City of Montclair

STANDARDS AND SPECIFICATIONS

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STD No. 103	CROSS GUTTER
STD No. 104	ALLEY INTERSECTION
STD No. 105	RIBBON GUTTER
STD No. 106	STREET CROSS SECTION
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STD No. 202	SIDEWALK DRAIN OUTLET AND PIPE DRAIN THRU CURB
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STD No. 301	STANDARD PAVING AND TRENCH REPAIR
STD No. 302	PLATE BRIDGING
STD No. 401	ECCENTRIC CONE PRECAST CONCRETE MANHOLE
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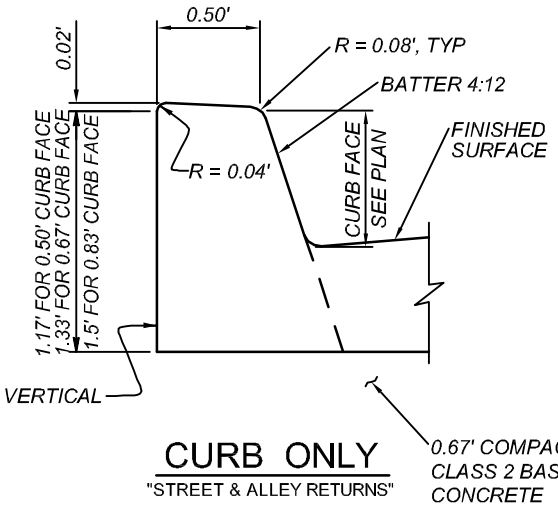
STD No. 403	STANDARD CHIMNEY PIPE
STD No. 404	SEWER SADDLE
STD No. 405	SEWER TERMINAL CLEAN-OUT
STD No. 406	CONCRETE CRADLE & ENCASEMENT
STD No. 407	MANHOLE FRAME AND COVER
STD No. 408	STANDARD PLAN DROP MANHOLE
STD No. 409	BREAK INTO AND RECHANNEL AN EXISTING MANHOLE
STD No. 410	SAMPLING STATION
STD No. 411	SAND INTERCEPTOR
STD No. 412	GREASE INTERCEPTOR
-	STANDARD TREE WELL WITH TREE – SEE APWA SPPWC PLANS 518, 519 & 520
-	ROOT BARRIER – SEE APWA SPPWC PLAN 523
STD DETAIL A	MICRO TRENCHING

CITY OF MONTCLAIR

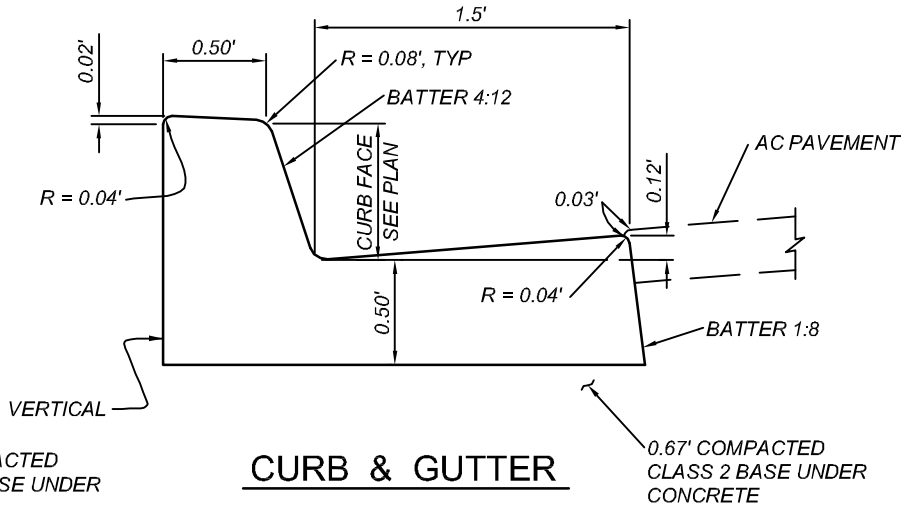
ABBREVIATIONS

AC	= ASPHALT CONCRETE
BCR	= BEGIN CURB RETURN
BLDG	= BUILDING
CONC	= CONCRETE
CF	= CUBIC FEET
℄	= CENTER LINE
CONST	= CONSTRUCTION
DIAM	= DIAMETER
ECR	= END CURB RETURN
EJ	= EXPANSION JOINT
FL	= FLOW LINE
FT	= FEET
GA	= GAUGE
GALV	= GALVANIZED
MAX	= MAXIMUM
MH	= MANHOLE
MIN	= MINIMUM
NO	= NUMBER
OC	= ON CENTER
OD	= OUTSIDE DIAMETER
P	= PLATE
PCC	= PORTLAND CEMENT CONCRETE
PKWY	= PARKWAY
PL	= PROPERTY LINE
PT	= POINT OF TANGENCY
PWC	= POINT WORKING CENTER
R	= RADIUS
R/W	= RIGHT OF WAY
SC	= SAW CUT
SQFT	= SQUARE FEET
STD	= STANDARD
TC	= TOP OF CURB
TI	= TRAFFIC INDEX
TYP	= TYPICAL
VCP	= VITRIFIED CLAY PIPE
WPJ	= WEAKEND PLANE POINT

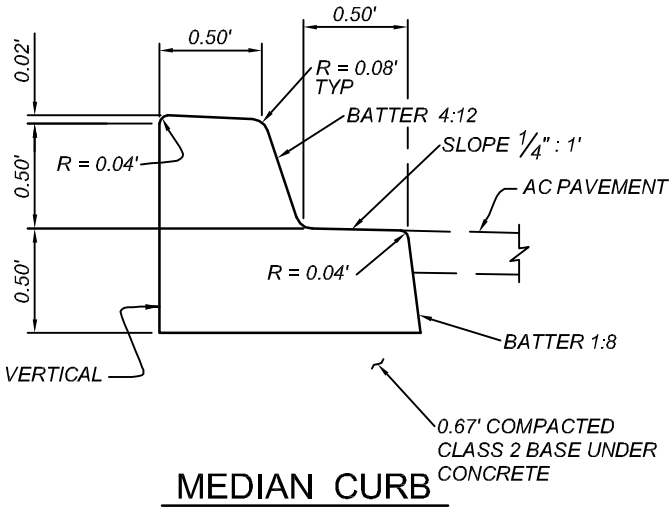


CURB ONLY

"STREET & ALLEY RETURNS"



CURB & GUTTER



MEDIAN CURB

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.
2. CURB SHALL BE SCORED AT 5' INTERVALS TO CONFORM TO SIDEWALK SCORING, WITH EXPANSION JOINTS PLACED NOT TO EXCEED 60' OC AND @ BCR'S / ECR'S. WEAKENED PLANE JOINTS NOT TO EXCEED 15 FT OC.
3. IMMEDIATELY AFTER COMPLETION OF FINISHING, CONCRETE CURING COMPOUND SHALL BE APPLIED AS A FINE SPRAY TO ALL EXPOSED SURFACES INCLUDING THE BACK OF CURB.
4. CURB AND GUTTER SHALL NOT BE CONSTRUCTED MONOLITHIC WITH SIDEWALK OR DRIVE APPROACH.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19

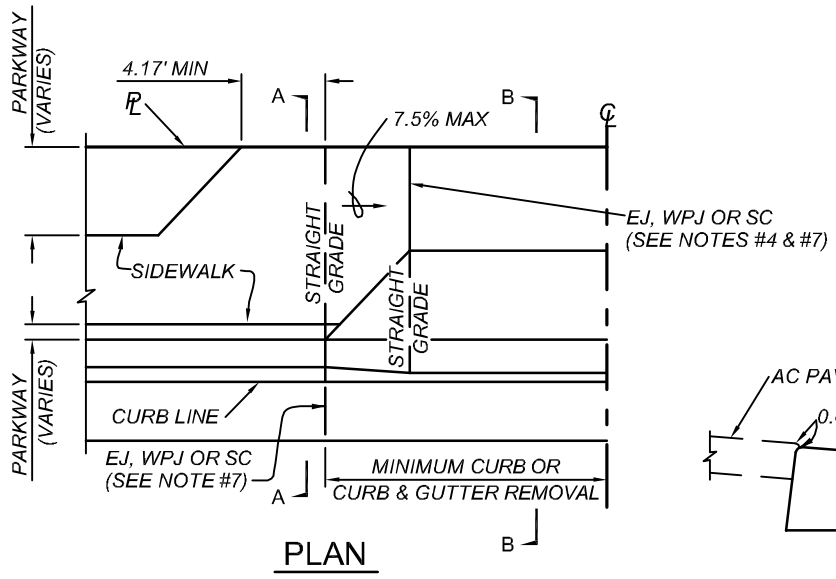
NOEL A. CASTILLO
CITY ENGINEER

DATE

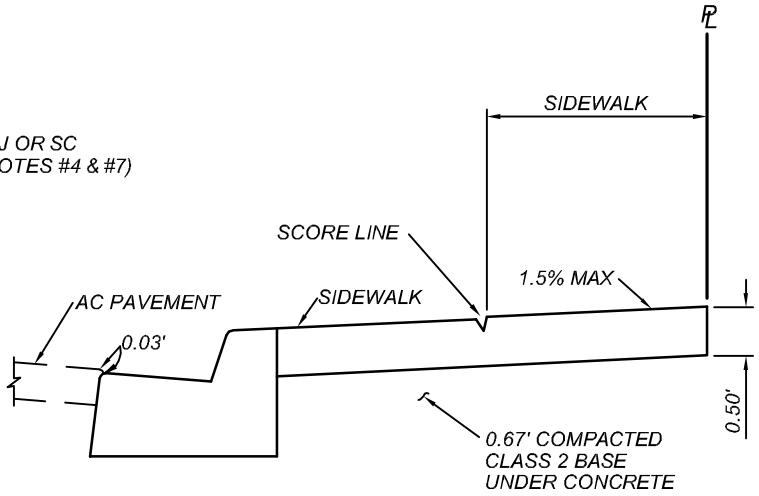
CITY OF MONTCLAIR

CURB DETAILS

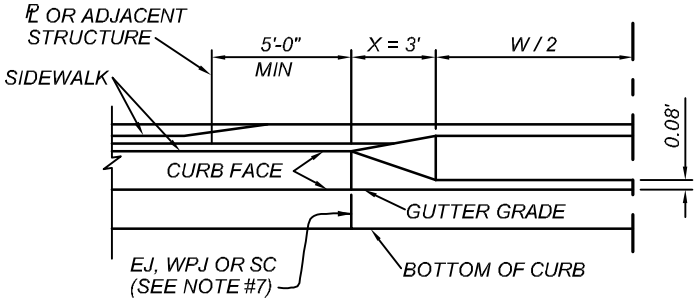
NO SCALE	STD. PLAN NO. 101	SHEET 1 of 1



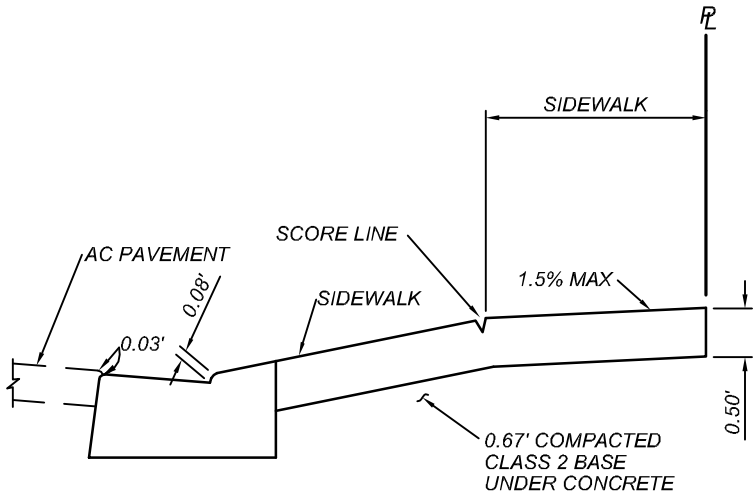
PLAN



SECTION A-A



FRONT ELEVATION



SECTION B-B

NOTES:

1. MINIMUM "W" SHALL BE 12'.
2. MINIMUM FULL CURB BETWEEN DRIVEWAYS SHALL BE 1.50'.
3. MINIMUM CLEARANCE TO FIRE HYDRANTS, LIGHT STANDARDS, POWER POLES AND OTHER OBSTRUCTIONS SHALL BE 5' FROM TOP OF "X".
4. WHERE SIDEWALKS EXIST, WITHIN LIMITS OF A RESIDENTIAL DRIVEWAY, THE WALK MAY REMAIN AT OPTION OF THE INSPECTOR IF IN GOOD CONDITION.
5. WHERE CURB & GUTTER EXIST, BOTH SHALL BE REMOVED AND REPLACED AS SHOWN FOR MINIMUM CURB REMOVAL.
6. ALL DRIVEWAYS SHALL BE 560-C-3250 PCC.
7. CURB AND WALK SHALL BE REMOVED BY SAW CUTTING OR, IF WITHIN 5', TO A WEAKENED PLANE OR EXPANSION JOINT.
8. FOR CURB-ADJACENT PARKWAY WITH SIDEWALK, SEE CALTRANS STANDARD PLAN A87A FOR SIDEWALK TRANSITIONS.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19
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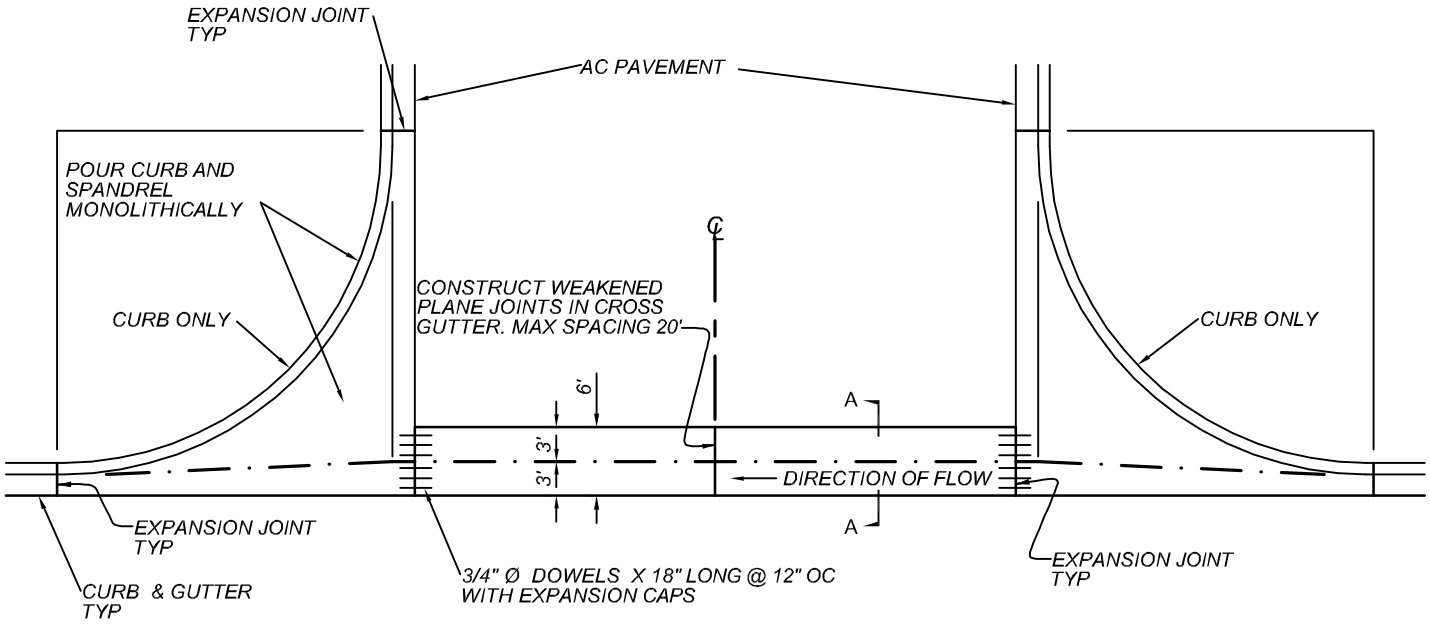
CITY ENGINEER

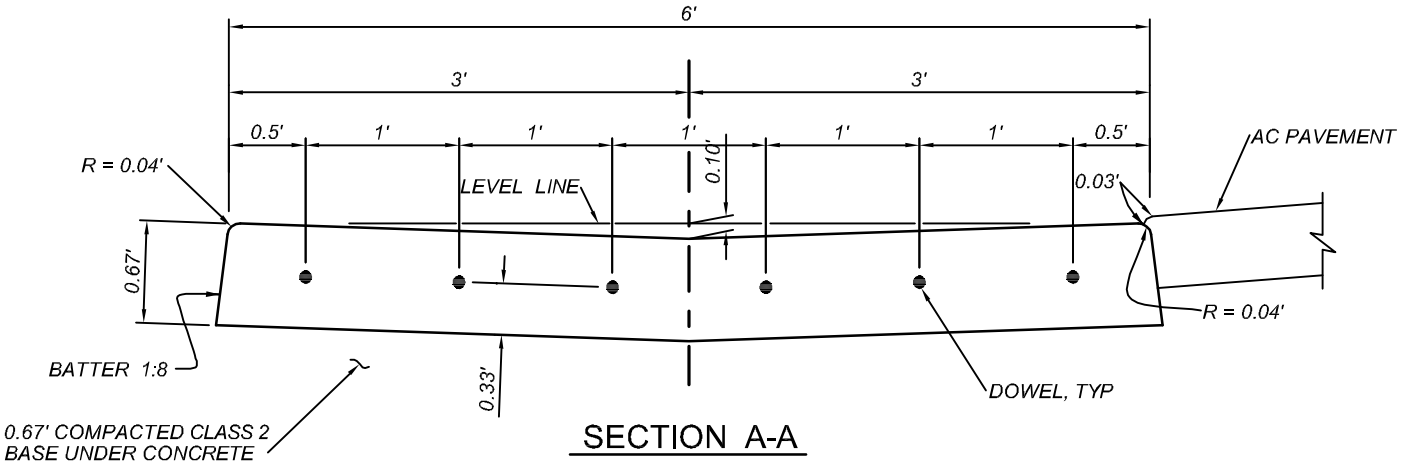
CITY OF MONTCLAIR

DRIVE APPROACH

NO SCALE	STD. PLAN NO. 102	SHEET 1 of 1



PLAN



SECTION A-A

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED FOR COMPLETE SPANDREL AND CROSS GUTTER AS SHOWN.
2. IMMEDIATELY AFTER COMPLETION OF FINISHING, CONCRETE CURING COMPOUND SHALL BE APPLIED AS A FINE SPRAY TO ALL EXPOSED SURFACES INCLUDING THE BACK OF CURB.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19
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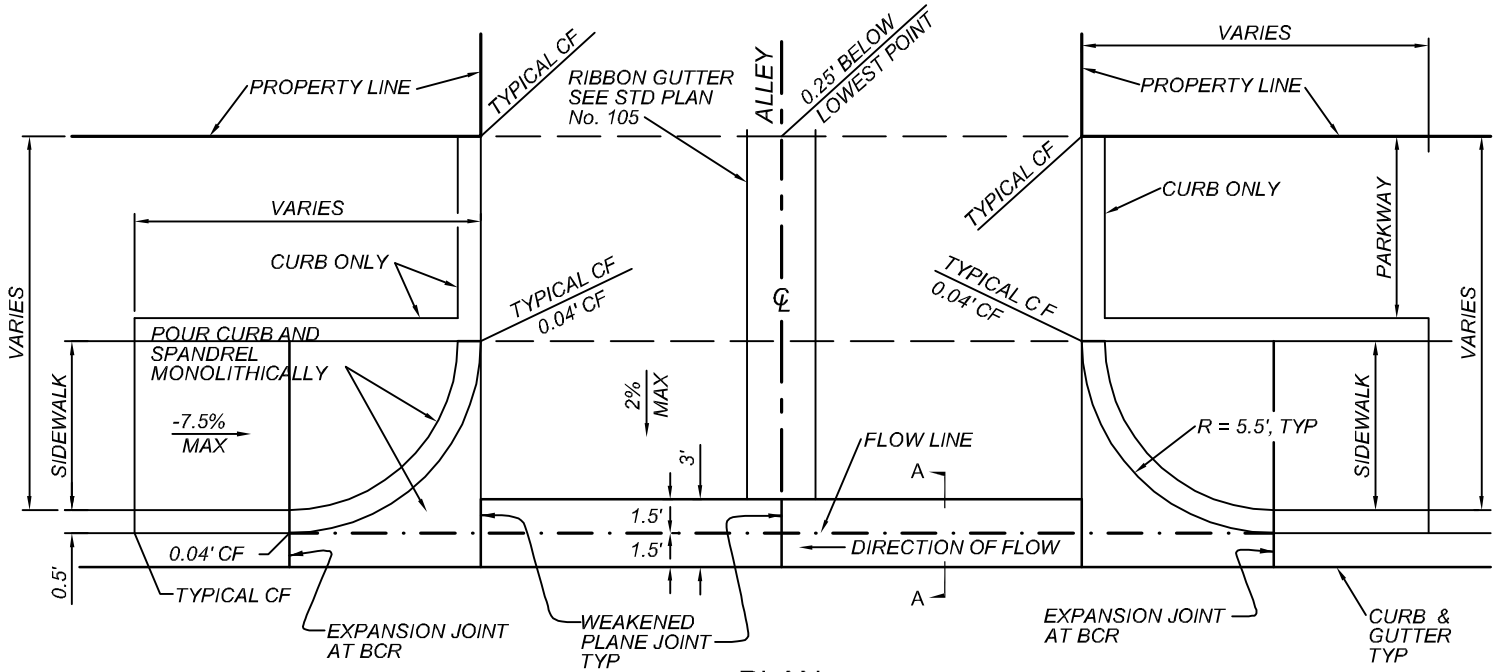
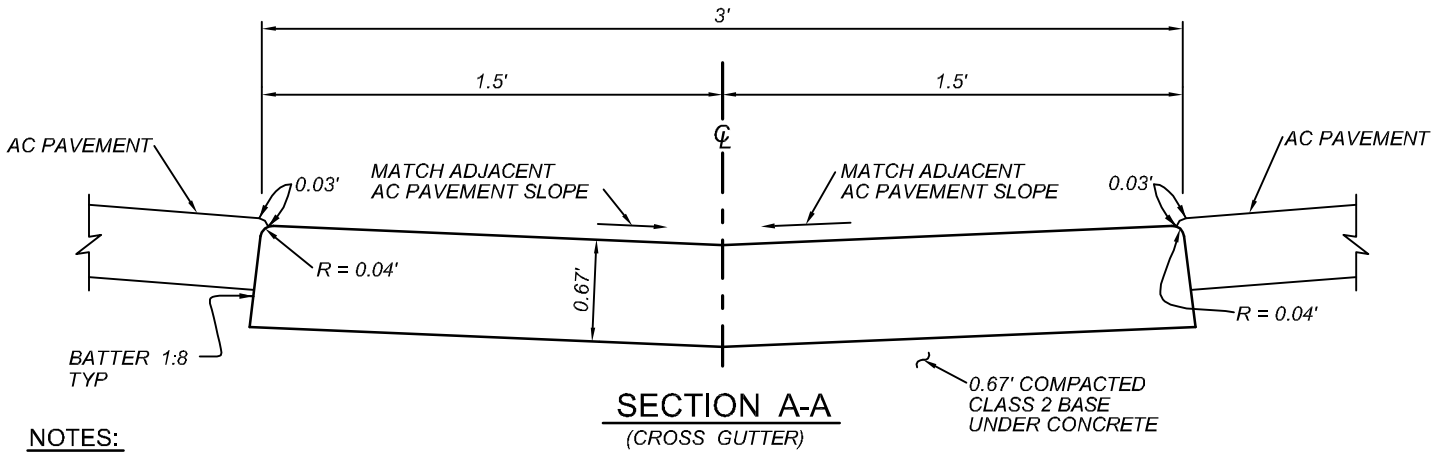
CITY OF MONTCLAIR

SPANDREL AND CROSS GUTTER

NO SCALE	STD. PLAN NO. 103	SHEET 1 of 1



PLAN
CURB-ADJACENT SIDEWALK



SECTION A-A
(CROSS GUTTER)

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.
2. NOTE TOP OF CURB ELEVATION AT PROPERTY LINE TO BE SET AT 1/4" PER FT SLOPE ABOVE TC AT CURB PI.
3. CURB FROM ECR TO PROPERTY LINE TO BE SET AT 1/4" PER FT SLOPE.
4. IMMEDIATELY AFTER COMPLETION OF FINISHING, CONCRETE CURING COMPOUND SHALL BE APPLIED AS A FINE SPRAY TO ALL EXPOSED SURFACES INCLUDING THE BACK OF CURB.
5. WHEN CROSS GUTTER IS TO BE POURED SEPARATE FROM SPANDREL, PLACE 3- 3/4"Ø DOWELS, 1.5' LONG TOGETHER WITH EXPANSION JOINTS AT EDGE OF SPANDREL.
6. WHEN CROSS GUTTER IS POURED MONOLITHIC WITH SPANDREL, A WEAKENED PLANE JOINT SHALL BE CONSTRUCTED AT CENTER OF CROSS GUTTER.

