

APPENDIX – SURVEY #1 RESPONSES

Q1 & Q2: Residency and Age



Resident of Miamisburg



Visitor to Miamisburg



Own or operate a business in Miamisburg



Work but don't live in Miamisburg



Other



Age 65+



Ages 55-64



Ages 45-54



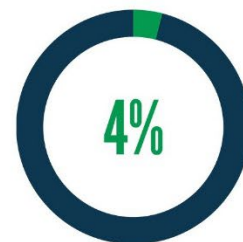
Ages 35-44



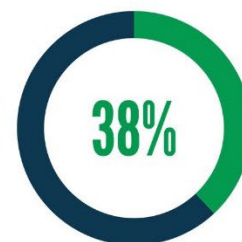
Under 35

Q3: Satisfaction w/ Current System

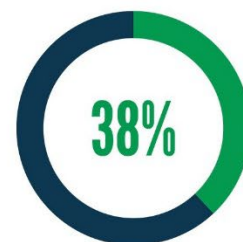
HOW SATISFIED ARE YOU WITH THE TRANSPORTATION SYSTEM IN MIAMISBURG?



Very satisfied



Satisfied



Neutral



Unsatisfied

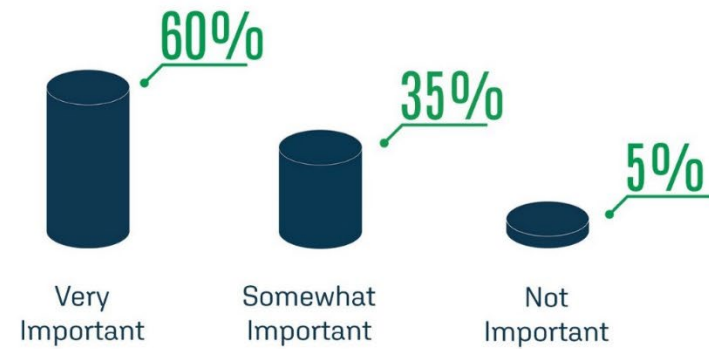


Very unsatisfied

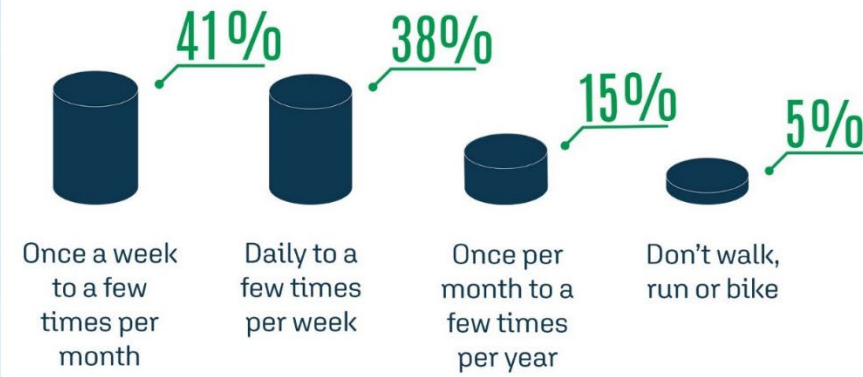
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Q4 & Q7: Non-Motorized Transportation

HOW IMPORTANT IS HAVING TRANSPORTATION OPTIONS (BIKING, WALKING, TRANSIT, ETC.)?



HOW OF DO YOU WALK, RUN, OR BIKE IN MIAMISBURG?



Q4 & Q7: Non-Motorized Transportation

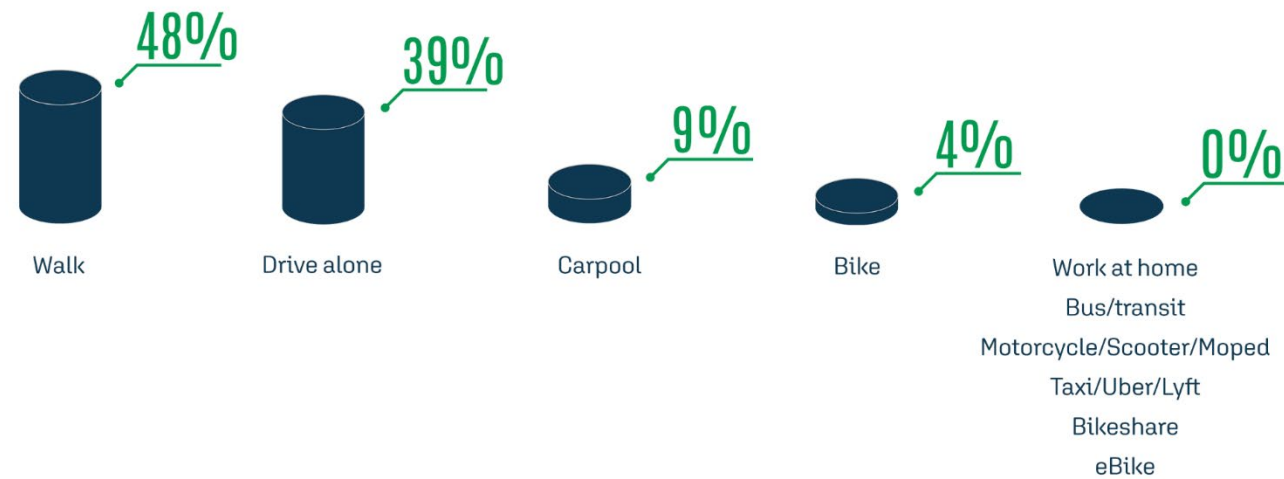
WHAT FACTORS CONTRIBUTE TO YOU NOT WALKING OR USING A BIKE AS A TRAVELING OPTION?



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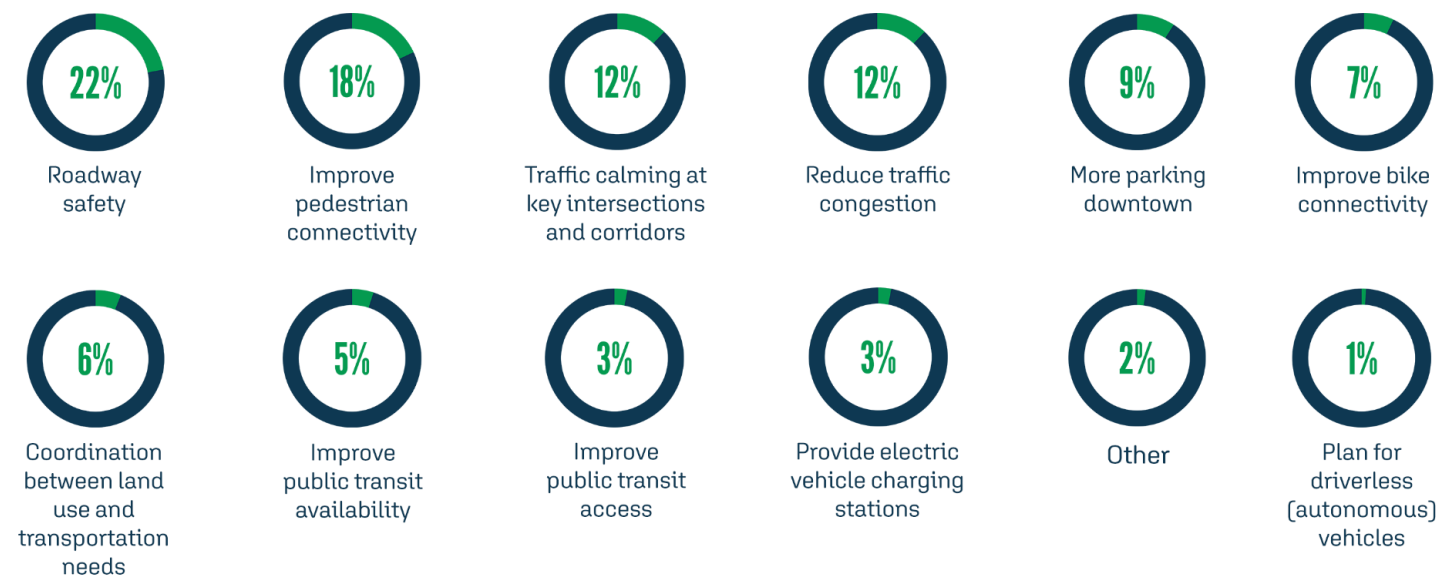
Q5: Popular Modes of Transportation

WHICH OF THE FOLLOWING TRANSPORTATION MODES DO YOU NORMALLY USE IN A TYPICAL WEEK?



Q6: Most Important for Transportation

WHAT ARE THE MOST IMPORTANT THINGS TO CONSIDER FOR THE FUTURE OF MIAMISBURG TRANSPORTATION FACILITIES?



Q9: Comments

Topics for comments included:

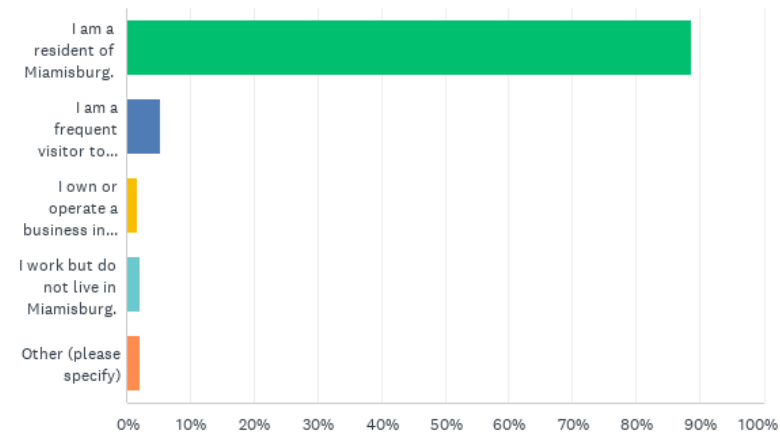
- Connectivity
- Transit
- Emerging Technology
- Maintenance
- Speed
- Safety
- Multi-modal Accommodations
- Intersection Improvements
- Parking
- Traffic

The top 4 categories were:

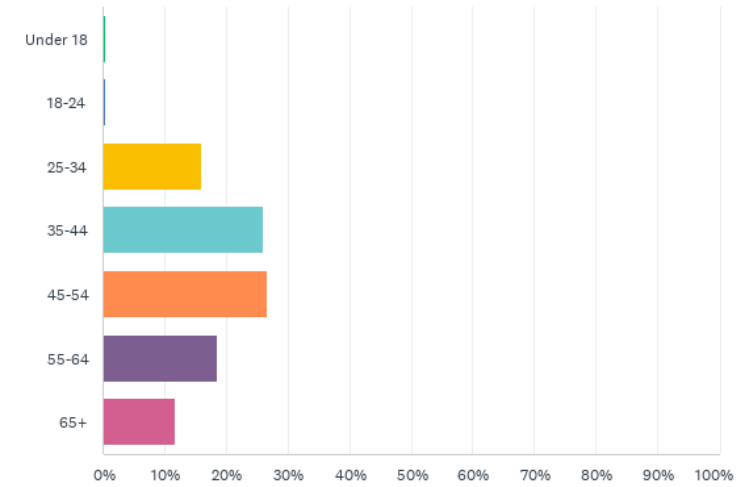
- Connectivity
- Safety
- Multi-Modal Accommodations
- Intersection Improvements

APPENDIX – SURVEY #2 RESPONSES

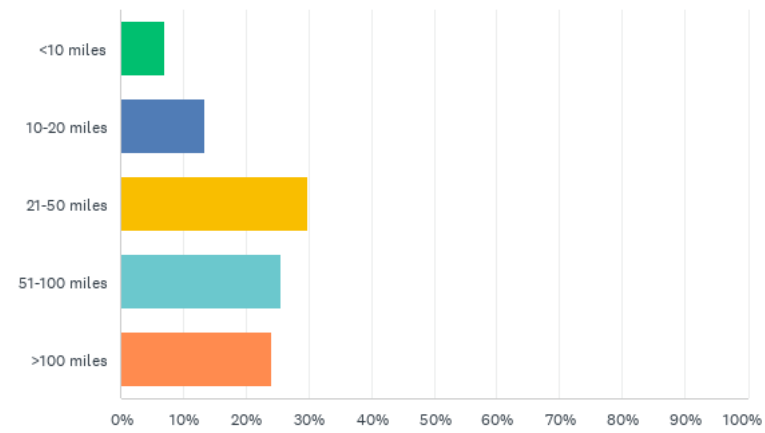
Q1 Check all that describe you.



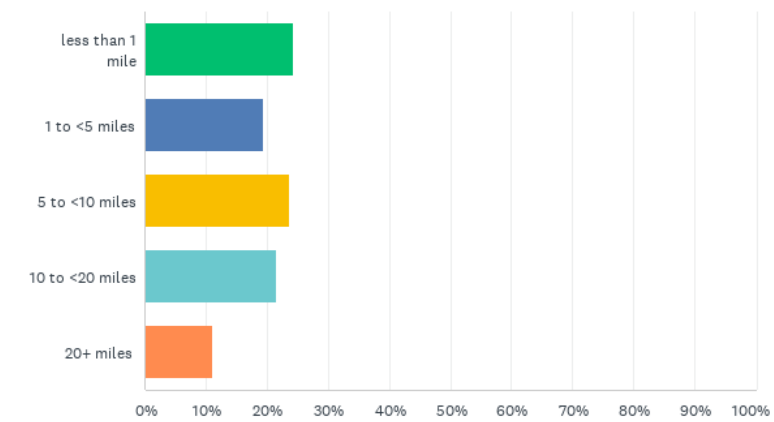
Q2 What is your age?



Q3 How many miles do you drive a vehicle per week?

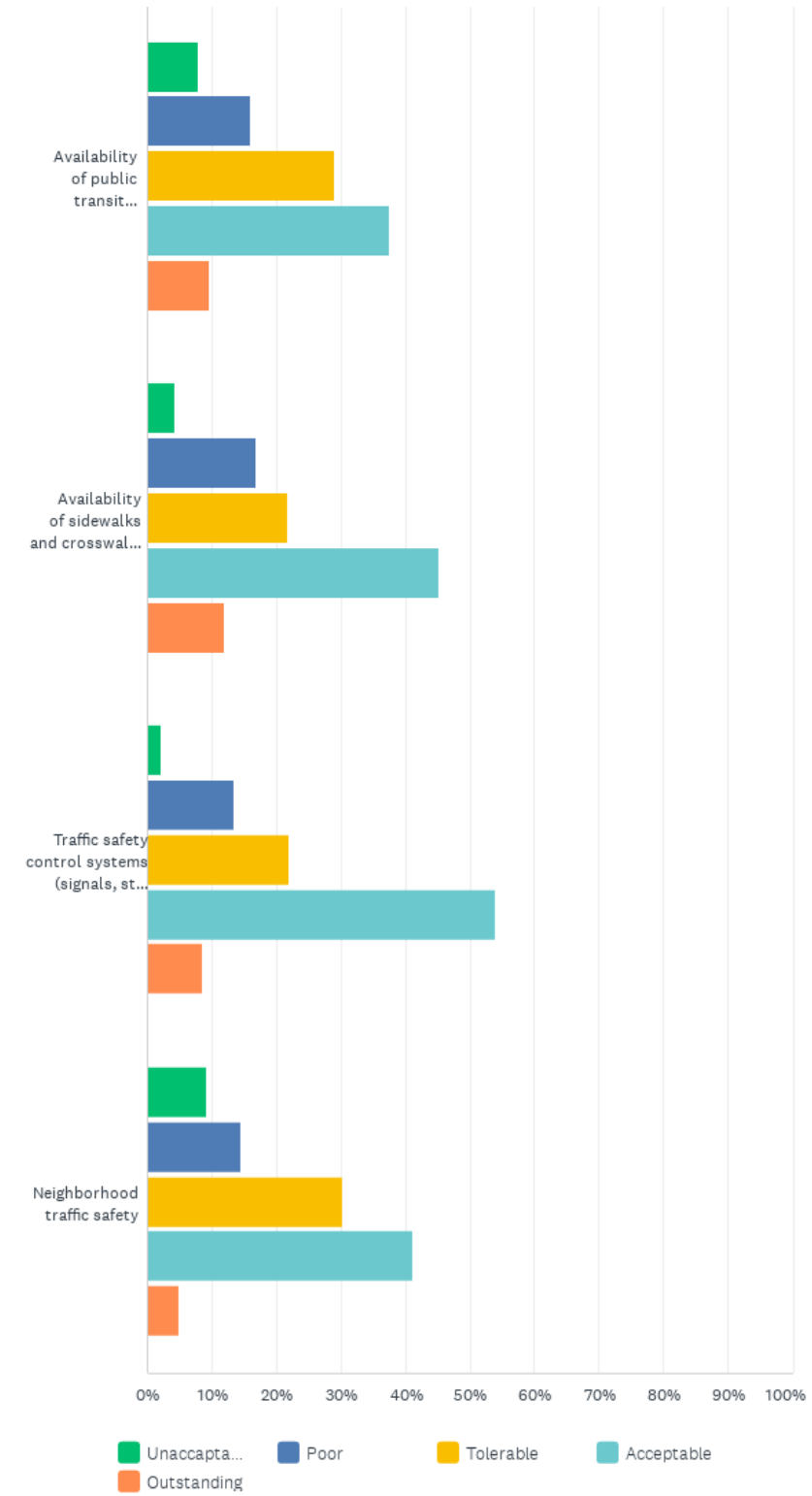
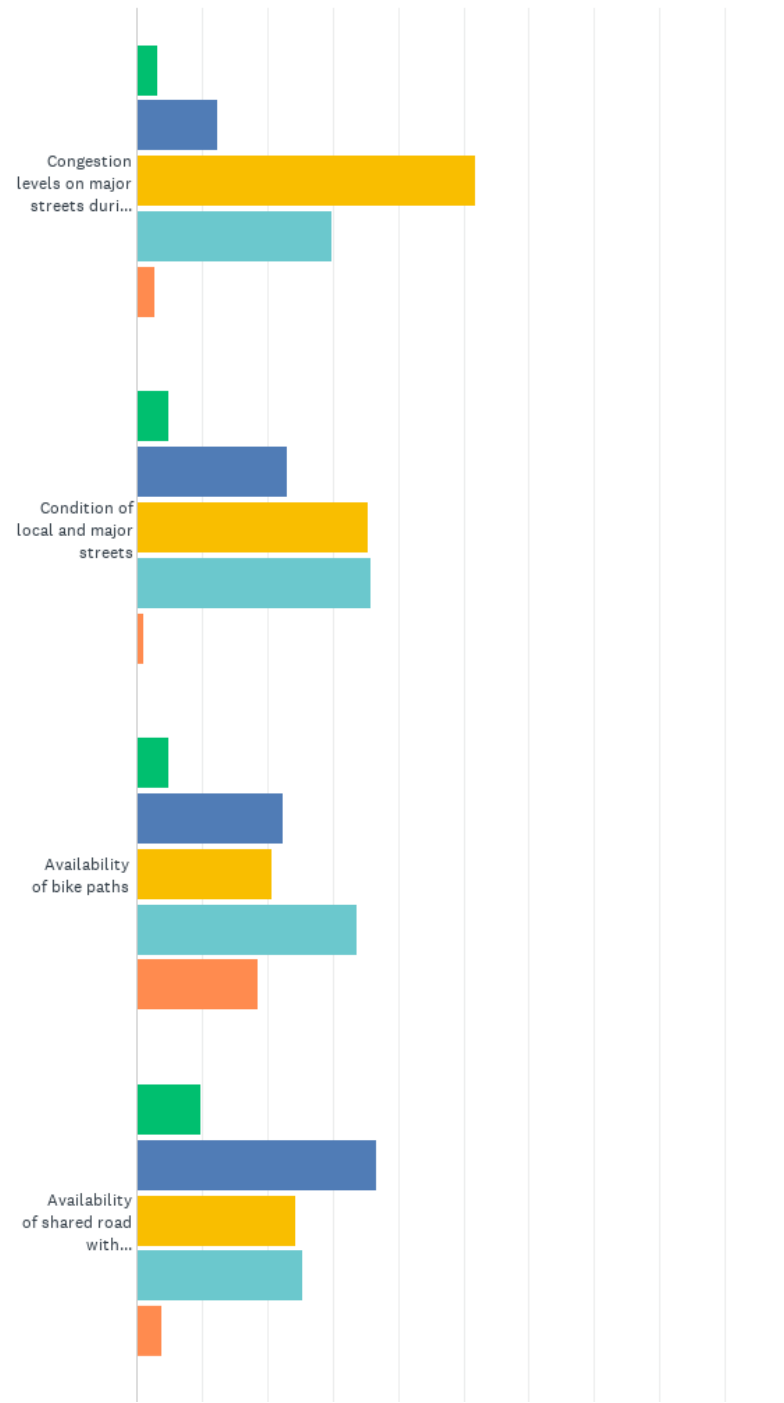


