

The drawing consists of two parts: a PLAN view and a cross-section view labeled SECTION B-B.

**PLAN View:**

- Shows a central **6" CONCRETE APPROACH** flanked by **4" THICK SIDEWALK** sections.
- Dimensions include **10' MIN.** and **18' MAX.** for the approach width, and **4' TYP.** for the sidewalk width.
- A **1'-0" TRANSITION TYP.** is shown at the edge of the approach.
- Section lines **A-A** and **B-B** are indicated.
- A **PULL OFF PAD** is shown at a **45°** angle, with a note **60° PREFERRED**.
- SAW CUT** locations are marked at the corners of the concrete approach.
- Expansion joints are marked with asterisks (\*).

**SECTION B-B:**

- Shows the cross-section of the curb and gutter.
- The curb height is **6"**.
- The gutter width is **12"**.
- A **SAW CUT** is indicated at the top of the curb.
- A note refers to **SECTION A-A FOR DEPRESSED CURB HEIGHT**.

**Legend:**

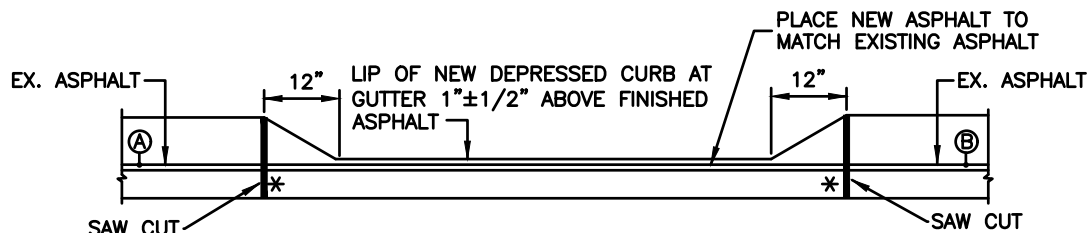
- \* 1/2" EXPANSION JOINT MATERIAL

SECTION A-A



Diagram illustrating the cross-section of a driveway and curb area, labeled SECTION A-A.

Key dimensions and features shown:

- TOP OF DEPRESSED CURB**: Indicated by a vertical line and arrow.
- DRIVE SURFACE**: The sloped area leading to the driveway.
- DRIVE APPROACH/TREE LAWN WIDTH**: The horizontal distance from the curb to the driveway.
- MATCH EXISTING**: The point where the new construction meets the existing driveway.
- SIDEWALK WIDTH**: The width of the sidewalk area.
- SAW CUT**: Indicated by a vertical line and arrow.
- R/W TYP.**: Right of Way Typical.
- Dimensions**:
  - 7'-1/2" (vertical distance from curb top to driveway surface)
  - 21" (horizontal distance from curb face to start of drive approach)
  - 8'-1/2" (vertical distance from curb top to driveway surface)
  - 24" (horizontal distance from curb face to match existing)
  - 1%-2% (slope of the driveway surface)



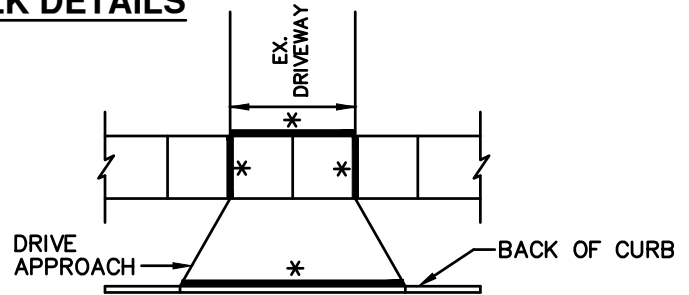
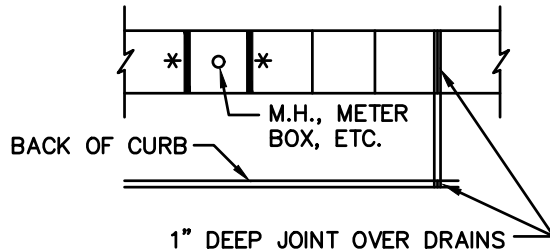
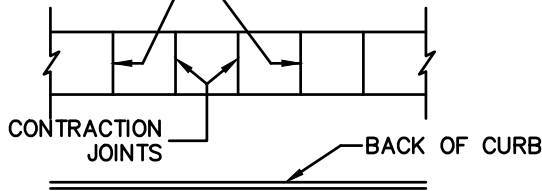
THIS SHEET IS A GUIDE FOR CONTRACTOR'S USE. CITY SPECIFICATIONS OR CITY ENGINEER'S DISCRETION MAY SUPERSEDE THIS SHEET.

STANDARD DRAWING			CONTRACTOR GUIDE SHEET FOR SIDEWALK AND DRIVE APPROACH	APPROVAL 	
DATE	REVISION	BY		CITY ENGINEER	
			 City of Kettering	JANUARY 2020	
				SHEET 1 of 2	
				SEE BACK	

## SIDEWALK DETAILS

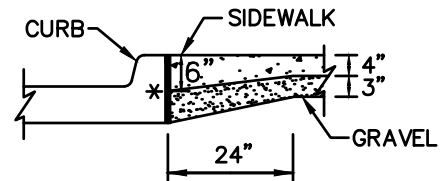
JOINTS 1" DEEP EVERY  
15' FINISHED WITH A  
RADIUS OF 3/8"

JOINT SPACING = 5'



\* 1/2" EXPANSION JOINT MATERIAL

## WALK ADJACENT TO CURB DETAIL



(TO BE USED WHERE CURB & SIDEWALK ARE ADJOINING)