90° INTERSECTION - CROSS GUTTER AREA - SQ.FT.	
20' ALLEY	89.5

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19
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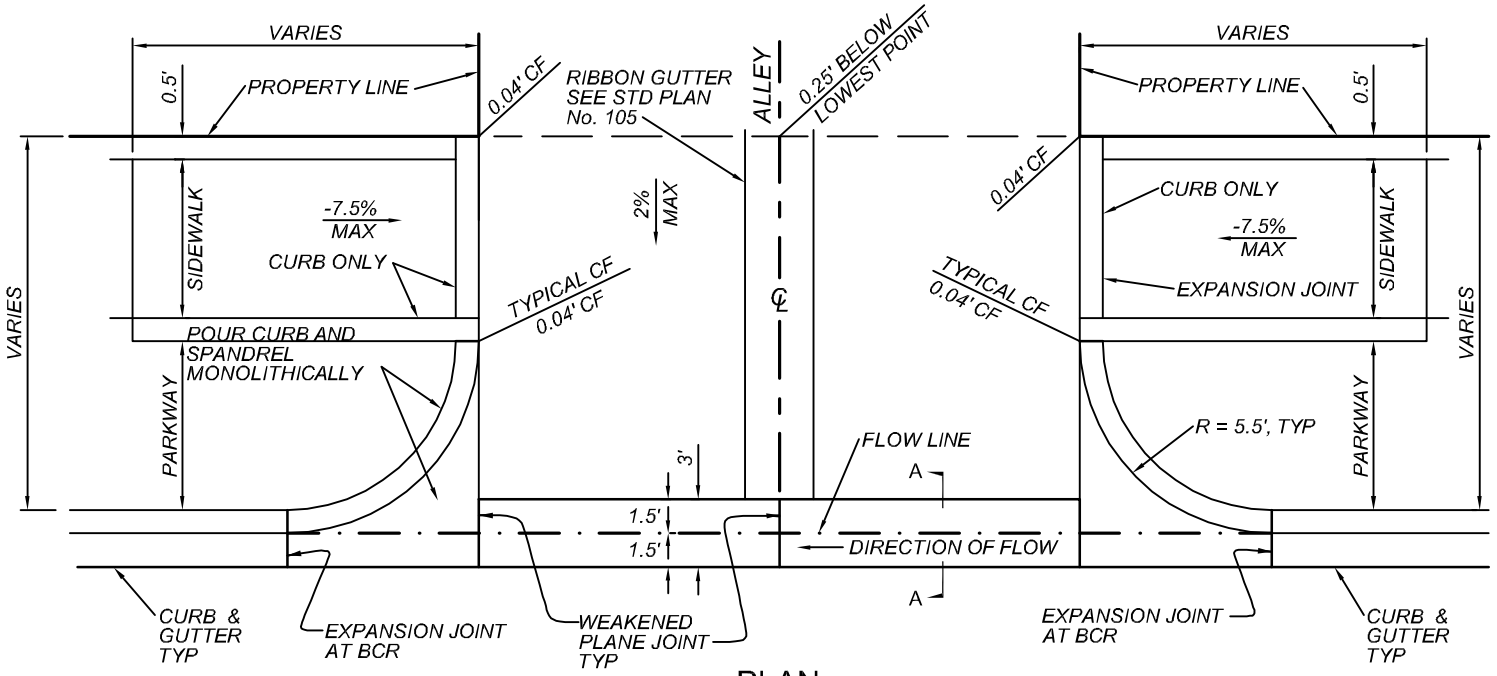
CITY ENGINEER

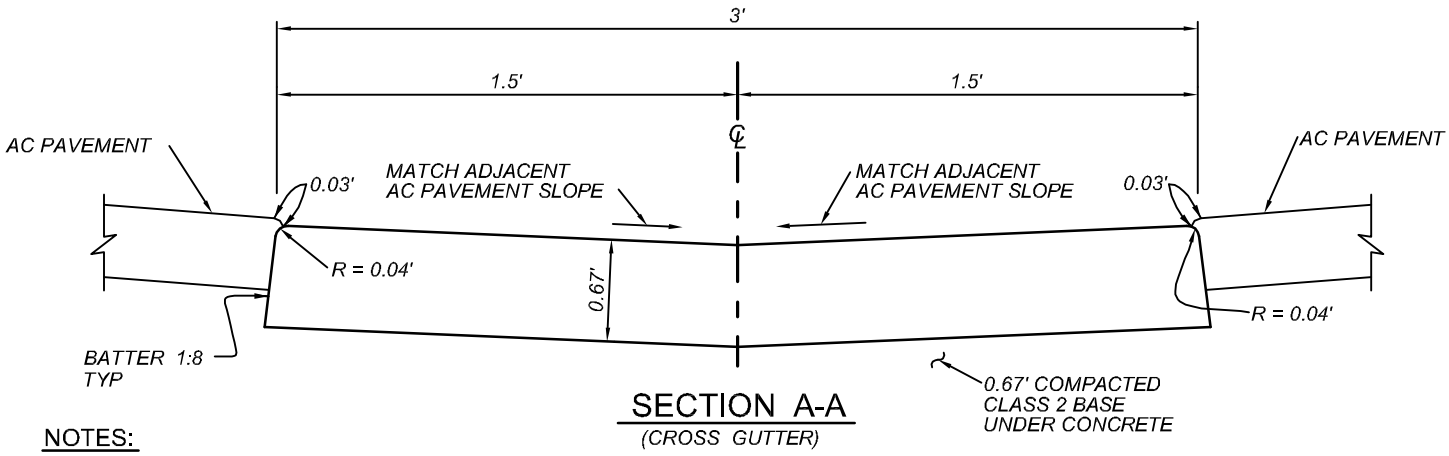
CITY OF MONTCLAIR

ALLEY INTERSECTION

NO SCALE	STD. PLAN NO. 104	SHEET 1 of 2



PLAN
CURB-ADJACENT PARKWAY



SECTION A-A
(CROSS GUTTER)

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.
2. NOTE TOP OF CURB ELEVATION AT PROPERTY LINE TO BE SET AT 1/4" PER FT SLOPE ABOVE TC AT CURB PI.
3. CURB FROM ECR TO PROPERTY LINE TO BE SET AT 1/4" PER FT SLOPE.
4. IMMEDIATELY AFTER COMPLETION OF FINISHING, CONCRETE CURING COMPOUND SHALL BE APPLIED AS A FINE SPRAY TO ALL EXPOSED SURFACES INCLUDING THE BACK OF CURB.
5. WHEN CROSS GUTTER IS TO BE POURED SEPARATE FROM SPANDREL, PLACE 3- 3/4"Ø DOWELS, 1.5' LONG TOGETHER WITH EXPANSION JOINTS AT EDGE OF SPANDREL.
6. WHEN CROSS GUTTER IS POURED MONOLITHIC WITH SPANDREL, A WEAKENED PLANE JOINT SHALL BE CONSTRUCTED AT CENTER OF CROSS GUTTER.

90° INTERSECTION - CROSS GUTTER AREA - SQ.FT.	
20' ALLEY	89.5

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

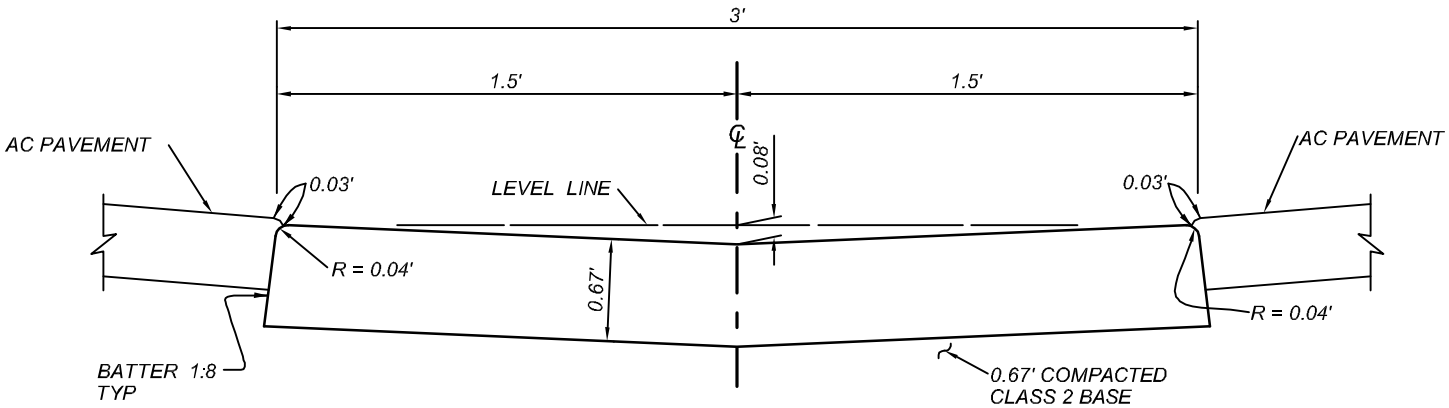
APPROVED BY:

09-20-19
NOEL A. CASTILLO — DATE
CITY ENGINEER

CITY OF MONTCLAIR

ALLEY INTERSECTION

NO SCALE	STD. PLAN NO. 104	SHEET 2 of 2



SECTION

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.
2. EXPANSION JOINTS SHALL BE PLACED NOT TO EXCEED 60' OC & WEAKENED PLANE JOINTS NOT TO EXCEED 15' OC.
3. IMMEDIATELY AFTER COMPLETION OF FINISHING, CONCRETE CURING COMPOUND SHALL BE APPLIED AS A FINE SPRAY TO ALL EXPOSED SURFACES INCLUDING THE BACK OF CURB.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19

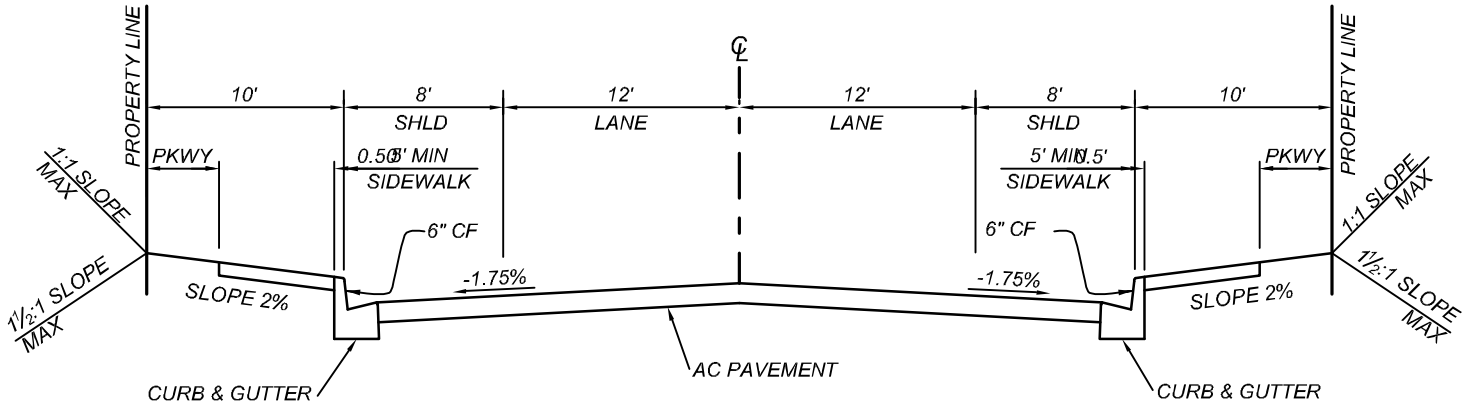
NOEL A. CASTILLO
CITY ENGINEER

DATE

CITY OF MONTCLAIR

RIBBON GUTTER

NO SCALE	STD. PLAN NO. 105	SHEET 1 of 1



SECTION

MINIMUM A.C. PAVEMENT THICKNESS REQUIREMENTS

MAJOR STEET	6"
SECONDARY STREET	5"
COLLECTOR STREET	4"
INDUSTRIAL STREET	5"
LOCAL STREET	4"

NOTES:

1. CROSS SECTIONS AND CURB LOCATIONS ARE TO BE DETERMINED BY THE CITY ENGINEER FOR ALL OTHER THAN 60' R/W.
2. SIDEWALK SHALL BE ADJACENT TO CURB UNLESS IT IS NECESSARY TO PROVIDE A PARKWAY TO MATCH EXISTING CONDITIONS, AS DETERMINED BY THE CITY ENGINEER.
3. THICKNESS OF PAVEMENT AND BASE SHALL BE DETERMINED FROM TI & R VALUE FROM SOILS TEST AND SO INDICATED ON THE PLANS.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

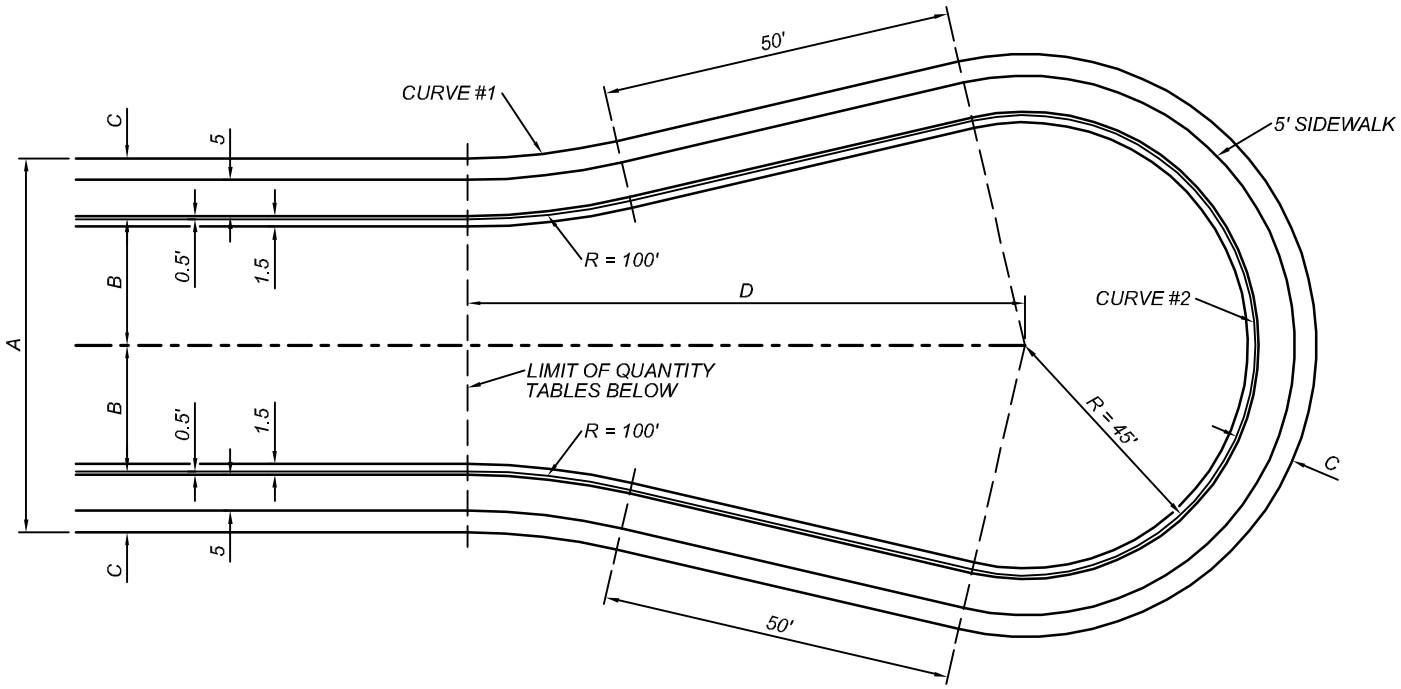
APPROVED BY:

09-20-19
NOEL A. CASTILLO — DATE
CITY ENGINEER

CITY OF MONTCLAIR

STREET CROSS SECTION

NO SCALE	STD. PLAN NO. 106	SHEET 1 of 1



A	B	C	D	CURVE #1 Δ	CURVE #1 CURB R	CURVE #1 CURB L	CURVE #1 CURB T	CURVE #1 ℄ R	CURVE #1 ℄ L	CURVE #1 ℄ T	CURVE #2 Δ	CURVE #2 CURB R	CURVE #2 CURB L	CURVE #2 ℄ R	CURVE #2 ℄ L
50	18	7	97.99	20°40'48"	100	36.09	18.25	93	33.57	16.97	221°21'35"	45	173.86	52	200.90
60	18	12	97.99	20°40'48"	100	36.09	18.25	88	31.76	16.06	221°21'35"	45	173.86	57	220.22
60	20	10	95.53	19°29'44"	100	34.03	17.18	90	30.62	15.46	218°59'28"	45	172.00	55	210.22

A	B	CURB LENGTH FT.	SIDEWALK - SQ. FT. ADJACENT TO CURB	SIDEWALK - SQ. FT. ADJACENT TO ℄	PAVEMENT SQ. FT. 1.5' GUTTER
50	18	346.04	1777.33	1793.04	8770.15
60	18	346.04	1777.33	1871.58	8770.15
60	20	340.05	1747.36	1810.19	8841.78

NOTES:

1. SIDEWALK LOCATION SHALL BE ADJACENT TO THE CURB UNLESS DETERMINED OTHERWISE BY THE CITY ENGINEER.
2. ALL STREET RIGHT OF WAY WIDTHS SHALL BE 60' UNLESS OTHERWISE APPROVED.

REGISTERED CIVIL ENGINEER
MONICA HEREDIA
No. C 56547
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

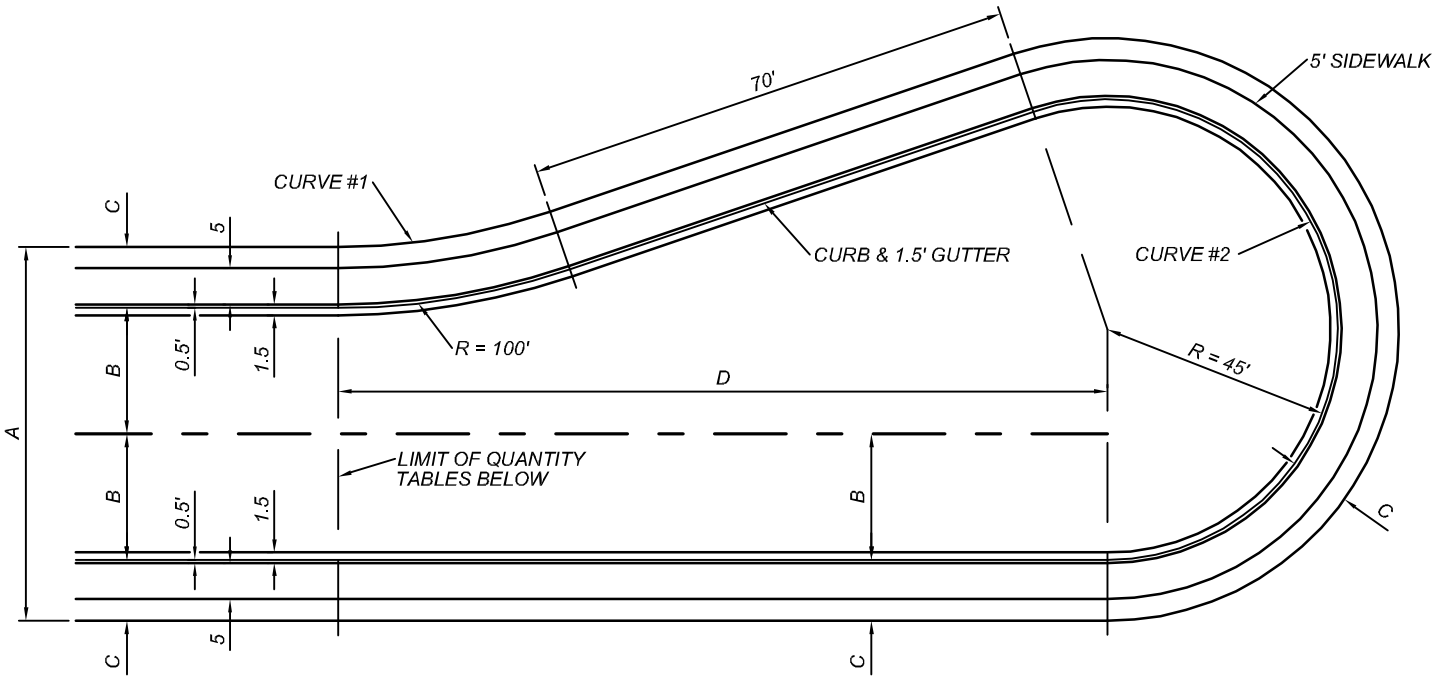
MONICA HEREDIA
CITY ENGINEER

05-14-24
DATE

CITY OF MONTCLAIR

STANDARD CUL-DE-SAC

NO SCALE | STD. PLAN NO. 107 | SHEET 1 of 1



A	B	C	D	CURVE #1 Δ	CURVE #1 CURB R	CURVE #1 CURB L	CURVE #1 CURB T	CURVE #1 ℄ R	CURVE #1 ℄ L	CURVE #1 ℄ T	CURVE #2 Δ	CURVE #2 CURB R	CURVE #2 CURB L	CURVE #2 ℄ R	CURVE #2 ℄ L
50	18	7	132.83	29°48'59"	100	52.04	26.62	93	48.40	24.76	209°48'59"	45	164.79	52	190.42
60	18	12	132.83	29°48'59"	100	52.04	26.62	88	45.79	23.43	209°48'59"	45	164.79	57	208.73
60	20	10	130.00	28°04'21"	100	49.00	25.00	90	44.10	22.50	208°04'21"	45	163.42	55	199.74

A	B	CURB LENGTH FT.	SIDEWALK - SQ. FT. ADJACENT TO CURB	SIDEWALK - SQ. FT. ADJACENT TO ℄	PAVEMENT SQ. FT. 1.5' GUTTER
50	18	419.66	2145.42	2161.13	10783.89
60	18	419.66	2145.42	2239.67	10783.89
60	20	412.42	2109.20	2172.03	10916.31

NOTES:

1. SIDEWALK LOCATION SHALL BE ADJACENT TO THE CURB UNLESS DETERMINED OTHERWISE BY THE CITY ENGINEER.
2. ALL STREET RIGHT OF WAY WIDTHS SHALL BE 60' UNLESS OTHERWISE APPROVED.

REGISTERED CIVIL ENGINEER
MONICA HEREDIA
No. C 56547
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

M Heredia

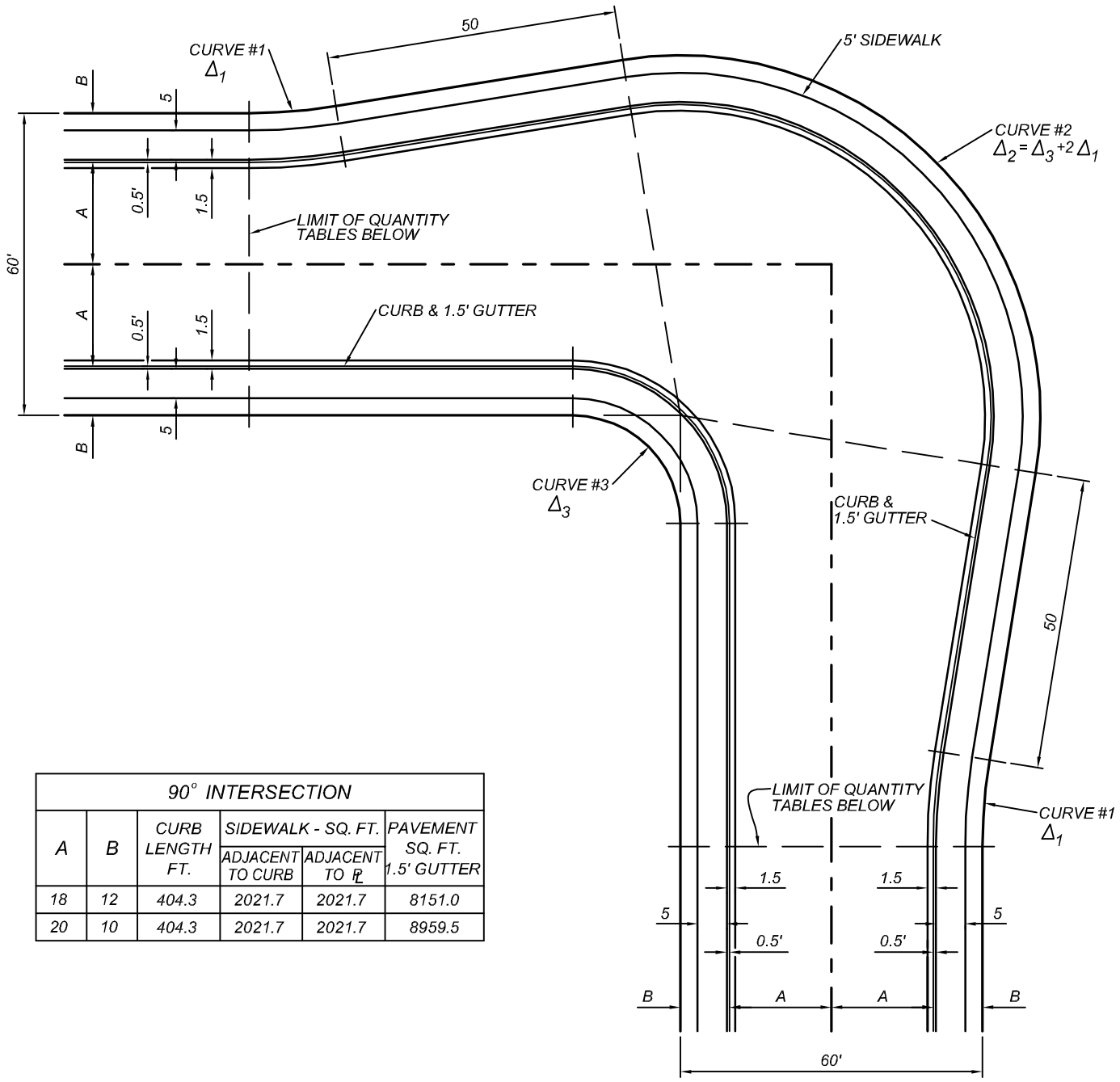
05-14-24
DATE

MONICA HEREDIA
CITY ENGINEER

CITY OF MONTCLAIR

OFFSET CUL-DE-SAC

NO SCALE | STD. PLAN NO. 108 | SHEET 1 of 1



90° INTERSECTION					
A	B	CURB LENGTH FT.	SIDEWALK - SQ. FT.		PAVEMENT SQ. FT. 1.5' GUTTER
			ADJACENT TO CURB	ADJACENT TO ℄	
18	12	404.3	2021.7	2021.7	8151.0
20	10	404.3	2021.7	2021.7	8959.5

			CURVE #1							CURVE #2			CURVE #3		
A	B	C	Δ_1	CURB			℄			Δ_2	CURB		Δ_3	CURB	
				R	L	T	R	L	T		R	℄		R	℄
18	12	74.83	9°09'36"	102	16.31	8.17	90	14.39	7.21	VARIES	58	70	VARIES	27	15
20	10	74.83	9°09'36"	100	15.99	8.01	90	14.39	7.21	VARIES	60	70	VARIES	25	15

NOTE:

1. SIDEWALK LOCATION SHALL BE ADJACENT TO THE CURB UNLESS DETERMINED OTHERWISE BY THE CITY ENGINEER.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