Q4 How far is your commute to work?



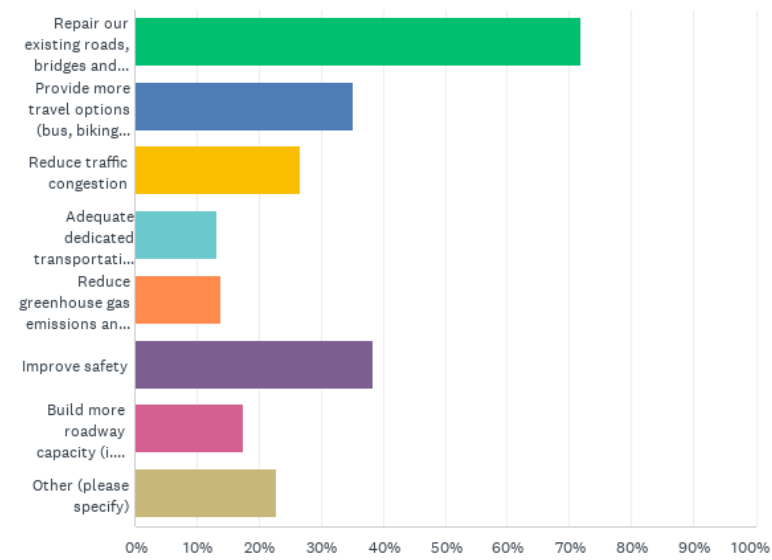
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Q5 Rate the following aspects of the transportation system in Miamisburg area. (Not an issue, Acceptable, Tolerable, Poor, Unacceptable)

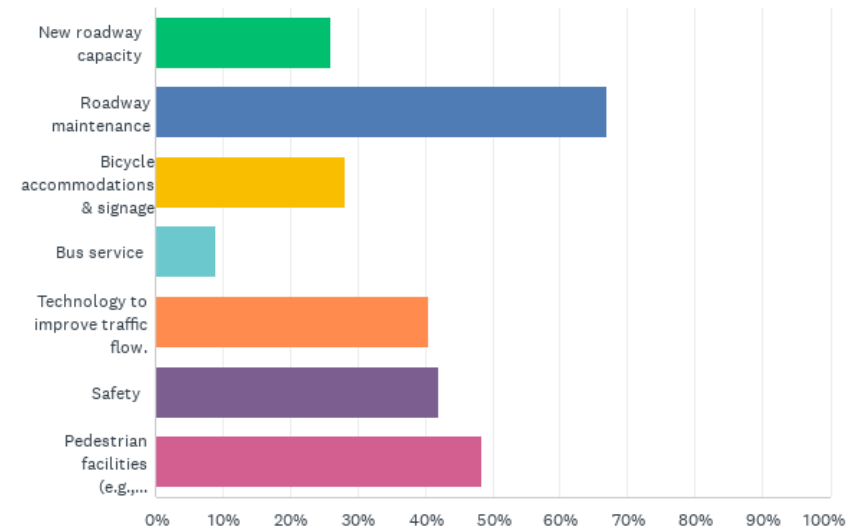


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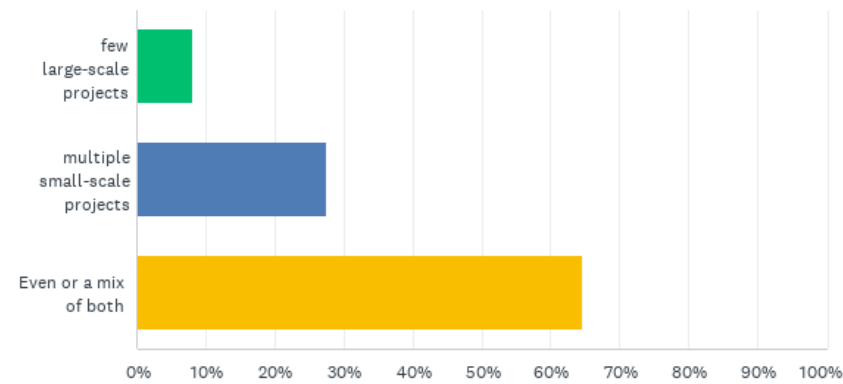
Q6 What do you think is the most important transportation need in Miamisburg today?
Choose three (3)



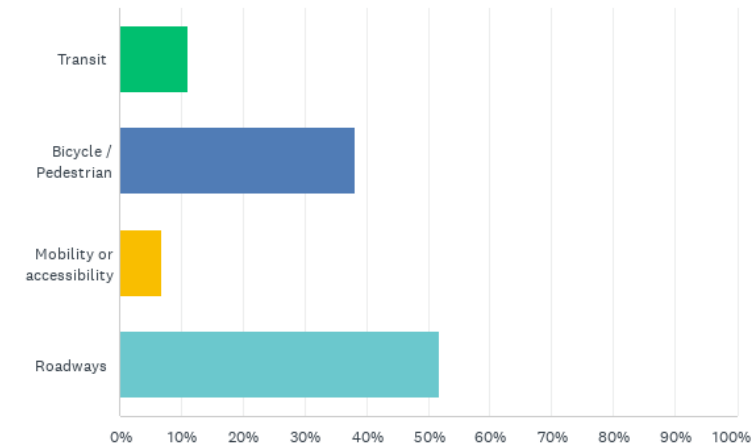
Q7 Which THREE of the items listed in the below would you be MOST WILLING to fund with your tax dollars?



Q8 During the next 25 years, would you focus funding on a few large-scale projects or multiple small-scale projects?

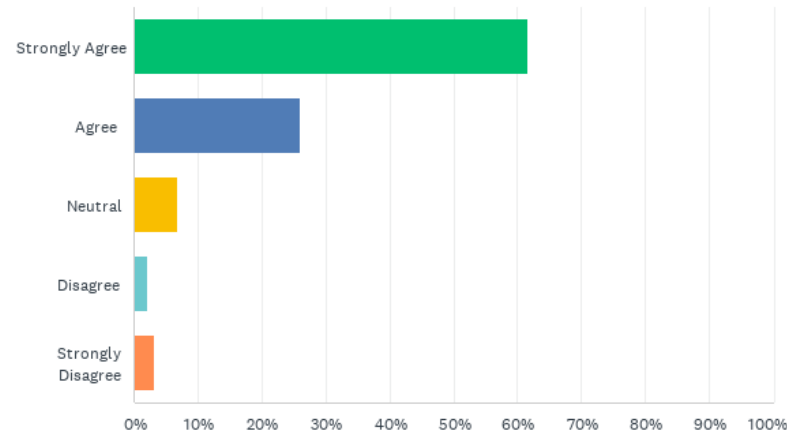


Q9 What personal need of yours is not being met by Miamisburg's transportation system?

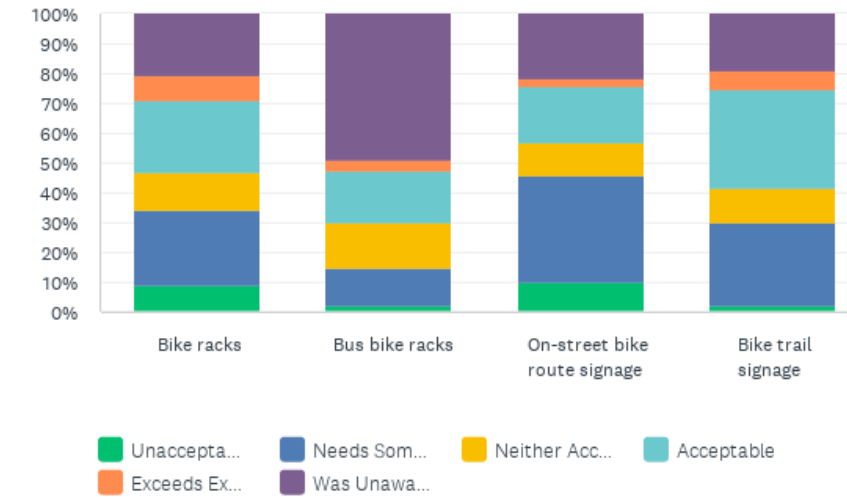


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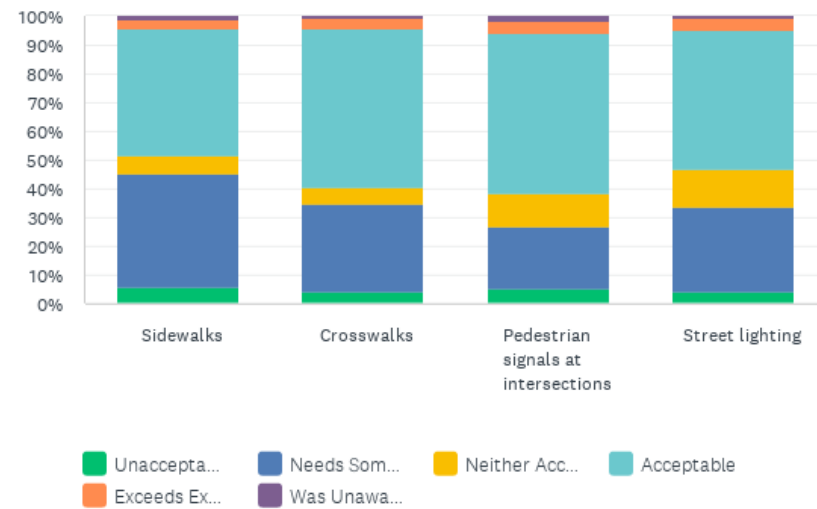
Q10 Please rate your level of agreement with the following statement: "A public street serves the community best when people in cars, on bicycles, using public transportation, or walking all feel safe and welcomed." (Strongly agree, Agree, Neutral, Disagree, Strongly Disagree)



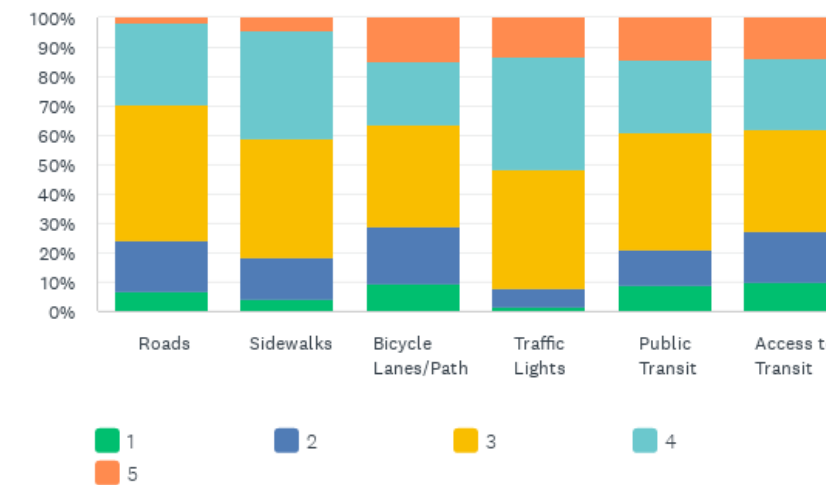
Q11 Rate the existing bicycle infrastructure in Miamisburg. (Was unaware, Unacceptable, Needs some improvement, Neither acceptable or unacceptable, Acceptable, Exceeds expectations)



Q12 Rate the existing pedestrian infrastructure in Miamisburg. (Was unaware, Unacceptable, Needs some improvement, Neither acceptable or unacceptable, Acceptable, Exceeds expectations)

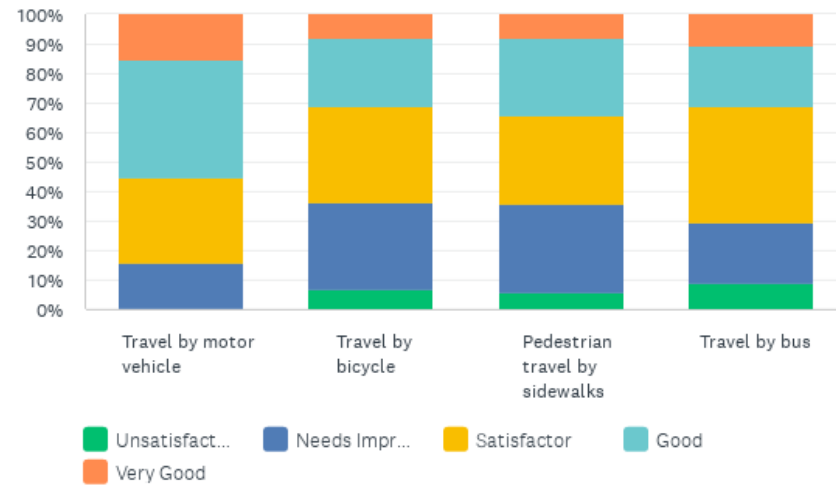


Q13 Rate the physical condition of the following facilities or services (1 = poor and 5 = excellent).

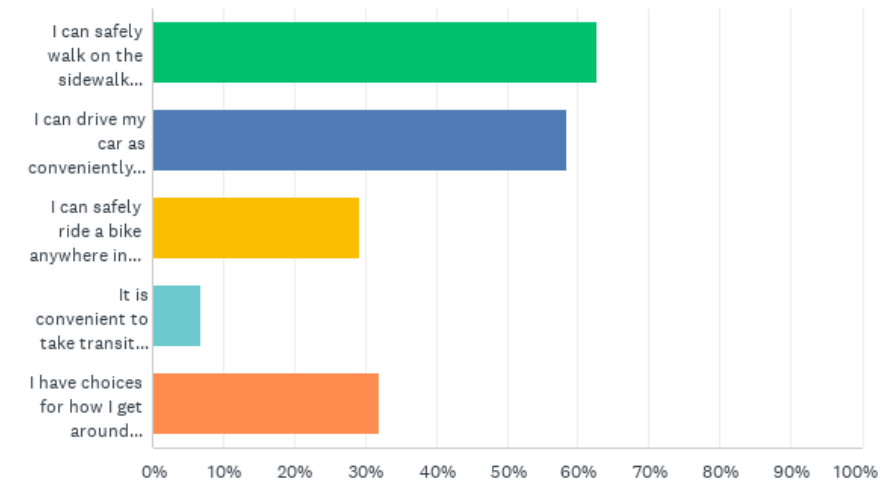


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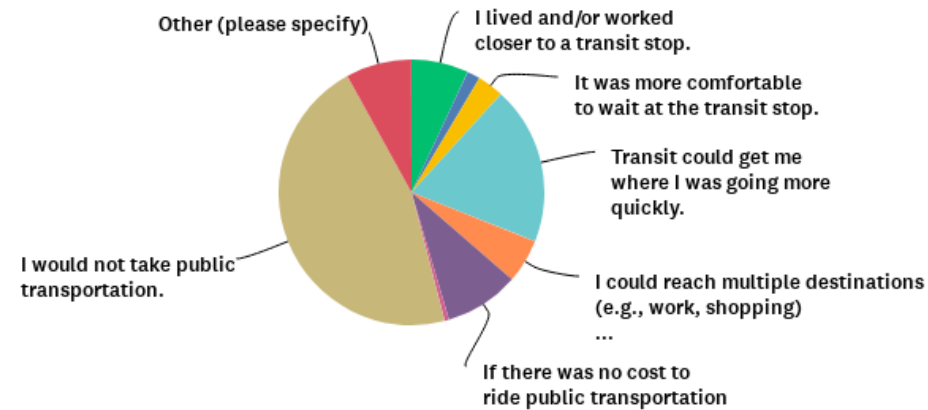
Q14 How do you rate the quality and ease of use of each available mode of transportation in Miamisburg? (Very Good, Good, Satisfactory, Needs Improvement, Unsatisfactory)



Q15 Which TWO of the items listed in below are MOST IMPORTANT to you and the members of your household?

