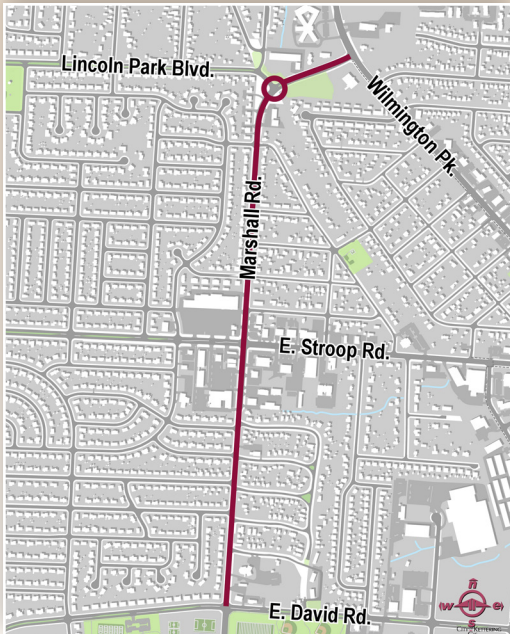




## Marshall Road Improvements PID 116891



**The City of Kettering proposes  
to improve Marshall Road from  
Wilmington Pike to East David Road.**

### What does this project involve?

- Resurfacing Marshall Road from Wilmington Pike to East David Road.
- Constructing an 11-foot wide sidewalk along the south side of Marshall Road from Wilmington Pike to Lincoln Park Blvd, and along the north side of Lincoln Park Blvd from Marshall Road to North Marshall Road.
- Modifying the intersection at Marshall and Lincoln Park to improve safety and better accommodate pedestrians and bicyclists.
- Upgrading curb ramps to meet Americans with Disabilities Act (ADA) requirements along Marshall from Stroop Road to Wilmington Pike.
- Repairing/replacing deteriorated curb and drive approaches, as needed.

### Why is this project needed?

#### The purposes of this project include:

- Resurfacing and remarking deteriorated pavement on Marshall Road.
- Improving safety at the Marshall Road and Lincoln Park Blvd intersection.
- Improving active transportation options in this area of Kettering by making safer corridors and connections for bicyclists and pedestrians.

### How much will this project cost, and how is it funded?

The current estimated project cost, including both right-of-way acquisition and construction, is approximately \$2.3 million dollars, costing the City \$1.3 million dollars after federal funding. Additional grant funding will be sought to reduce the City's cost for the project.

### Who proposed this project and who is responsible for oversight?

The City applied for funds for the project from the Miami Valley Regional Planning Commission (MVRPC) and was awarded Federal Surface Transportation Program and Transportation Alternatives Program funds. The City is managing the project and the Ohio Department of Transportation (ODOT) is providing oversight.

### What intersection modifications are being considered at Marshall Road and Lincoln Park Blvd?

The City will complete a detailed feasibility study analyzing a range of alternatives, including a modern roundabout, traffic signal and all-way stop at this intersection.

### Why is the City considering improvements beyond addressing the deterioration of the roadway?

Consistent with industry best practices, the City is exploring alternatives that best address the overall needs within the corridor, including improved safety and improved connectivity for all roadway users.

### Traffic on Marshall Road is not currently delayed at the intersection with Lincoln Park Blvd. Will the various alternatives delay traffic on Marshall Road and cause congestion?

A feasibility study will assess alternatives, including a capacity analysis to ensure the selected alternative will not negatively impact traffic movement through the intersection.

### Why is the City considering a roundabout at Marshall Road and Lincoln Park Blvd?

**Roundabouts have many safety benefits, including:**

- Reducing the number of conflict points for both drivers and pedestrians.
- Creating a low speed environment at the intersection, reducing the likelihood of high speed crashes.
- Preventing high-angle crashes such as "T-bone" and left turn crashes. Lower angle, low speed crashes tend to be less severe.
- Experiencing fewer and less severe crashes than traffic signals.
- Promoting continuous and efficient operations, with fewer stops, fewer hard accelerations and reduced delays for left turns.

### If selected as a preferred alternative, how would a roundabout fit into the existing intersection?

Roundabouts have flexibility in terms of placement and can be shifted to avoid or minimize impacts to adjacent properties.

### How will large and emergency vehicles be able to negotiate a roundabout?

The design of the intersection will allow semi-trucks, school buses and other large vehicles to navigate the roundabout while providing adequate visual and physical indicators to guide and slow passenger vehicles.

### Where can I find more information about roundabouts?

Additional information is available from the Federal Highway Administration at [www.safety.fhwa.dot.gov/intersection/innovative/roundabouts](http://www.safety.fhwa.dot.gov/intersection/innovative/roundabouts).

### How will a roundabout affect traffic speeds and noise levels?

It is anticipated that traffic speed and noise will be reduced with construction of a roundabout.