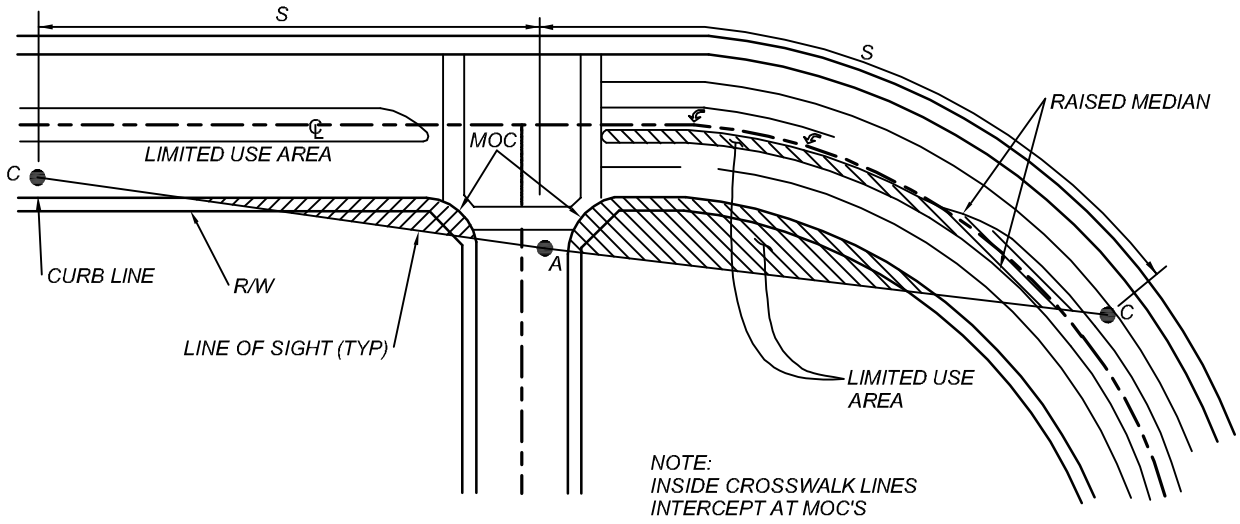
09-20-19
DATE

NOEL A. CASTILLO
CITY ENGINEER

CITY OF MONTCLAIR

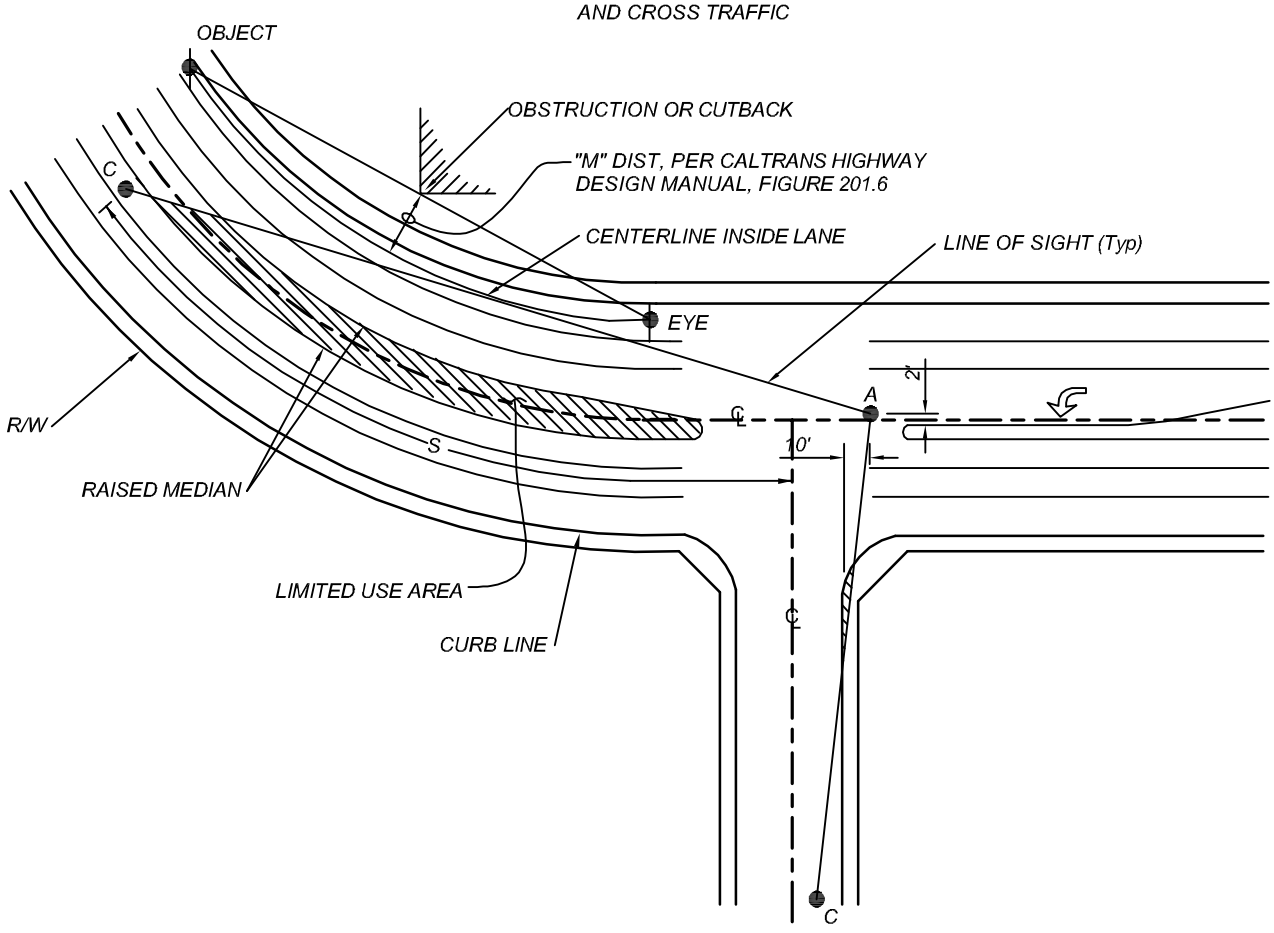
STANDARD KNUCKLE (60' RIGHT-OF-WAY)

NO SCALE | STD. PLAN NO. 109 | SHEET 1 of 1



SIGHT DISTANCE

LEFT AND RIGHT TURN OUT AND CROSS TRAFFIC



SIGHT DISTANCE

LEFT TURN IN

APPROVED BY:

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

09-20-19

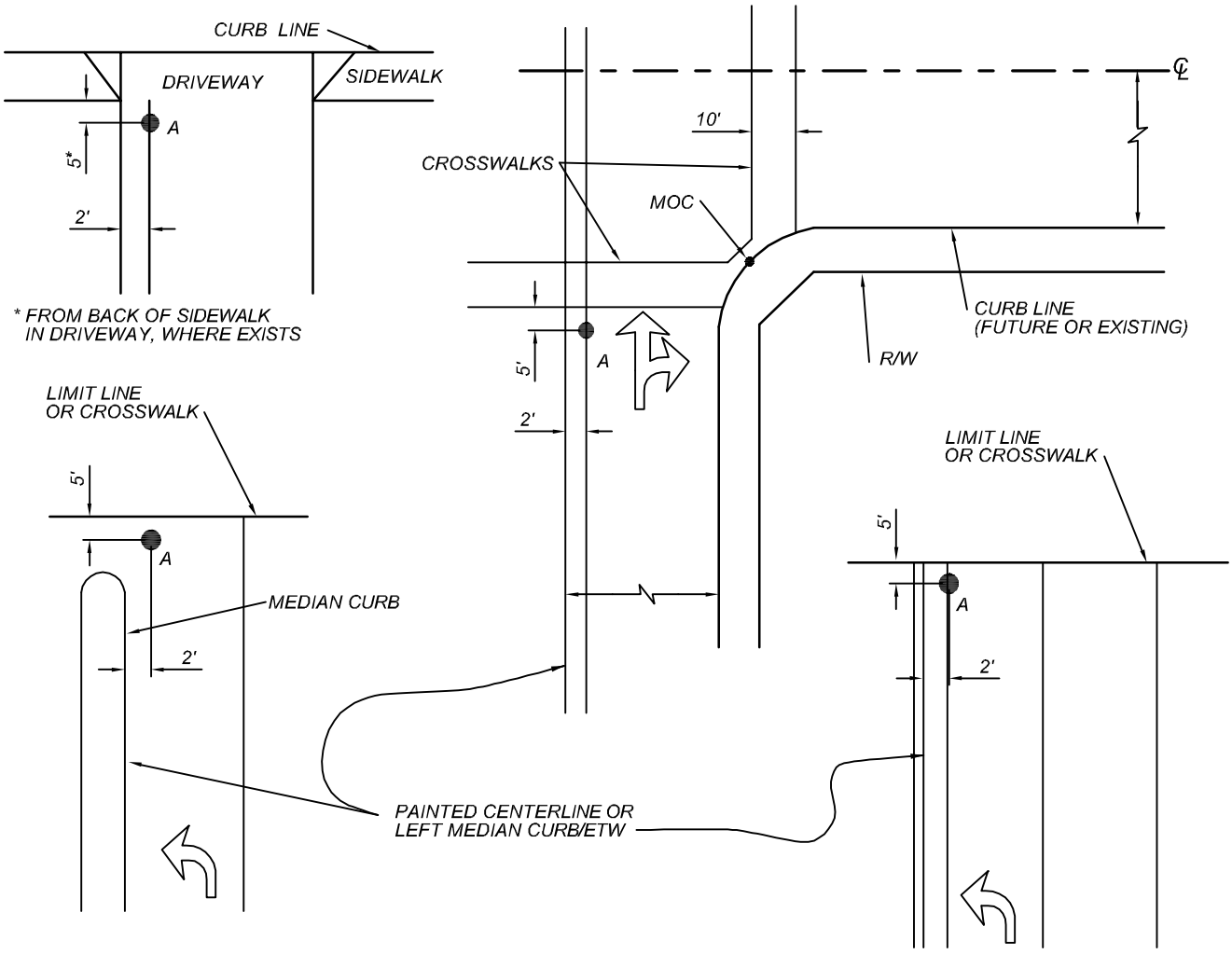
NOEL A. CASTILLO
CITY ENGINEER

DATE

CITY OF MONTCLAIR

INTERSECTION SIGHT DISTANCE

NO SCALE	STD. PLAN NO. 110	SHEET 1 of 3



DESIGN SPEED OR 85TH PERCENTILE (MPH)	STOPPING DISTANCE (S) (FT)
0	0
15	80
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

(1) STOPPING DISTANCE = SIGHT DISTANCE = S

SOURCE: AASHTO GEOMETRIC DESIGN
OF HIGHWAYS AND STREETS
DATED: 2018

NOTES:

1. ● = POINT OF MEASUREMENT.
2. REFER TO SHEET 1 FOR LINE OF SITE APPLICATION TO DISTANCE (S).
3. WHERE VEHICLES ARE BACKING INTO ON COMING TRAFFIC, OFFSET DISTANCE FOR POINT "A" SHALL BE 13' VERSUS 5'.
4. MOC - MIDDLE OF CURB RETURN.

REGISTERED CIVIL ENGINEER
NOEL CASTILLO
No. C 78044
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

09-20-19
DATE

NOEL A. CASTILLO
CITY ENGINEER

CITY OF MONTCLAIR

INTERSECTION SIGHT DISTANCE

NO SCALE	STD. PLAN NO. 110	SHEET 2 of 3

NOTES:

1. THE DISTANCE "S" REPRESENTS THE INTERSECTION SIGHT DISTANCE MEASURED ALONG THE CENTERLINE OF THE ROAD. THE INTERSECTION SIGHT DISTANCE IS THE DISTANCE REQUIRED TO ALLOW STOPPING DISTANCE FOR THE DRIVER ON THE CROSS ROAD (OR LEFT TURN POCKET) TO SAFETY CROSS THE MAIN ROADWAY OR TURN LEFT WHILE THE APPROACH VEHICLE TRAVELS AT THE ASSUMED DESIGN SPEED OF THE MAIN ROADWAY.

2. THE DISTANCE S SHOULD BE INCREASED BY 20% FROM THE AMOUNT SHOWN ON THE STOPPING DISTANCE TABLE ON SUSTAINED DOWNGRADES STEEPER THAN 3% AND LONGER THAN ONE MILE. INTERSECTIONS SHALL BE ILLUMINATED.

3. POINT "A" IS THE LOCATION OF A DRIVER'S LINE OF SIGHT (3.5 FOOT EYE HEIGHT) TO ONCOMING VEHICLES (4.25 FOOT OBJECT HEIGHT) LOCATED AT POINT "C" WHILE IN A VEHICLE AT AN INTERSECTION 5 FEET BACK FROM THE PROJECTION OF THE FIRST CROSSWALK LINE LEADING INTO THE INTERSECTION. IN NO CASE SHALL POINT A BE LESS THAN FIFTEEN FEET FROM THE EDGE OF THE TRAVELED WAY. POINT "C" IS LOCATED AT THE CENTER OF LANE.

4. THE LIMITED USE AREA IS DETERMINED BY THE GRAPHICAL METHOD USING THE APPROPRIATE SIGHT DISTANCES. IT SHALL BE USED FOR THE PURPOSE OF PROHIBITING OR CLEARING OBSTRUCTIONS IN ORDER TO MAINTAIN ADEQUATE SIGHT DISTANCE AT INTERSECTIONS.

5. THE LINE OF SIGHT LINE SHALL BE SHOWN AT INTERSECTIONS ON ALL LANDSCAPING PLANS, GRADING PLANS, AND TENTATIVE TRACT MAPS. IN CASES WHERE AN INTERSECTION IS LOCATED ON A VERTICAL CURVE, A PROFILE OF THE LINE OF SIGHT MAY BE REQUIRED. THE LANDSCAPE PLAN SUBMITTED SHALL SHOW THE NAME, LOCATION AND MATURE DIMENSIONS, PLOTTED TO SCALE OF ALL THE PROPOSED TREES WITHIN THE LIMITED USE AREA.

6. OBSTRUCTIONS SUCH AS BUS SHELTERS, WALLS, COMMERCIAL SIGNAGE OR LANDSCAPING WITHIN THE LIMITED USE AREA WHICH COULD RESTRICT THE LINE OF SIGHT SHALL NOT BE PERMITTED. DRIVEWAYS ARE NOT PERMITTED WITHIN INTERSECTION AREA DUE TO SIGHT DISTANCE RESTRICTION BY ENTERING VEHICLES.

   a. PLANTS AND SHRUBS WITHIN THE LIMITED USE AREA SHALL BE OF THE TYPE THAT WILL GROW NO HIGHER THAN 30 INCHES ABOVE THE TOP OF CURB AND SHALL BE MAINTAINED AT A HEIGHT WHICH WILL ASSURE THAT THE 30 INCH MAXIMUM HEIGHT IS NOT EXCEEDED BETWEEN MAINTENANCE INTERVALS. MAINTENANCE AT A LOWER HEIGHT MAY BE REQUIRED ON CREST VERTICAL CURVES PER NOTE 5 ABOVE.

   b. A PROFILE DETAIL OF THE LINE OF SIGHT MAY BE REQUIRED TO VERIFY 12" MINIMUM VERTICAL CLEARANCE ABOVE VARIABLE HEIGHT OBSTRUCTIONS SUCH AS SLOPE LANDSCAPING, PLANTS, SHRUBS AND PERIMETER WALLS.

   c. THE TOE OF SLOPE MAY NOT ENCROACH INTO THE LIMITED USE AREA UNLESS THE REQUIREMENTS OF (b) ABOVE ARE SATISFIED.

   d. IN LIEU OF PROVIDING A PROFILE OF THE LINE OF SIGHT PER NOTE 6.b. ABOVE, THE TOE OF SLOPE SHALL NOT ENCROACH INTO THE LIMITED USE AREA, AND THE LIMITED USE AREA SHALL SLOPE 2% MAXIMUM BETWEEN THE LINE OF SIGHT AND THE BACK OF SIDEWALK.

7. NO PARKING OF ANY KIND IS TO BE ALLOWED WITHIN THE LIMITED USE AREA.

8. TREES ARE GENERALLY NOT PERMITTED WITHIN ANY PORTION OF THE LIMITED USE AREA. EXCEPTIONS ARE ALLOWED WHEN THE SPECIES HAS A MATURE DIAMETER OF LESS THAN 6 INCHES.

9. MEDIAN AREAS LESS THAN FIVE (5) FEET IN WIDTH SHALL NOT BE LANDSCAPED.

10. INTERSECTION SIGHT DISTANCE AT RIGHT ANGLE INTERSECTIONS IS MEASURED FROM THE IDENTIFIED MEASUREMENT POINT "A", IN ACCORDANCE WITH THE DIAGRAMS ON SHEET 2.

APPROVED BY:

09-20-19

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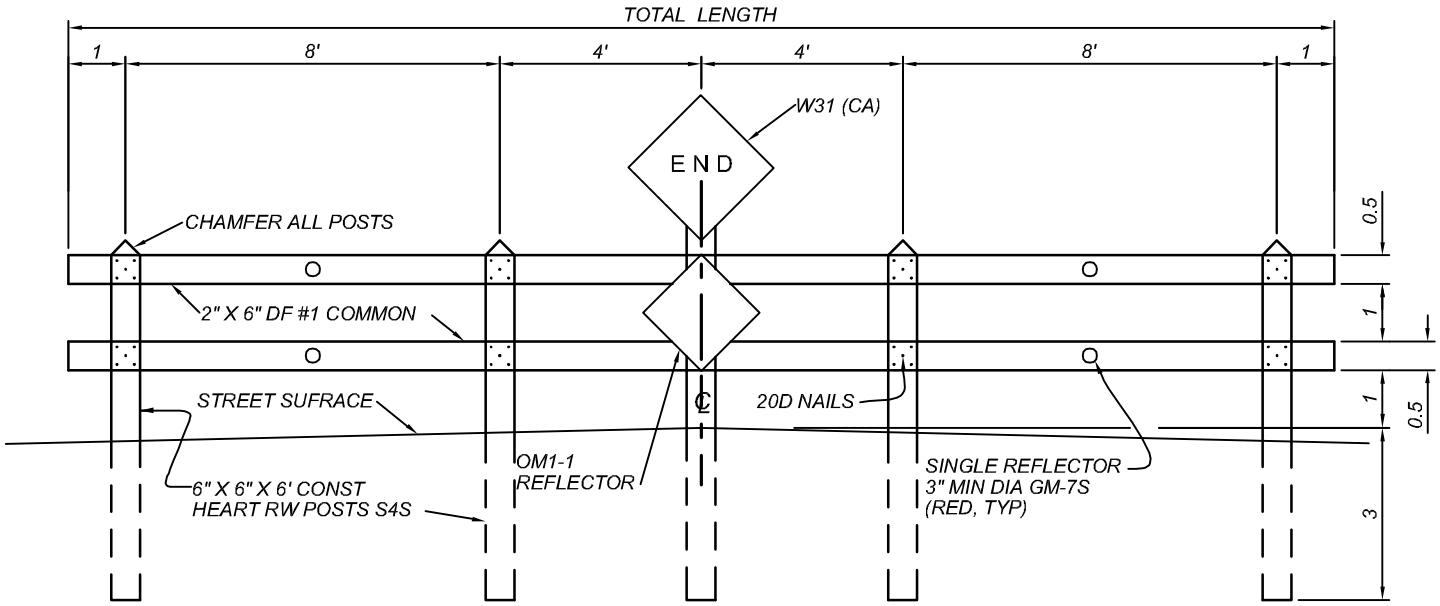
CITY ENGINEER

DATE

CITY OF MONTCLAIR

INTERSECTION SIGHT DISTANCE

NO SCALE	STD. PLAN NO. 110	SHEET 3 of 3



ELEVATION

TABLE OF PANELS FOR VARIOUS ROADWAY WIDTHS

8' SECTIONS SHALL BE ADDED OR DELETED TO GIVE THE FOLLOWING WIDTHS:		
WIDTH OF ROADWAY	NO. OF 8' SECTIONS	TOTAL LENGTH OF PANELS
20' ALLEY	2	18'
36'	3	26'
40'	4	34'
44'	4	34'
64'	7	58'

NOTES:

1. OM1-1 REFLECTOR IS TO BE A 1.5' X 1.5' SIGN, AND IS TO BE BOLTED ON THE CENTER OF THE BARRICADE AS SHOWN.
2. SIGNS TO BE W31 (CA) & OM1-1 OR APPROVED EQUAL.
3. ALL WOOD ASSEMBLY IS TO BE PAINTED WITH ONE COAT OF PRIMER AND TWO COATS OF WHITE EXTERIOR PAINT.

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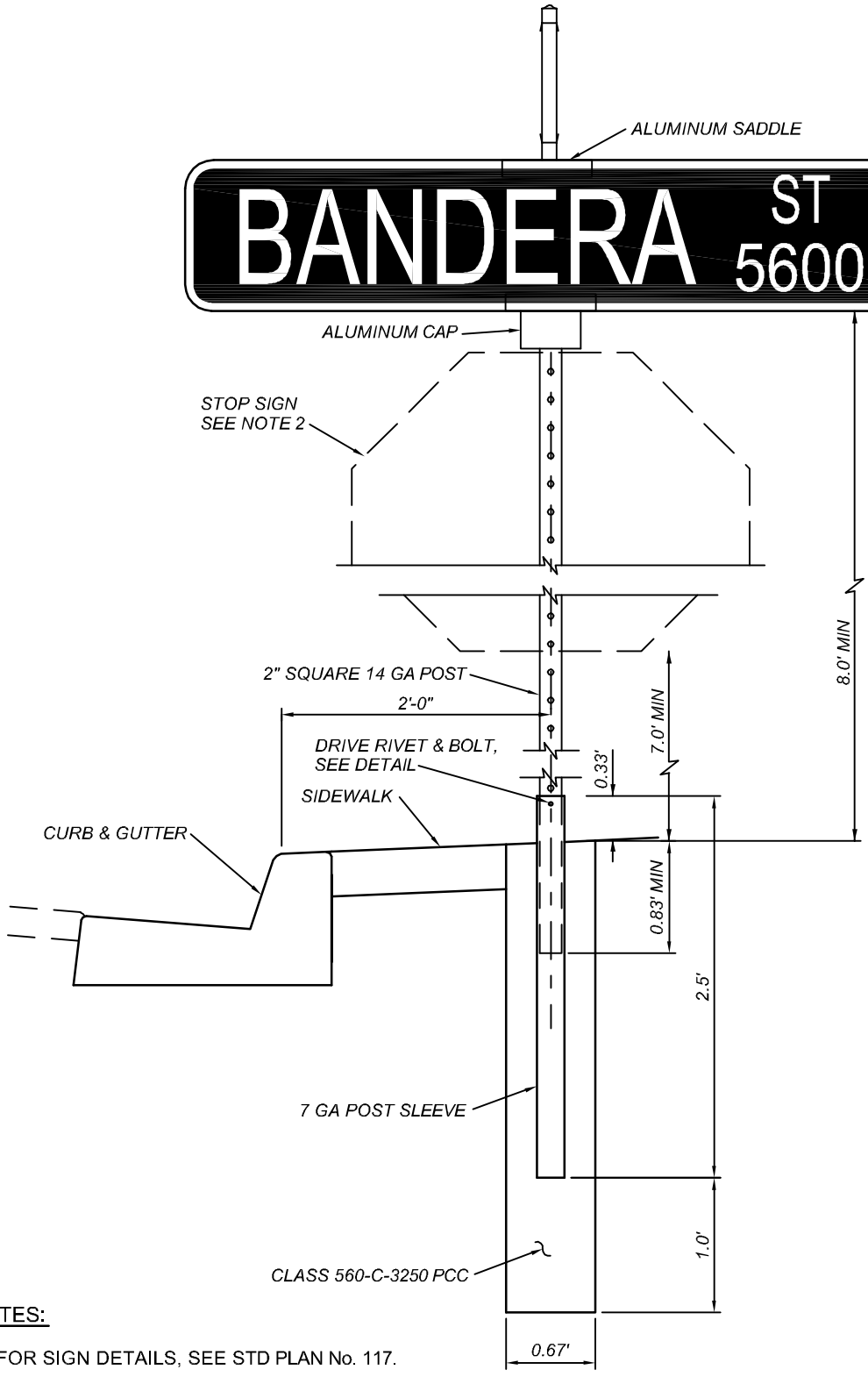
CITY ENGINEER

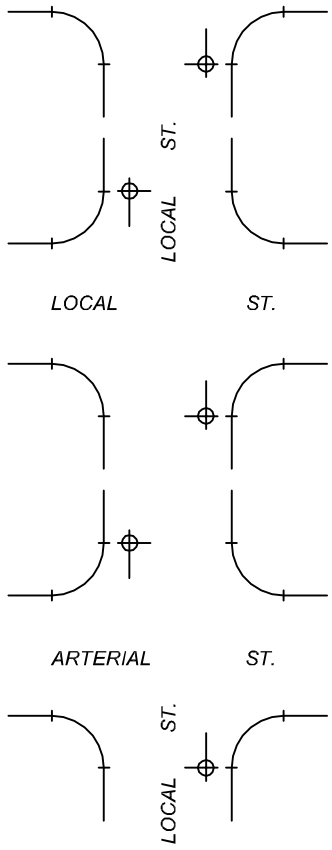
CITY OF MONTCLAIR

BARRICADE

NO SCALE	STD. PLAN NO. 111	SHEET 1 of 1



POST LOCATIONS



NOTES:

1. FOR SIGN DETAILS, SEE STD PLAN No. 117.
2. WHEN A STOP SIGN IS ATTACHED TO THE SIGN POST THERE MUST BE SEVEN (7) FEET OF CLEARANCE BETWEEN THE BOTTOM OF THE SIGN AND THE WALKING SURFACE.