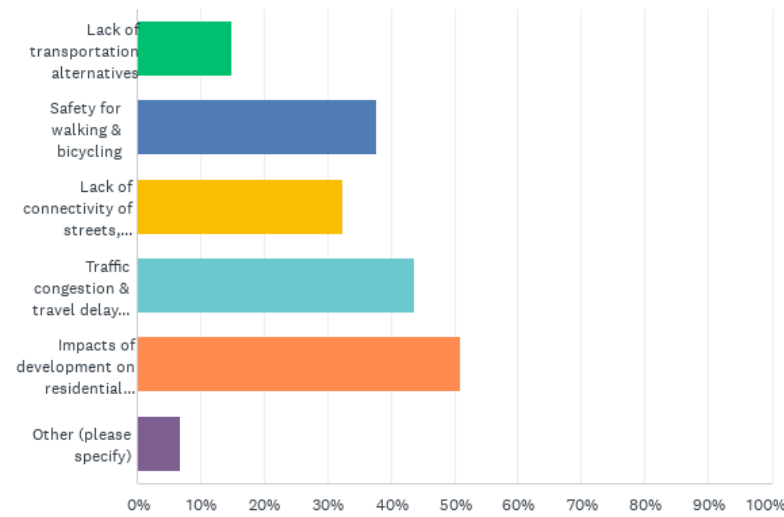


Q16 Please indicate which ONE of the following items would most increase the likelihood that you would take public transportation. I would be more likely to take public transportation if...

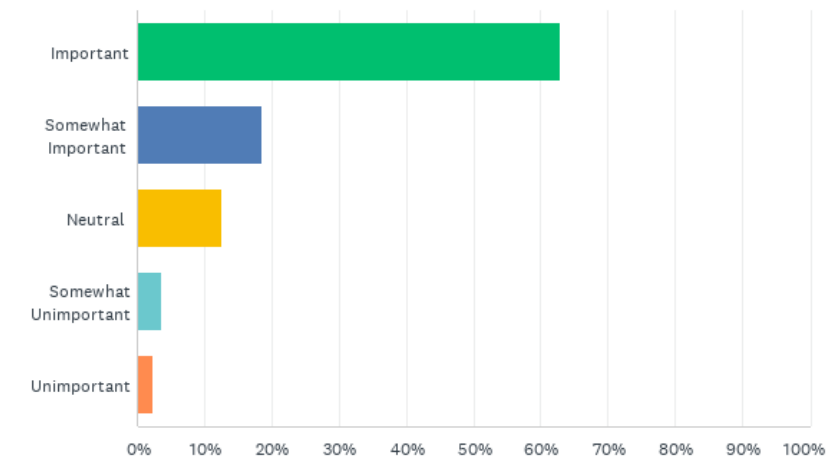


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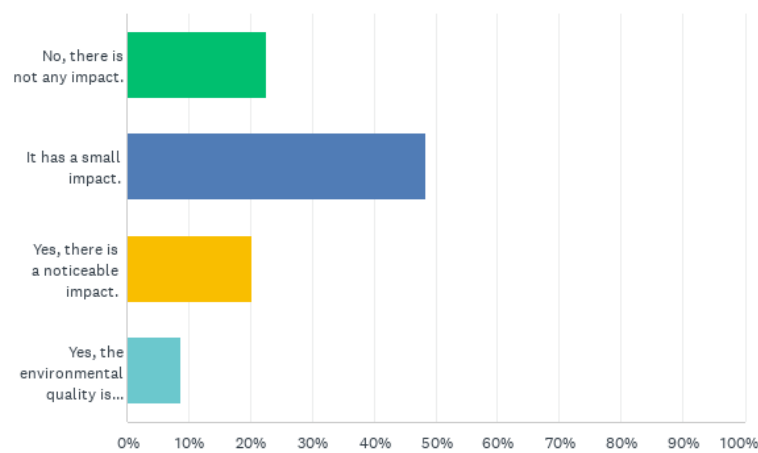
Q17 Which TWO items below are the MOST PRESSING TRANSPORTATION CHALLENGES facing Miamisburg over the next 5-10 years?



Q18 Please indicate how important it is for Miamisburg to work together with other jurisdictions (such as neighboring communities, Montgomery County, ODOT, etc.) to address shared transportation needs.



Q19 Do you feel that Miamisburg's existing transportation infrastructure has a significant impact on the environmental quality of the region? In other words, do you feel that the existing transportation infrastructure in Miamisburg significantly contribute to degrade the air and water quality of the region?



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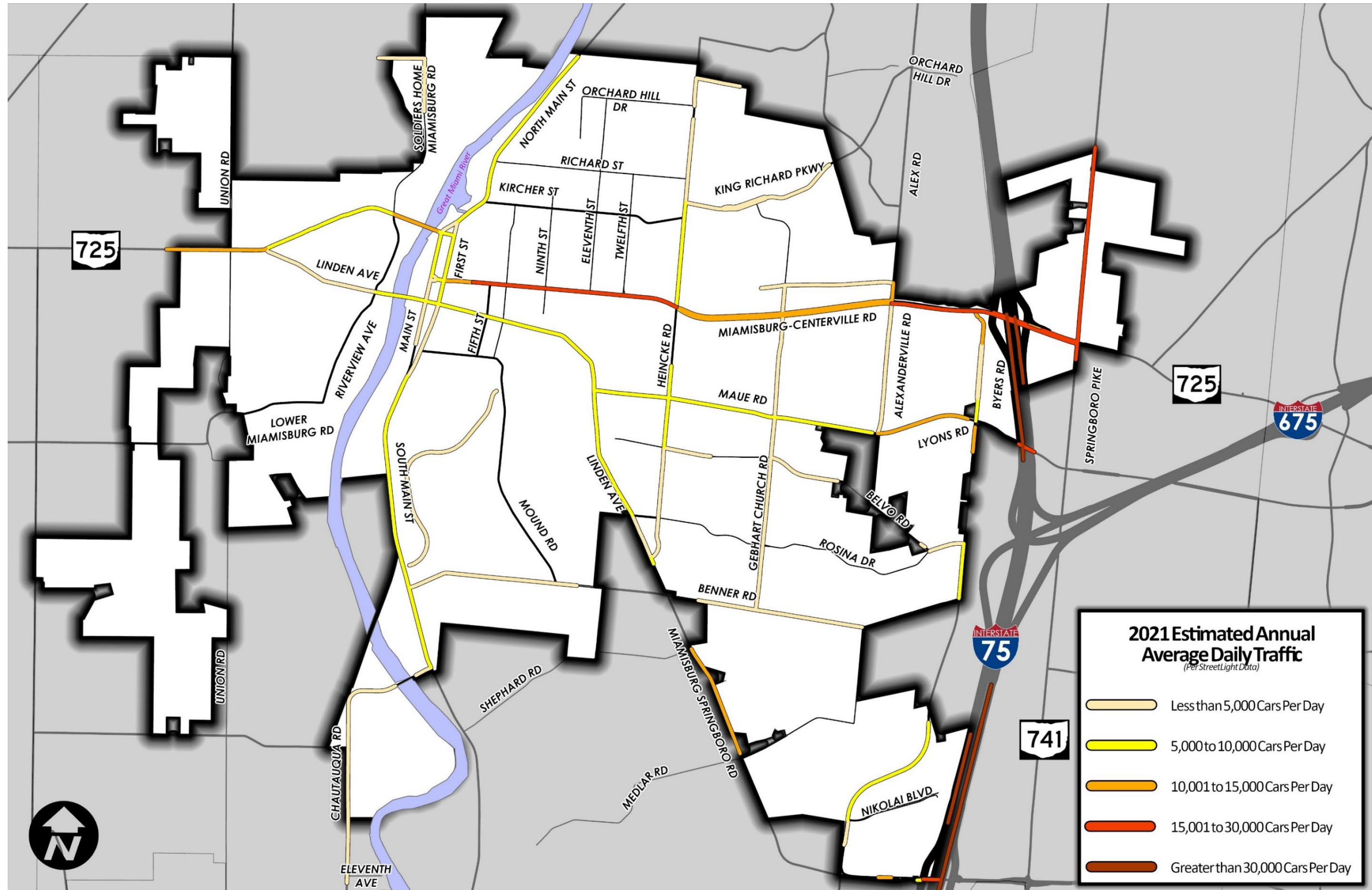
Q20 Use this space to provide any additional comments regarding the transportation system in Miamisburg.

1. Fix Linden Ave from Bennet towards downtown
2. Love the city!
3. Some roads, especially Gebhart Church Road and Heincke Road are in real need of repaving. I hope those roads are on the schedule for re-pavement.
4. Add speed bumps around school zones for safety!!
5. I would like to feel safe when walking, but some of the dog owners do not keep their dogs contained. I have been on many walks with my dog, on a leash, and other people's dogs come running over to my dog and I as we're trying to walk down the street. I don't want to be attacked or my dog attacked while I'm trying to take a walk! These dogs could hurt me and my dog. Allowing dogs to run out to people on the sidewalk should not be allowed.
6. The slip lanes need to go and a no turn on red needs to be implemented and enforced downtown along with signage for drivers to watch out for pedestrians. Also, please have an in person meeting that isn't on a Thursday night, or at least give more than a week and a half notice of the meeting.
7. Please setup a police camera over the Hamburger wagon to ticket those that park on the crosswalk.
8. No off-road bike lanes other than metro parks, no connectivity to main bike lanes from main rt on old 45. No real infrastructure to support that bike lane into downtown. We are evolving our community, public recreation is completely revamped and encouraged with new river side park. We need connectivity to gtown bike path and a easier safer way to ride bikes/walk in general. Recreation will drive more people downtown. Note parking downtown is horrible. Not enough parking spots.
9. Please add the RTA bus line to Austin Landing and the apartment buildings on 741 south of the Dayton Mall.
10. I will not support higher taxes
11. I think it needs to be considered how and where new housing developments could be developed and it be prepared now in order to save money later, I also think some roads needs expanded so there is more of a shoulder (like Benner)
12. I think during rush hour times it would be a great opportunity for the police department to make alot of cash for the community towards these projects you speak of. Giving tickets and citations to speeders violating the speed limits of our awesome community, cars flying down the double lane at 60 miles an hour in a 35 zone and cutting over last minute to the only straight single lane it is on 725. I know you know where i mean! I almost get hit daily , I am tired of that if anything if something could be enforced I see a great money making opportunity!!! Would probably pay for every project you want to fund!!!
13. Educate people to use the provided sidewalks per ORC and stay out of the streets
14. I would choose to avoid all public transportation
15. The RTA brings in a lot of homeless and others that do not pay taxes for any of this.
16. I think the alley ways is a big problem, almost seems forgotten about by the city. There are also a lack of sidewalks in parts of the neighborhoods which can be unsafe especially with people speeding around turns. I love the bike path availability and condition.
17. Reduce public transportation
18. There need to be pedestrian has right away signs set in multiple places. Some of the residential neighborhoods are becoming drag strips for these kids
19. I would love to see a passenger train connecting Miamisburg with Cincinnati, Dayton, Columbus, and Cleveland! It would be great to get rid of the low dam and have the river be a more abundant activity/transportation area. Would also love more horse riding trails/access. It would be great to get the neighborhood alleys repaved as well. If we need a new bridge in the future it would be great if we could have a beautiful structure over the Great Miami River into downtown. Miamisburg is great and it can be even greater! Love the work that you all are doing.
20. With Kroger being a major stopping point, it would be nice to have full sidewalks on N. Heincke. There is a section where there are no sidewalks on either side. Very unsafe to walk on the road there, and there's no other option.
21. Small, residential roads are in terrible condition and make traveling through our community an unpleasant experience. It's unacceptable how poorly these streets are maintained for the exorbitant tax rates we pay.
22. Doesn't seem like any stop lights are timed together to let a group of traffic proceed through area without stopping again. Also would like to see the use of roundabout in areas where this is possible.
23. The area in King Richard that keeps having watermain breaks is a disaster. IT doesn't seem like much care is given to it. Gamewell is unlikeable and almost undeliverable at its break a few years ago.
24. The intersection of byers and Benner is horrible makes me nervous with small kiddos
25. A free trolley system or shuttles might ease vehicle congestion
26. Miamisburg's existing roadways and sidewalks are all in terrible condition, this includes those in neighborhoods
27. Since you are asking, Linden that turns into Miamisburg Springboro Rd is in dire need of repaving.
28. I have inquired about bike lanes on Linden, but it was turned down, not sure why after Linden was widened. Miamisburg needs to enhance and implement lanes and accommodations to support its bicycle friendly claim. The pedestrian route through the Byers road to the mall, or from the mall to downtown on Rte 725 is a disgrace. There's been talk about making improvements for years but it never happens. Parking downtown is getting more difficult as time goes on due to more people enjoying the business improvements.
29. Miamisburg has been doing a good job making downtown more walkable and enjoyable, but the surrounding areas are all very car dependent. It would be nice to be able to travel anywhere in Miamisburg without having to drive.
30. With all the improvements to downtown lately, the biggest issue I see is having a safer ability to walk all over downtown. There are a good number of sidewalks, but safety sometimes feels lack luster.
31. It is no or a pedestrian- friendly community, as 'walkable' as it is
32. Please aim to improve Miamisburg's transportation conditions by designing for people first, not cars. I highly recommend the book Strong Towns by Charles L. Mahron, Jr.