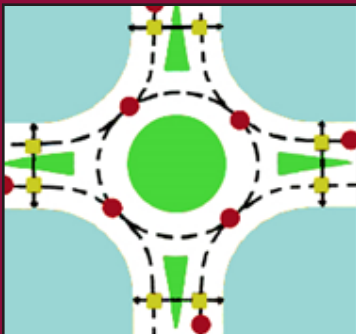
### Since there are already sidewalks, why are bike lanes not being considered instead of a widened sidewalk?

Off street bicycle facilities better accommodate young and inexperienced riders compared to on-street facilities. The traffic volumes and speed limit on Marshall Road warrant shared-use traffic lanes that mainly favor experienced adult bicyclists.

### What right-of-way acquisition is required for this project?

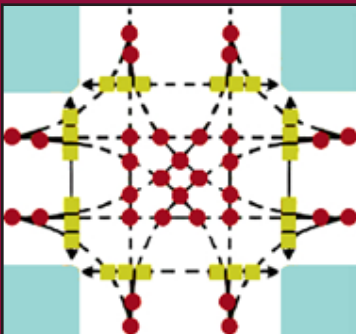
Most improvements will occur on land already in existing right-of-way or otherwise owned by the City of Kettering. Minor right-of-way acquisition may be necessary for construction.

## Roundabout



● 8 Vehicle conflicts  
■ 8 Pedestrian conflicts

## Intersection



● 32 Vehicle conflicts  
■ 24 Pedestrian conflicts



Will the project affect historic resources?

An evaluation of the corridor for historic resource and the project's impact on any identified historic properties will be undertaken as part of the environmental review of the corridor. At this time, there are no properties within the corridor that are listed on, or known to be eligible for, the National Register of Historic Places.

If you are concerned that the project will affect historic properties and would like to have an opportunity to comment on our decision-making regarding historic properties, please email **Tricia Bishop** with ODOT District 7 at [tricia.bishop@dot.ohio.gov](mailto:tricia.bishop@dot.ohio.gov).

What types of amenities will be offered?

The proposed widened sidewalk and ADA compliant curb ramps will improve mobility for active transportation and persons with disabilities. To improve safety, conventional street lights will be constructed with the proposed modifications to the Marshall and Lincoln Park intersection.

What is the schedule for the project?

A final decision regarding the preferred alternative is expected in early 2024. Right-of-way acquisition, based on the selected alternative, is expected to begin in mid-2024. Under the current schedule, construction is expected to begin in 2026.

Why is the project not being undertaken sooner?

In addition to providing time for data collection, project design and quality control review, the schedule must also provide for public involvement, environmental review, environmental permitting, right-of-way negotiations and utility relocations. Federal funding for construction is available is 2026.

How will traffic on Marshall Road and cross streets be maintained during construction?

Traffic may be detoured around the Marshall and Lincoln Park intersection during construction, using Wilmington Pike and East Stroop Road as the detour for Marshall traffic, and Ackerman Blvd, East Stroop Road, and Wilmington Pike for Lincoln Park traffic. Resurfacing Marshall Road will require temporary lane closures, and traffic will be maintained during this phase of the project.

Will sidewalk access be maintained during construction?

Portions of the sidewalk system will need to be closed during curb ramp upgrades. Curb ramp construction will be phased to ensure that access to the sidewalk along one side of Marshall is maintained at all times.

Why are funds not being used elsewhere, on more urgently needed improvements?

This project is being funded with non-transferrable Federal Surface Transportation Program funds and Transportation Alternatives funds awarded to the City by MVRPC.

Will the project be ADA-compliant?

All pedestrian facilities within the corridor will be evaluated and upgraded, as needed, for ADA-compliance.

Will property access/driveway configurations be affected?

Driveway approaches right-of-way will be replaced, as needed. Access to properties will be maintained while the project is under construction.

Will trees be removed under this project?

Limited tree removal will be needed to construct the project.

How will utilities be impacted by this project? When will utility relocations occur?