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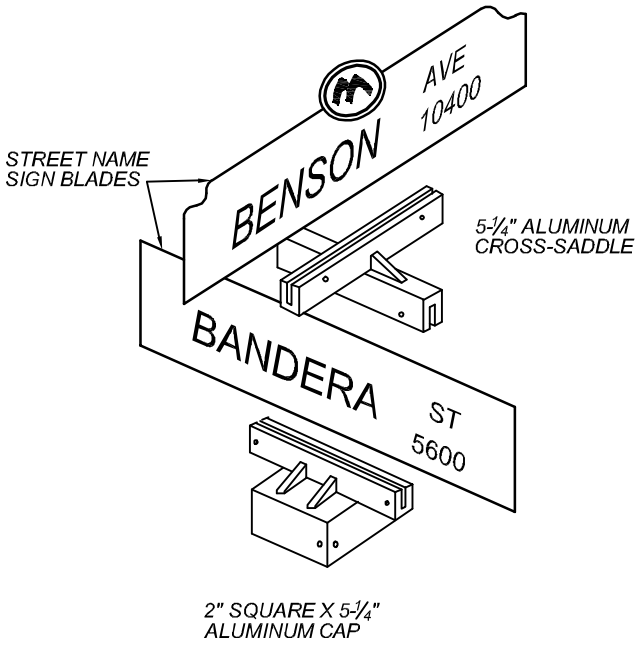
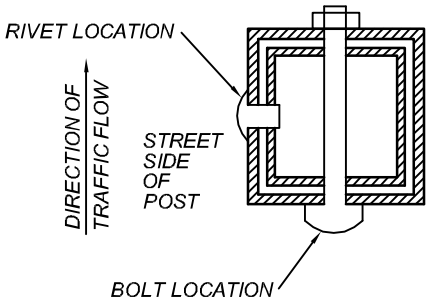
CITY OF MONTCLAIR

STREET NAME SIGN

NO SCALE	STD. PLAN NO. 112	SHEET 1 of 2





NOTE:

1. EACH SIGN POST SHALL ACCOMMODATE TWO STREET NAME SIGN BLADES.

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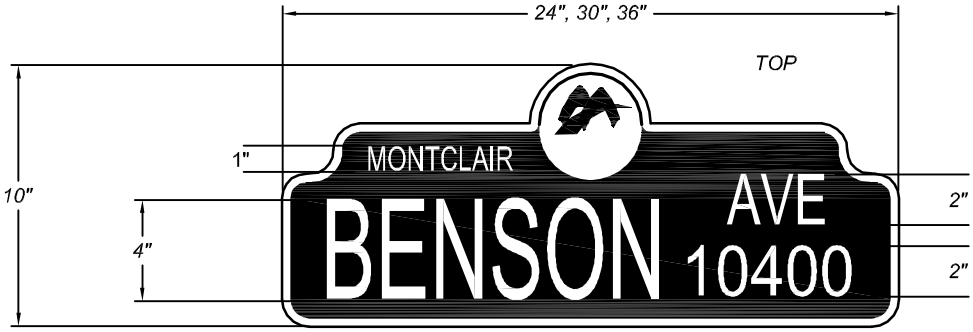
CITY ENGINEER

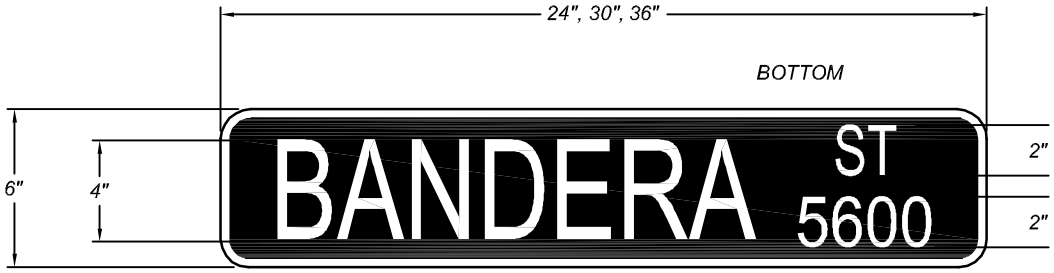
DATE

CITY OF MONTCLAIR

STREET NAME SIGN

NO SCALE	STD. PLAN NO. 112	SHEET 2 of 2





MONTCLAIR STREET SIGNS

MONTCLAIR OVERHEAD SIGNS

NOTE:

CA MUTCD SUPERSEDES
SIGN LAYOUTS AS NEEDED

LOGO

MAIN STREET TEAL AND MAIN STREET BURGUNDY
3 7/8" TALL LOGO GRAPHICS
MONTCLAIR LETTERS 1/2" TALL

STREET SIGN MATERIAL

0.100" ALUMINUM
3M HIP SHEETING OR EQUAL
BLUE UC FILM
ANTI-GRAFFITTI COATTING 3M1160 OR EQUAL
WHITE LETTERS AND 1/4" MARGIN
ON BLUE BACKGROUND ANTI-GRAFFITI

OVERHEAD SIGN MATERIAL

3M VIP SHEETING OR EQUAL
BLUE UC FILM
ANTI-GRAFFITTI COATTING 3M1160 OR EQUAL
WHITE LETTERS, ARROWS AND 1/2" MARGIN
ON BLUE BACKGROUND ANTI-GRAFFITI
WITH FRAME

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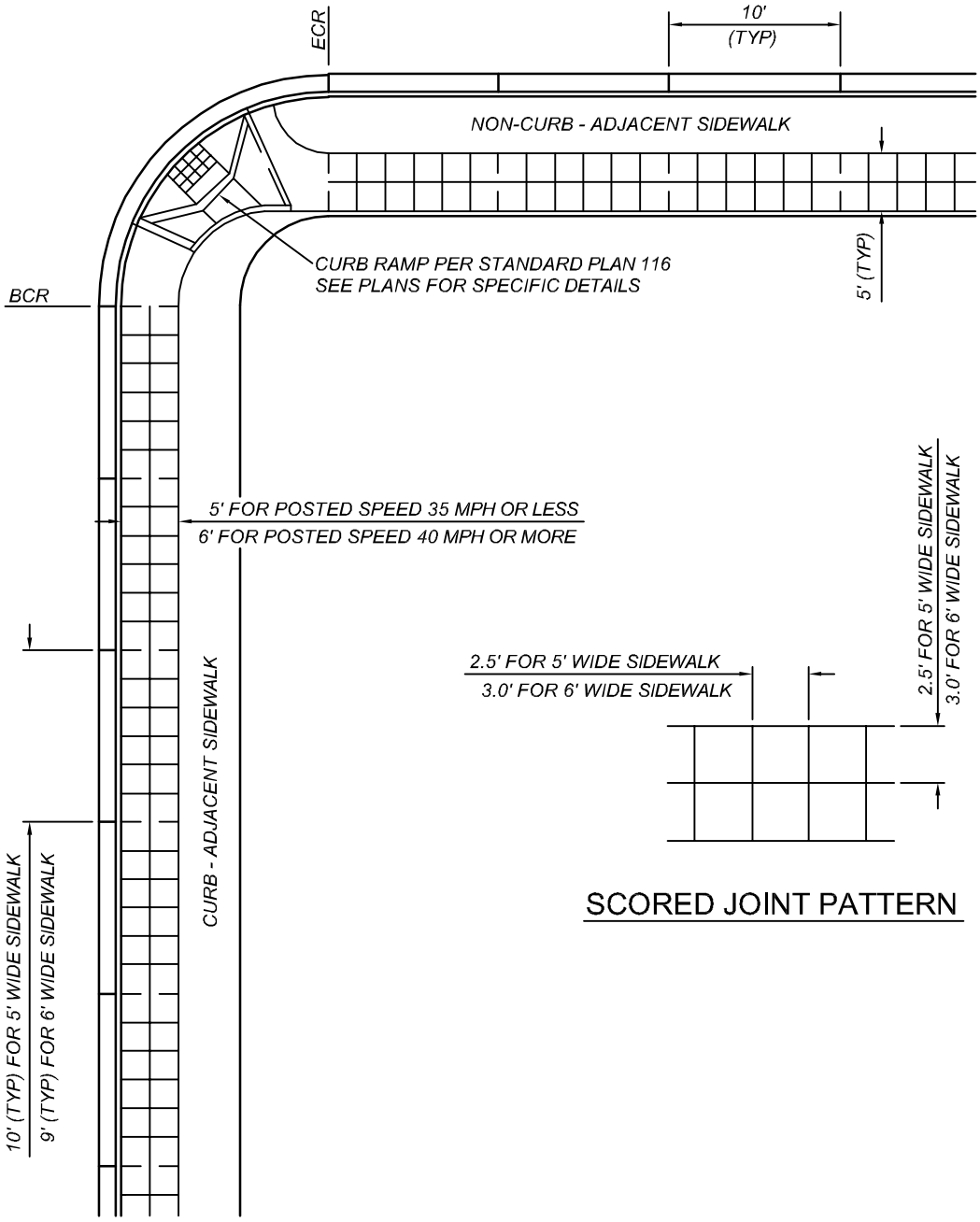
CITY ENGINEER

CITY OF MONTCLAIR

STREET AND OVERHEAD SIGNS

NO SCALE	STD. PLAN NO. 113	SHEET 1 of 1



SCORED JOINT PATTERN

NOTES:

1. EXPANSION JOINTS: AT CURB RETURNS, ADJACENT TO STRUCTURES AND 45' (6' WIDE SIDEWALKS) OR 50' (5' WIDE SIDEALKS).
2. TOOLED JOINTS WITH ¼" GROVES AND ¼" RADIUS.
3. MEDIUM BROOM FINISH. FINISH QUALITY AND WORKMANSHIP SHALL BE PER STAFF-PROVIDED REFERENCE AREA AND PER STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION. THE FIELD ENGINEERING INSPECTOR SHALL HAVE THE FINAL DETERMINATION OF APPROVAL.
4. CONCRETE SHALL BE 560-C-3250 PER STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION AND SHALL BE 4" THICK.
5. CURB JOINTS ARE TO BE ALIGNED WITH SIDEWALK JOINTS.

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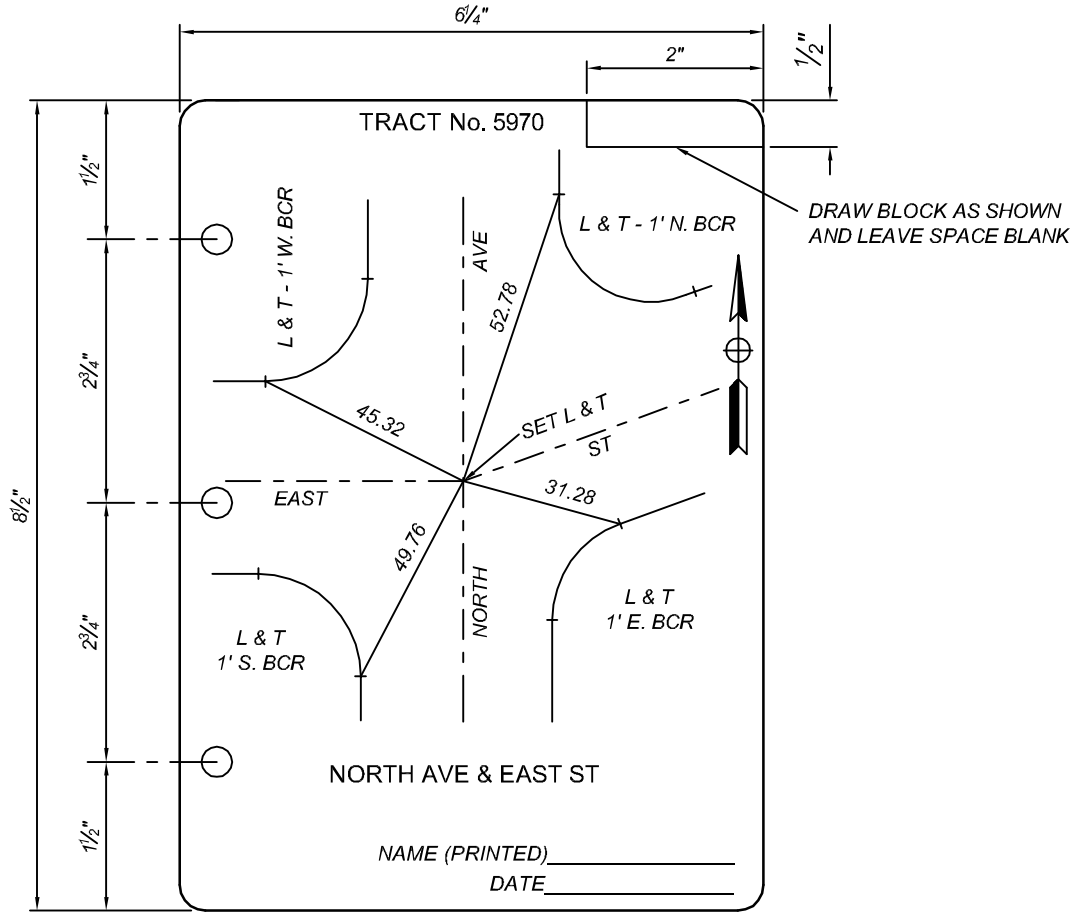
CITY ENGINEER

CITY OF MONTCLAIR

SIDEWALK JOINT LOCATION (NMDSP AREA)

NO SCALE	STD. PLAN NO. 114	SHEET 1 of 1



½ ACTUAL SIZE

NOTES:

1. REFERENCE POINTS SHALL BE L & Ts IN SIDEWALKS OR IN TANGENT PORTION OF THE CURB AND A MINIMUM OF 3 REFERENCE POINTS FOR EACH INTERSECTION.
2. CENTER LINE MONUMENTS SHALL BE:
   a. CEMENT CONCRETE - LEAD & TACK.
   b. MACADAM OR PLANT MIX - 6" RR SPIKE.
   c. OIL & ROCK, GRAVELLED & OTHER - 1" IP 12" DOWN.
   d. WHERE MANHOLES EXIST - 4 PUNCH MARKS ON MH RING.
3. TANGENT TIES AND POINTS ON C PRODS ARE PREFERRED.
4. ONLY ONE STREET INTERSECTION SHALL BE SHOWN ON EACH SHEET.
5. SHEETS SHALL BE WET-SIGNED BY LICENSED ENGINEER OR LAND SURVEYOR ON GOOD QUALITY BOND PAPER.
6. SUBMITTAL SHALL INCLUDE AN ELECTRONIC PDF FILE.

REGISTERED CIVIL ENGINEER
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APPROVED BY:

M Heredia

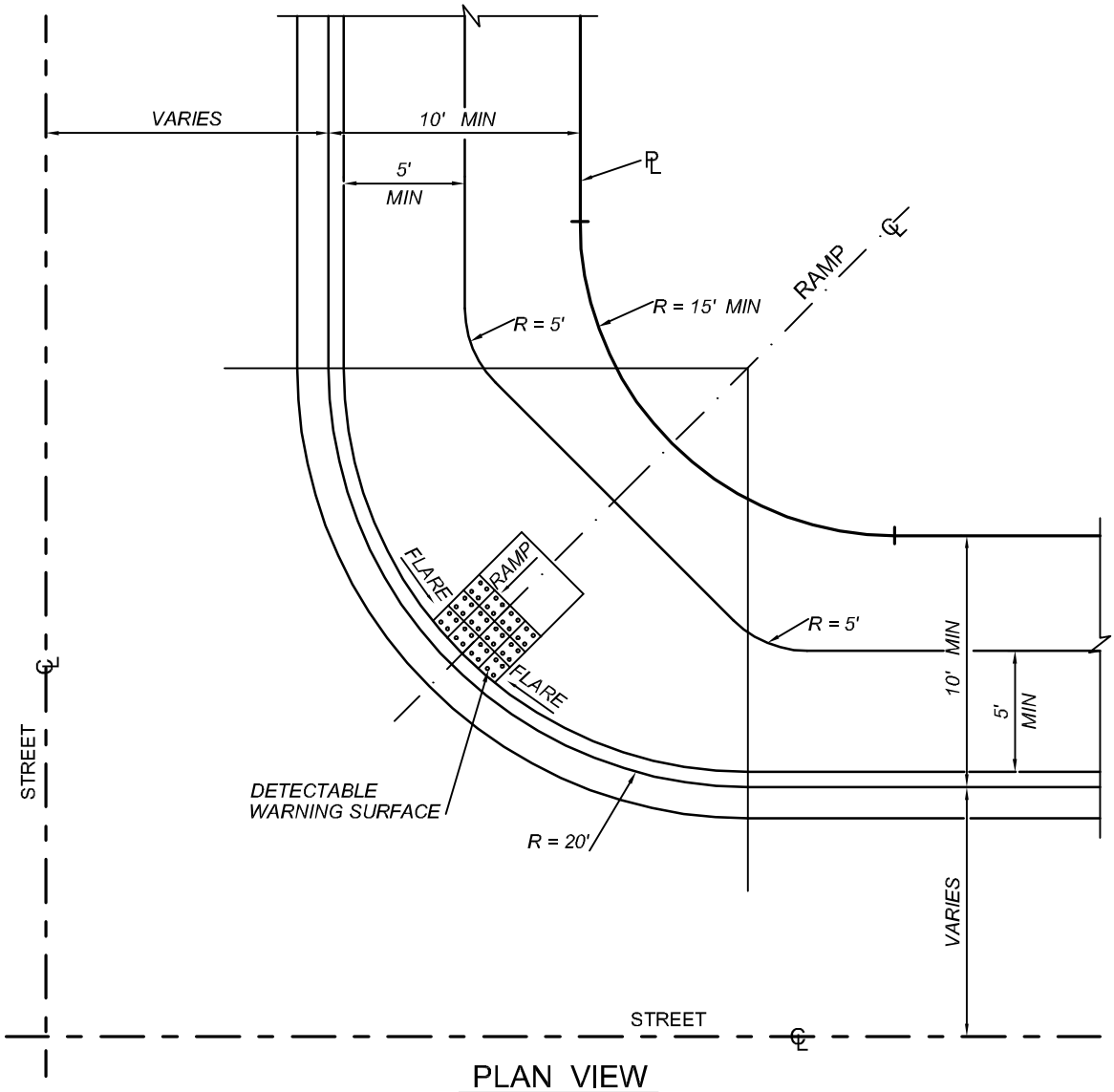
05-14-24
DATE

MONICA HEREDIA
CITY ENGINEER

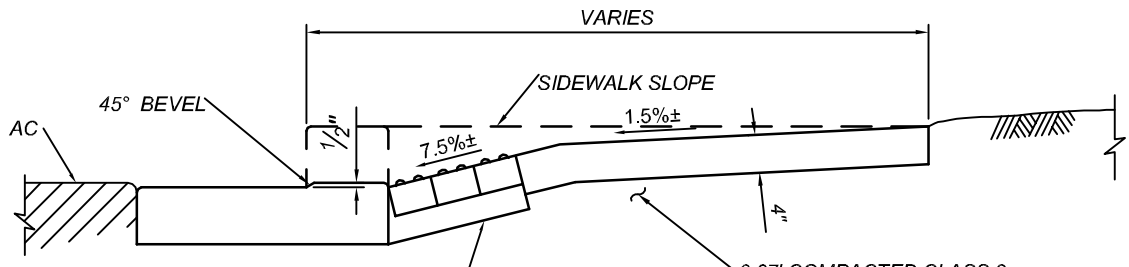
CITY OF MONTCLAIR

CENTER LINE TIE NOTES

NO SCALE	STD. PLAN NO. 115	SHEET 1 of 1



PLAN VIEW



ELEVATION

NOTES:

1. FOR CURB RAMP GEOMETRY AND DETAILS, SEE CALTRANS STANDARD PLAN A88A AND A88B.
2. THE DETECTABLE WARNING SURFACE SHALL BE WAUSAU ADA-2 12" SQUARE PAVERS, COLOR A-80, THICKNESS = 2"
3. RAMP SHALL HAVE A HEAVY BROOM FINISH.
4. CLASS 560 - C - 3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.

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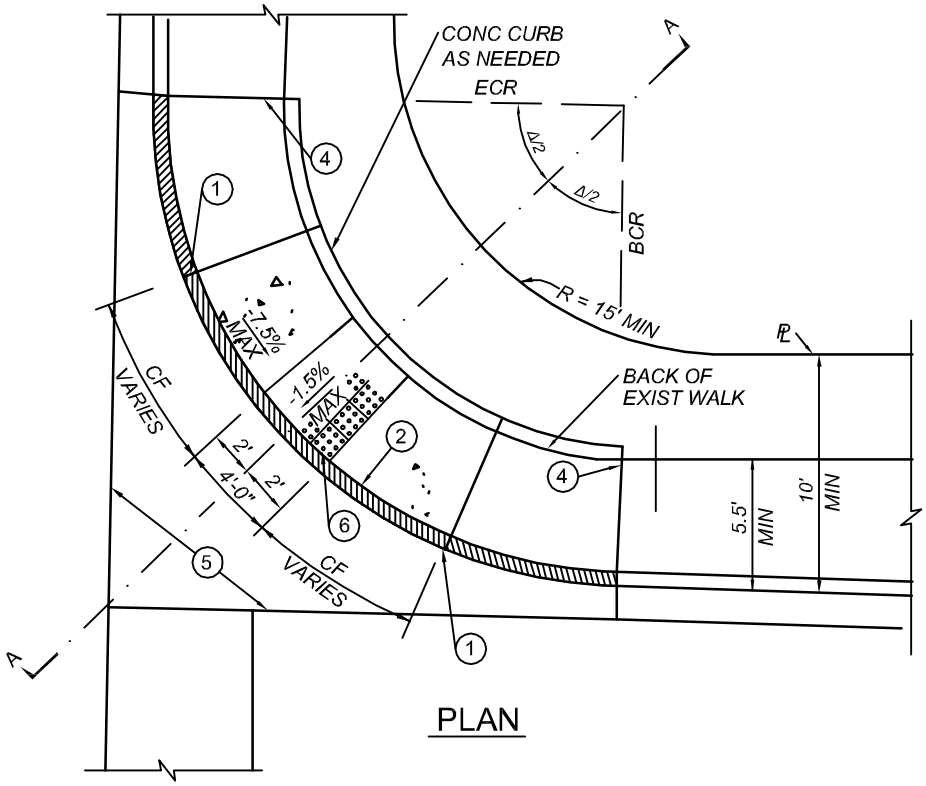
CITY ENGINEER

DATE

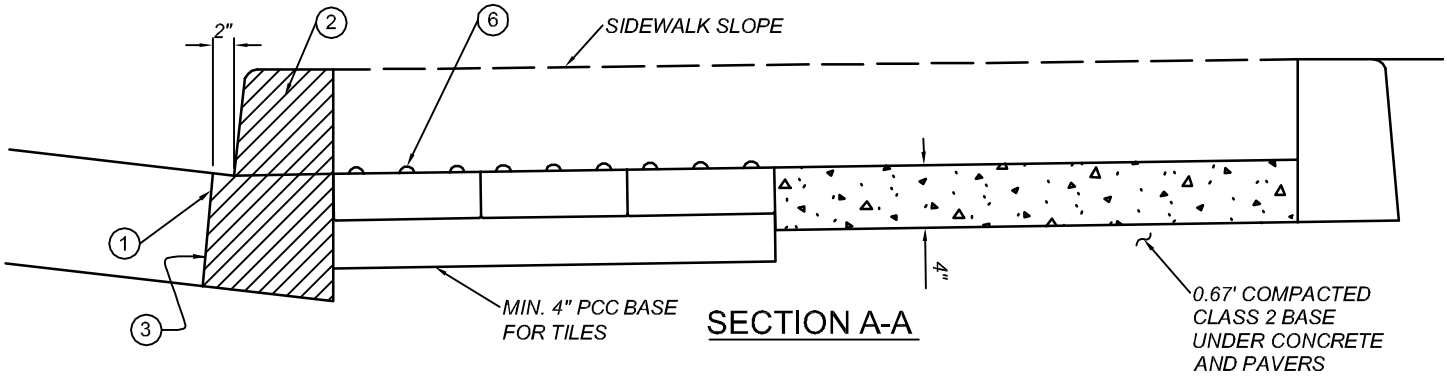
CITY OF MONTCLAIR

WHEELCHAIR RAMP

NO SCALE	STD. PLAN NO. 116	SHEET 1 of 1



PLAN



SECTION A-A

CONSTRUCTION NOTES:

(1) SAW CUT A MINIMUM OF 6" DEPTH BEFORE BREAKING OFF CURB PORTION.

(2) REMOVE EXISTING CURB SHOWN CROSS-HATCHED AND EXISTING SIDEWALK

(3) THOROUGHLY CLEAN WITH WIRE BRUSH AND APPLY HUNT 101 EPOXY RESIN BOND OR APPROVED EQUAL TO MANUFACTURER'S RECOMMENDATION AND TO SATISFACTION OF THE CITY ENGINEER.

(4) REMOVE SIDEWALK & CURB BCR TO ECR OR AS NECESSARY TO GET TRANSITION SLOPE (8.33% MAX)

(5) REMOVE PCC SPANDREL IF CRACKED OR SETTLED PER PUBLIC WORKS INSPECTOR'S REQUIREMENTS

(6) THE DETECTABLE WARNING SURFACE SHALL BE WAUSAU ADA-2 12" SQUARE PAVERS, COLOR A-80, THICKNESS = 2".

NOTES:

1. RAMP SHALL HAVE A HEAVY BROOM FINISH
2. CLASS 500-C-3250 CONCRETE SHALL BE USED UNLESS OTHERWISE SPECIFIED

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CITY ENGINEER

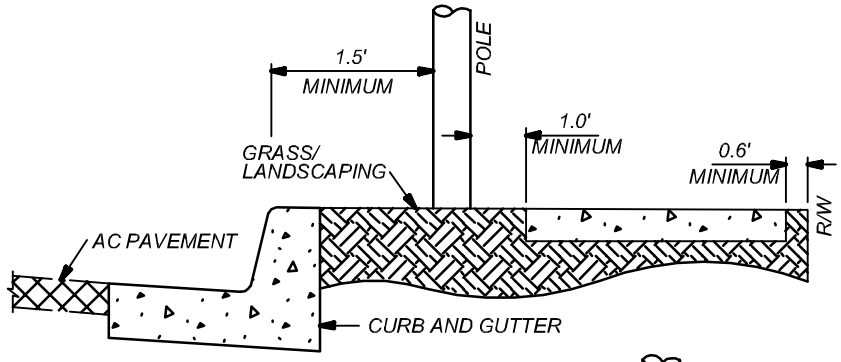
DATE

CITY OF MONTCLAIR

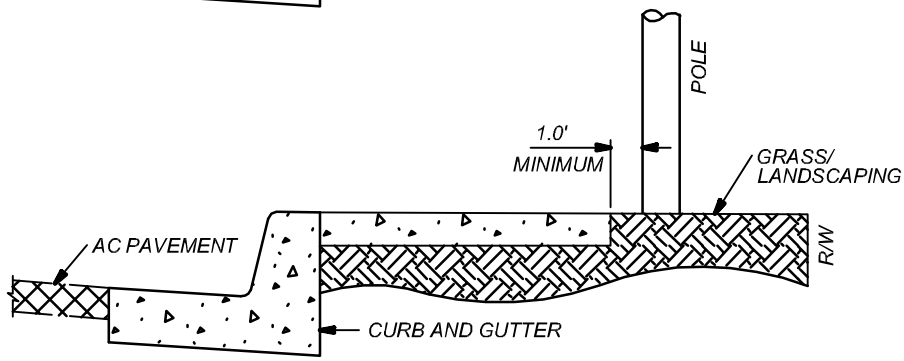
WHEELCHAIR RAMP AT EXISTING CURB RETURN

NO SCALE	STD. PLAN NO. 117	SHEET 1 of 1

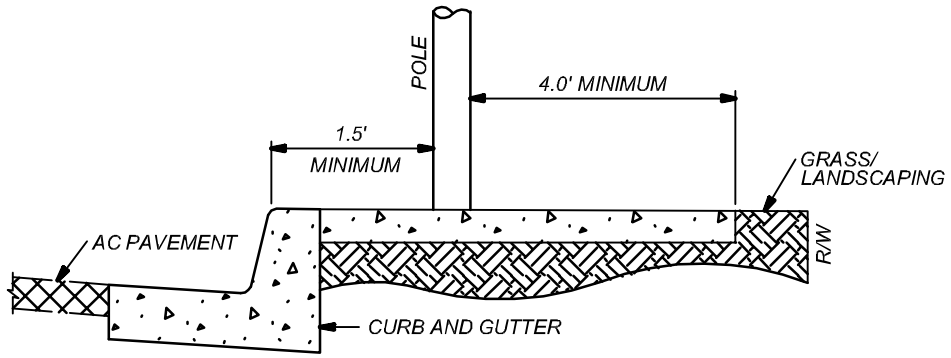
CASE 1
LANDSCAPING ADJACENT TO CURB



CASE 2
SIDEWALK ADJACENT TO CURB



CASE 3
SIDEWALK ADJACENT TO CURB



STREET LIGHT GENERAL NOTES:

1. LIGHTING LAYOUT SHALL COMMENCE AT STREET INTERSECTIONS. STREET LIGHT SPACING BETWEEN INTERSECTIONS SHALL BE AS SPECIFIED IN THE TABLE ON SHEET 2 AND LOCATED AT THE PROLONGATION OF PROPERTY LINES. STREET LIGHT LAYOUTS SHALL BE REVIEWED FOR CONFLICTS WITH OTHER UTILITIES (CATCH BASINS, FIRE HYDRANTS, TRANSFORMERS, ETC.)

2. USE OF BRAND NAMES IN THESE STANDARDS IS NOT INTENDED TO RESTRICT COMPETITION. EQUIVALENT PRODUCTS MAY BE SUBSTITUTED WITH THE APPROVAL OF THE CITY ENGINEER.

3. SIDEWALK WIDTHS TO BE PER APPROVED STREET IMPROVEMENT PLANS. A MINIMUM OF 4' UNOBSTRUCTED SIDEWALK CLEARANCE IS REQUIRED.

4. ANY CHANGE IN LOCATION OF STREET LIGHTS FROM THE APPROVED PLANS OR STANDARDS IS SUBJECT TO WRITTEN APPROVAL BY THE CITY ENGINEER.