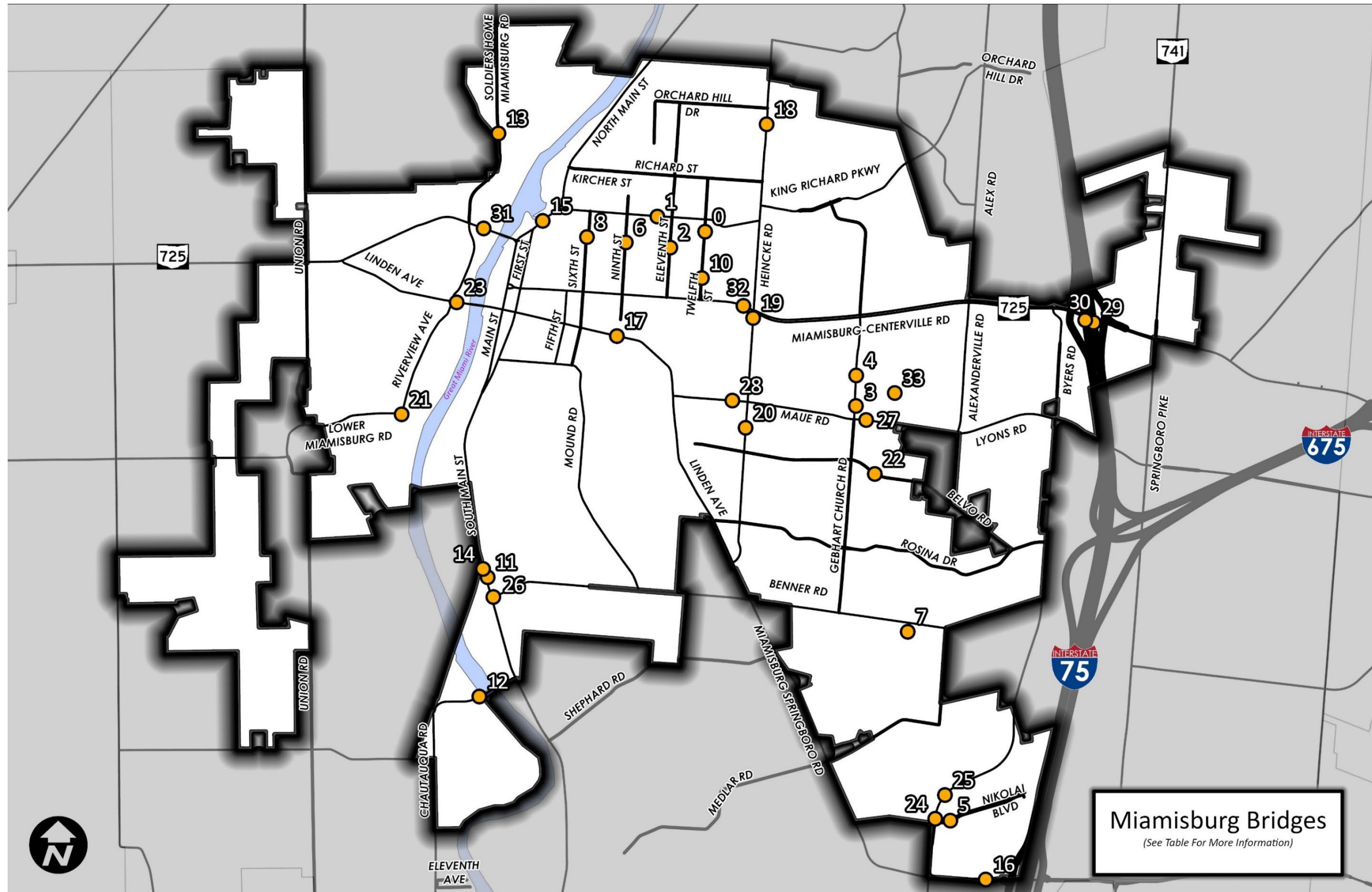
APPENDIX – SURVEY #2 RESPONSES

33. My neighbors & I would love speed bumps in the surrounding neighborhoods. So many of our neighborhoods have families with little kids. It would help so much in stopping unnecessary speeding.
34. Need a lot more parking downtown
35. I would like to see a better sequence of the traffic lights on 725 between Alex Rd. and 741 or really 675. If I'm at the light at Alex it turns green. I should be able to make all the way to the mall with green lights traveling the speed limit.
36. Overall our transportation and road crews have been doing a great job.
37. There are many streets that do not have sidewalks. I see kids walking to school in the morning while its still dark and they must walk in the street because there are no sidewalks. Adult's walking to and from stores walks next to the road instead because no sidewalks.
38. Prefer if transportation was for locals only
39. Roundabouts are evil. Do not install roundabouts.
40. I would love to see the city encourage more biking and walking. We have a great trail system by the river, but it's not always easy to ride from your home to the trail. It would also be great to encourage people to bike/walk to the Kroger area. Cutting down on car usage for short trips like that would be amazing!
41. Sidewalk needed on both sides of Heincke so people don't have to cross in the middle of the street to walk on a sidewalk, or walk through people's flowerbeds where there's no sidewalk on that side of the street
42. The biggest issue is the intersections on 725 between 75 & Alex. There's so many accidents and it gets backed up so quickly. Especially at the 725/Byers intersection.
43. I don't want more taxes. What we have going on is good.
44. I'm saying make pedestrians safe, a child is going to be hit.
45. Don't add bike paths on main road I've seen so many cities do this a no one uses them waste of tax dollars. Wider roads. Stop light timers set appropriately... some of the side streets that come out onto 725 at lights take forever. Ie Lawrence cross over to 12th vice versa needs attention as well as 9th & 6th lights.
46. Please focus on the basics and not create an overly complicated system that benefits few. Focus on keeping the roadways in good shape, plan for major capital projects needed to repair/redesign roadways as necessary. Let's not spend a bunch of money to make miamisburg something it isn't.
47. Miamisburg does a wonderful job clearing off and treating the roads during snowy weather. Thank you!
48. Please add linden to resurfacing list asap. It's the main road I take to and from work every day and it is going to set my vehicle
49. Bicycles need to stay on the bike trails NOT on major roadways.
50. I appreciate the opportunity to share my thoughts and look forward to a focus on more complete streets and safer infrastructure for bike/peds which will have a huge return in quality of life and livability in Miamisburg making it an attractive destination for residents and visitors.
51. Especially by the schools, there needs to be more clearly marked street crossing for student walkers. We live by the high school and my daughter has been almost hit many times due to the inability to cross at Belvo/Dunaway. I lighted crosswalk sign would help alert drivers that they need to yield to a pedestrian. This would also be helpful when groups of residents walk to the stadium. Also, more speed signs need to be posted in neighborhoods alerting drivers that they are speeding. Drag racing has also been an issue. Maybe just more police presence would help on that front.
52. Painted bike lanes and painted symbols aren't helpful, only protected bike lanes are helpful, if you wouldn't let your middle school kid ride on it then it isn't safe for anyone
53. Treat all residents the same when it comes to the condition of their street and sidewalks.
54. Better pedestrian signage on main st, middle school, connecting bike trails to schools
55. We desperately need a walking and/or biking path on Medlar Road between the back entrance to the Pipestone residential development at Fairwood Drive and the existing bike path at Medlar Elementary. This is an extremely dangerous stretch of road for pedestrians with virtually no road shoulder to walk on and several blind spots for drivers. Such a path would also connect people in Pipestone to the new neighborhood on Aberdeen Lane.
56. Wish there was a direct line from Downtown Miamisburg to Dayton. Would take me 3 hrs currently. Plus a sidewalk on 725 past Heincke heading east would be appreciated. Same for a second crosswalk at Heincke and 725.
57. The roads need repair period! We fund schools, libraries etc but not the roads. The cost to fix vehicles from bad roads here is getting out of control.
58. There needs to be more enforcement of existing traffic laws for the safety of pedestrians. Ie speeding, running lights, pedestrian right of way in crosswalks,.
59. Downtown NEEDS a dedicated and PROTECTED bike lane that connects to shops.
60. - The intersection at Medlar Road and Miamisburg-Springboro Road is a death trap for cyclists and pedestrians. Please add stoplights and crosswalks. - Please build a bike trail and sidewalks to connect downtown Miamisburg with downtown Germantown. - Please add sidewalks on 721 so pedestrians can walk to Cox Arboretum. - Please add crosswalks and a traffic light at Heincke and King Richard. It is a death trap there for pedestrians with cars zooming up and down the hill. - Please add sidewalks and bike paths to connect downtown Miamisburg to the Dayton Mall area. Most of it is ditches and cars are going too fast. - Please offer affordable way to get from Miamisburg to the Dayton Airport. It's unacceptable to pay \$55 to take an Uber there or spend 2 hours on buses that don't even get you there early in the morning. - Please arrange an option for a future connecting bus from Miamisburg to the future Amtrak 3C-D connection.
61. Something needs to be done to address the intersection of East Linden and 725 at the top of the hill on the way to Germantown. The traffic at the watertower is a Hazard now and is only going to get worse as the new Housing is built on S Union