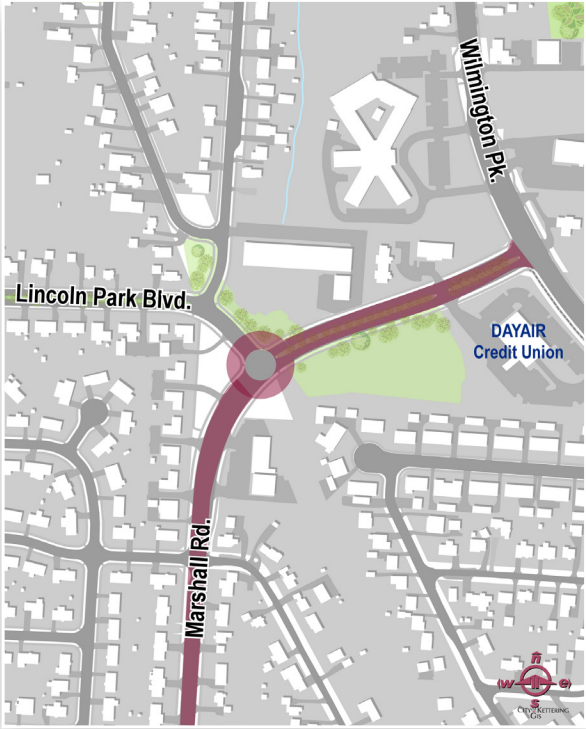
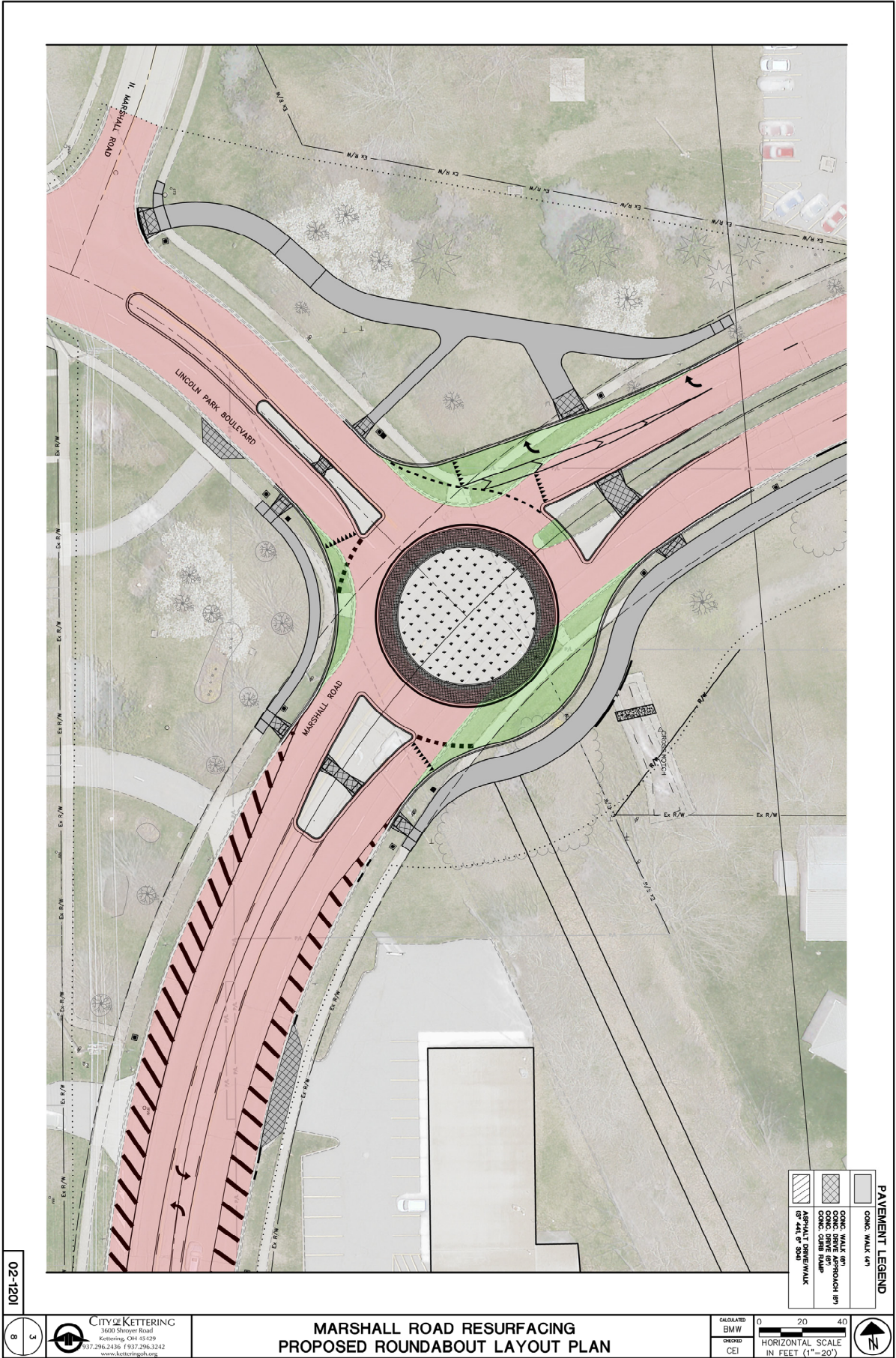
Impacts to utilities are expected to be limited to the street lighting and storm sewers along Marshall Road. Any necessary utility adjustments will be completed prior to or during construction of the project.

What is the current status of decision-making on this project?

No final decisions regarding proposed improvements have been made at this time. We are seeking public input to ensure the best possible decisions are made for our community.

How can I submit comments?

Email, mail or call one of the individuals in the contact box. Please reference “Marshall Road Improvement Project” in any emails or letters. Comments are requested no later than December 31, 2023.



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