5. ALL STREET LIGHTS SHALL HAVE AN ANTI-GRAFFITI COATING.

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CITY ENGINEER

CITY OF MONTCLAIR

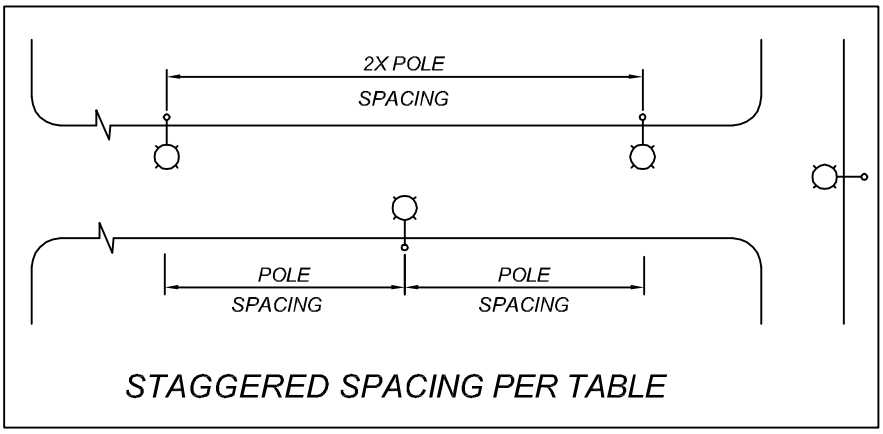
STREET LIGHT INSTALLATION DETAILS

NO SCALE	STD. PLAN NO. 118	SHEET 1 of 2

SPACING, PART NUMBERS, WATTAGES						
STREET CLASSIFICATION	POLE SPACING	HPS WATTAGE EQUIVALENT	LUMINAIRE, LED, COBRAHEAD		LUMINAIRE, LED, DECORATIVE**	
			SCE PART #	WATTAGE	SCE PART #	WATTAGE
LOCAL	250' ± 50'	100W	10205809	41W	10184035	75W
COLLECTOR	250' ± 50'	100W	10205809	41W	10184035	75W
ARTERIAL	125' ± 50'*	200W	10205811	90W	10184043	165W

* 100' MAX SPACING FOR COMMERCIAL ZONES

** DECORATIVE LUMINAIRE IS KING K804 'CARPINTERIA' TEARDROP PENDANT



STAGGERED SPACING PER TABLE

NOTES:

1. SEE PROJECT PLANS AND SPECIFICATIONS FOR DETAILS REGARDING INSTALLATION OF STANDARD OR DECORATIVE STREET LIGHT POLES.
2. PLANS SHALL INCLUDE POINT BY POINT FOOT-CANDLE VALUES ARRANGED IN A GRID VERIFYING A MINIMUM LIGHTING LEVEL OF 21 LUX (2 FOOT-CANDLES) THROUGHOUT EVERY INTERSECTION.

REGISTERED CIVIL ENGINEER
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09-20-19
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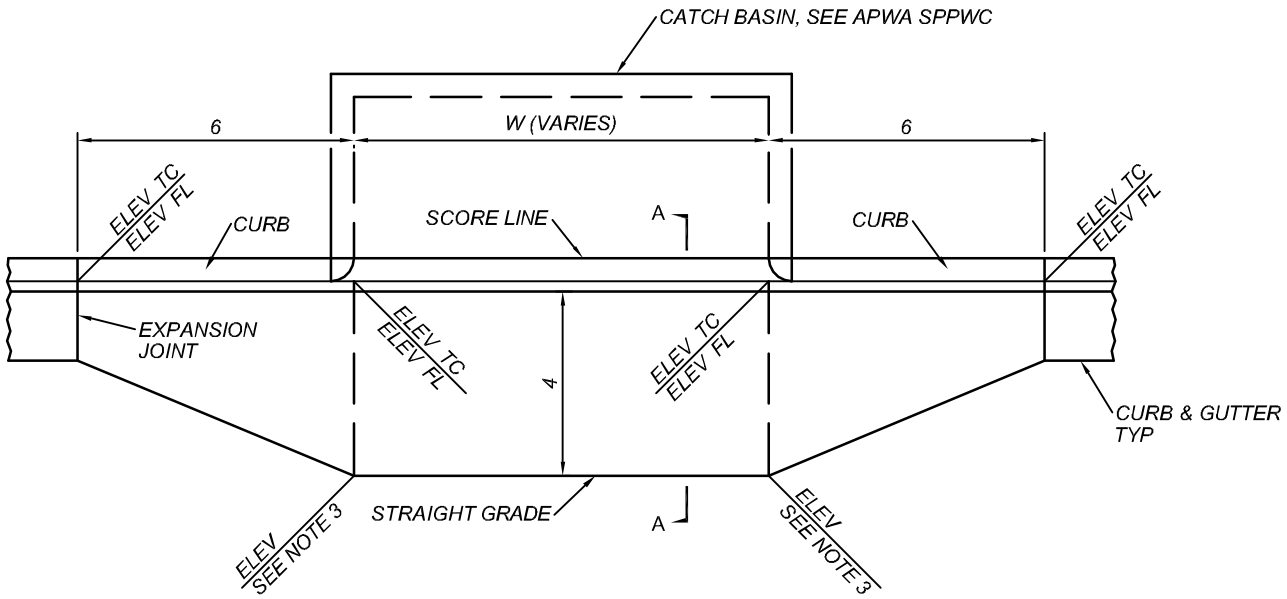
CITY ENGINEER

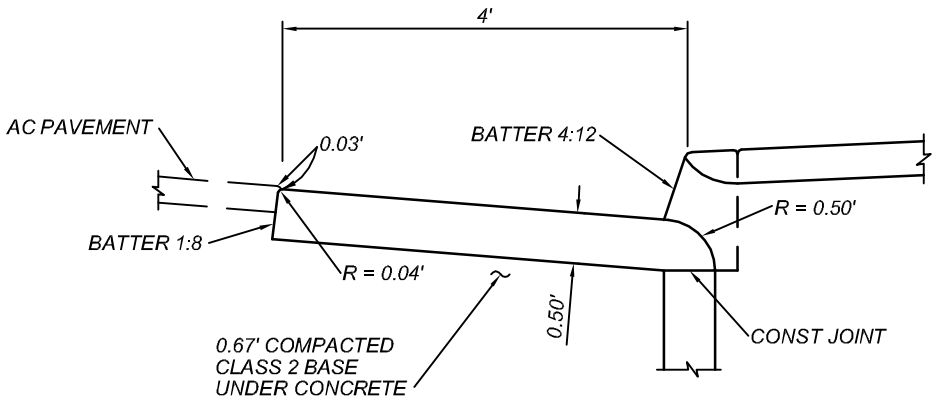
CITY OF MONTCLAIR

STREET LIGHT INSTALLATION DETAILS

NO SCALE	STD. PLAN NO. 118	SHEET 2 of 2



PLAN



SECTION A-A

NOTES:

1. CLASS 560-C-3250 PCC SHALL BE USED UNLESS OTHERWISE SPECIFIED.
2. ALL NECESSARY CONTROL ELEVATIONS SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
3. TOP OF LOCAL DEPRESSION ELEVATIONS SHALL CONFORM TO STREET SECTION.
4. FOR ADDITIONAL LOCAL DEPRESSION CONFIGURATIONS, SEE APWA SPPWC PLAN 313.

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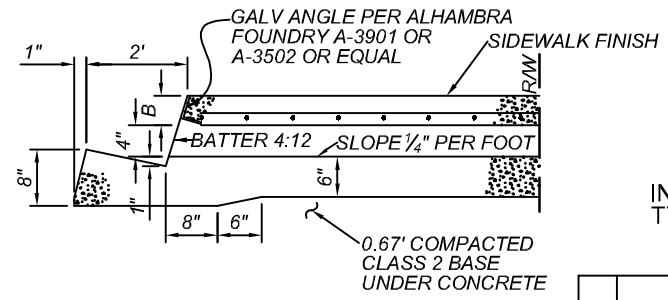
CITY ENGINEER

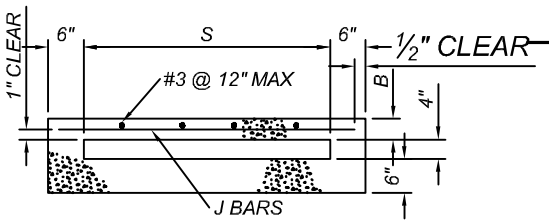
CITY OF MONTCLAIR

LOCAL DEPRESSION

NO SCALE	STD. PLAN NO. 201	SHEET 1 of 1

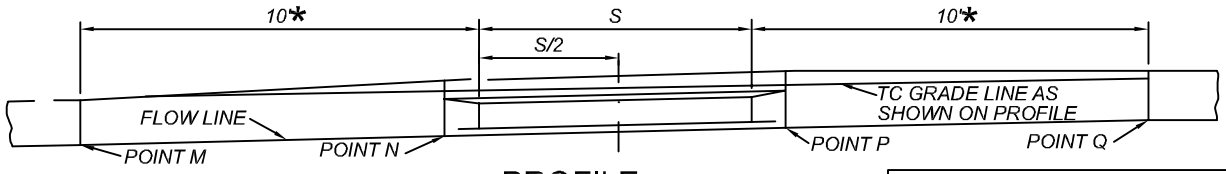


SECTION A-A



SECTION B-B

PLAN

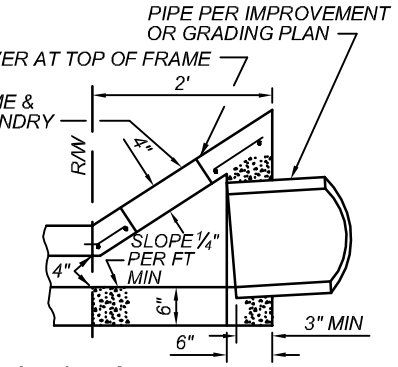


PROFILE

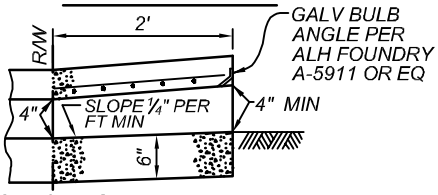
* TRANSITION SHALL BE MEASURED ALONG BACK OF SIDEWALK

* TRANSITION IS NOT REQUIRED WHEN CURB FACE = 5" + B OR GREATER

* TRANSITION SHALL NOT BE WITHIN DRIVEWAY OR CURB RETURNS



SECTION-INLET TYPE I SECTION C-C



SECTION-INLET TYPE II SECTION D-D

* PIPE DRAIN THRU CURB

CURB FACE	MAX PIPE DIA
6"	3"
8"	4"
10"	6"

*PIPE SHALL BE DI, STEEL OR SCH 80 PVC

NOTES:

1. FLOOR OF BOX TO BE TROWELED SMOOTH.
2. WHEN THE TOE OF SLOPE IS WITHIN THE R/W, INLET TYPE I BEGINS AT THE TOE RATHER THAN AT THE R/W LINE.
3. FOR OPEN DITCH APPROACH (TYPE II) THE 2' EXTENSION IS NOT REQUIRED WHEN THE BACK OF WALK IS 2' OR MORE FROM THE R/W LINE.
4. TOP OF INLET STRUCTURE (TYPE I & II) TO BE FLUSH WITH ADJACENT SURFACE WHERE PRACTICABLE.
5. A HEADED STEEL STUD 5/8" x 6 3/8" WITH HEAD D=1" ATTACHED BY A FULL PENETRATION BUTT WELD MAY BE USED AS AN ALTERNATE ANCHOR.
6. NORMAL CURB FACE AT POINT M AND Q, B+5" AT POINT N AND P.
7. CLASS 560-C-3250 PCC SHALL BE USED.

STEEL LIST

S	B	GALVANIZED STEEL ANGLE	ANCHOR	J BAR SIZE	J BAR SPACING	J BAR LENGTH
1'-0"	3"	2½"x2"x⅜"	2	#3	7"	1'-9"
1'-6"	"	"	"	"	"	2'-3"
2'-0"	"	"	"	"	"	2'-9"
2'-6"	"	"	"	"	"	3'-3"
3'-0"	"	"	3	"	"	3'-9"
3'-6"	"	"	"	"	6"	4'-3"
4'-0"	"	"	"	"	5"	4'-9"
4'-6"	4"	3½"x3"x½"	"	"	6½"	5'-3"
5'-0"	"	"	"	"	5"	5'-9"
5'-6"	"	"	"	"	4"	6'-3"
6'-0"	"	"	"	"	3½"	6'-9"

REGISTERED CIVIL ENGINEER
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STATE OF CALIFORNIA

APPROVED BY:

MONICA HEREDIA
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05-14-24
DATE

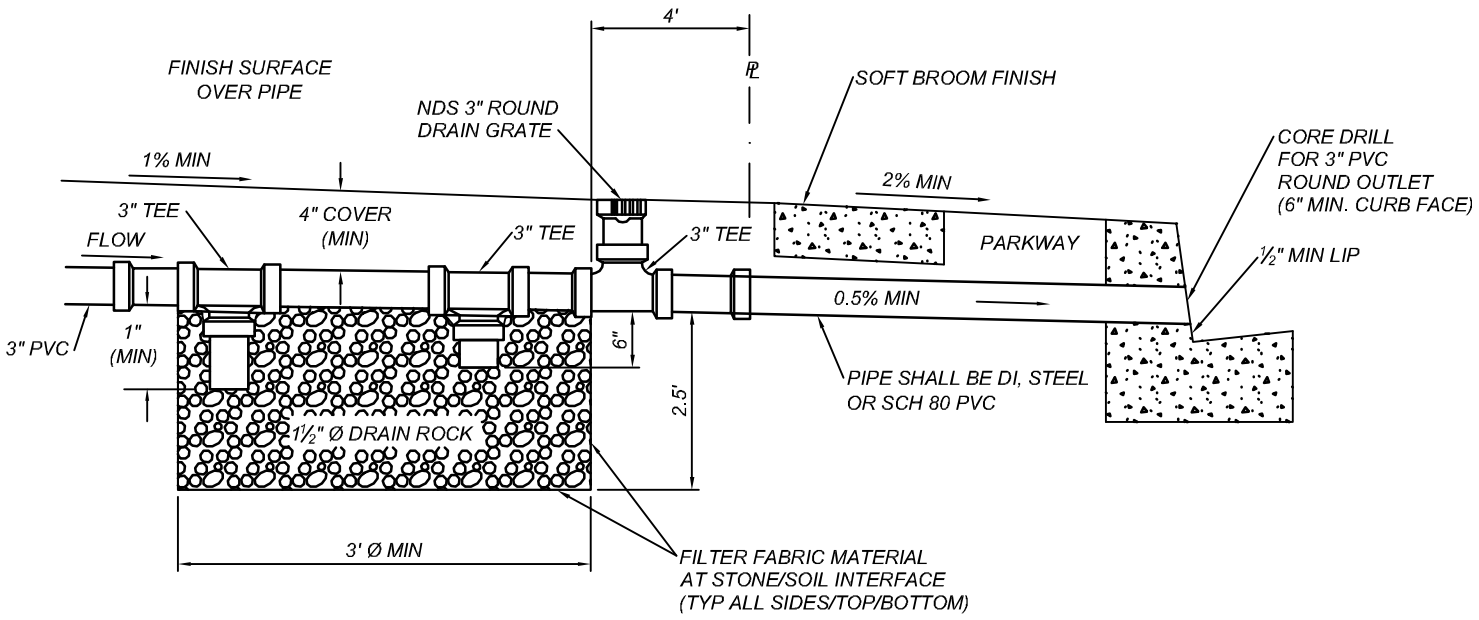
CITY OF MONTCLAIR

SIDEWALK DRAIN OUTLET AND PIPE DRAIN THRU CURB

NO SCALE

STD. PLAN NO. 202

SHEET 1 of 1



NOTES:

1. A CONSTRUCTION PERMIT MUST BE OBTAINED FOR ANY WORK DONE IN THE CITY RIGHT OF WAY. PERMITS ARE ISSUED THROUGH THE ENGINEERING DIVISION. CALL (909) 625-9440 FOR ADDITIONAL INFORMATION.

2. REMOVE SIDEWALK PANEL FROM JOINT TO JOINT OR SCORE LINES BEFORE CORING CURB. REPLACEMENT CONCRETE SHALL BE CLASS 560-C-3250.

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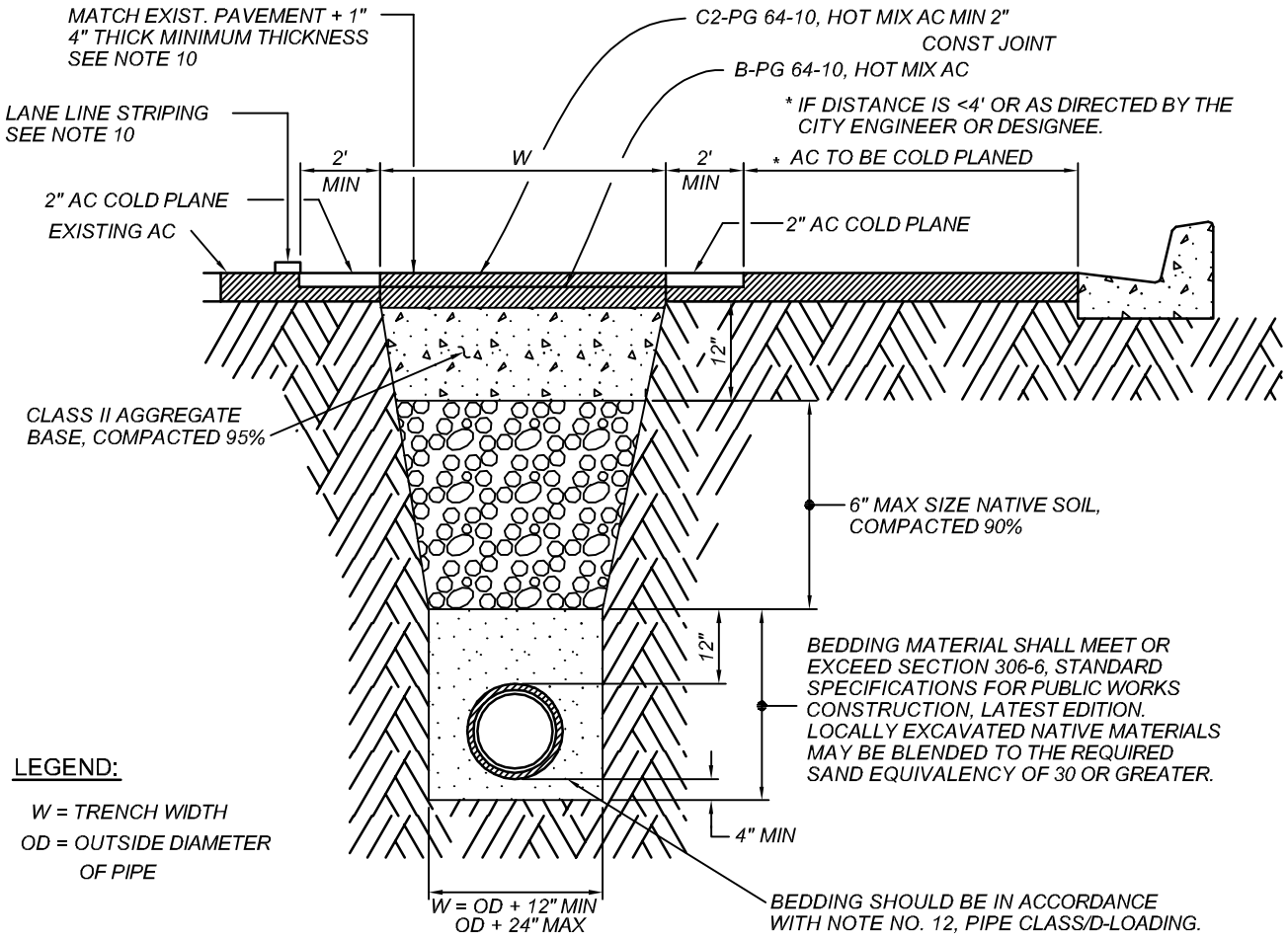
CITY OF MONTCLAIR

WQMP GRAVEL FILTER DETAIL

NO SCALE	STD. PLAN NO. 203	SHEET 1 of 1



GENERAL NOTES:

1. ALL EXCAVATION WITHIN THE CITY OF MONTCLAIR RIGHT-OF-WAY REQUIRES AN EXCAVATION PERMIT FROM THE ENGINEERING DIVISION.
2. UNDERGROUND SERVICE ALERT SHALL BE NOTIFIED 2 WORKING DAYS PRIOR TO START OF WORK 811.
3. ALL PUBLIC WORKS INSPECTION REQUESTS WILL BE MADE 2 WORKING DAYS AHEAD OF SCHEDULED WORK.
4. COMPACTION OF BACKFILL SHALL BE VERIFIED BY REGISTERED GEOTECHNICAL ENGINEERING AND A COMPACTION REPORT SUBMITTED TO THE CITY FOR APPROVAL PRIOR TO THE PLACING OF THE PERMANENT PAVEMENT. IN LIEU OF COMPACTION TESTING CONTRACTOR MAY USE 2-SACk PCC SLURRY.
5. ALL EXCAVATION SHALL BE MADE, PROTECTED AND SUPPORTED AS REQUIRED FOR SAFETY AND IN A MANNER SET FORTH IN OPERATIONS, RULES, ORDERS, AND REGULATIONS PRESCRIBED BY THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY. A COPY OF THE CAL OSHA EXCAVATION PERMIT, IF APPLICABLE, SHALL BE FURNISHED TO THE PROJECT INSPECTOR PRIOR TO BEGINNING THE WORK. STEEL PLATE BRIDGING SHALL BE IN ACCORDANCE WITH CITY STD. PLAN NO. 302.
6. PERMANENT PAVEMENT SHALL BE REPLACED WITHIN 30 DAYS OF EXCAVATION, A MINIMUM 2-INCH THICKNESS OF TEMPORARY ASPHALT PAVING SHALL BE PLACED WITHIN THE TRENCH AREA UNTIL PERMANENT REPAIR IS COMPLETED OR AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE. THE TEMPORARY ASPHALT PAVING SHALL BE FLUSH WITH EXISTING PAVEMENT.
   THE TEMPORARY PAVING SHALL BE PLACED AND COMPACTED IN SUCH A MANNER AS TO PROVIDE A SAFE AND SMOOTH TRAVELED SURFACE. PERMITTEE SHALL MAINTAIN THE TEMPORARY PAVEMENT IN A SAFE AND SMOOTH CONDITION UNTIL PERMANENT PAVING IS IN PLACE.

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CITY OF MONTCLAIR

STANDARD PAVING AND TRENCH REPAIR

NO SCALE	STD. PLAN NO. 301	SHEET 1 of 2

GENERAL NOTES: Continued

7. PRIOR TO PLACEMENT OF PERMANENT PAVING, EXISTING PAVEMENT SHALL BE CUT TO A NEAT STRAIGHT LINE. ALL PAVEMENT JOINTS OR CRACKS WITHIN 2 FEET OF TRENCH IN ALL DIRECTIONS, OR AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE, SHALL BE REMOVED AND REPLACED.

8. ALL EDGES OF EXISTING PAVEMENT BEING JOINED AND SURFACE BEING OVERLAID SHALL RECEIVE A TACK COAT OF ASPHALT EMULSION.

9. ANY STREET PAVED OR RESURFACED IN THE PREVIOUS 60 MONTHS SHALL BE SUBJECT TO SPECIAL PAVING REQUIREMENTS.

10. ANY TRENCH EXTENDING FROM THE CURB INTO PARKING LANE, SHALL REQUIRE A COMPLETE COLD PLANE AND OVERLAY OF THE PARKING LANE. ALL TRENCHES EXTENDING LONGITUDINALLY IN THE DRIVING LANE SHALL REQUIRE THAT THE ENTIRE LANE BE COLD PLANED AND OVERLAID. ALL TRENCHES EXTENDING INTO THE TRAVELED LANE TRANSVERSELY WILL REQUIRE THE ENTIRE LENGTH OF THE TRENCH UP TO THE NEAREST LANE LINE BE COLD PLANED AND OVERLAID 10 FEET IN BOTH DIRECTIONS FROM THE CENTERLINE OF THE TRENCH.

11. IF TRENCH FAILURE SHOULD OCCUR, THE PERMITTEE/DEVELOPER WILL BE NOTIFIED OF SUCH DEFICIENCIES AND DIRECTED TO REMOVE, REPLACE, REMEDY THIS WORK. UPON FAILURE OF THE CONTRACTOR TO PROMPTLY COMPLY AND UNDER ORDER OF THE CITY ENGINEER, TRENCH SHALL BE REMEDIED, REMOVED, REPLACED AT PERMITTEE/ DEVELOPER'S SOLE EXPENSE.

12. BEDDING MATERIAL SHALL BE SAND, GRAVEL, CRUSHED MISCELLANEOUS BASE OR NATIVE FREE-DRAINING GRANULAR MATERIAL. HAVING A SAND EQUIVALENT OF NOT LESS THAN 30, AND SHALL HAVE A PERCENTAGE COMPOSITION BY WEIGHT WHICH CONFORMS TO THE FOLLOWING GRADING:

SIEVE SIZES	% PASSING SIEVES
1 - ½"	-
1"	-
¾"	100
½"	90 - 100
⅜"	20 - 60
No. 4	0 - 15
No. 8	0 - 5

13. THE CONTRACTOR/PERMITEE SHALL BE REQUIRED TO FOLLOW THE WORK AREA TRAFFIC CONTROL HANDBOOK ("WATCH" MANUAL) OR AS DIRECTED BY THE CITY TRAFFIC ENGINEER OR DESIGNEE.

14. FULL STREET CLOSURES REQUIRE CITY TRAFFIC ENGINEER APPROVAL AND REQUEST FOR SUCH CLOSURES SHALL BE SUBMITTED 20 DAYS IN ADVANCE OF CLOSURE.

15. THE WORKING HOURS ON ARTERIAL AND MAJOR STREETS, SCHOOLS, HOSPITALS, AND FREEWAY RAMPS SHALL BE SUBJECT TO APPROVAL BY CITY TRAFFIC ENGINEER'S OFFICE.

16. ALL STRIPING AND MARKINGS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED IN KIND AS DIRECTED BY THE CITY ENGINEER OR DESIGNEE.

17. ALL POTHOLES/SERVICE CUTS/ETC. SHALL BE BACKFILLED WITH A 2-SACK SLURRY OR AS SPECIFIED BY THE CITY ENGINEER OR DESIGNEE.