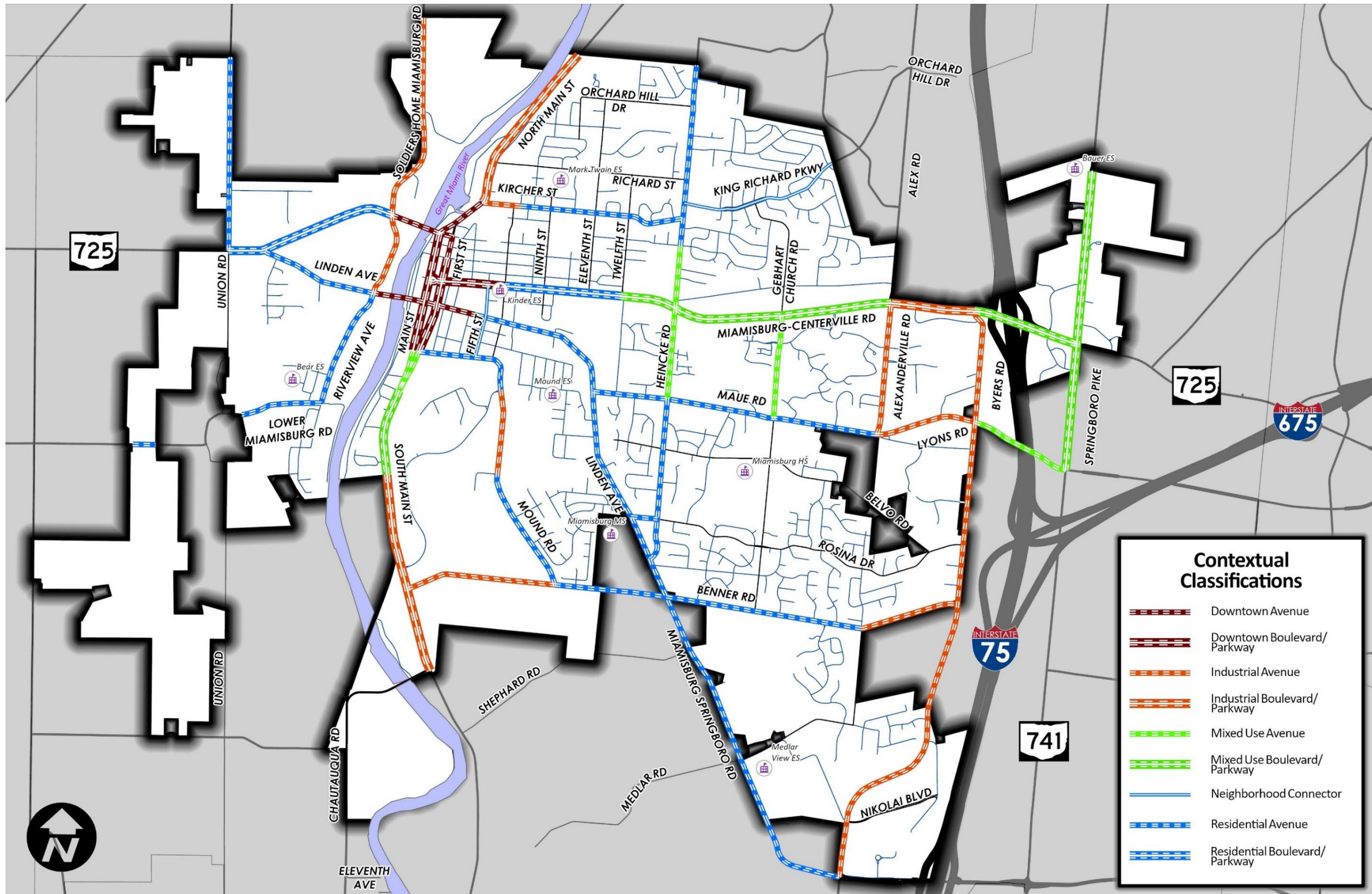
APPENDIX – EXISTING CONDITION MAPS



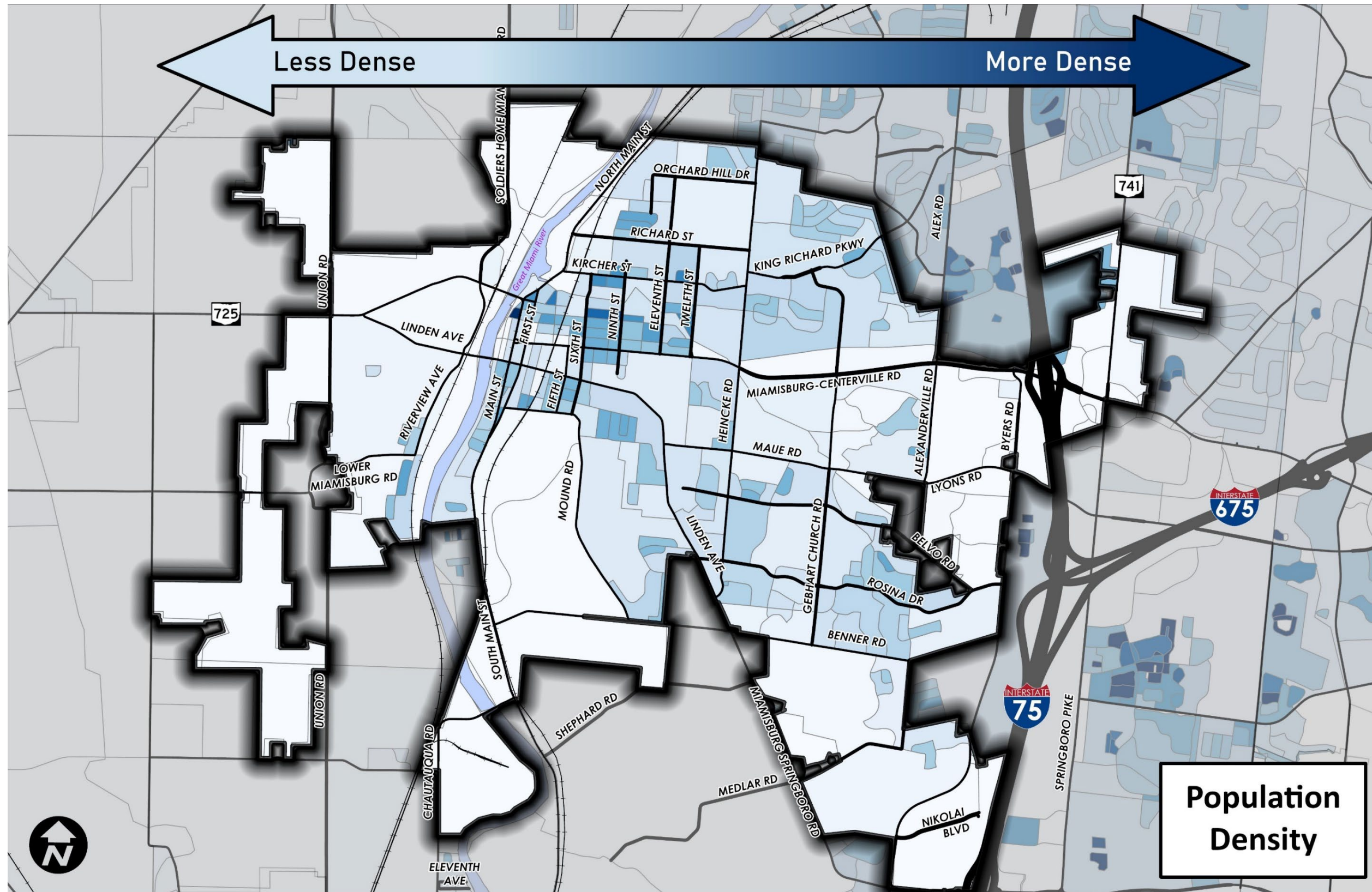
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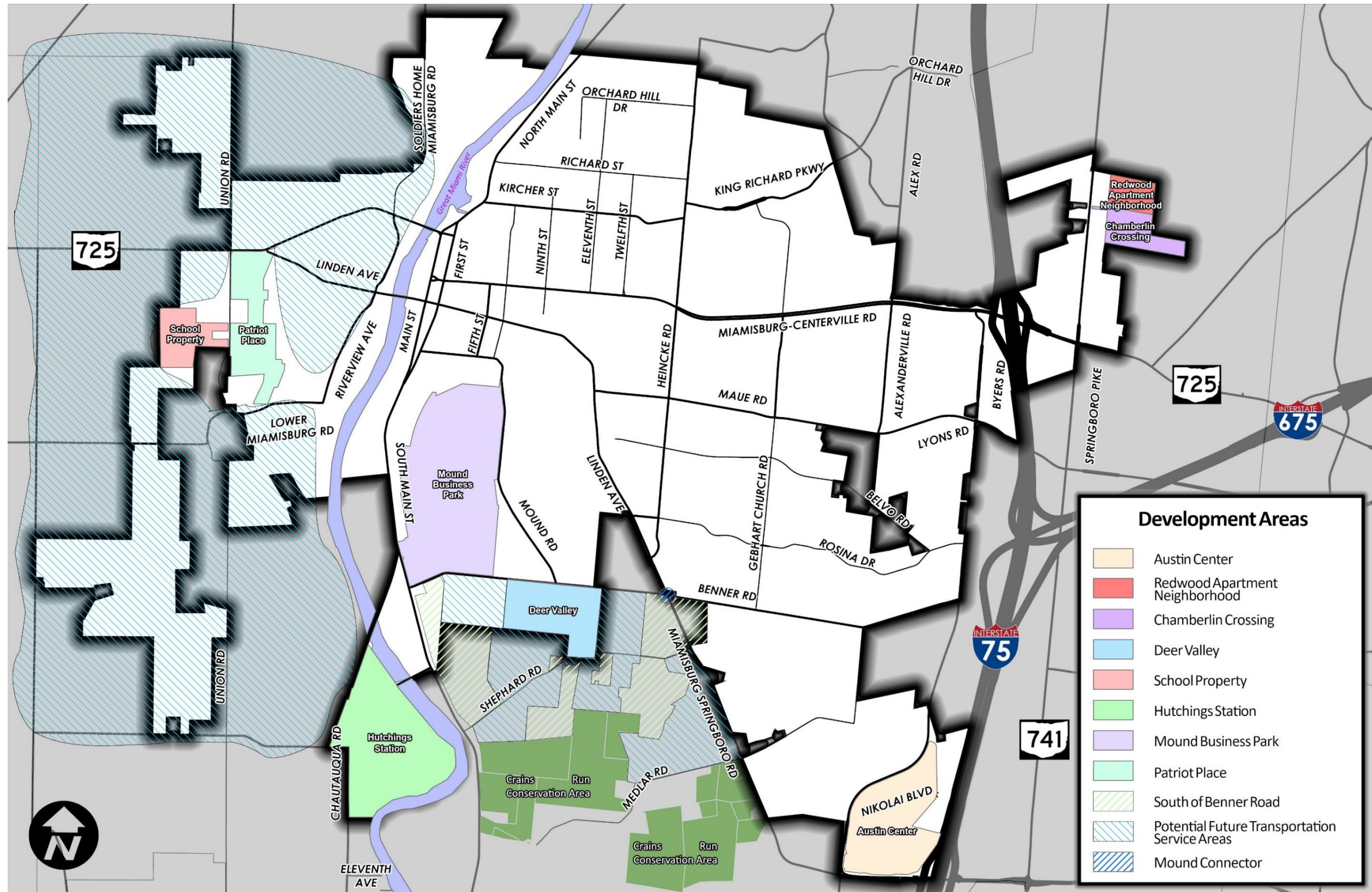
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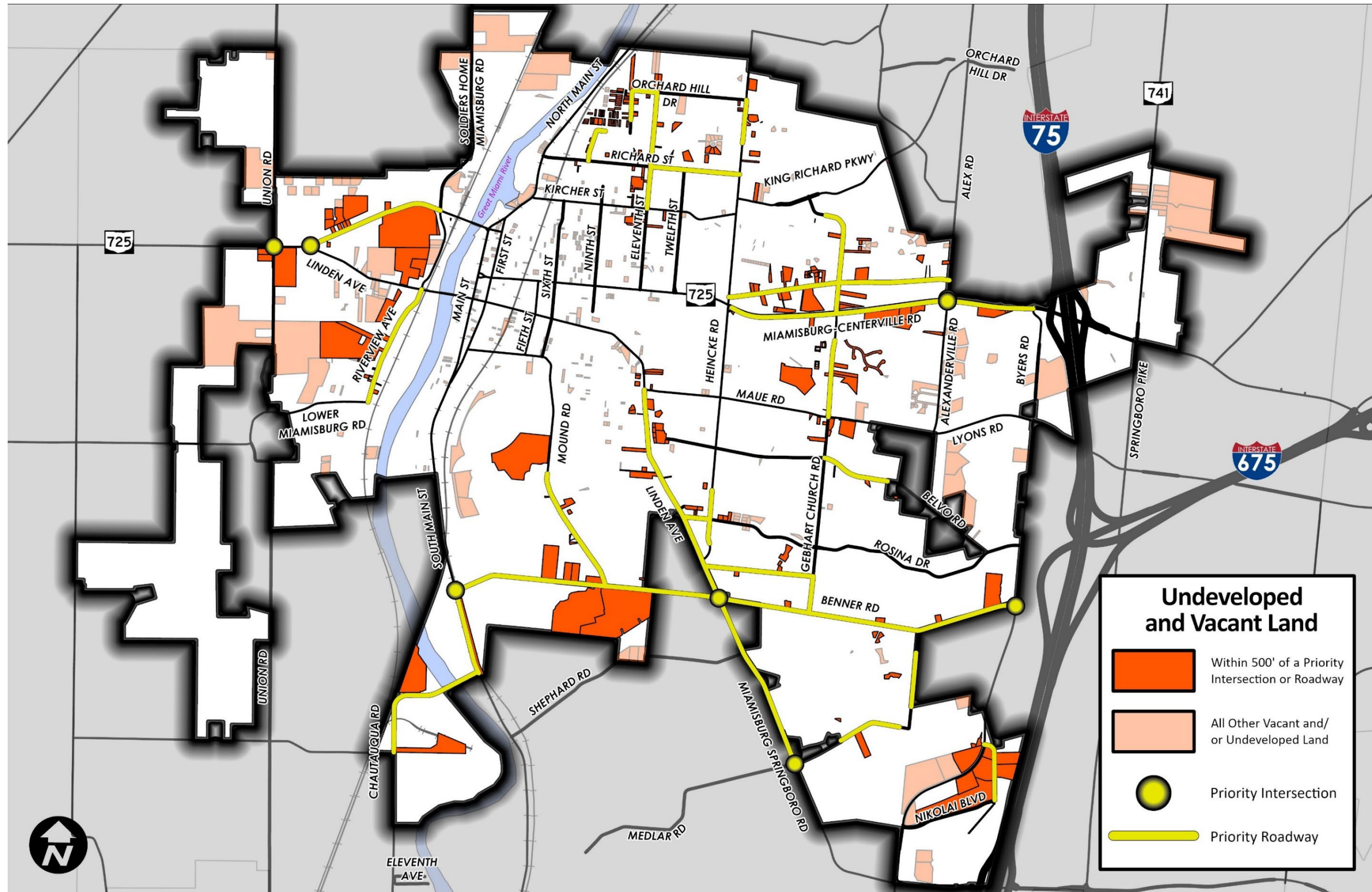
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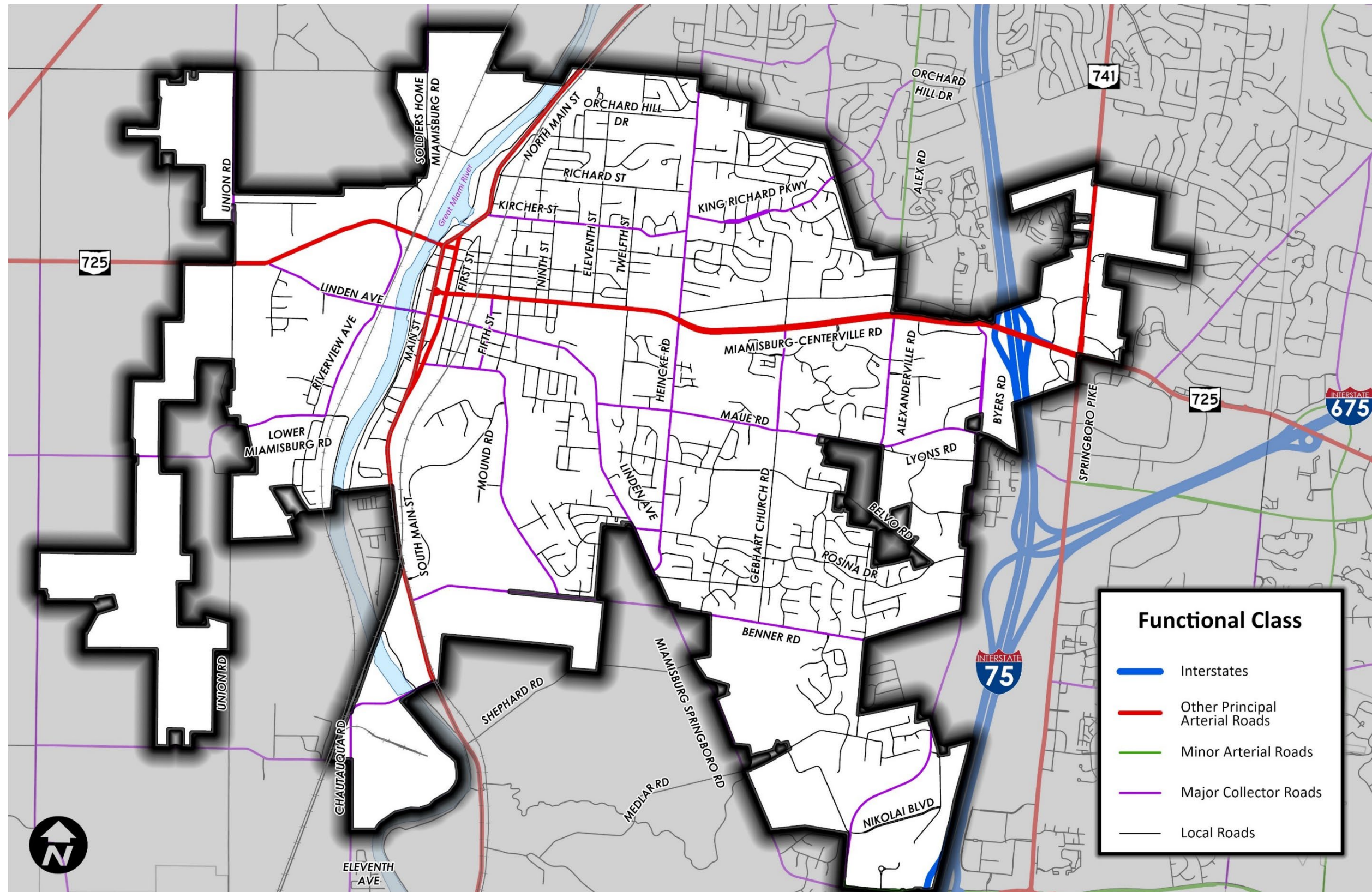
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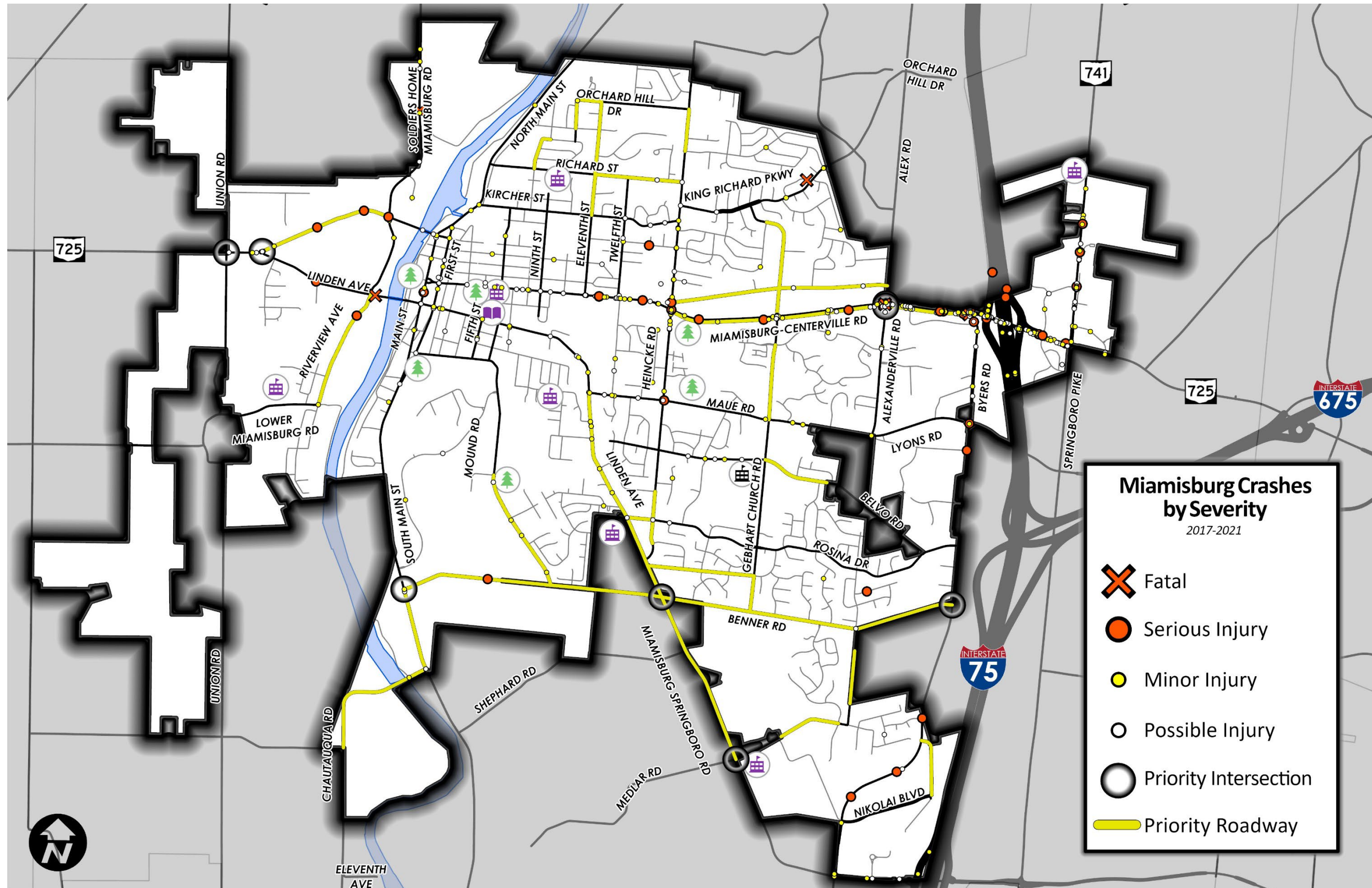
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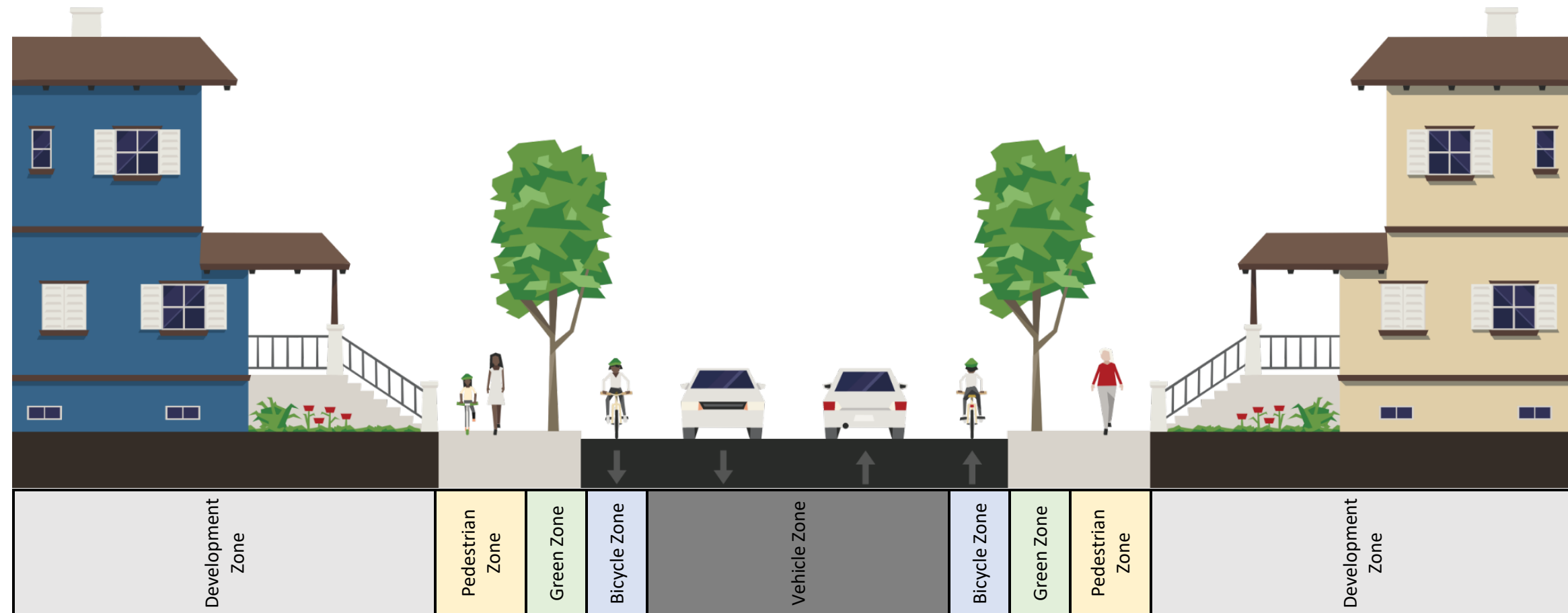


APPENDIX – EXISTING CONDITION MAPS



APPENDIX – STREET DESIGN GUIDELINES

Streets: Residential Typical Zones



	<p>Development Zone</p> <p>The basic intent for the Development Zone is that buildings orient toward and have good functional and visual connections to the street. Within the Development Zone, the building setbacks, site design and land uses will vary based on the context.</p>		<p>Green Zone</p> <p>Landscaping and trees in the Green Zone serve multiple purposes:</p> <ul style="list-style-type: none"> ➤ Buffering for pedestrians from weather and automobile traffic ➤ Green Infrastructure to mitigate stormwater and summer heat/glare ➤ Underlying support for property values/desirability of real estate 		<p>Bicycle Zone</p> <p>The Bicycle Zone is sometimes adjacent to the Pedestrian Zone. This is an essential component to many Complete Streets. In the chart, look for standards under “Bike Zone.”</p>
	<p>Pedestrian Zone (Frontage, Through, Furnishing Zones)</p> <p>Street design privileges walking as a travel option. Therefore, the Pedestrian Zone should include unobstructed sidewalks at appropriate widths for adjacent land uses.</p>		<p>Parking Zone</p> <p>The need for the Parking Zone varies on Streets. The benefits of the Parking Zone include traffic calming, buffering between vehicles and pedestrians, and easy “in and out” access to adjacent land uses.</p>		<p>Vehicle Zone</p> <p>The Vehicle Zone serves motor vehicles, with a variety of lane configurations, to accommodate higher volumes than local streets. Narrower lanes will help to slow traffic and provide other modes of transportation additional room within the right-of-way.</p>

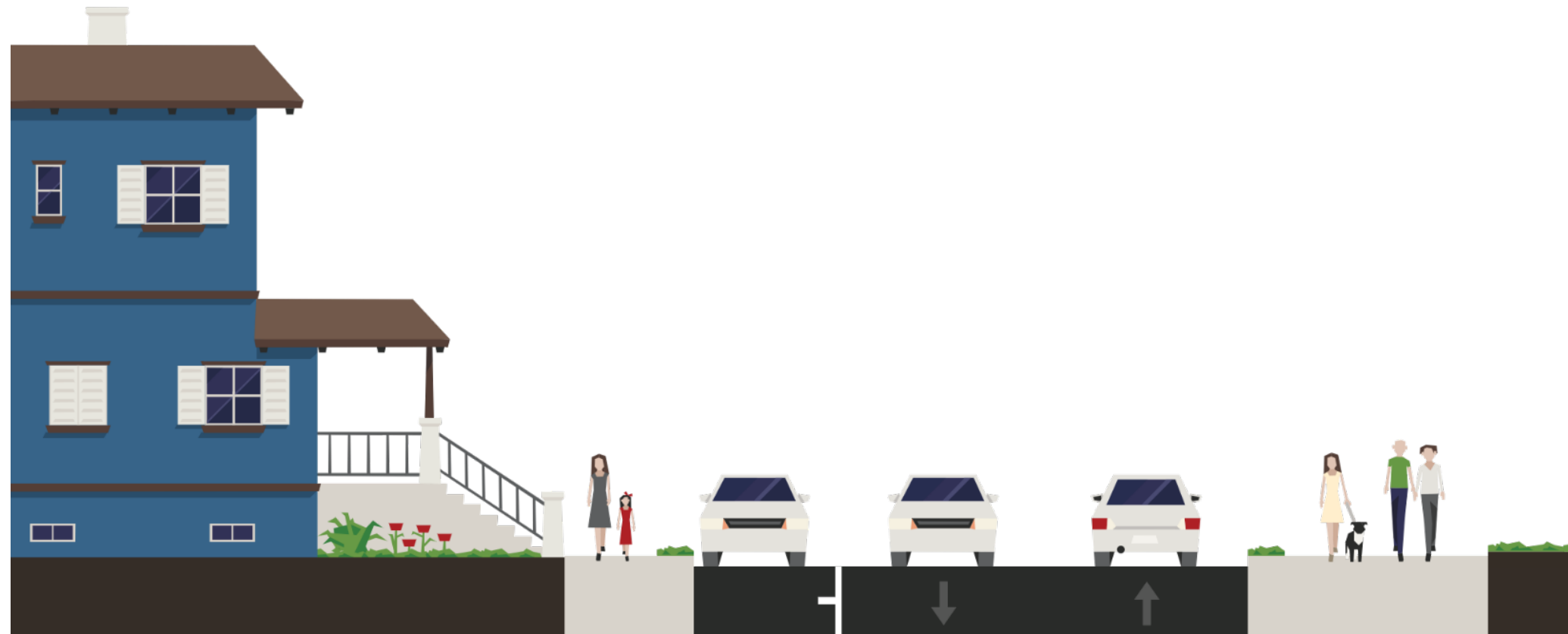
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Street Typical Example 1: Residential Street with Shared Lanes



