 21. Gardiner buildings, Race Lane



Fig. 22. Barnes Brothers masonry yard complex, 9 Fresno Place

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## GINGERBREAD COMMERCIAL DISTRICT

### Development

Like the Railroad Avenue District, the development of this area is tied to the arrival of the railroad in 1895. Businesses served by railroad spur lines were the first to be established here and these were followed by other construction, service and manufacturing concerns.

The 1929 Sanborn Map shows the railroad spur lines that carried materials to Barnes Brothers mason contractor; wood and coal to J.F. Gilmartin Co.; fuel to the Long Island Lighting Co. substation; and materials to S.J. Lynch mason contractor. Other companies established by this time within the triangle formed by the railroad tracks, Gingerbread Lane and Race Lane included East Hampton Steam Laundry; E.L. Philips Gas & Electric Supplies; Frank Johnson Contractor; and O'Brien's Garage. These small-scale businesses were mixed in with modest residences.

### Existing Character

The Gingerbread District today largely retains its early character of small scale commercial buildings interspersed with modest houses with significant space between buildings.

The district contains a diverse collection of buildings including: modest houses; 1920s and 1930s small-scale commercial buildings; recent small-scale sheds; recent large commercial buildings and the LIPA substation.

Many buildings contribute to the desirable character of this district and its compatibility with Village as a whole. Especially important are a collection of buildings from 1910 to 1930 which exemplify the principles of simple, utilitarian design and recall the history of this "working" part of the Village as distinct from Main Street, the "Summer Colony" and the residential streets. Among these are: the c. 1913 East Hampton Steam Laundry complex at 29-31 Race Lane (Fig. 20); the c.1927 Barnes Brothers masonry yard complex at 9 Fresno Place (Fig. 22) which includes an important 19<sup>th</sup>-century timber-frame barn; the complex of former East Hampton Town and Village highway barns at 3 Toilsome Lane; and the c.1930 O'Brien's Garage at 14 Gingerbread Lane with a front facade of over-burnt brick which originally had a distinctive color and texture.

Race Lane in particular has a character that exemplifies many qualities important to the Village: a mixture of building types and uses; a small scale; quality commercial buildings with a simple, utilitarian design; and significant open space. Buildings which contribute to the character of this street include the East Hampton Steam Laundry complex with a c. 1913 brick building and a c. 1920 wood-frame addition clad with painted fiber-cement shingles, the c. 1910 Thomas Atkins House on the corner with Gingerbread Lane and three buildings moved to the east side of Race Lane by Winthrop Gardiner in the 1920s (Fig. 21), at least one of which is an 18-th century building from the Gardiner "Brown House" farm complex. The front lawns of the three former Gardiner buildings and the yard at the Atkins House are important open spaces.

## **Goals for Future Development**

**The group of pre-1930 small-scale commercial buildings, the modest houses and generous open spaces establish the desirable character of the Gingerbread Commercial District.**

- To retain the architectural integrity of the buildings that give the Gingerbread District its desirable character and guide development of other properties to be compatible with that character.
- In particular to retain the ensemble of buildings and open spaces that give Race Lane its distinct rural character.
- To encourage a rhythm of small-scale buildings separated by significant open space. This will enhance the character of this district and is a pattern that is compatible with the modest residences in the adjoining residential neighborhood.
- To encourage buildings of a utilitarian design using quality materials. A context for building forms, proportions, scale, massing and materials is provided by the East Hampton Steam Laundry complex (29-31 Race Lane), the Barnes Brothers masonry yard complex (9 Fresno Place), the East Hampton Town and Village highway barns (3 Toilsome Lane) and O'Brien's Garage (14 Gingerbread Lane).
- To promote a streetscape with grass edges, sidewalks and street trees that will continue the character of the adjacent residential neighborhood and make this district more pedestrian friendly.

## **GUIDELINES FOR BUILDINGS**

### **Alterations and additions**

- Buildings that make a positive contribution to the desirable character of the Gingerbread District should be retained and their form and important architectural features and materials kept intact. Alterations or additions to these buildings should be compatible with their architectural character.
- Other alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Gingerbread District as described in these guidelines.

### **New construction**

- New construction should be compatible with the desirable character of the Gingerbread District as described in these guidelines.