18. SOIL-STERILANT IS REQUIRED PRIOR TO PLACEMENT OF PERMANENT PAVING.

19. CONTRACTOR/PERMITTEE SHALL CLEAN UP TRACKING BY ANY MEANS NECESSARY.

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CITY ENGINEER

CITY OF MONTCLAIR

STANDARD PAVING AND TRENCH REPAIR

NO SCALE	STD. PLAN NO. 301	SHEET 2 of 2

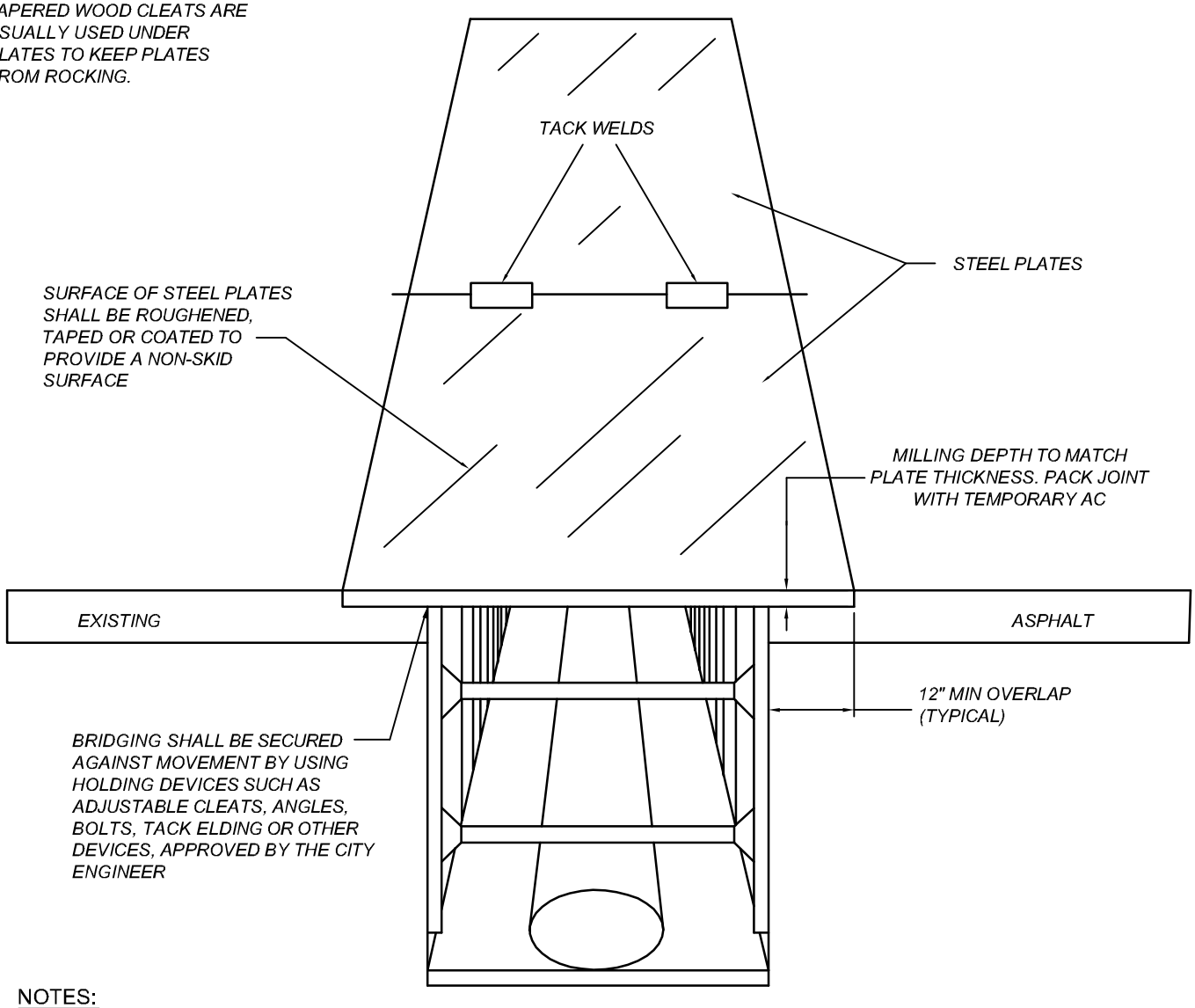
PLATE BRIDGING

WIDTH OF TRENCH	MINIMUM PLATE THICKNESS
1.0 FOOT TO 3 FOOT	1 INCH
4.0 FEET	1 -¼ INCH

FOR SPANS GREATER THAN 4 FEET, A STRUCTURAL DESIGN SHALL BE PREPARED BY A REGISTERED CIVIL ENGINEER AND APPROVED BY CITY ENGINEER.

NOTE:

TAPERED WOOD CLEATS ARE USUALLY USED UNDER PLATES TO KEEP PLATES FROM ROCKING.



NOTES:

1. TRENCH WALLS AND ADJACENT SOIL SHALL BE SUFFICIENTLY STABLE FOR THE USE OF THE ABOVE PLATE.
2. CONTRACTOR/PERMITTEE SHALL BE RESPONSIBLE AT ALL TIMES FOR MAINTENANCE OF THE PLATES FOR THE SAFE OPERATION OF TRAFFIC AND PEDESTRIAN SAFETY.
3. CONTRACTOR/PERMITTEE SHALL COLD PLANE TO RECESS PLATES.
4. MINIMUM PLATE DIMENSION SHALL BE 8' X 10'.

REGISTERED CIVIL ENGINEER
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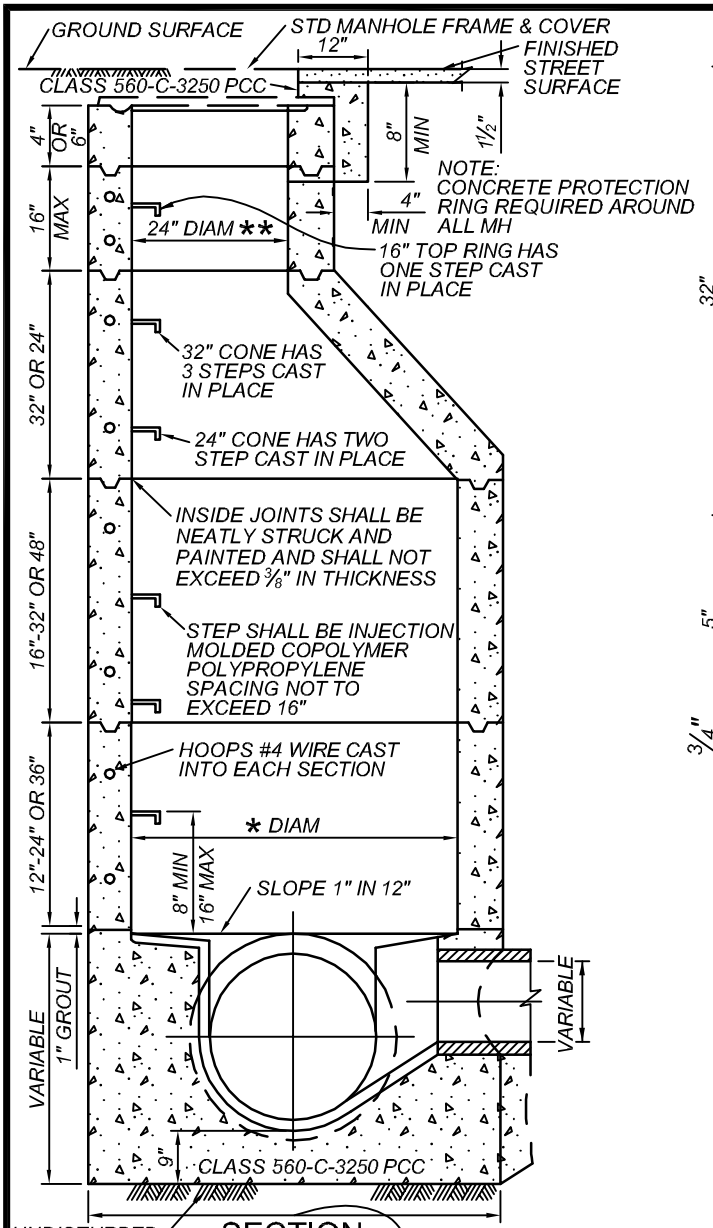
CITY ENGINEER

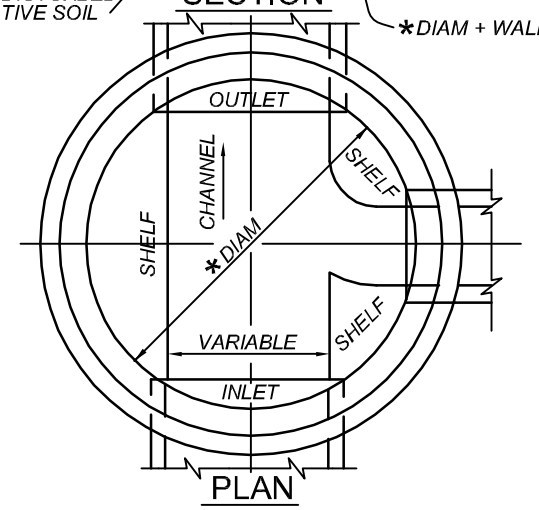
DATE

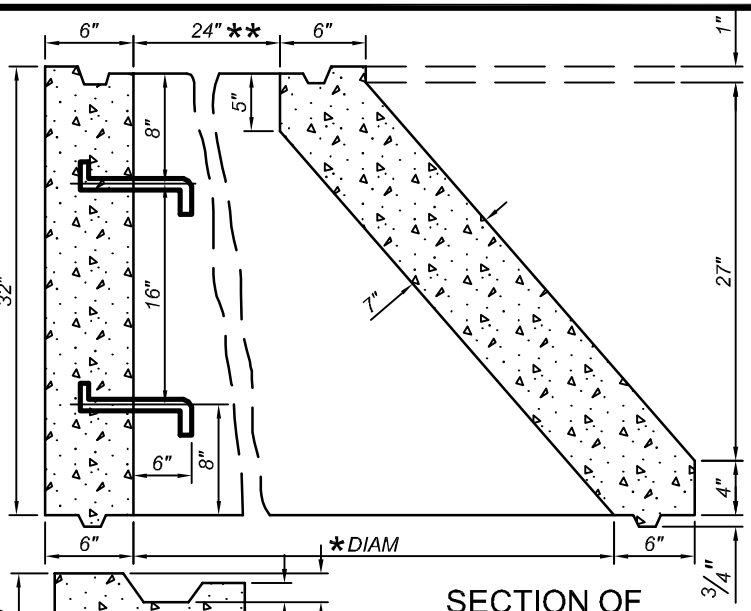
CITY OF MONTCLAIR

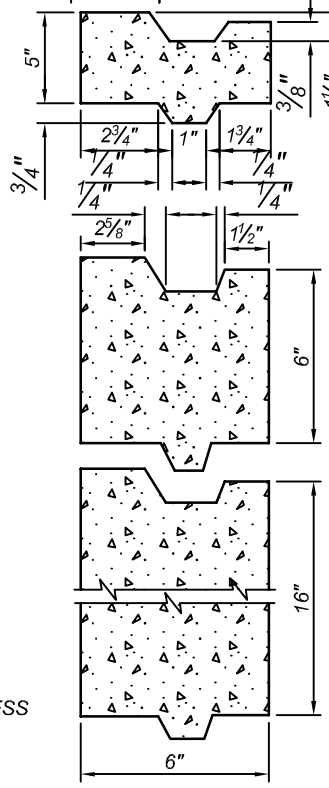
PLATE BRIDGING

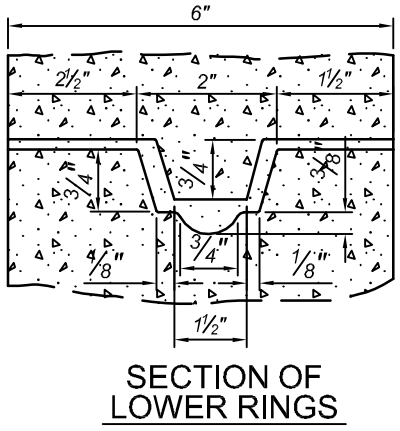
NO SCALE	STD. PLAN NO. 302	SHEET 1 of 1











NOTES:

1. CRADLE FIRST LENGTH OF PIPE AT ALL INLETS AND OUTLETS WITH CLASS 450-C-2000 PCC ON UNDISTURBED NATIVE SOIL.
2. NON-REINFORCED PRECAST MANHOLES SHALL BE 560-C-3250 PCC. REINFORCED PRECAST MANHOLES SHALL BE PER ASTM C-478 SPECIFICATIONS.
* 3. 48" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 18" OR LESS AND A DEPTH OF 12' OR LESS UNLESS OTHERWISE DIRECTED.
* 4. 60" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 20" OR MORE OR WITH A DEPTH GREATER THAN 12' UNLESS OTHERWISE DIRECTED.
* 5. 72" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 30" OR LARGER.
** 6. A 24" FRAME AND COVER SHALL BE USED ON 48" I.D. MANHOLES.
** 7. A 30" FRAME AND COVER SHALL BE USED ON 60" I.D. MANHOLES.
** 8. A 36" FRAME AND COVER SHALL BE USED ON 72" I.D. MANHOLES.
9. STEPS SHALL BE INJECTION MOLDED COPOLYMER POLYPROPYLENE, MAXIMUM SPACING 16".

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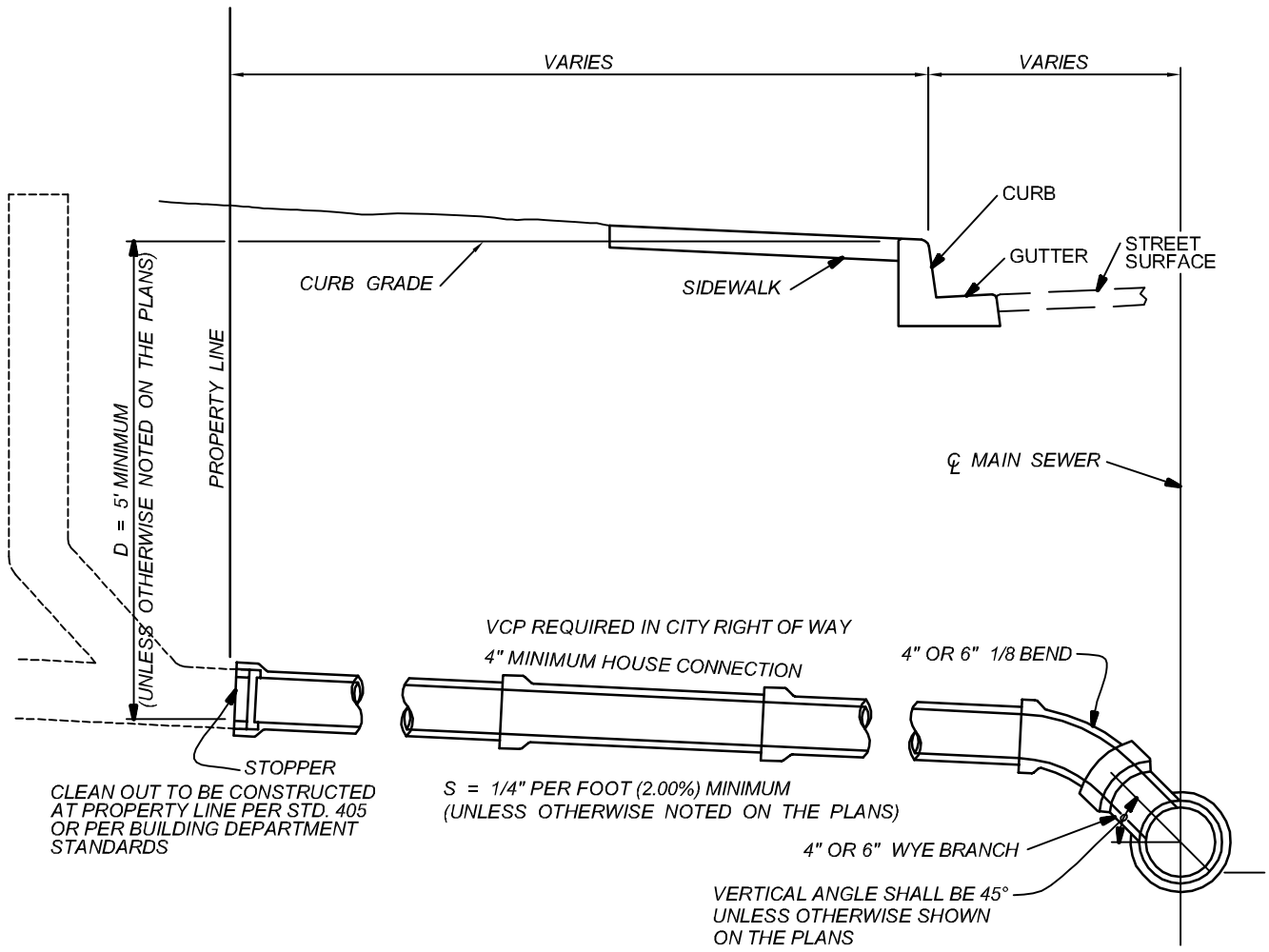
CITY OF MONTCLAIR

ECCENTRIC CONE PRECAST CONCRETE MANHOLE

NO SCALE	STD. PLAN NO. 401	SHEET 1 of 1



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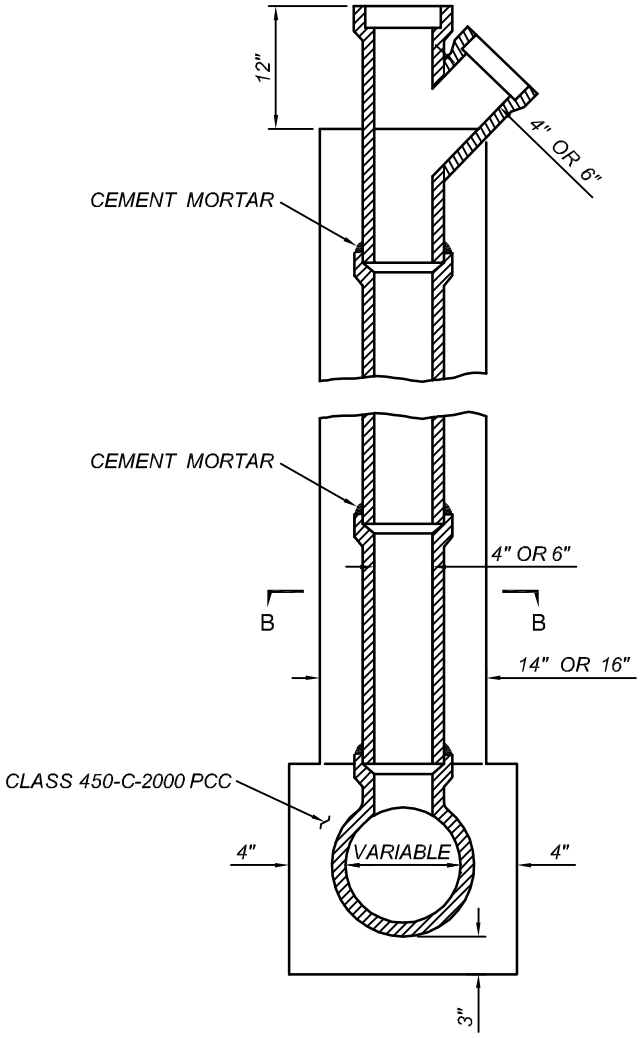
CITY ENGINEER

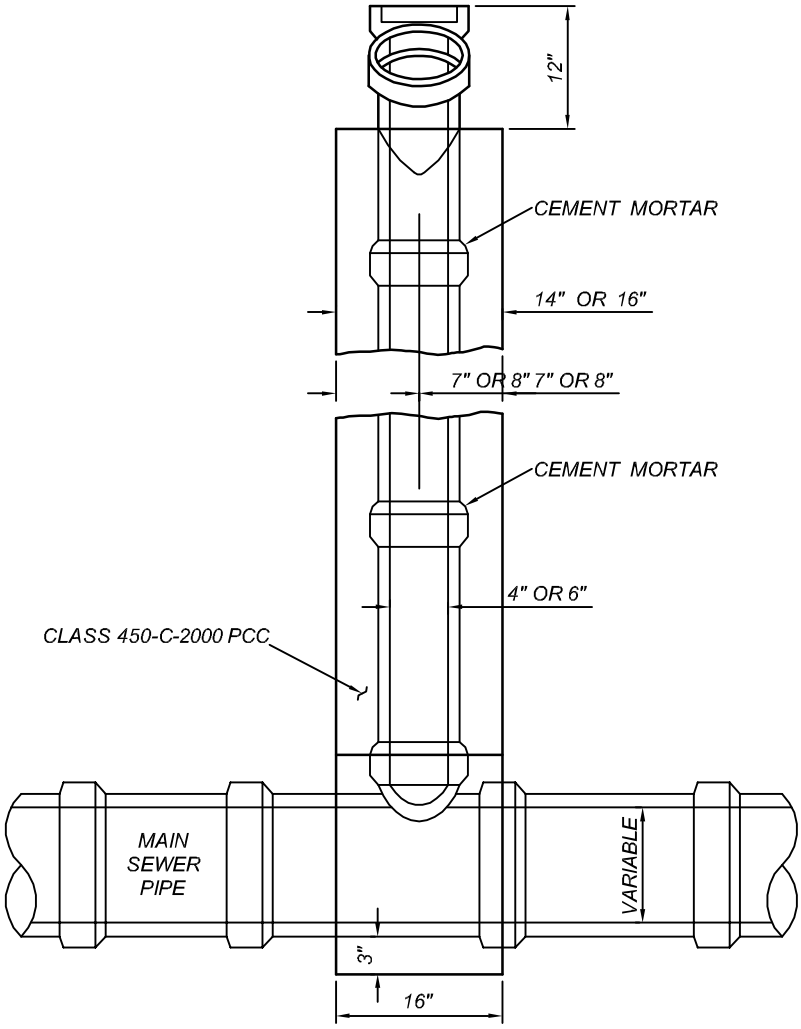
CITY OF MONTCLAIR

SEWER LATERAL

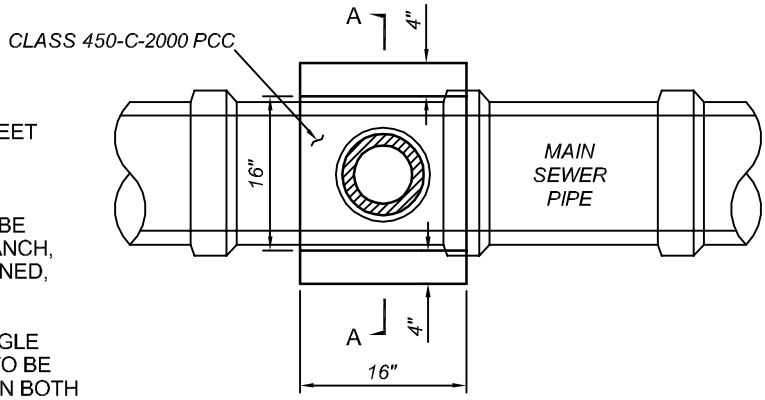
NO SCALE	STD. PLAN NO. 402	SHEET 1 of 1



SECTION A-A



ELEVATION



SECTION PLAN B-B

NOTES:

1. THE UPPER END OF THE CHIMNEY PIPE SHALL BE 8 FEET BELOW THE GRADE OF THE LOWER CURB, UNLESS OTHERWISE SPECIFIED.
2. WHERE ONE OR TWO HOUSE CONNECTIONS ARE TO BE JOINED TO THE CHIMNEY PIPE, USE A SINGLE "Y" BRANCH, WHERE THREE HOUSE CONNECTIONS ARE TO BE JOINED, USE A DOUBLE "Y" BRANCH.
3. WHERE THE CHIMNEY PIPE IS TO BE USED FOR A SINGLE HOUSE CONNECTION FACE "Y" TOWARD PROPERTY TO BE SERVED; WHERE USED FOR HOUSE CONNECTIONS ON BOTH SIDES OF THE SEWER, THE HOUSE CONNECTION ON THE RIGHT SIDE OF THE SEWER (LOOKING UP GRADE), SHALL BE CONNECTED TO THE "Y" BRANCH BY A 1/8 BEND AND THE HOUSE CONNECTION ON THE LEFT SIDE SHALL BE CONNECTED TO THE UPPER END OF THE CHIMNEY BY A 4" OR 6" 1/4 BEND UNLESS OTHERWISE NOTED.

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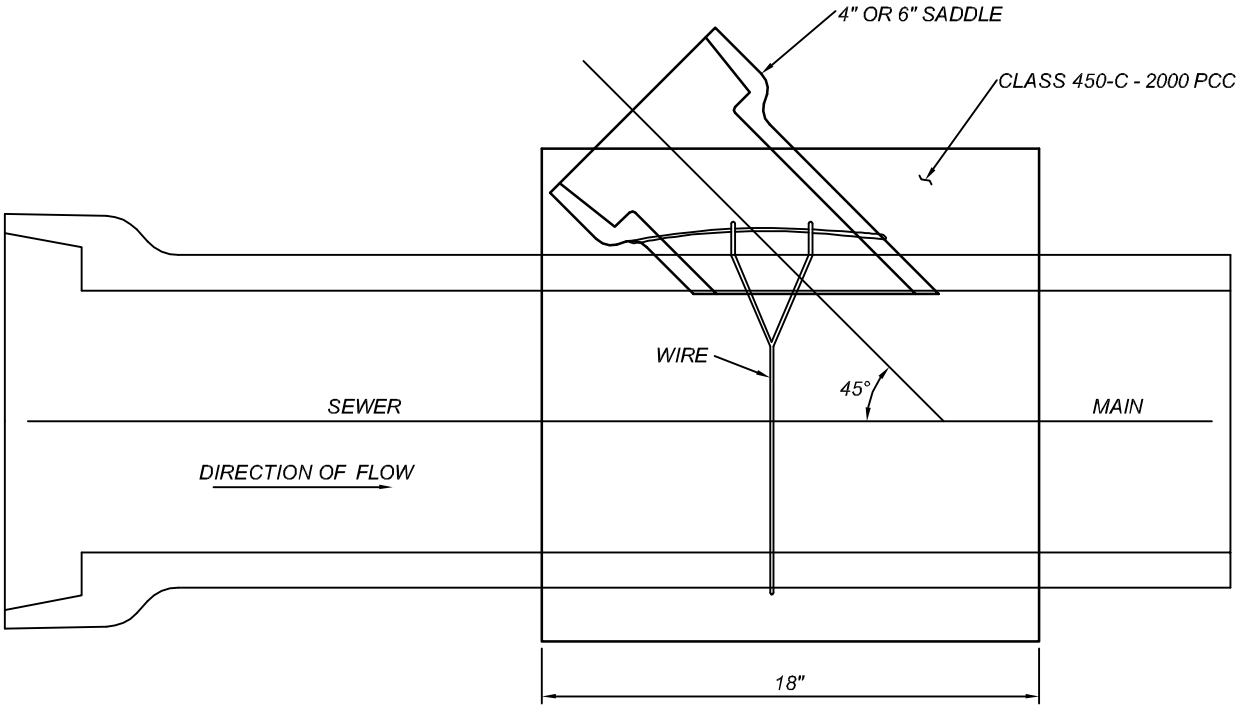
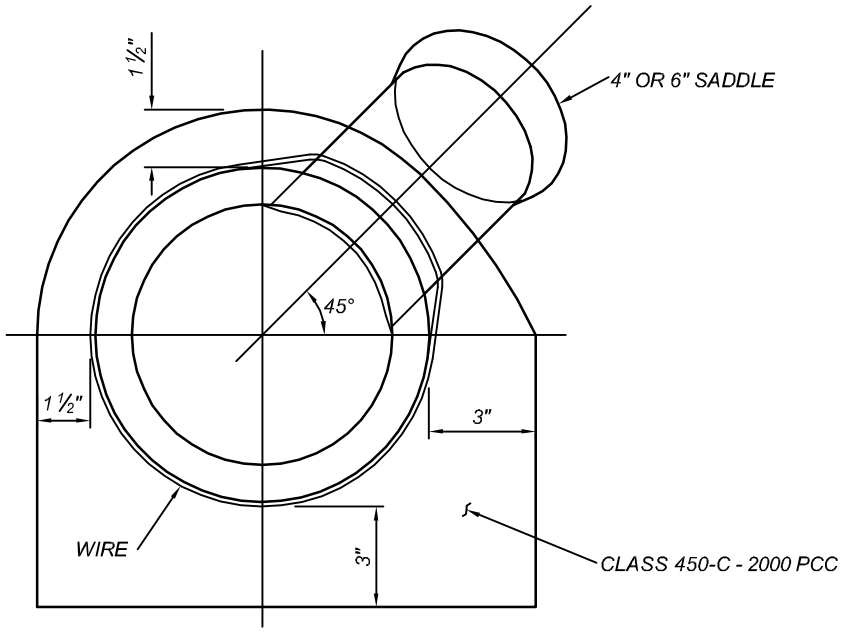
CITY OF MONTCLAIR

STANDARD CHIMNEY PIPE

NO SCALE	STD. PLAN NO. 403	SHEET 1 of 1



PLAN VIEW



SECTION

NOTES:

1. INSPECTION MUST BE OBTAINED FOR SADDLE PRIOR TO PLACING PCC AND AGAIN BEFORE BACKFILLING OVER LATERAL.
2. TIE WIRE SHALL BE 12 GA GALVANIZED WIRE.