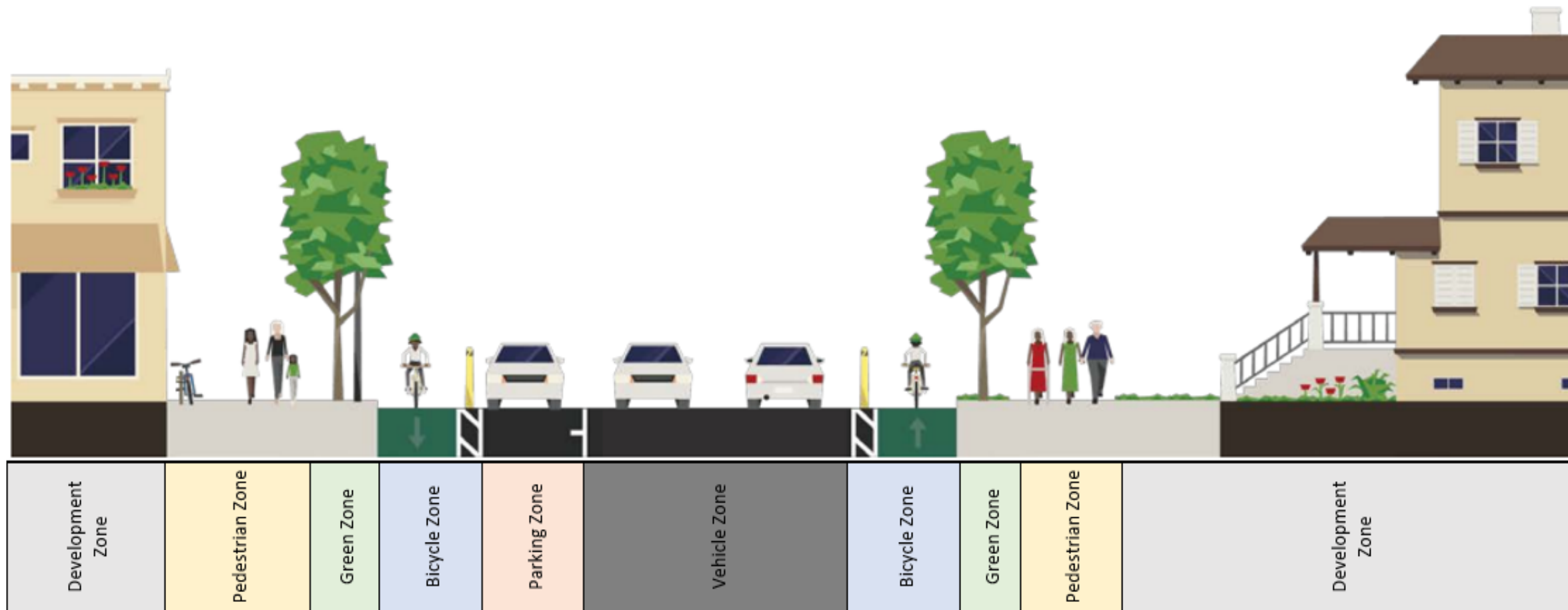
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Street Typical Example 2: Mixed Use Street with Multi-use Path



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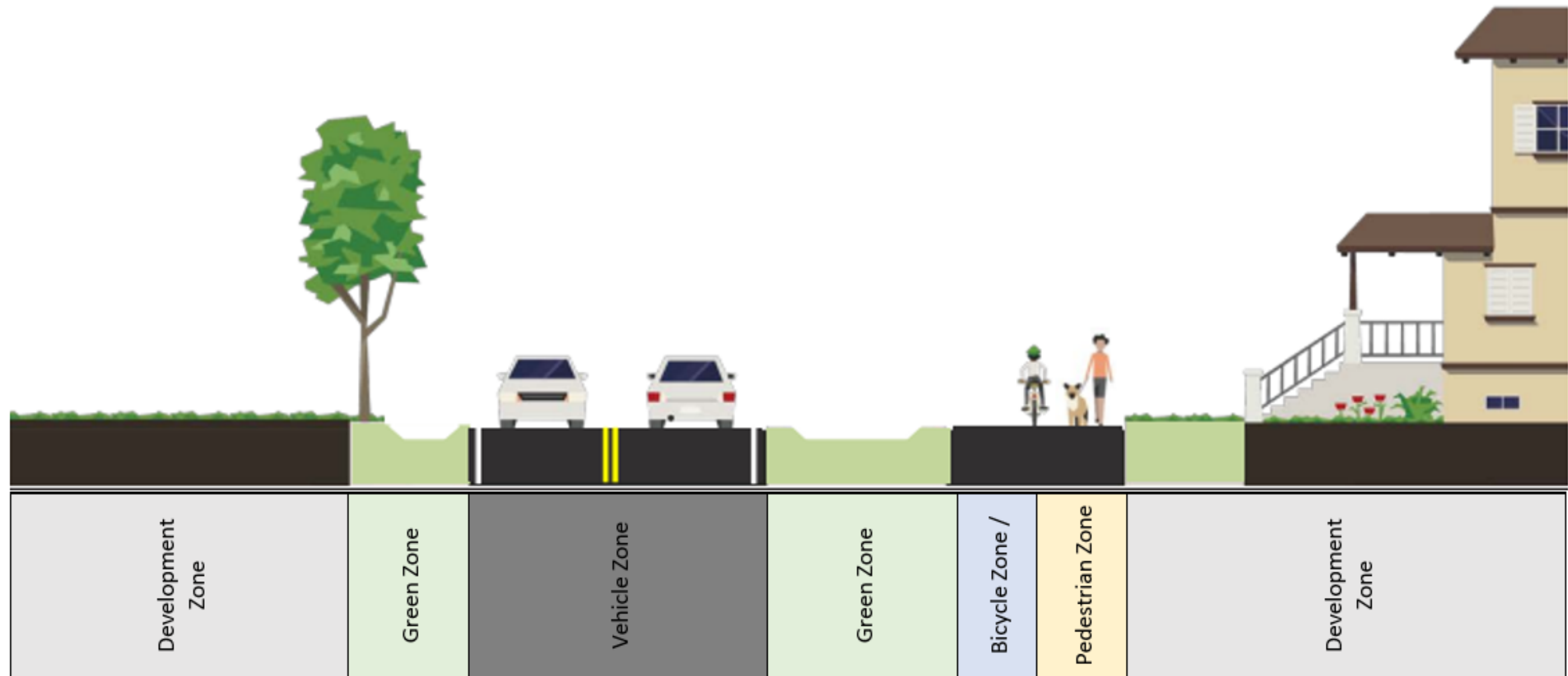
Avenue: Typical Zones



	Development Zone		Green Zone		Bicycle Zone
	The basic intent for the Development Zone is that buildings orient toward and have good functional and visual connections to the street. Within the Development Zone, the building setbacks, site design and land uses will vary based on the context.		Landscaping and trees in the Green Zone serve multiple purposes: <ul style="list-style-type: none"> • Buffering for pedestrians from weather and automobile traffic • Green Infrastructure to mitigate stormwater and summer heat/glare. • Underlying support for property values/desirability of real estate 		The Bicycle Zone is sometimes adjacent to the Pedestrian Zone. This is an essential component to many Complete Streets. In the chart, look for standards under “Bike Zone.”
	Pedestrian Zone (Frontage, Through, Furnishing Zones)		Parking Zone		Vehicle Zone
	Avenues design privileges walking as a travel option. Therefore, the Pedestrian Zone should include unobstructed sidewalks at appropriate widths for adjacent land uses.		The need for the Parking Zone varies on Collector-Avenues. The benefits of the Parking Zone include traffic calming, buffering between vehicles and pedestrians, and easy “in and out” access to adjacent land uses.		The Vehicle Zone serves motor vehicles, with a variety of lane configurations, to accommodate higher volumes than local streets. Narrower lanes will help to slow traffic and provide other modes of transportation additional room within the right-of-way.

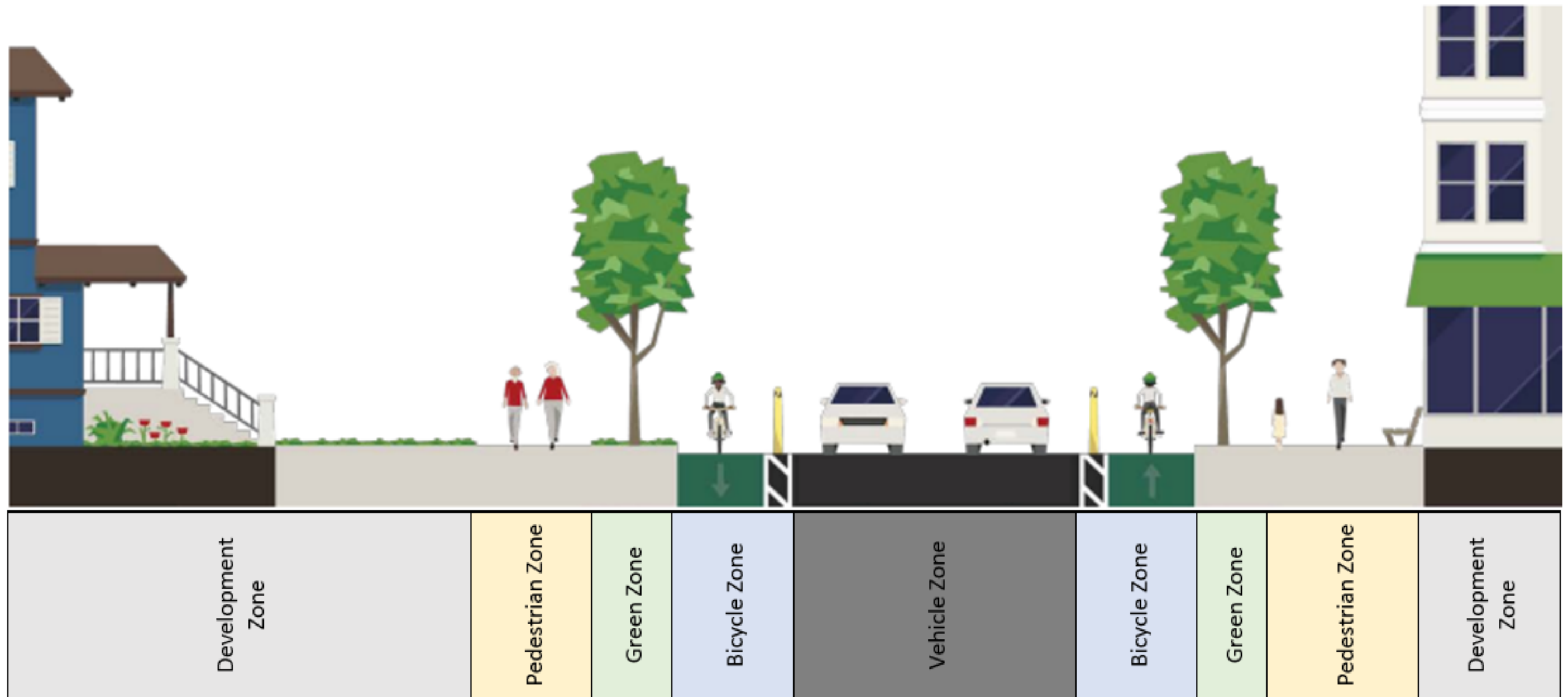
APPENDIX – STREET DESIGN GUIDELINES

Avenue Typical Section Example 1: Avenue with Swales and Multi-use Path



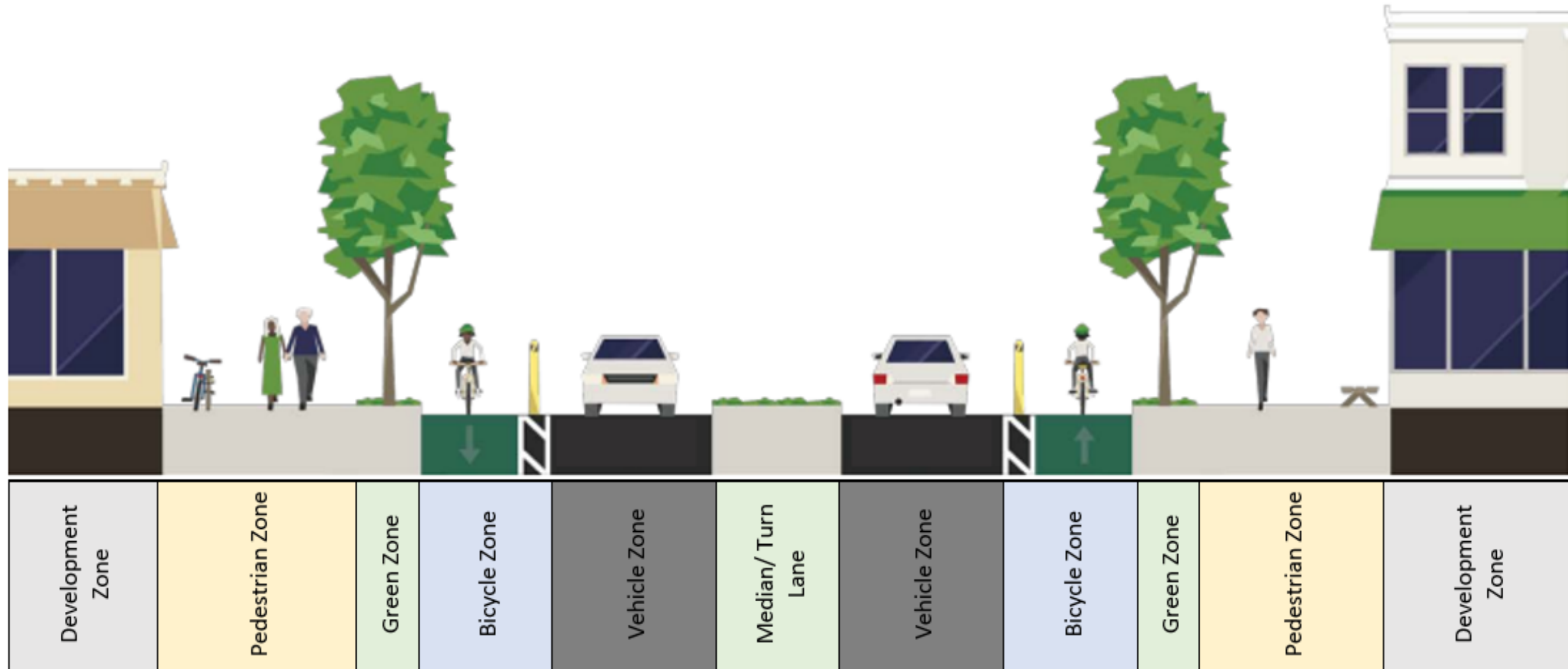
APPENDIX – STREET DESIGN GUIDELINES

Avenue Typical Section Example 2: Mix Use Avenue with Protected Bike Lane



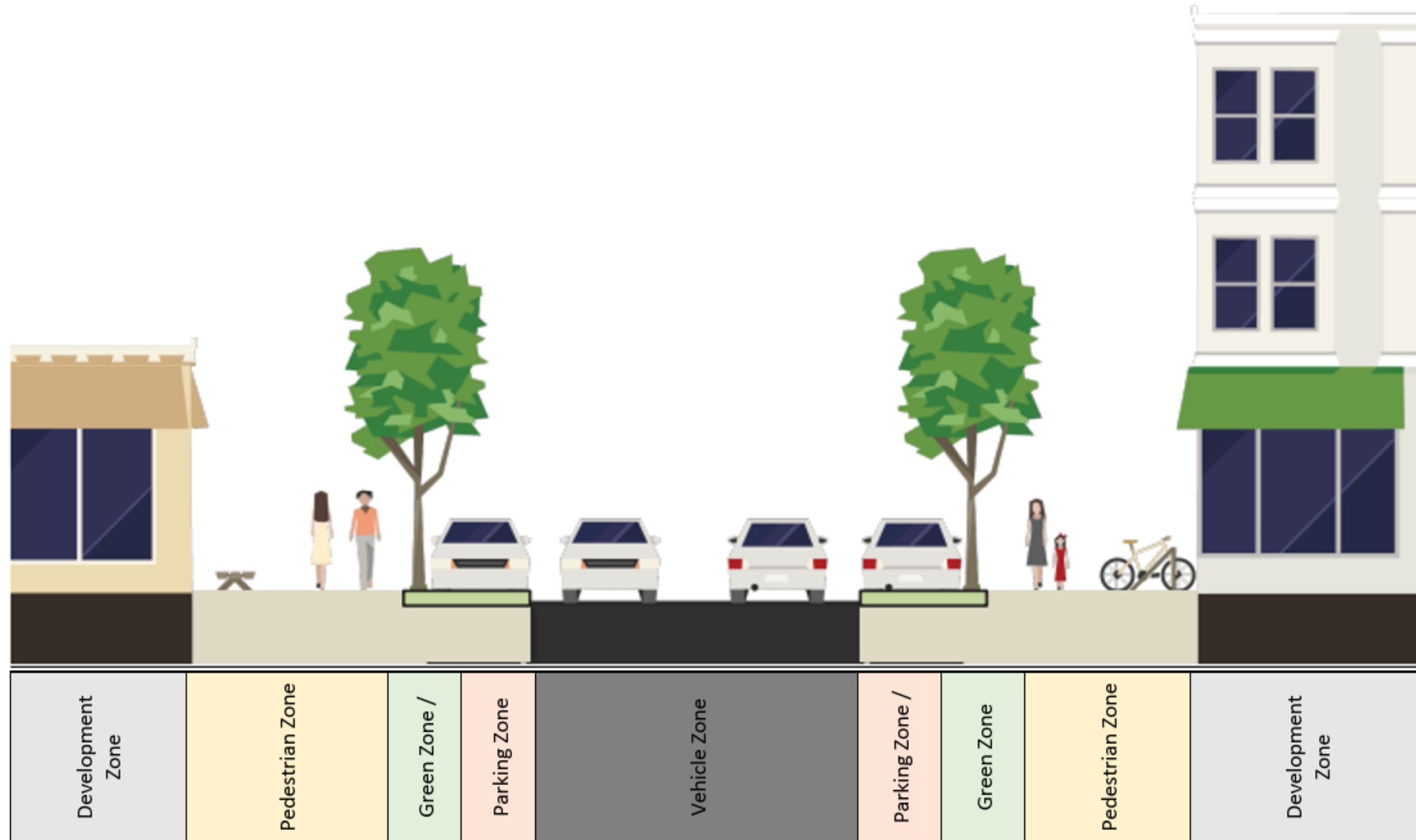
APPENDIX – STREET DESIGN GUIDELINES

Avenue Typical Section Example 3: Mixed Use Avenue with Median and Protected Bike Lanes