## SIDEWALK NOTES

1. SIDEWALK SHALL BE A MINIMUM OF 5' WIDE OR MATCH EXISTING ON 50' R/W STREETS, OR WIDTH AS DIRECTED BY THE ENGINEER.
2. SIDEWALK SHALL BE 4" THICK, EXCEPT WHEN THROUGH DRIVEWAYS (6" THICK), AND WHEN ADJACENT TO CURB (VARIABLE THICKNESS AS SHOWN ABOVE).
3. EXPANSION JOINT MATERIAL OR APPROVED EQUAL, SHALL BE PLACED AS SHOWN. A MAXIMUM SPACING OF 50' FOR NEW CONSTRUCTION, BOTH SIDES OF EXISTING CONCRETE FOR REPLACEMENT SECTIONS EQUAL TO OR GREATER THEN 15' OR ONE SIDE OF EXISTING CONCRETE FOR REPLACEMENT SECTION LESS THEN 15'.
4. THE SUBGRADE BENEATH THE PROPOSED SIDEWALK IS TO BE COMPACTED TO THE MAXIMUM EXTENT PRACTICAL. A MINIMUM OF 3" OF GRAVEL (OR CRUSHED AGGREGATE) BASE SHALL BE PLACED UNDER THE SIDEWALK UNLESS DETERMINED UNNECESSARY BY THE ENGINEER.
5. CONCRETE SHALL MEET SPECS K-499 AND SHALL ONLY BE SUPPLIED THROUGH A CITY OF KETTERING APPROVED READY MIX SUPPLIER.
6. SIDEWALKS SHALL BE SLOPED BETWEEN 1 AND 2% TOWARD THE STREET, OR AS APPROVED BY THE ENGINEER.
7. ANY TOOL MARKS LEFT BY EDGING SHALL BE ELIMINATED BY TEXTURING THE SURFACE. THE FINAL SURFACE SHALL BE BROOM FINISHED. RETOOL JOINTS SHALL NOT BE CONSTRUCTED UNLESS APPROVED BY THE ENGINEER.

## DRIVE APPROACH NOTES

1. MAXIMUM JOINT SPACING SHALL BE 8 FEET LONGITUDINALLY AND TRANSVERSELY.
2. THE FLARE ON RESIDENTIAL DRIVE APPROACHES SHALL BE A MINIMUM OF 4 FEET. ON RESIDENTIAL STREETS WHERE THE TREE LAWN IS LESS THAN 4 FEET, THE FLARE SHALL BE A 45° ANGLE. ON ALL OTHER STREETS (I.E. NON-RESIDENTIAL) WHERE THE TREE LAWN IS LESS THAN 8 FEET, THE FLARE SHALL BE A 45° ANGLE. IF THE TREE LAWN IS GREATER THAN 8 FEET THE FLARE SHALL BE AT THE DISCRETION OF THE ENGINEER.
3. THE SUBGRADE BENEATH THE PROPOSED DRIVE APPROACH IS TO BE COMPACTED TO THE MAXIMUM EXTENT PRACTICAL. A MINIMUM OF 3" OF GRAVEL (OR CRUSHED AGGREGATE) BASE SHALL BE PLACED UNDER THE DRIVE APPROACH UNLESS DETERMINED UNNECESSARY BY THE ENGINEER.
4. CONCRETE SHALL MEET SPECS K-499 AND SHALL ONLY BE SUPPLIED THROUGH AN APPROVED READY MIX SUPPLIER.
5. WHERE ASPHALT CONCRETE PAVEMENT IS DISTURBED, THE ASPHALT SHALL HAVE A SAWED EDGE, BE REPLACED AND SEALED AS DIRECTED BY THE ENGINEER.
6. SAW CUT CURB AT BOTH ENDS. SAW CUT SHALL HAVE CLEAN AND STRAIGHT EDGES, AND SHALL BE THE FULL DEPTH OF THE CURB.
7. FOR CURB MONOLITHIC WITH CONCRETE STREET, SAW CUT PAVEMENT 21" FROM BACK OF CURB. SAW CUT TO HAVE CLEAN AND STRAIGHT EDGES, AND SHALL BE FULL DEPTH OF CURB AND PAVEMENT.
8. EXPANSION JOINT MATERIAL SHALL BE PLACED AS SHOWN ON SHEETS 18-21.
9. ANY TOOL MARKS LEFT BY EDGING SHALL BE ELIMINATED BY TEXTURING THE SURFACE. THE FINAL SURFACE SHALL BE BROOM FINISHED. RETOOL JOINTS SHALL NOT BE CONSTRUCTED UNLESS APPROVED BY THE ENGINEER.
10. IF AN EXISTING SIDEWALK IS PRESENT THROUGH THE DRIVE, THE THE DRIVE APPROACH SHALL BE PLACED IN CONCRETE.
11. PATCH REPAIRS MAY ONLY BE USED AS A TEMPORARY MEASURE UNTIL REPLACEMENT IS POSSIBLE. ONCE PART OF A DRIVE APPROACH IS EXCAVATED, THE FULL DRIVE APPROACH IS TO MEET CURRENT SPECIFICATIONS. NO EXCAVATION MAY BE LESS THAN AN AREA CREATED BY STANDARD JOINT CONSTRUCTION. INTEGRAL DRIVE APPROACHES ARE NOT PERMITTED.
12. SEPARATE CURB IS REQUIRED FOR BOTH CONCRETE OR ASPHALT DRIVE APPROACHES IF THE STREET IS CURBED.

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### STANDARD DRAWING

DATE	REVISION	BY

### CONTRACTOR GUIDE SHEET FOR SIDEWALK AND DRIVE APPROACH



City of Kettering

APPROVAL

*John E. Ryan*  
CITY ENGINEER

JANUARY 2020

SHEET  
2 of 2  
SEE FRONT