### **Orientation**

- The front facade, containing the primary entrance, should face the street. On corner lots buildings should face the street with the highest priority in the following list: (1) Race Lane; (2) Gingerbread Lane; (3) Fresno Place; and (4) King Street.
- The front facade should be parallel to the street.
- Buildings on corner lots should also have a side facade that continues the material and details of the front wall.

### **Setback from the street**

- The setback from Gingerbread Lane, Race Lane, Fresno Place and King Street should be 15' to 20'. The setback from Railroad Avenue should be 10'.

### **Rhythm of buildings on the street**

- An open rhythm of small-scale buildings should be achieved by limiting the width of the building facade facing the street and by providing a significant space between buildings.

### **Size**

- Buildings with a footprint of 2,000 s.f. or less enhance the desirable character of this district. Breaking up the allowable building area for a parcel into individual buildings of this size is preferred.
- Larger buildings should have a maximum footprint of 3,000 s.f. and/or a maximum gross floor area of 5,000 s.f. and should have complex massing that diminishes the apparent size.

### **Height**

- There is no preference for one-story or two-story buildings. It is appropriate to give a one-story building extra height by using higher ceilings, a pitched roof or a parapet front facade.

### **Width of the front facade**

- The width of the front facade or of each individual unit of the front facade of a building with complex massing should not be greater than 35'.

### **Proportion of the front facade**

- The width of the front facade should not exceed 1 ½ times the height of the building.

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### **Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- Buildings with a footprint greater than 2000 s.f. should have a complex massing, breaking the building down into separate small-scale components. A main block should be closest to the street and contain the primary entrance. Secondary wings can project from the sides or rear. Different units can have different heights and roof forms. Some of the best buildings in the district have additive massing that illustrates this principle: East Hampton Steam Laundry complex (29-31 Race Lane), the Barnes Brothers masonry yard complex (9 Fresno Place), the East Hampton Town and Village highway barns (3 Toilsome Lane).

### **Roof form**

- Gable roofs with a pitch not less than 7:12 are preferred.
- A flat roof is acceptable for secondary components of a building or for buildings with a footprint not greater than 1,500 s.f. provided they have a parapet on the front facade.

### **Materials**

Acceptable siding materials:

- Wood shingles
- Painted wood clapboards
- Cement stucco
- Brick

Acceptable siding materials where wood is not allowed by the building code:

- Painted fiber-cement shingles.
- Painted fiber-cement clapboards.

Acceptable materials for pitched roofs:

- Asphalt shingles.
- Wood shingles.

Acceptable material for doors, windows and trim:

- Wood.

## **Color**

Acceptable paint schemes:

- Natural shingle siding and painted trim.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guidelines:

- Trim colors can be darker and more intense than siding colors.
- A small building can have greater latitude with color than a large building.
- The color of a large building should help to make it blend into the environment.
- Color should contribute to the streetscape and district as a whole and not draw undue attention to one building.
- The East Hampton Steam Laundry complex (29-31 Race Lane), the Barnes Brothers masonry yard complex (9 Fresno Place), the East Hampton Town and Village highway barns (3 Toilsome Lane) and the Atkins House are all good examples of color contributing to the desirable character of this district.

## **Proportion and arrangement of windows**

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Windows should be placed in side elevations which face a street.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.

## **Storefronts**

- Storefronts here may have a simpler treatment but should have the features, relationships and proportions of the traditional storefronts found in the Village Center District (see page 9).

## **Building details**

- The utilitarian buildings that contribute to the context of this district have restrained detailing and rely on the quality of materials and design for their character. Following this principle will help to make a new building or a renovation enhance the desirable character of this district.



## GUIDELINES FOR THE SITE

### Allocation of open space

Open space should be distributed to achieve the best balance between the following priorities:

- A front setback from Gingerbread Lane, Race Lane, Fresno Place and King Street of 15' to 20' for compatibility with the residential neighborhood and to allow a sidewalk with a generous grass edge on either side.
- Open space that contributes to the setting of buildings in the district.
- Separation between buildings to contribute to an open rhythm.
- Separation between buildings sufficient to allow wood exteriors by the requirements of the building code.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

### Landscaping

Landscaping should contribute to an open character that is compatible with the adjacent residential neighborhood.