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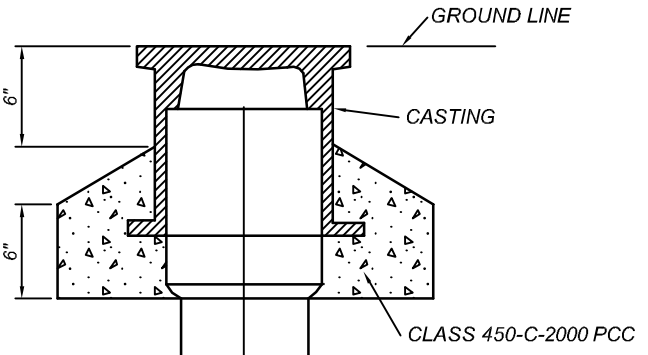
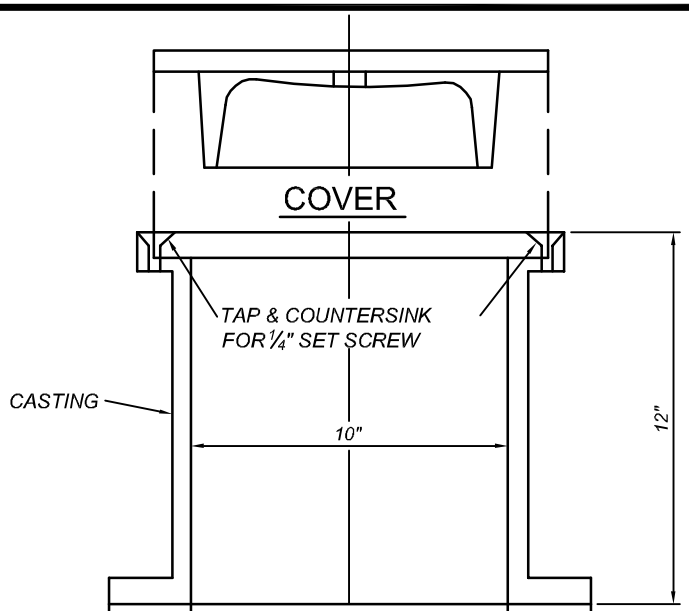
CITY OF MONTCLAIR

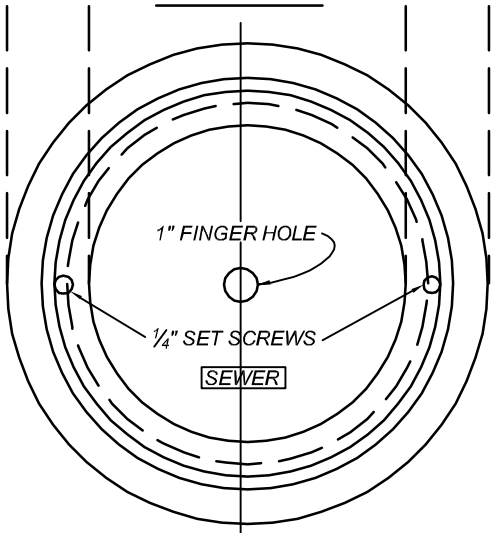
SEWER SADDLE

NO SCALE	STD. PLAN NO. 404	SHEET 1 of 1





SIDE VIEW



TOP VIEW

TOP VIEW

NOTES:

1. CLEAN-OUT PIPE MUST BE SAME DIAMETER AS MAIN LINE SEWER.
2. CASTING MAY BE: ALHAMBRA FOUNDRY A-1240, EJ 3510, SOUTH BAY FOUNDRY A-1240 OR NEENAH FOUNDRY R-1976

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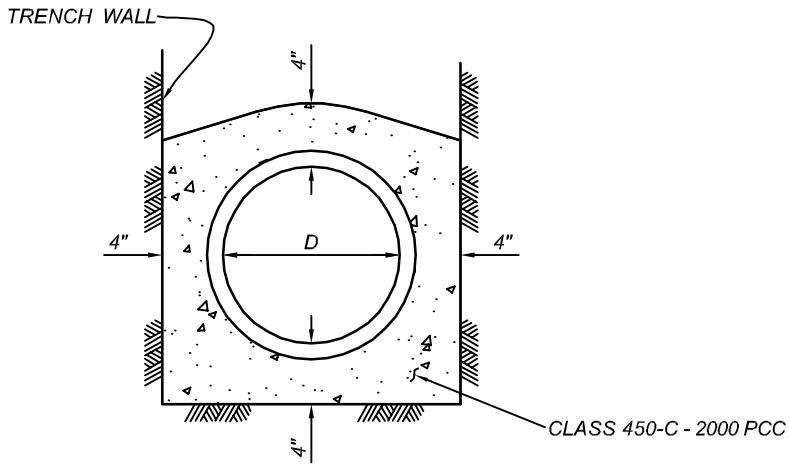
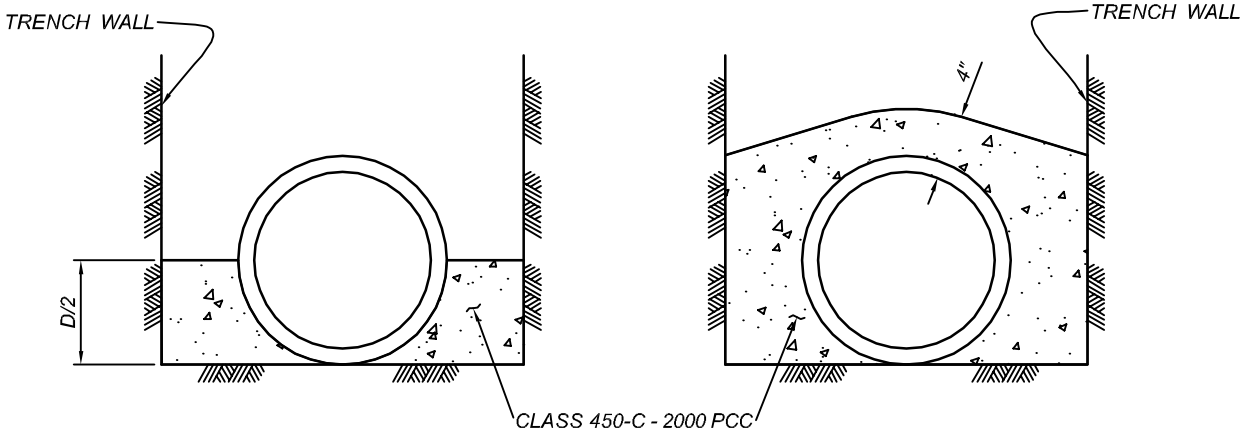
CITY OF MONTCLAIR

SEWER TERMINAL CLEAN-OUT

NO SCALE	STD. PLAN NO. 405	SHEET 1 of 1



NO. 1
CONCRETE ENCASEMENT



CONCRETE CRADLE

NO. 2
CONCRETE ENCASEMENT

NOTE:

1. WHERE TRENCH WIDTH EXCEEDS THE WIDTH SPECIFIED IN THE SPECIFICATIONS, CONTRACTOR SHALL FURNISH AND PLACE AT HIS OWN EXPENSE, CRADLE AS SHOWN ABOVE.

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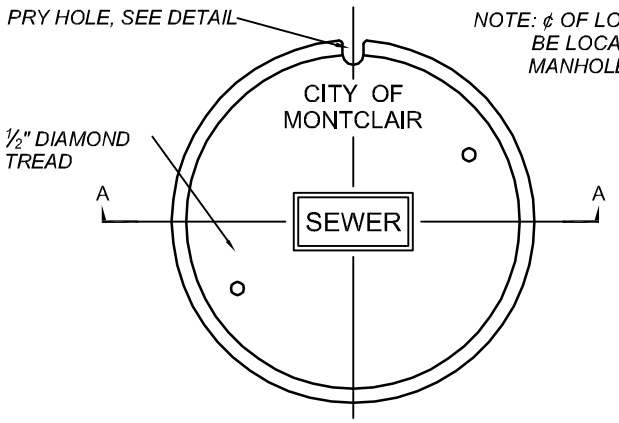
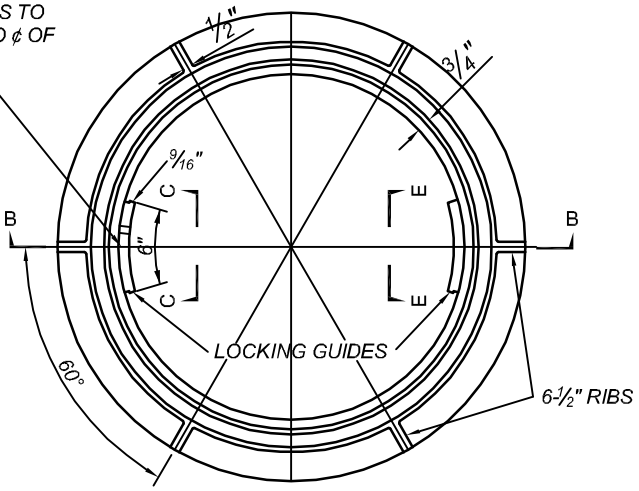
CONCRETE CRADLE & ENCASEMENT

NO SCALE	STD. PLAN NO. 406	SHEET 1 of 1

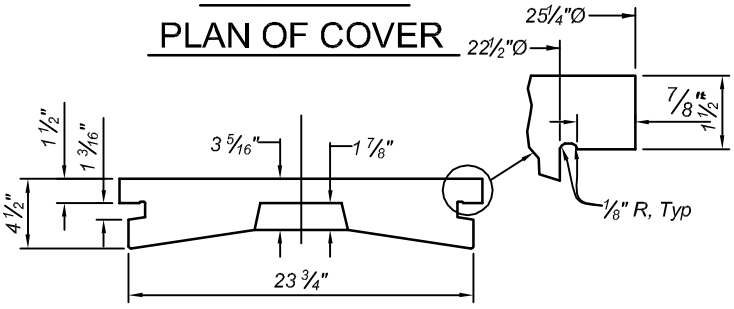


TOP VIEW

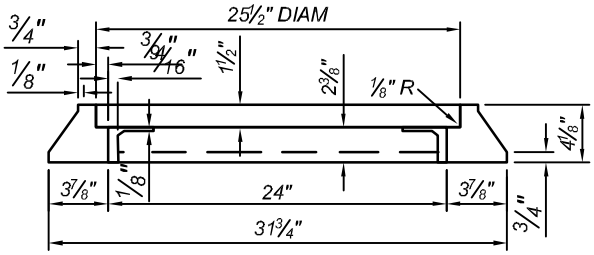
PLAN OF COVER



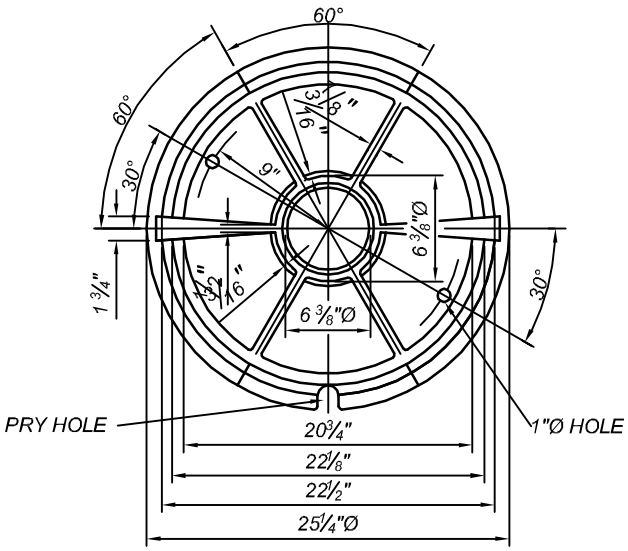
PLAN OF FRAME



SECTION A-A

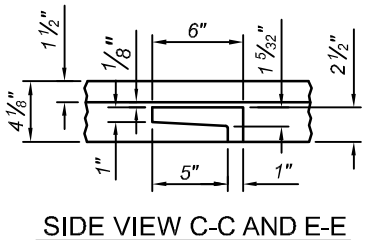


SECTION B-B



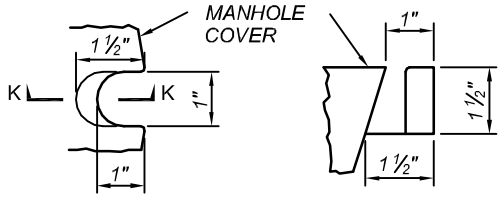
BOTTOM VIEW

PLAN OF COVER



SIDE VIEW C-C AND E-E

LOCKING GUIDE



PLAN TOP VIEW SECTION K-K

PRY HOLE DETAIL

NOTES:

MACHINE SEATS AND GRIND LUGS SMOOTH.
PERIMETER OF COVER SHALL BE GROUND SMOOTH, DIAMETER TOLERANCE ± 1⁄16"
WEIGHT OF MANHOLE FRAME = 150 LBS.
WEIGHT OF MANHOLE COVER = 215 LBS.
THE CAST IRON USED SHALL HAVE A TENSILE STRENGTH OF 50,000 LBS. PER SQ. IN.
ALHAMBRA FOUNDRY NO. A 1175 OR EQUAL

MANUFACTURER	24" COVER	30" COVER	36" COVER
ALHAMBRA FOUNDRY	A-1495	A-1252	A-1251
EJ	3160	3163	3106
SOUTH BAY FOUNDRY	A-1176	A-1252	A-1251
NEENAH FOUNDRY	1574	1577	1578

REGISTERED CIVIL ENGINEER
MONICA HEREDIA
No. C 56547
CIVIL
STATE OF CALIFORNIA

APPROVED BY:

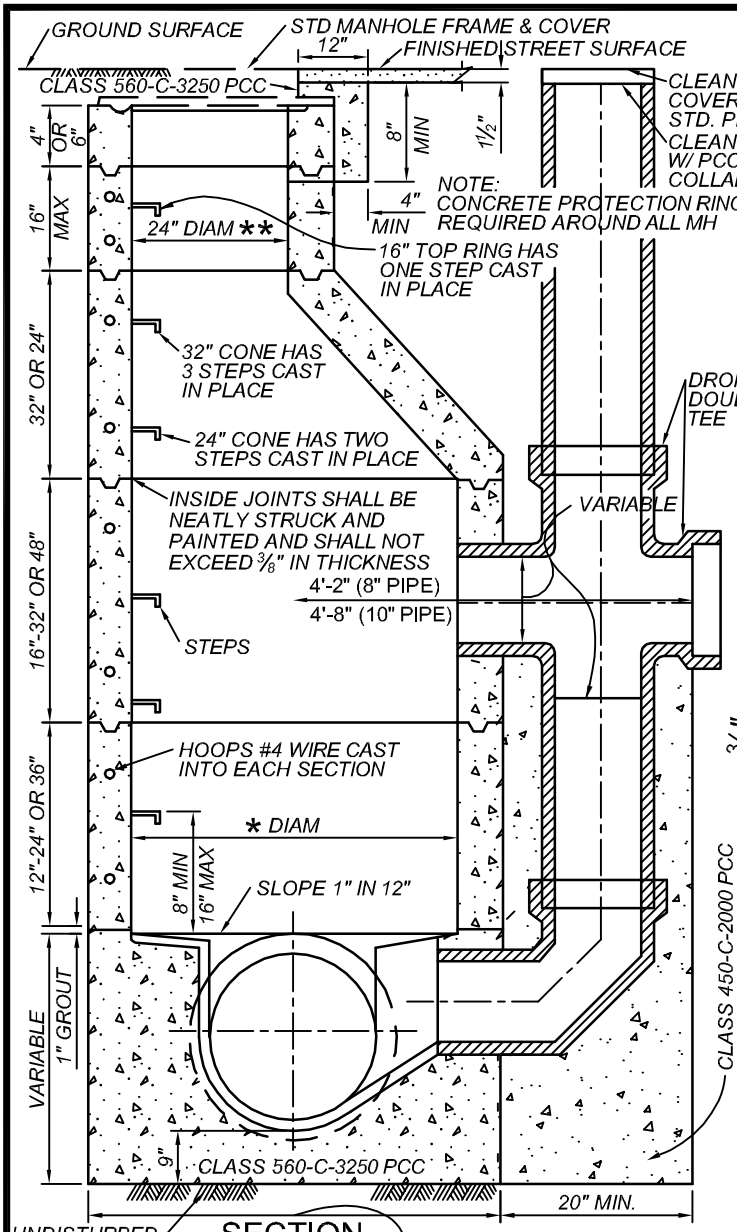
MONICA HEREDIA
CITY ENGINEER

05-14-24
DATE

CITY OF MONTCLAIR

MANHOLE FRAME & COVER

NO SCALE	STD. PLAN NO. 407	SHEET 1 of 1



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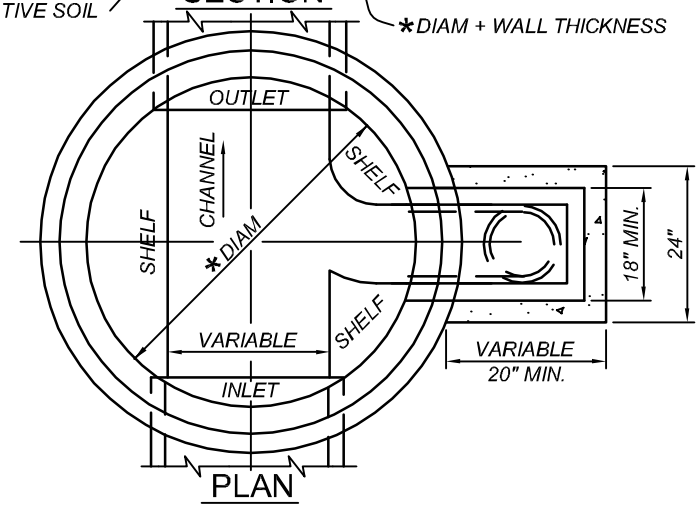

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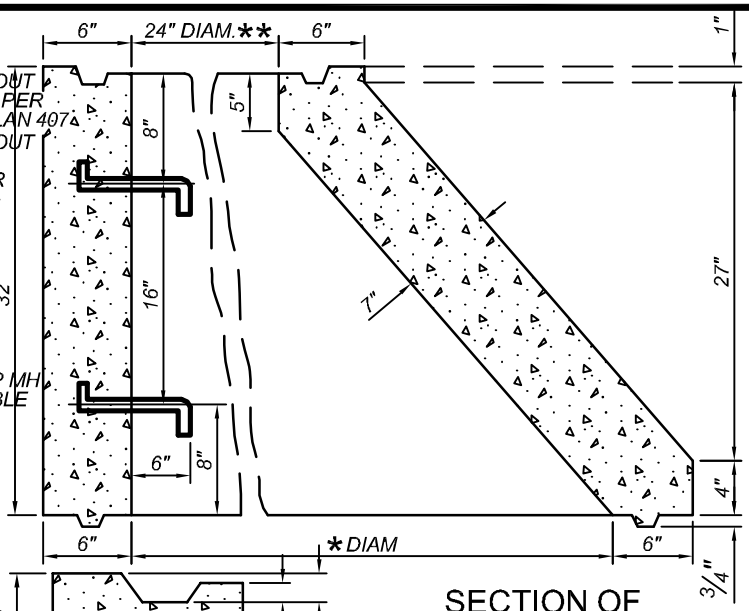
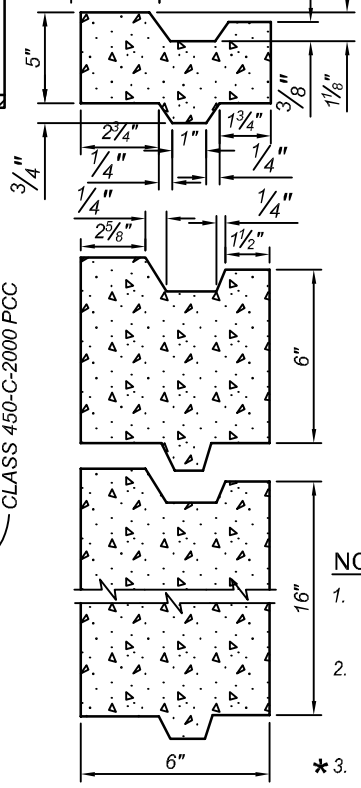
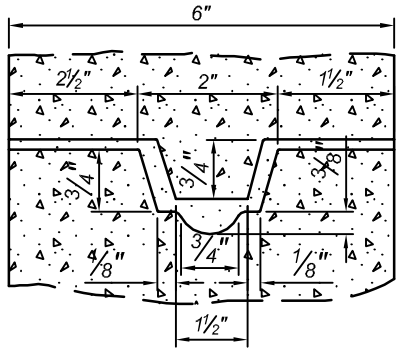


SECTION OF ECCENTRIC CONE



SECTIONS OF TOP RINGS



SECTION OF LOWER RINGS

NOTES:

1. CRADLE FIRST LENGTH OF PIPE AT ALL INLETS AND OUTLETS WITH CLASS 450-C-2000 PCC ON UNDISTURBED NATIVE SOIL.
2. NON-REINFORCED PRECAST MANHOLES SHALL BE 560-C-3250 PCC. REINFORCED PRECAST MANHOLES SHALL BE PER ASTM C-478 SPECIFICATIONS.
3. * 48" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 18" OR LESS AND A DEPTH OF 12' OR LESS UNLESS OTHERWISE DIRECTED.
4. * 60" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 20" OR MORE OR WITH A DEPTH GREATER THAN 12' UNLESS OTHERWISE DIRECTED.
5. * 72" I.D. MANHOLE TO BE USED ON SEWERS WITH A DIAMETER OF 30" OR LARGER.
6. ** A 24" FRAME AND COVER SHALL BE USED ON 48" I.D. MANHOLES.
7. ** A 30" FRAME AND COVER SHALL BE USED ON 60" I.D. MANHOLES
8. ** A 36" FRAME AND COVER SHALL BE USED ON 72" I.D. MANHOLES.
9. STEPS SHALL BE INJECTION MOLDED COPOLYMER POLYPROPYLENE, MAXIMUM SPACING 16".

REGISTERED CIVIL ENGINEER
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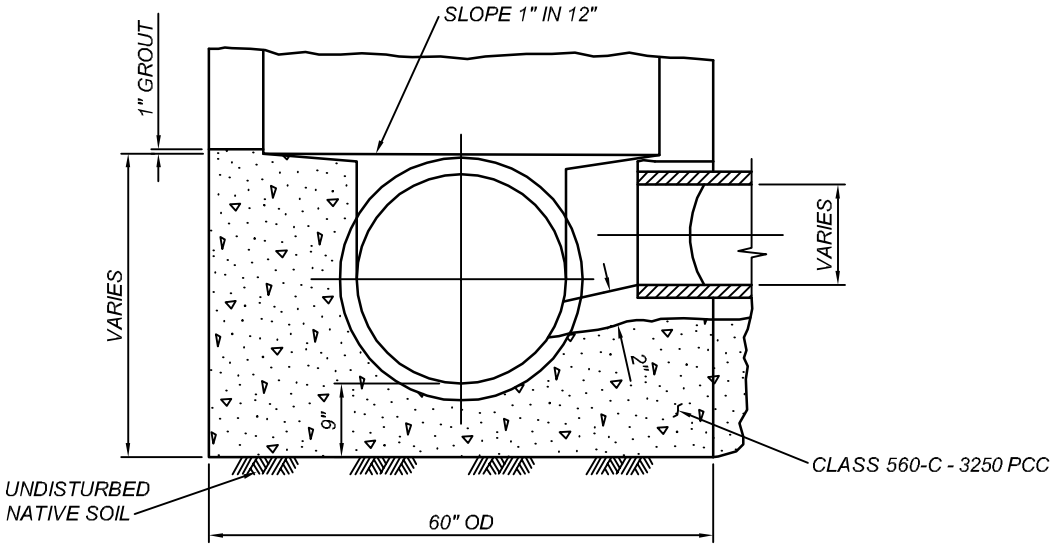
05-14-24
DATE

CITY OF MONTCLAIR

STANDARD PLAN DROP MANHOLE

NO SCALE	STD. PLAN NO. 408	SHEET 1 of 1



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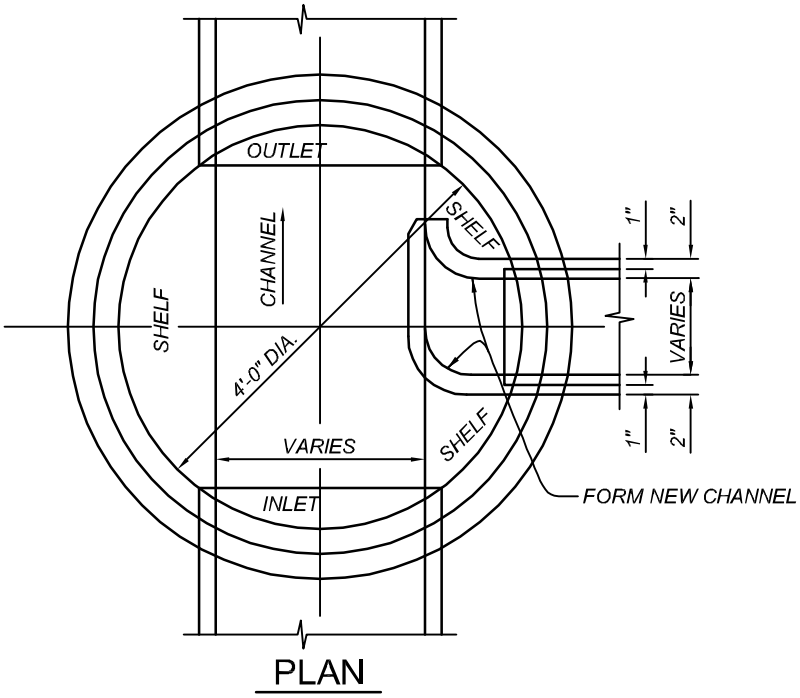

PLAN

NOTES:

1. CORE DRILL THROUGH EXISTING MANHOLE WALL AND OPEN CIRCULAR HOLE TO PROVIDE 1" CLEAR AROUND PIPE INLET TO BE INSTALLED.
2. BREAK OUT CHANNEL TO 2" BELOW PROPOSED FLOWLINE.
3. PAINT EXISTING EXPOSED CONCRETE SURFACES WITH EPOXY RESIN BOND AS RECOMMENDED BY THE MANUFACTURER AND TO SATISFACTION OF THE CITY ENGINEER.
4. SEAL PIPE STUB AND FORM NEW CHANNEL WITH CONCRETE MORTAR AND EPOXY RESIN MIXED AS DIRECTED BY THE CITY ENGINEER.