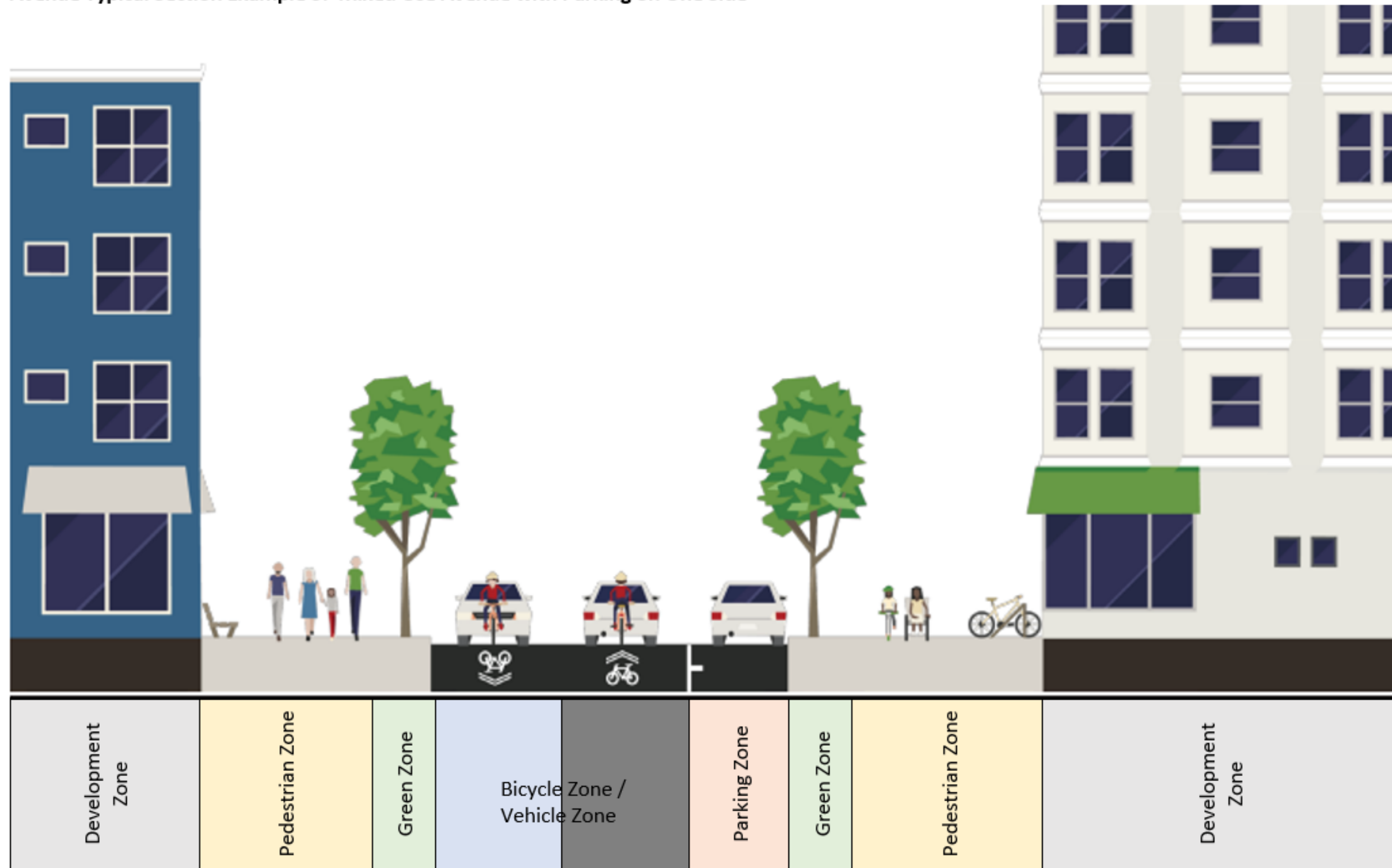
APPENDIX – STREET DESIGN GUIDELINES

Avenue Typical Section Example 4: Mixed Use Avenue with On-street Parking and Curb Extensions



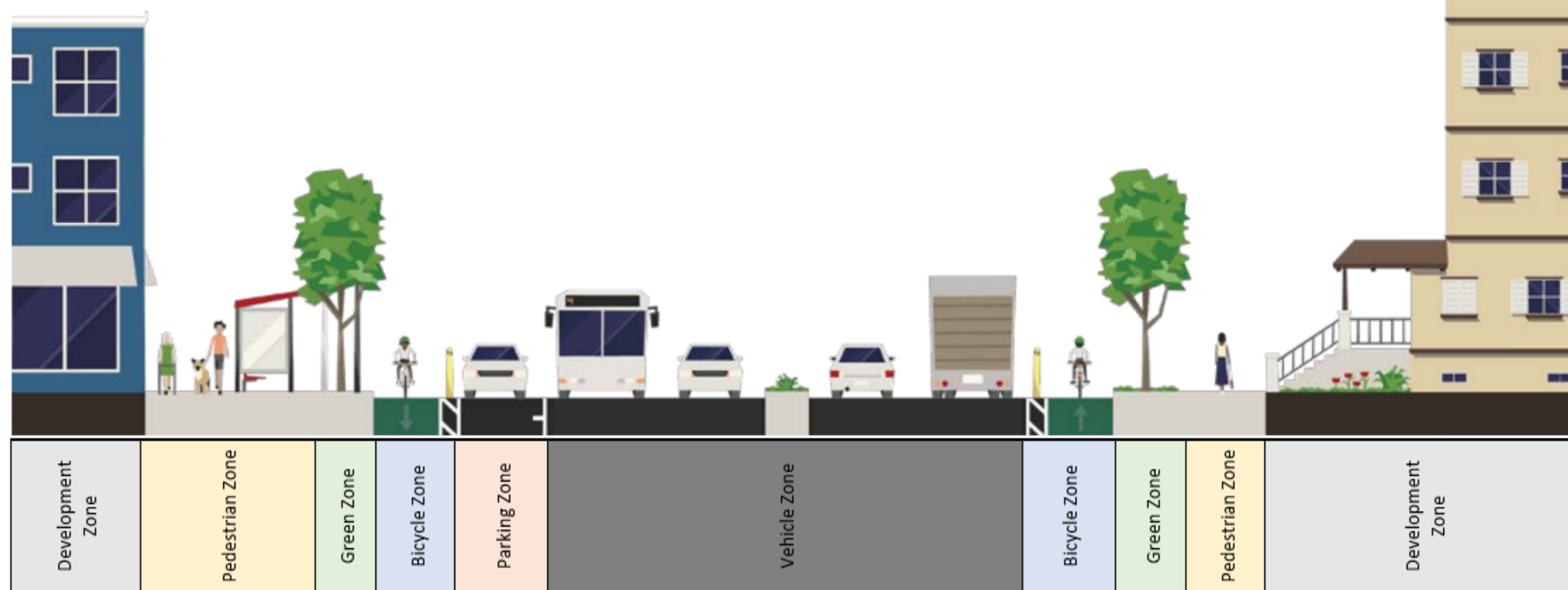
APPENDIX – STREET DESIGN GUIDELINES

Avenue Typical Section Example 5: Mixed Use Avenue with Parking on One Side



APPENDIX – STREET DESIGN GUIDELINES

Arterial-Boulevard: Typical Zones



Development Zone	Green Zone	Bicycle Zone	Vehicle Zone	Bicycle Zone	Green Zone	Pedestrian Zone	Development Zone
<p>Development Zone</p> <p>The basic intent for the Development Zone is that buildings orient toward and have good functional and visual connections to the street. Within the Development Zone, the building setbacks, site design and land uses will vary based on the context.</p>		<p>Green Zone</p> <p>Landscaping and trees in the Green Zone serve multiple purposes:</p> <ul style="list-style-type: none"> • Buffering for pedestrians from weather and automobile traffic • Green Infrastructure to mitigate stormwater and summer heat/glare • Underlying support for property values/desirability of real estate 			<p>Bicycle Zone</p> <p>Arterial-Boulevards typically have lower traffic speeds and volumes so bicyclists are less likely to feel comfortable in mixed traffic. The Bicycle Zone is essential to encourage cycling.</p>		
	<p>Pedestrian Zone (Frontage, Through, Furnishing Zones)</p> <p>Pedestrian travel should be a prominent option on Arterial-Boulevards. This zone should include unobstructed sidewalks at appropriate widths for adjacent and surrounding land uses.</p>		<p>Parking Zone</p> <p>The need for the Parking Zone varies on Arterial-Boulevards. The benefits of the Parking Zone include traffic calming, buffering between vehicles and pedestrians, and easy “in and out” access to adjacent land uses.</p>		<p>Vehicle Zone</p> <p>The Vehicle Zone serves motor vehicles, with a variety of lane configurations, to accommodate higher volumes than Avenues. Narrow lanes should be considered to slow traffic and provide for the expansion of other zones within the right-of-way.</p>		

APPENDIX – STREET DESIGN GUIDELINES

Arterial–Boulevard Typical Section Example 1: Residential Arterial–Boulevard with Swales and Multi-use Path



APPENDIX – STREET DESIGN GUIDELINES

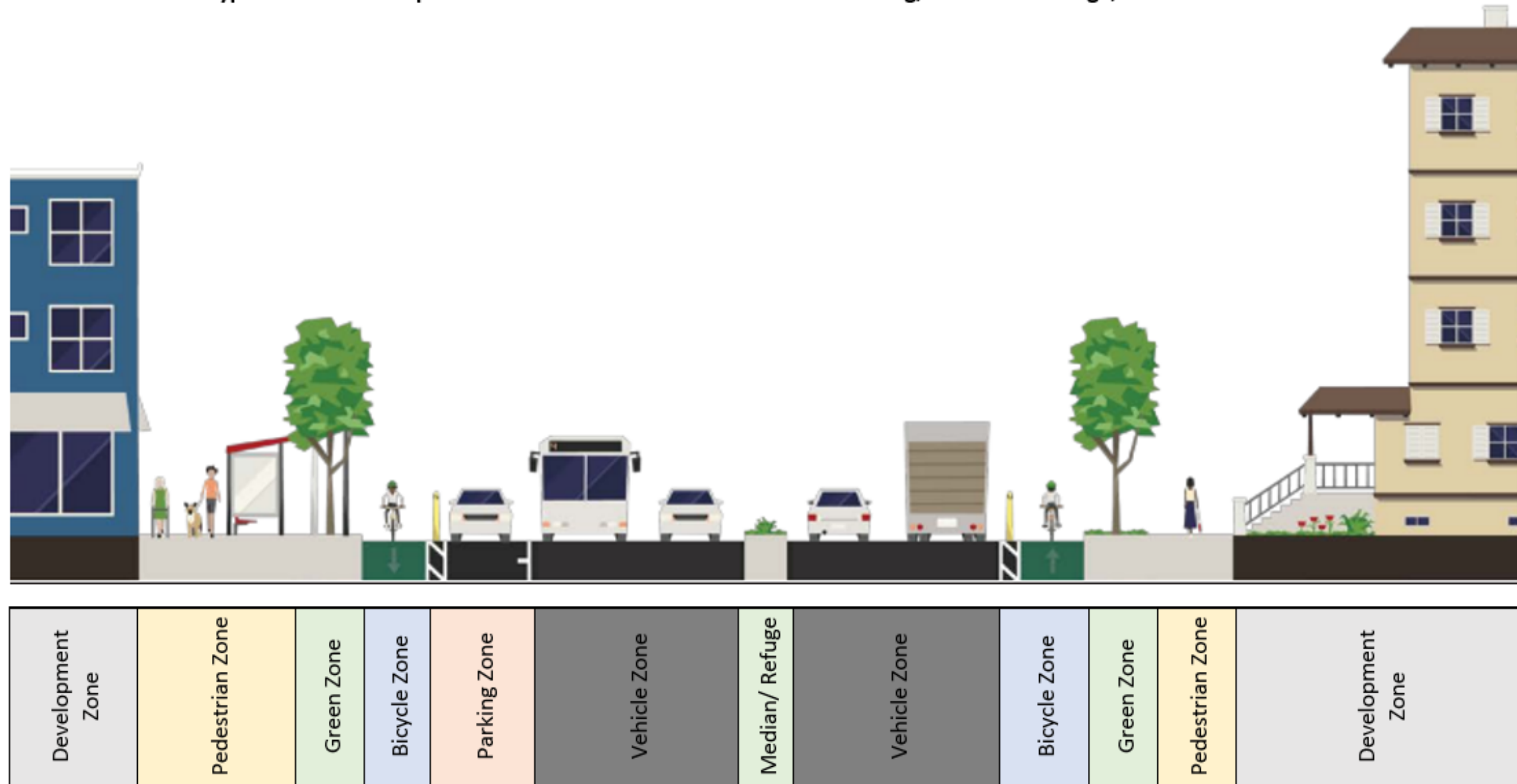
Arterial–Boulevard Typical Section Example 2: Mixed Use Arterial–Boulevard with Median and Protected Bike Lanes



Development Zone	Pedestrian Zone	Green Zone	Bicycle Zone	Vehicle Zone	Median	Vehicle Zone	Green Zone	Bicycle Zone	Pedestrian Zone	Development Zone
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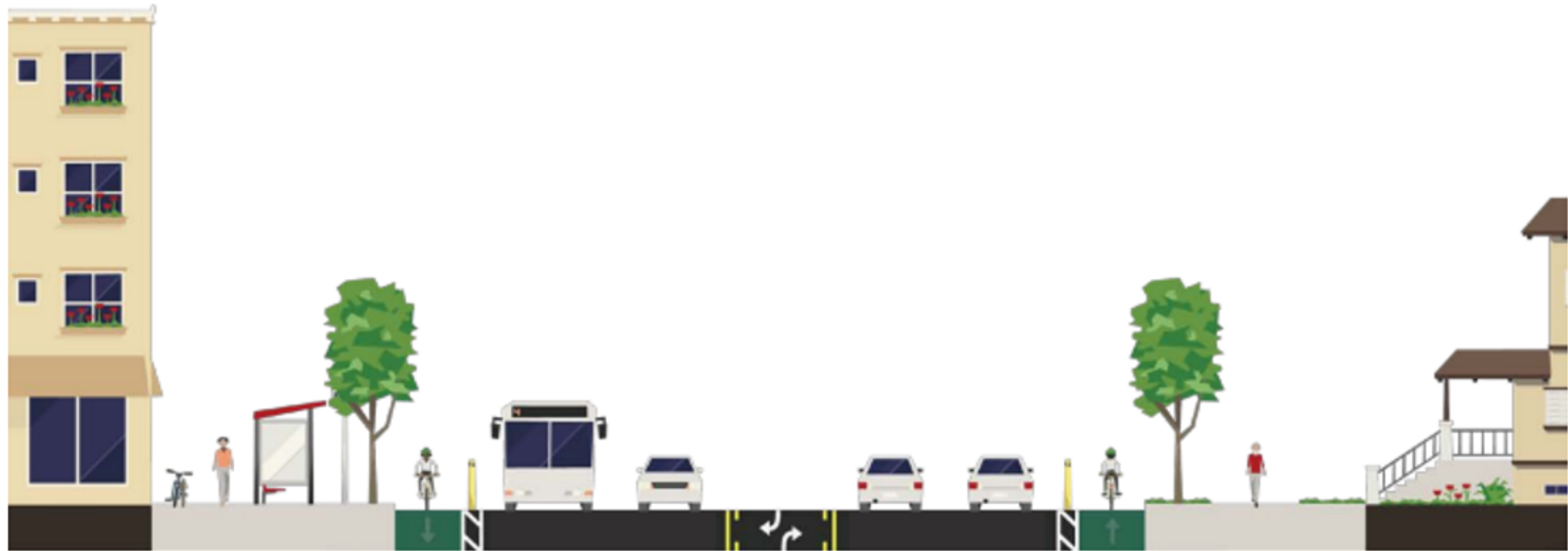
APPENDIX – STREET DESIGN GUIDELINES

Arterial–Boulevard Typical Section Example 3: Mixed Use Arterial–Boulevard with Parking, Pedestrian Refuge, and Protected Bike Lanes



APPENDIX – STREET DESIGN GUIDELINES

Arterial–Boulevard Typical Section Example 4: Mixed Use Arterial–Boulevard with Turn Lane and Protected Bike Lanes