Appropriate treatments include:

- Lawns, shade trees, shrubs and planting beds at the natural grade.
- Privet hedges and low evergreen hedges set well back from the street to screen parking.

### Parking

- As a general principle, buildings that front on Gingerbread Lane, Race Lane, Fresno Place and King Street should have parking to the rear of the building.
- In some instances the Design Review Board may determine that placing all or some parking to the side may allow a building that better meets the guidelines for this district. In these instances, the parking should be set back 15' to 20' from the street.

# COVE HOLLOW COMMERCIAL DISTRICT

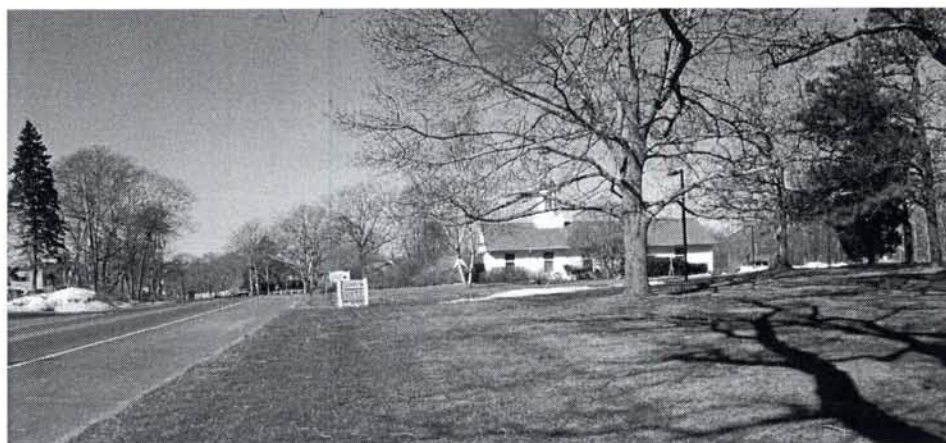
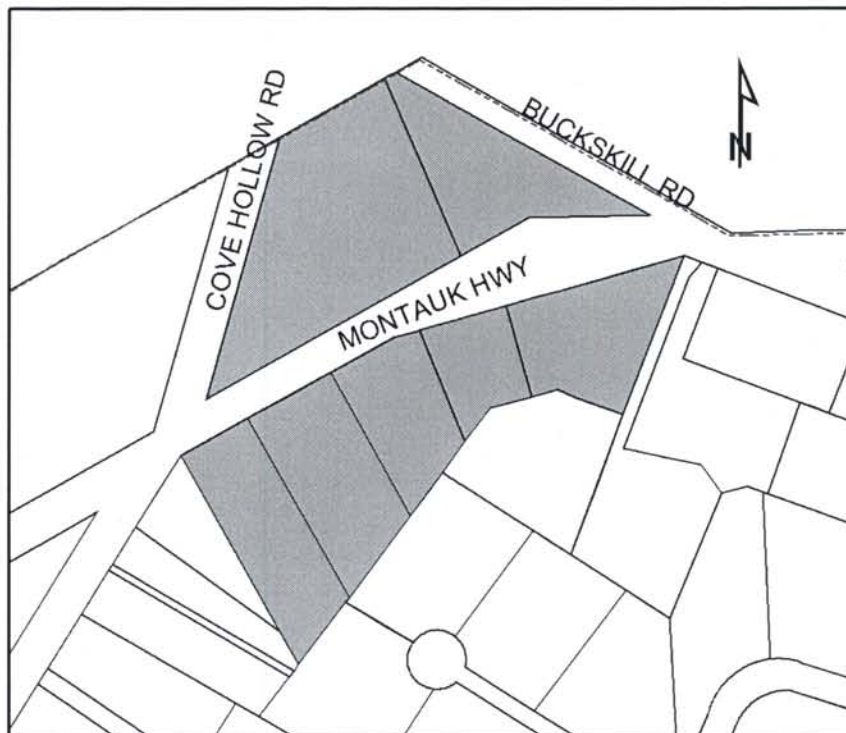


Fig. 23. Apple Bank, 50 Montauk Highway



Fig. 24. Red Horse complex, 74 Montauk Highway

## COVE HOLLOW COMMERCIAL DISTRICT

### Development

This district is within an area annexed to the Village in 1948 which was originally zoned entirely residential. Existing businesses within the present Cove Hollow District at that time included The Oaks Inn on the north side of Montauk Highway and William Boone's gas station at 47 Montauk Highway. Because of these existing businesses, the area was eventually zoned for commercial use.