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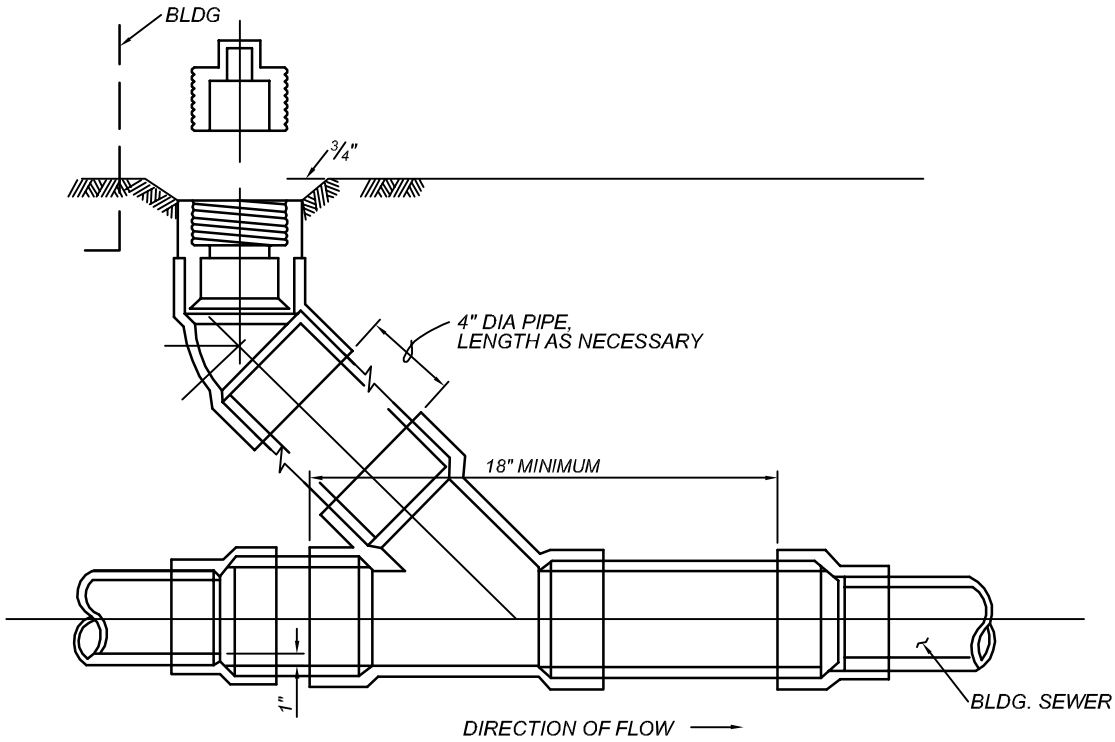
CITY ENGINEER

DATE

CITY OF MONTCLAIR

BREAK INTO AND RECHANNEL AN EXISTING MANHOLE

NO SCALE	STD. PLAN NO. 409	SHEET 1 of 1



NOTES:

1. BARREL DIAMETER OF SAMPLING WYE TO BE A MINIMUM OF 2" LARGER THAN BLDG. DISCHARGE LINE.
2. DIAMETER OF RISER PIPE TO BE 4".
3. MUST BE ACCESSIBLE AT ALL TIMES TO CITY PERSONNEL.
4. NOT FOR USE IN TRAVELED WAYS.
5. IF NECESSARY TO PLACE CAP IN SIDEWALK, USE FLUSH CAP.

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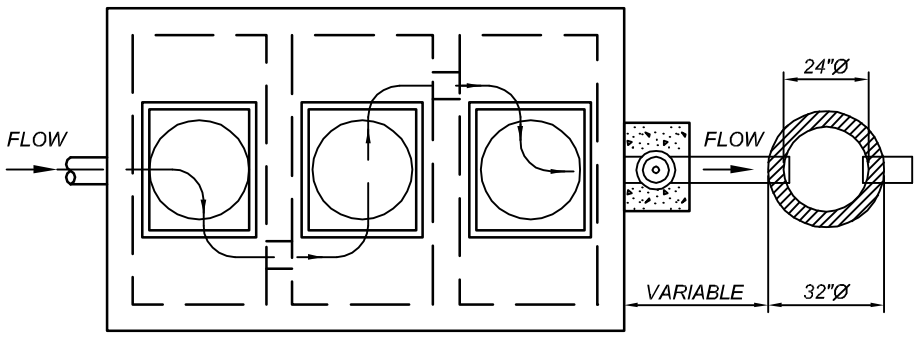
CITY ENGINEER

CITY OF MONTCLAIR

SAMPLING STATION

NO SCALE	STD. PLAN NO. 410	SHEET 1 of 1



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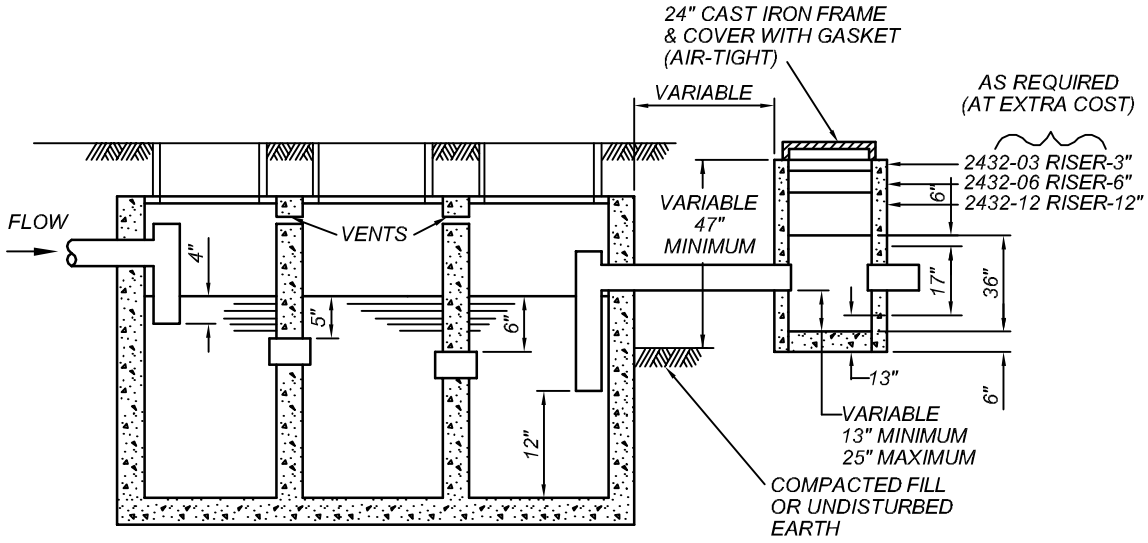

SECTION

NOTES:

1. THIS INSTALLATION SHALL BE ACCESSIBLE FOR MAINTENANCE AND INSPECTION AT ALL TIMES.
2. WHERE SUBJECT TO VEHICLE LOADING, DESIGN ADEQUACY SHALL BE VERIFIED.
3. THE PERIMETER SHALL SLOPE AWAY FROM THE INTERCEPTOR. FLOW SHALL NOT INCLUDE SANITARY SEWAGE OR SURFACE DRAINAGE.
4. EACH INSTALLATION SHALL BE SUBJECT TO REVIEW FOR ADEQUATE CAPACITY PRIOR TO CONSTRUCTION. MINIMUM CAPACITY SHALL BE 750 GALLONS.
5. A SAMPLING BOX IS REQUIRED IN ADDITION TO INTERCEPTOR.
6. A TWO COMPARTMENT INTERCEPTOR IS THE MINIMUM STANDARD.
7. EACH COMPARTMENT SHALL HAVE A SEPARATE RING AND COVER.

REGISTERED CIVIL ENGINEER
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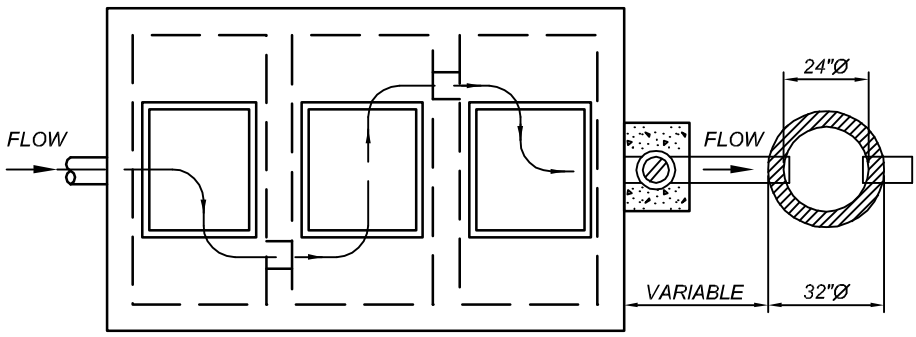
CITY ENGINEER

CITY OF MONTCLAIR

SAND INTERCEPTOR

NO SCALE	STD. PLAN NO. 411	SHEET 1 of 1



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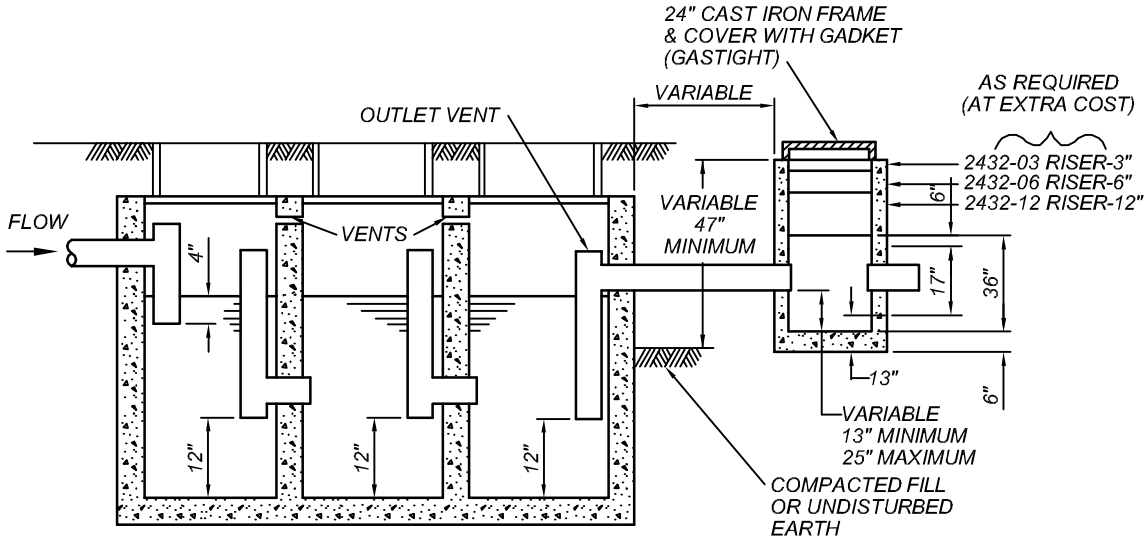

SECTION

NOTES:

1. THIS INSTALLATION SHALL BE ACCESSIBLE FOR MAINTENANCE AND INSPECTION AT ALL TIMES.
2. WHERE SUBJECT TO VEHICLE LOADING, DESIGN ADEQUACY SHALL BE VERIFIED.
3. THE PERIMETER SHALL SLOPE AWAY FROM THE INTERCEPTOR. FLOW SHALL NOT INCLUDE SANITARY SEWAGE OR SURFACE DRAINAGE.
4. EACH INSTALLATION SHALL BE SIZED BY THE CITY FOR ADEQUATE CAPACITY PRIOR TO CONSTRUCTION. MINIMUM CAPACITY SHALL BE 750 GALLONS.
5. A SAMPLING BOX IS REQUIRED IN ADDITION TO INTERCEPTOR.
6. A TWO COMPARTMENT INTERCEPTOR IS THE MINIMUM STANDARD.
7. EACH COMPARTMENT SHALL HAVE A SEPARATE RING AND COVER.

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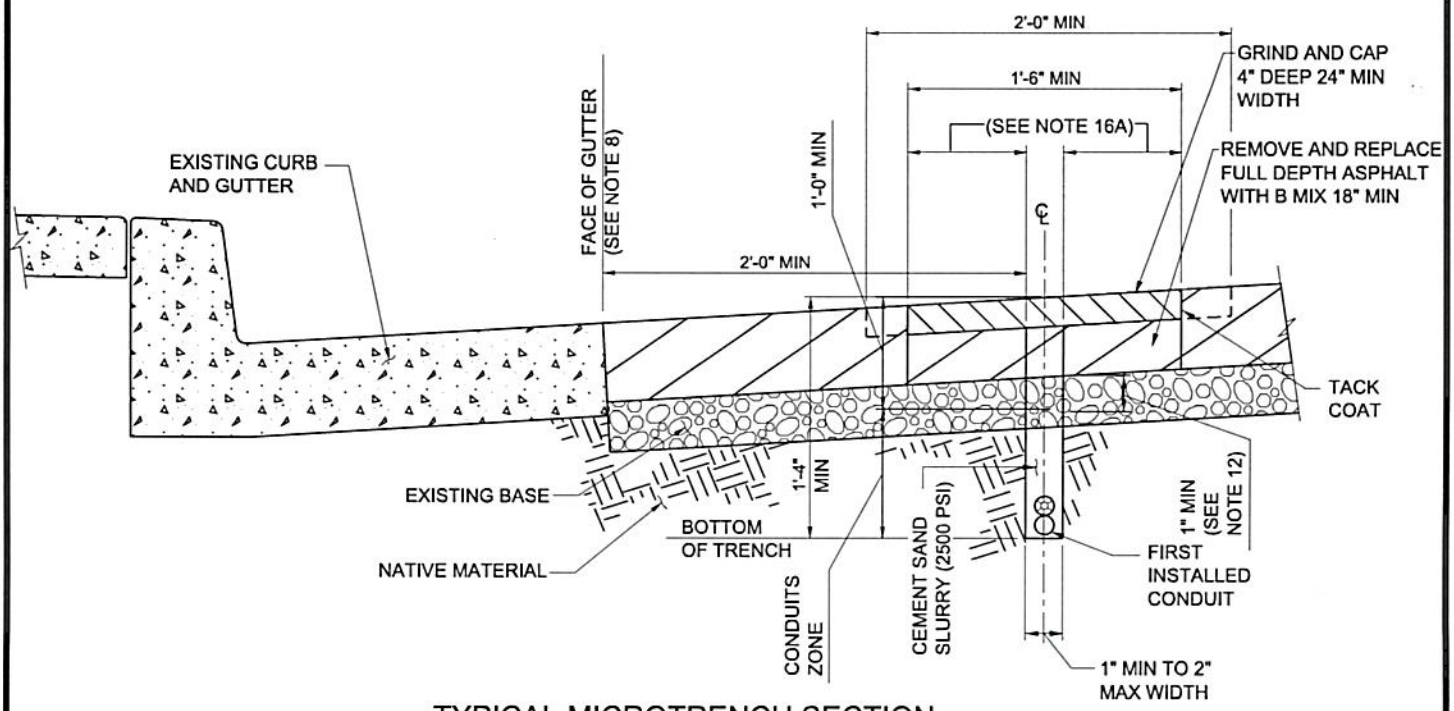
CITY OF MONTCLAIR

GREASE INTERCEPTOR

NO SCALE	STD. PLAN NO. 412	SHEET 1 of 1



TYPICAL MICROTRENCH SECTION

(INTEGRAL CURB AND GUTTER SHOWN)

NOTES:

ALL WORK SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION (SSPWC) "GREENBOOK" ADOPTED BY THE CITY OF MONTCLAIR.

GENERAL:

1. MICRO-TRENCHING SHALL ONLY BE USED TO INSTALL TELECOMMUNICATION CONDUITS.
2. MICRO-TRENCHING SHALL NOT BE ALLOWED IN CONCRETE PAVED STREETS, NOR SIDEWALKS, PARKWAYS, CURBS AND GUTTERS.
3. THE CONTRACTOR SHALL IDENTIFY ALL EXISTING UTILITIES, INCLUDING SERVICE CONNECTIONS IN THE FIELD. THE CONTRACTOR SHALL CONTACT UNDERGROUND SERVICE ALERT (U.S.A.) AT LEAST 48 HOURS PRIOR TO START OF WORK AT 8-1-1, OR TOLL-FREE AT 1-800-422-4133. THE CONTRACTOR SHALL FURTHER SUPPLEMENT THE FINDINGS OF U.S.A. TO DETERMINE THE EXACT LOCATIONS AND DEPTHS OF ALL UTILITIES BY USING A MOBILE GROUND PENETRATING RADAR SYSTEM. THE CONTRACTOR SHALL POTHOLE ALL CROSSING UTILITIES AND PARALLEL UTILITIES WITHIN 18-INCHES OF THE PROPOSED ALIGNMENT TO A DEPTH OF 6-INCHES BELOW THE BOTTOM OF THE MICRO-TRENCH, TO DETERMINE THE EXISTING UTILITY ALIGNMENT AND ELEVATION. POTHOLES SHALL BE IMMEDIATELY BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE SPECIFICATIONS OR RESTORED AS DIRECTED BY THE ENGINEER.
4. IF EXISTING UTILITIES ARE DAMAGED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE INSPECTOR, ENGINEER AND UTILITY OWNER TO PERFORM THE REPAIRS PROMPTLY ACCORDING TO THEIR REQUIREMENTS AND PER ASSOCIATED CITY PERMITS.
5. THE FOLLOWING ITEMS SHALL BE SUBMITTED BY THE CONTRACTOR TO THE ENGINEER FOR APPROVAL:
   - A. PROVIDE A DETAILED SITE PLAN INCLUDING EXISTING CONDITIONS AND PROPOSED SCOPE OF WORK IN DETAIL.
   - B. A STREET CROSS-SECTION THAT INCLUDES THE FOLLOWING INFORMATION:
     - (1) THE CURB AND GUTTER, ASPHALT CONCRETE (AC) ROADWAY APPROXIMATE THICKNESS, BASE, DEPTH AND WIDTH OF MICRO-TRENCH, DEPTH OF TOPMOST CONDUIT, DISTANCES OF MICRO-TRENCH TO FACE OF GUTTER, CURB, CONCRETE PAVEMENT OR STRUCTURE AS APPLICABLE, AND BACKFILL MATERIAL.
     - (2) AC REINSTATEMENT INCLUDING WIDTH AND DEPTH OF GRIND AND CAP, STARTING FROM GUTTER FACE, CURB, SLAB OR STRUCTURE AS APPLICABLE.
   - C. DETAIL SHOWING CONDUIT FROM MAIN MICRO-TRENCH ALIGNMENT TO LATERAL SURFACE CONNECTIONS INCLUDING TO ANY JUNCTION/PULL BOX. INCLUDE SPECIFIC INFORMATION OF DEPTH, SIZE, AND METHOD OF EXCAVATION BELOW EXISTING CURB AND GUTTER.
   - D. CUT SHEETS OF THE PROPOSED EQUIPMENT PARTICULARLY SUITABLE FOR MICRO-TRENCHING, INCLUDING:
     - (1) MICRO-TRENCHER CAPABLE OF MEETING TARGET DEPTH AND WIDTH IN A SINGLE PASS WITH AN INTEGRAL HOOD AND ASSOCIATED VACUUM SYSTEM. SELECTION OF CUTTING WHEEL SHALL BE SUCH THAT IT MINIMIZES DAMAGE TO THE ADJACENT AC SURFACE.
     - (2) MOBILE CONCRETE/SLURRY PLACEMENT WITH AN ON-BOARD VIBRATOR AND NARROW TROUGH TO MATCH MICRO-TRENCH WIDTH.
     - (3) MOBILE GROUND PENETRATING RADAR SYSTEM THAT IS CAPABLE OF LOCATING BOTH METALLIC AND NON-METALLIC PIPES AND CABLES TO A DEPTH OF 24-INCHES.
   - E. OTHER SITE SPECIFIC ITEMS AS REQUIRED BY THE ENGINEER.

REGISTERED CIVIL ENGINEER
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CIVIL
STATE OF CALIFORNIA

APPROVED BY:

3/9/2021

NOEL A. CASTILLO
CITY ENGINEER

DATE

CITY OF MONTCLAIR

MICROTRENCHING DETAIL

NO SCALE	STD. DETAIL A	SHEET 1 of 2

LIMITS OF REMOVALS, TRENCH WIDTH, AND LOCATION

6. THE MICRO-TRENCH SHALL BE CONSTRUCTED WITH CONTINUOUS UNIFORM STRAIGHT AND NEAT EDGES.
7. MICRO-TRENCH ALIGNMENTS SHALL CONSIST OF RUNS PARALLEL TO THE CENTERLINE OF THE STREET. STREET CROSSING MAY BE DONE PROVIDED THE ALIGNMENT IS PERPENDICULAR TO THE STREET CENTERLINE TO THE EXTENT POSSIBLE.
8. THE EDGE OF THE MICRO-TRENCH SHALL BE A MINIMUM OF 24-INCHES FROM THE EXISTING FACE OF THE GUTTER, EXISTING CONCRETE STRUCTURE, OR CURB IF GUTTER IS NOT PRESENT.
9. THE MICRO-TRENCH WIDTH SHALL BE A MINIMUM OF 1-INCH AND A MAXIMUM OF 2-INCHES.
10. MICRO-TRENCHING MAY BE PERMITTED UPON THE ENGINEER'S DISCRETION ON SPECIAL PAVEMENTS SUCH AS DECORATIVE ASPHALT PAVING, AND THROUGH EXISTING IMPROVEMENTS SUCH AS PERPENDICULAR TO SPEED BUMPS. SPECIAL PAVEMENTS AND EXISTING IMPROVEMENTS SHALL BE RESTORED IN KIND AS APPROVED BY THE ENGINEER. HOWEVER, MICRO-TRENCHING THROUGH EXISTING CURB, GUTTER, CROSS GUTTER, BUS PAD, SIDEWALK, FLOATING CURB EXTENSION, BUS BULB, TRUCK PILLOW, RAISED CROSSWALK, ISLAND, MINI-ROUNDABOUT, OR SIMILAR ELEMENTS IS NOT PERMITTED.
11. UP TO TWO (2) VERTICALLY STACKED CONDUITS CAN BE PLACED WITHIN A MICRO-TRENCH.
12. THE CONDUIT SHALL BE INSTALLED AT A MINIMUM DEPTH OF 24 INCHES BELOW THE EXISTING AC PAVEMENT SURFACE, AND THE BOTTOM OF THE MICRO-TRENCH SHALL BE AT A MINIMUM DEPTH OF 24 INCHES BELOW THE EXISTING AC PAVEMENT SURFACE.
13. ANCHORS/SPACERS SHALL BE PLACED AT A MAXIMUM OF 10-FEET APART ALONG THE ALIGNMENT TO ENSURE THE CONDUIT DOES NOT RISE FROM THE BOTTOM OF THE MICRO-TRENCH AND DOES NOT TOUCH THE WALLS OF THE MICRO-TRENCH DURING INSTALLATION.

BACKFILL

14. ALL MICRO-TRENCHES SHALL BE COMPLETELY BACKFILLED WITH A CEMENT SAND SLURRY 2500 PSI TO FINISH GRADE BY THE END OF THE WORK DAY.

GRIND AND RESURFACE SECTION

15. COMMENCEMENT OF SURFACE PREPARATION SUCH AS GRINDING/CHIPPING FOR ASPHALT CONCRETE PAVING REPLACEMENT WILL OCCUR NO SOONER THAN 48 HOURS AFTER SLURRY BACKFILL OF TRENCH. FIELD CONDITIONS OR MATERIAL USED MAY NECESSITATE A LONGER WAIT AS DETERMINED BY THE INSPECTOR.
16. AS SOON AS BACKFILL HAS CURED, NOT TO EXCEED 30 CALENDAR DAYS, ASPHALT CONCRETE SHALL BE GROUND AND CAPPED AS FOLLOWS:
    A. EXISTING AC AND SLURRY BACKFILL SHALL BE GROUND DOWN FULL DEPTH OF EXISTING ASPHALT FOR A WIDTH OF 18-INCHES FROM BOTH EDGES OF THE MICRO-TRENCH AND RESURFACED WITH CLASS B ASPHALT AND BINDER GRADE PER GREENBOOK.
    B. TACK COAT ALL EDGES WITH EITHER SS-1H EMULSIFIED ASPHALT OR PG 64-10 PAVING ASPHALT IMMEDIATELY BEFORE THE ADJOINING ASPHALT CONCRETE IS PLACED. "NO TRAK TAC".
    C. CAP COURSE SHALL BE GROUND 4" DEEP AT MIN 24" WIDE.
    D. WHERE ANGULAR CROSSING OR ANY LENGTH-WISE CUTS OF A BIKE LANE OCCUR BY MICROTRENCHING, THE CAPPING LIMITS SHALL EXTEND THE FULL WIDTH OF THE BIKE LANE. PERPENDICULAR CROSSINGS MAY RECEIVE TYPICAL CAPPING WIDTH PER NOTE 16.A. ABOVE. PAVEMENT MARKINGS SHALL BE RESTORED IN KIND. WHERE NO BIKE LANE MARKINGS EXIST, CONTRACTOR SHALL CONSULT WITH THE ENGINEER TO DETERMINE LOCATION OF ANY PLANNED BIKE LANES SO THAT IMPACT OF PAVEMENT SURFACE MAY BE AVOIDED.
    E. PAVEMENT SHALL BE LEVEL WITH ADJACENT ROADWAY ELEVATIONS AND SHALL PROVIDE A SMOOTH SURFACE PER GREENBOOK SECTION 302-5 AND SUBJECT TO ACCEPTANCE BY THE INSPECTOR.

VAULTS AND SERVICE CONNECTIONS

17. CONNECTION TO SERVICE LATERALS, JUNCTION BOXES, ETC., SHALL BE DONE SUCH THAT CURB AND GUTTER IS NOT DISTURBED, SETTLED OR DAMAGED. REMOVAL LIMITS OF SIDEWALK SHALL FOLLOW APPLICABLE STANDARDS AND REQUIREMENTS AS APPROVED BY THE ENGINEER.
18. THE USE OF HYDRO-JETTING IS NOT PERMITTED. TRENCHLESS METHODS SHALL NOT CREATE A VOID TWO TIMES GREATER THAN CONDUIT. VOID SHALL BE COMPACTED AND BACKFILLED WITH APPROVED CONTROLLED LOW-STRENGTH MATERIAL (CLSM).

IDENTIFICATION

19. SHALL INCLUDE TRACKER WIRE FROM VAULT TO VAULT.

APPROVED BY:

3/9/2021

NOEL A. CASTILLO
CITY ENGINEER

DATE

CITY OF MONTCLAIR

MICROTRENCHING DETAIL

NO SCALE	STD. DETAIL A	SHEET 2 of 2