APPENDIX – STREET DESIGN GUIDELINES

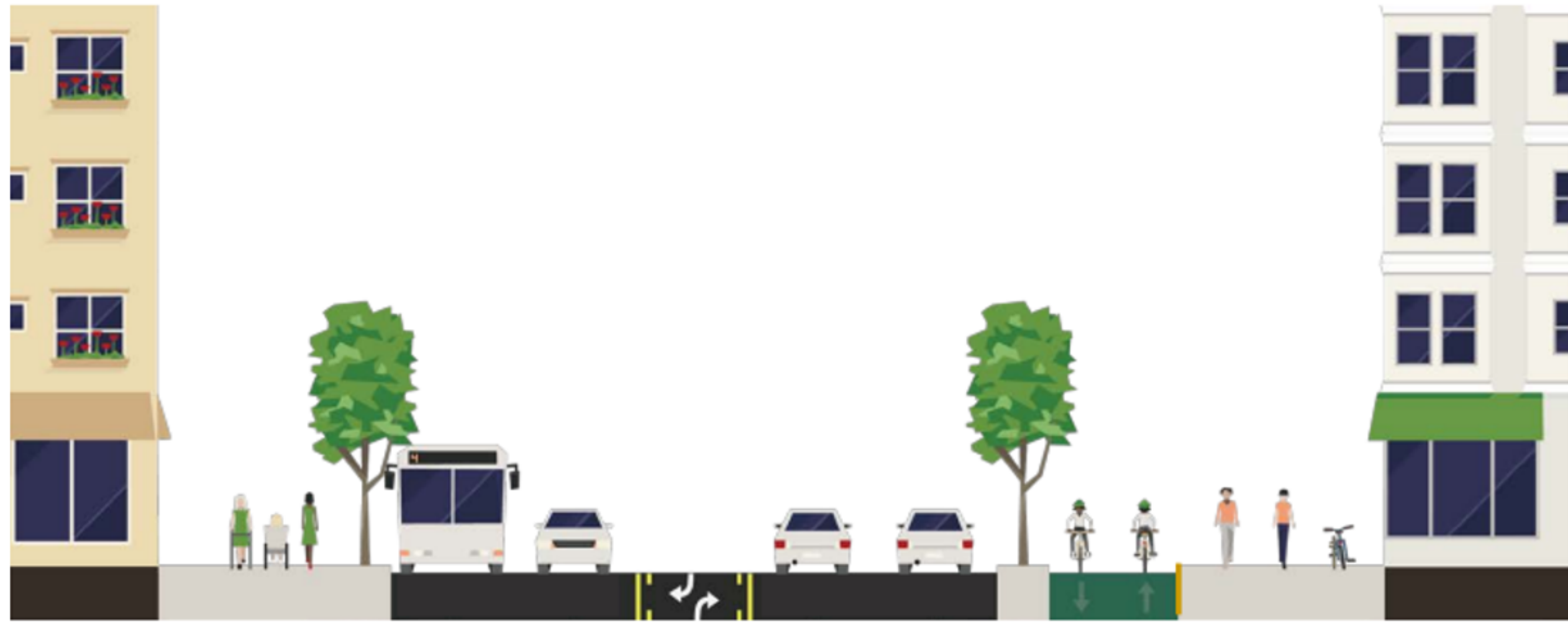
Arterial–Boulevard Typical Section Example 5: Mixed Use Arterial–Boulevard with Parking and Protected Bike Lanes



Development Zone	Pedestrian Zone	Green Zone	Bicycle Zone	Vehicle Zone	Parking Zone	Bicycle Zone	Green Zone	Pedestrian Zone	Development Zone
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APPENDIX – STREET DESIGN GUIDELINES

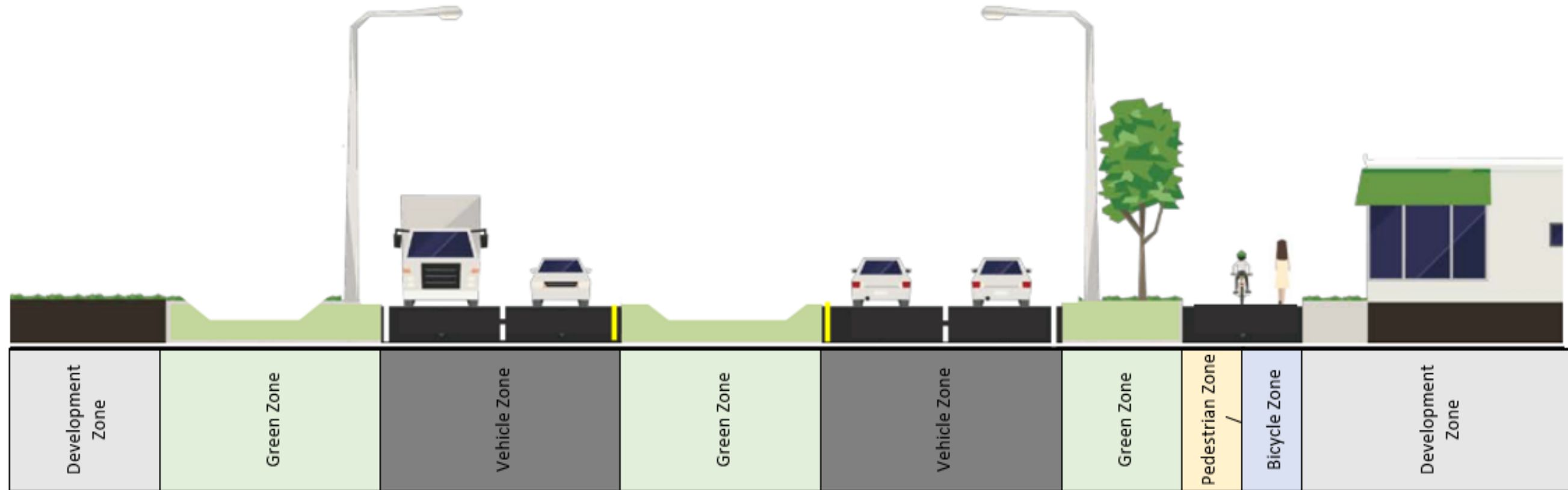
Arterial–Boulevard Typical Section Example 6: Mixed Use Arterial–Boulevard with Turn Lane and Cycle Track



Development Zone	Pedestrian Zone	Green Zone	Vehicle Zone	Green Zone	Bicycle Zone	Pedestrian Zone	Development Zone
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APPENDIX – STREET DESIGN GUIDELINES

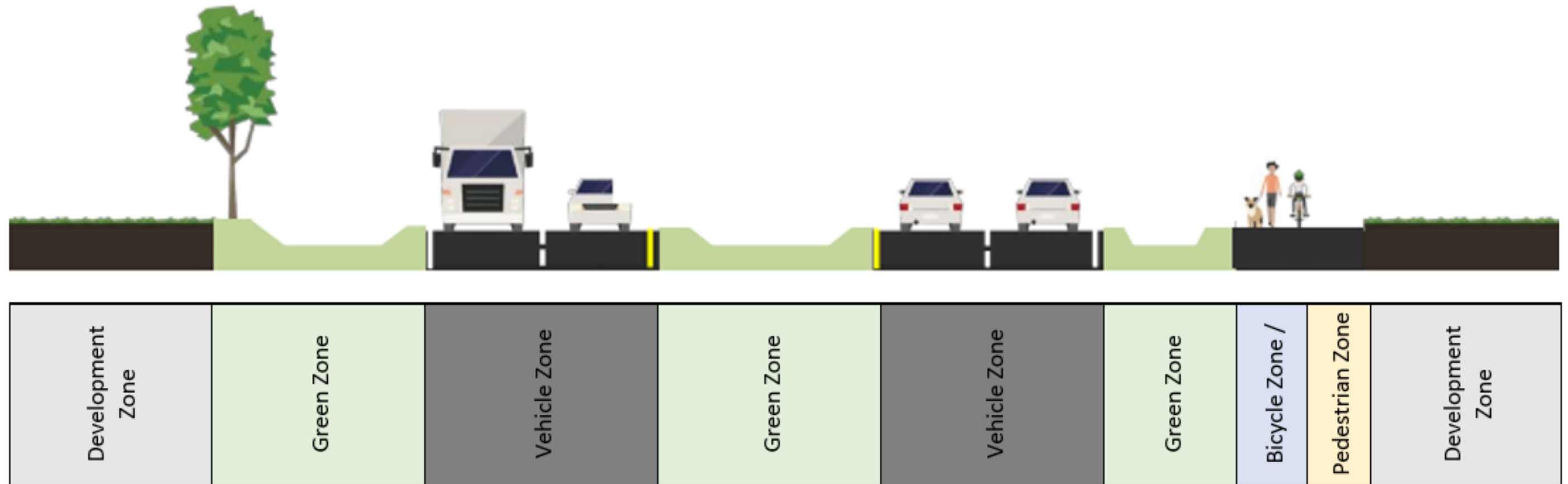
Arterial-Parkway: Typical Zones



Development Zone	Green Zone	Pedestrian Zone	Bicycle Zone
Setbacks, design, and land uses will vary, but the basic intent for this zone is to orient development toward the street. Access from individual properties to the Arterial-Parkway is very limited.	Landscaping and trees in the Green Zone serve multiple purposes: <ul style="list-style-type: none"> • Buffering for pedestrians from weather and automobile traffic • Green Infrastructure to mitigate stormwater and summer heat/glare • Underlying support for property values/desirability of real estate 	Arterial-Parkways typically have higher traffic speeds and volumes, so bicyclists are less likely to feel comfortable in mixed traffic; this zone is important for modal balance, safety and additional buffering between drivers and pedestrians. Bicycle facilities can be provided in a multi-use path.	
Pedestrian Zone (Frontage, Through, Furnishing Zones)	Parking Zone	Vehicle Zone	
Pedestrian travel should be a comfortable travel option on Arterial-Parkways. Therefore, this zone should include unobstructed sidewalks or multi-use paths at appropriate widths for adjacent and surrounding land uses.	This zone does not exist on Parkways given their higher speeds and limited-access nature.	The Vehicle Zone serves motor vehicles, with a variety of lane configurations, to accommodate high traffic volumes.	

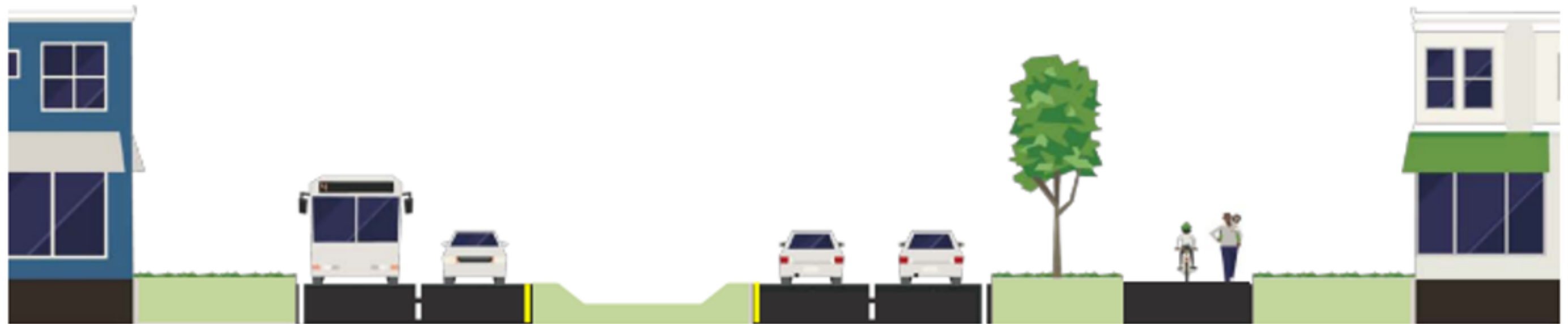
APPENDIX – STREET DESIGN GUIDELINES

Arterial–Parkway Typical Section Example 1: Industrial Arterial–Parkway with Median, Swales and Multi-use Path



APPENDIX – STREET DESIGN GUIDELINES

Arterial–Parkway Typical Section Example 2: Mixed Use Arterial–Parkway with Median and Multi-use Path



Development Zone	Green Zone	Vehicle Zone	Green Zone	Vehicle Zone	Green Zone	Bicycle Zone /	Pedestrian Zone	Green Zone	Development Zone
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APPENDIX – ROADWAY INTERSECTION CHARACTERISTICS

Roadway Intersection Characteristics

	Parkway	Boulevard	Avenue	Other
Medians	Medians are recommended for all intersections. Arterial-Parkways are typically wide, with multiple lanes for pedestrians to cross. Medians provide a needed refuge for pedestrians mid-crossing.	Medians are not typical at smaller intersections, those generally involving two lane roads. Medians are recommended as pavement widths increase and crossing distances for pedestrians are lengthened, as medians provide a refuge for pedestrians mid-crossing.	Medians are not typical at smaller intersections, those generally involving an Avenue and other two-lane roads. Medians are recommended as pavement widths increase and crossing distances for pedestrians are lengthened, as medians provide a refuge for pedestrians mid-crossing.	Medians are not typical.
Left-turn Lanes	Left turn lanes are typical on Arterial-Parkways. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the number and use of left turn lanes	Left turn lanes are typical for Arterial-Boulevards with three lanes or more. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the number and use of left turn lanes	Left turn lanes are typical for Avenues with three lanes or more. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the number and use of left turn lanes	Left turn lane are not typical. An early determination of the type of intersection control must be made (i.e. stop signs, signal, or roundabout). Whenever there is doubt as to the adequacy of stop control during the design life of the project, traffic signal warrants, as outlined in the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) should be investigated. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the number and use of left turn lanes.
Right-turn Lanes	Right turn lanes are not recommended at Arterial-Parkway intersections with Collector-Avenues. Right turn lanes are typical where Arterial-Parkways intersect with Arterial-Boulevards. When right-turn lanes are necessary a pedestrian island is preferred to increase pedestrian safety. Islands should be a minimum of 50 square feet and landscaped to provide a buffer between vehicles and pedestrians. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the need for right turn lanes	When right-turn lanes are necessary a pedestrian island should be considered to increase pedestrian safety. Islands should be a minimum of 50 square feet and landscaped to provide a buffer between vehicles and pedestrians. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the need for right turn lanes.	Right turn lanes are not typical on Avenues in areas of high connectivity in the surrounding street network. When right-turn lanes are necessary, a pedestrian island should be considered to increase pedestrian safety. Islands should be a minimum of 50 square feet and landscaped to provide a buffer between vehicles and pedestrians. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the need for right turn lanes	Right turn lanes are not typical. An early determination of the type of intersection control must be made (i.e. stop signs, signal, or roundabout). Whenever there is doubt as to the adequacy of stop control during the design life of the project, traffic signal warrants, as outlined in the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) should be investigated. Intersection capacity analysis procedures of the current edition of the Highway Capacity Manual should be used to determine the need for right turn lanes.
Curb Extensions	Not typical on Arterial-Parkways, as these streets have higher speeds and no on-street parking.	Boulevard intersections transitioning from on-street parking and those with large shoulders should include curb extensions. Curb extensions help reduce the intersection crossing distance for pedestrians and clearly define the limits of on-street parking.	Avenue intersections transitioning from on-street parking and those with large shoulders should include curb extensions. Curb extensions help reduce the intersection crossing distance for pedestrians and clearly define the limits of on-street parking. Curb Radii It is recommended to keep curb radii as small as possible. Curb radii should be measured by effective radius of turning movements, not actual curb radius.	Curb Extensions are not typical on Neighborhood Connectors or Local Streets. Intersections transitioning from on-street parking and those with large shoulders should include curb extensions. Care should be given to the design when the extension is on or beyond a crest of a vertical curve, or where there is a substantial horizontal curvature on the approach.

APPENDIX – ROADWAY INTERSECTION CHARACTERISTICS

Curb Radii	30 ft. radii or more should be used where feasible at major cross street intersections however it is recommended to keep curb radii as small as possible. Radii of 40 ft. or more, three-centered compound curves or simple curves with tapers to fit truck paths should be provided at intersections used frequently by buses or large trucks. Truck turning templates should be used to determine proper radii and stop bar location. When truck turning templates are used, a 2-foot clearance should be provided between the edge of traveled way and the closest tire path. Curb radii should be measured by effective radius of turning movements, not actual curb radius.		15 to 25 ft. radii are adequate for passenger vehicles and may be provided at minor cross streets where there are few trucks or at major intersections where there are parking lanes. 5 ft. or more radii should be provided at minor intersections on new or reconstruction projects where space permits. Radii of 40 ft. or more, three-centered compound curves or simple curves with tapers to fit truck paths should be provided at intersections used frequently by buses or large trucks. Truck turning templates should be used to determine proper radii and stop bar location. When truck turning templates are used, a 2-foot clearance should be provided between the edge of traveled way and the closest tire path.	
Crosswalks	Crosswalks are recommended at all intersections on all legs, unless pedestrian crossing would decrease safety. Crosswalks should be located outside of the curb radius where possible, while remaining as close to the intersection as possible. There are two types of standard crosswalks: 1) Standard (transverse) crosswalk markings – A standard crosswalk consists of two transverse (parallel) lines, each a minimum of 12 inches in width. 2) High-visibility (longitudinal) crosswalk markings – A high visibility ODOT Standard crosswalk consists of longitudinal lines only striped parallel to the direction of travel. Additionally, the OMUTCD allows longitudinal lines to be used alone or in addition to the transverse lines, thus creating a ladder-style crossing. Where the determination has been made to install crosswalk markings, the longitudinal bar crosswalk should be used in the following situations: 1) At intersections where a minimum of one approach has a speed limit of 35 mph or higher. 2) At all established mid-block pedestrian crossings and with appropriate signing accompaniment.			
Bike Signals	Arterial Parkways that intersect streets with high bike volumes or intersections that transition from one bike facility to another may need signalization for bicyclists.	Arterial-Boulevards with high bike volumes or intersections that transition from one bike facility to another may need signalization for bicyclists.	Avenues with high bike volumes or intersections that transition from one bike facility to another may need signalization for bicyclists.	Bike Signals are not typical on Neighborhood Connectors or Local Streets. A bicycle signal is typically used in the following situations: 1) Where the bikeway is a one-way or two-way separated bike lane; 2) Where bicyclists’ position in the bikeway does not allow them to see motor vehicle or pedestrian signals that may otherwise be able to control their movement, and 3) Where intersection complexity is such that signals may be helpful, as determined by engineering judgment. Traffic signal indications for a bicyclist along a corridor should be as uniform as possible.
Bike Boxes	Bike boxes are recommended at intersections where there may be right or left-turning conflicts between bicyclists and motorists. Two-stage turn boxes provide a formal queuing space for bicyclists needing a two-stage turn. Arterial-Boulevards with high traffic speeds and/ or traffic volumes and multi-lane roadways are often appropriate for two-stage turn boxes.		Bike boxes are recommended at intersections where there may be right or left-turning conflicts between bicyclists and motorists.	Bike Boxes are not typical on Neighborhood Connectors or Local Streets. The use of green-colored pavement to supplement other bikeway pavement markings, such as a shared lane marking, requires experimental approval from FHWA. Designers should look to IA-14 and FHWA requirements for experimentation when determining if an experimental design is appropriate to consider and feasible to implement.

References:

- ODOT Location & Design Manual, Volume 1 - Roadway Design: <https://www.transportation.ohio.gov/working/engineering/roadway/manuals-standards/location-design-vol-1/location-design-vol-1>
- ODOT Multimodal Design Guide: <https://www.transportation.ohio.gov/working/engineering/roadway/manuals-standards/multimodal>



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