### Existing Character

The Cove Hollow District has a diverse character. Buildings range in size from the diminutive flower shop of Buckley's at 75 Montauk Highway to the large-scale buildings of the Red Horse complex at 74 Montauk Highway (Fig. 24). Parking areas on the south side of Montauk Highway extend directly to the highway pavement while most parking on the north side is set back and screened.

Despite its highway location, this district is not yet a "commercial strip" completely at odds with the character and scale of the Village.

Existing buildings and properties possess some qualities that make them compatible with the character and scale of the Village including: a predominance of gable roofs; complex massing which joins individual smaller-scale units into one larger building; complex massing that joins two-story and one-story components; building details which include a fair number of residential-scale windows and display windows of proportions that relate to those in the Village Center; meaningful open spaces on the north side of Montauk Highway, especially at the corner with Buckskill Road (Fig. 23), and at Buckley's on the south side; parking areas that are setback from the highway and are well screened; and a fair number of mature deciduous trees.

### Goals for Future Development

**The qualities identified above of form, massing, proportions and open space which make development here compatible with the character and scale of the Village establish the desirable character of the Cove Hollow District.**

- To guide future development to be compatible with the character and scale of the Village.
- To retain and enhance the qualities and features that contribute to the desirable character of this district.
- To encourage large buildings to utilize complex massing of traditional building forms.
- To encourage placement of parking to have a minimal impact on the overall setting of the district.

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## GUIDELINES FOR BUILDINGS

### Alterations and additions

- Alterations or additions should relate harmoniously to the existing building and be compatible with the desirable character of the Cove Hollow District as described in these guidelines.

### New construction

- New construction should be compatible with the desirable character of the Cove Hollow District as described in these guidelines.

### Orientation

- The front facade, containing the primary entrance, should face Montauk Highway.
- For a building of complex massing or for a grouping of smaller buildings, the front facade of the primary component should face Montauk Highway.

### Setback from the street

- The setback from Montauk Highway should be a minimum of 25'.
- The setback from Buckskill Road and Cove Hollow Road should be a minimum of 15'.

### Rhythm of buildings on the street

- A rhythm of small-scale units in an open setting should be achieved by breaking development into a grouping of smaller individual buildings or by complex massing.

### Size

- Buildings with a footprint of 5,000 s.f. or less enhance the desirable character of this district. Breaking up the allowable building area for a parcel into individual buildings of this size is preferred.
- Larger buildings should have a maximum footprint of 7,500 s.f. and/or a maximum gross floor area of 10,000 s.f. and should have complex massing that diminishes the apparent size.

### Height

- A development with a combination of one-story and two-story components enhances the desirable character of this district.

**Width of the front facade**

- The width of the front facade or of each individual unit of the front facade of a building with complex massing should not be greater than 45'.

**Proportion of the front facade**

- The width of the front facade of a two-story building should not exceed 1 ½ times the height.
- The width of the front facade of a one-story building should not exceed 2 times the height.

**Massing and building form**

- Small-scale buildings with a basic rectangular footprint are preferred.
- Buildings with a footprint greater than 3000 s.f. should have a complex massing, breaking the building down into separate small-scale components.

**Roof form**

- Gable roofs with a pitch not less than 7:12 are preferred.

**Materials**

Acceptable siding materials:

- Wood shingles
- Painted wood clapboards
- Cement stucco
- Brick

Acceptable substitute materials where wood is not allowed by the building code:

- Painted fiber-cement shingles
- Painted fiber-cement clapboards

Acceptable materials for pitched roofs:

- Asphalt shingles
- Wood shingles

Acceptable material for doors, windows and trim:

- Wood

### **Color**

Acceptable paint schemes:

- Natural shingle siding and painted trim.
- Painting siding one color and painting trim a second color.
- Painting siding and trim white or another very light neutral color.

In considering colors the Board will apply the following guidelines:

- A small building can have greater latitude with color than a large building.
- The color of a large building should help to make it blend into the environment.
- Color should contribute to the streetscape and district as a whole and not draw undue attention to one building.

### **Proportion and arrangement of windows**

- When a building has a use where a “storefront” is not appropriate, first floor windows are recommended to be residential in scale and proportion. Window and door area should be at least 50% of the width of the front facade.
- Second-floor windows should have a residential character, vertical proportions and an orderly spacing.

### **Storefronts**

- Even though this is a highway district, existing storefronts are oriented toward shoppers approaching from a parking lot rather than to motorists on the highway. This is a characteristic that should be retained.
- Storefronts may have a simpler treatment but should have the features, relationships and proportions of the traditional storefronts found in the Village Center District (see page 9).

### **Building details**

- The design and detailing of building facades should be oriented toward persons approaching from a parking lot rather than to motorists on the highway.

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## GUIDELINES FOR THE SITE

### Allocation of open space

Open space should be distributed to achieve the best balance between the following priorities:

- A setback from Montauk Highway of 25'.
- A setback from Cove Hollow Road and Buckskill Road of 15'.
- Separation between buildings to contribute to an open setting.
- Separation between buildings sufficient to allow wood exteriors by the requirements of the building code.
- A setback for parking areas.
- A buffer zone adjoining a property in a residential district.

### Landscaping

Landscaping can play a major role in enhancing the compatibility of this highway commercial district with the character of the Village.

Appropriate treatments include:

- Shade trees along the roads and in open spaces within the properties.
- Lawn settings in front of the buildings and parking area with shrubs and planting beds at the natural grade.

### Parking

- As a general principle, parking should be to the rear of the building.
- In some instances the Design Review Board may determine that placing all or some parking to the side may allow a building that better meets the guidelines for this district. Parking should be set back 25' from Montauk Highway and 15' from Cove Hollow Road and Buckskill Road.

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## SIGNS

These guidelines are in addition to Village Code requirements that pertain to signs.

- Signs should be designed to be compatible with the surroundings and appropriate to the architectural character of the buildings on which they are placed. Sign panels and graphics should relate with and not cover architectural features, and should be in proportion to them.
- Layout should be orderly and graphics should be of simple shape, such as a rectangle, circle or oval.
- The number of colors used should be the minimum consistent with the design.
- Illumination should be minimal and appropriate to the character of the sign and surroundings. Neon signs are not permitted. Directly illuminated, which is considered to mean internally illuminated, signs are not permitted.
- Groups of related signs should be compatible and create a sense of harmonious appearance.
- On a historically or architecturally unified building containing two or more commercial uses, the Design Review Board may require signage to be uniform in style and color.
- Awning graphics shall be a single line or lettering applied directly to the awning fabric and shall be only the name of the enterprise or premises.



## AWNINGS

These guidelines are in addition to Village Code requirements that pertain to awnings.

- Awnings of one solid color are preferred.
- An individual awning for each framed unit of the storefront is preferred.
- Awnings should be set below a signboard
- When a display window has a transom, the awning should not extend below the transom bar.
- An awning should fit the scale and proportions of the building facade and of the elements to which it will relate.
- Awnings should not cover important features of a building.
- Retractable awnings are preferred in the Village Center District.

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**MECHANICAL SYSTEMS, UTILITIES, TRASH RECEPTACLES  
AND SERVICE AREAS**

- Any service area, loading dock, storage area, mechanical equipment, transformer, HVAC unit, utility panel, utility meter or trash receptacle should be on the rear wall, on a secondary side wall or be located to the rear of the property.
- HVAC units may be placed on the roof providing they are not visible from the ground.
- When the Design Review Board determines that rooftop units require screening, the screen should be set back from the cornice or the edge of the roof and should not diminish the appearance of the building
- Mechanical equipment, transformers, HVAC units and utility panels or meters should be located so that they do not need to be screened from view from the street or from residential properties.
- When the Design Review Board determines that screening is necessary or desirable, acceptable screening may include planting or a combination of a fence and plantings. With the exception of enclosures for trash receptacles, solid fences or walls should be avoided. Chain link fences should be avoided. Wood enclosures for trash receptacles are preferred.