

Arrow Highway Mixed-Use District

AHMUD

a specific plan

April 2025

01

04

INTRODUCTION

History, community profile, and planning process.

02

16

VISION

A shared future for preservation and growth.

03

38

POLICIES

Context & rationale of desired outcomes.

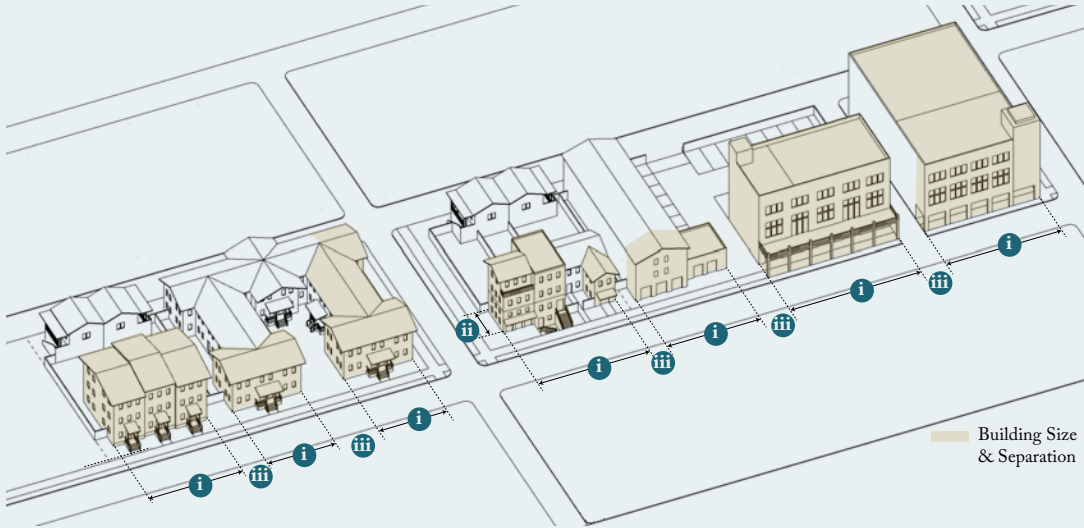


04

70

CODE

Development standards



05

154

IMPLEMENTATION

Partners, timeframes, funding sources.



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Mayor Pro Tem Bill Ruh
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Virginia “Ginger” Eaton
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Staff

Director of Economic Development Mikey D. Fuentes
Director of Community Development Michael Diaz
Economic Development Consultant Christine Caldwell
Senior Planner Silvia Gutiérrez
Director of Public Works/City Engineer Monica Heredia

Consultants

Rangwala Associates
in association with
Moule & Polyzoides
HR&A
Fehr & Peers
Rincon
Fuscoe
Alta Planning
ARG

... and thanks to numerous members of City Staff & thousands of Montclair residents.





**“Whatever you vividly imagine,
ardently desire, sincerely believe,
and enthusiastically act upon... must
inevitably come to pass!”**

Paul J. Meyer





1

Introduction

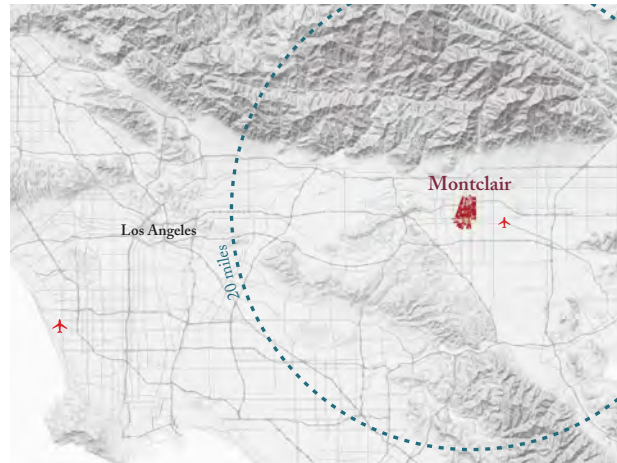


Figure 1.1: Montclair Regional Location.

The Arrow Highway Mixed-Use District (AHMUD) Specific Plan Area is composed of two distinct areas at the far northwest and northeast corners of Montclair. As its name indicates, the Study Area is located along the Arrow Highway corridor, and is developed with a variety of commercial and light industrial uses that generally date to the latter decades of the 20th century.

The AHMUD District is located on both sides of the boundaries of the North Montclair Downtown Specific Plan, which has some of the City's highest allowable densities and was developed to coordinate planning efforts and development around the City's transit hub.

The AHMUD East generally contains a varied mix of commercial and industrial uses, with the most notable exception being the Villa Montclair Mobile Home Park,

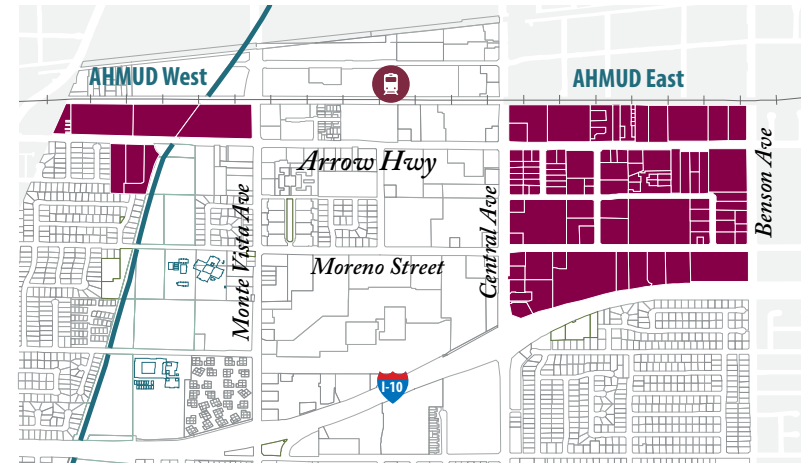


Figure 1.2: Arrow Highway Mixed-Use District.

located between Olive and Moreno Streets. In addition to the mobile home park, the AHMUD East retail uses are generally focused on the lots adjacent to Central Avenue, while industrial uses are located to the south of Arrow Highway.

The smaller AHMUD West is primarily composed of two business parks on Arrow Highway across from each other. The southern park contains mostly light industrial uses including auto repair shops, whereas the northern business park supports a wide variety of different commercial uses.

History

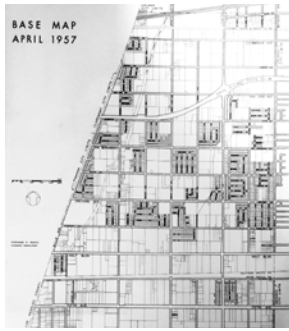
Through the mid-20th century, the Arrow Highway Mixed Use District (AHMUD) fell outside the boundaries of any established community: it occupied a swath of land located between Montclair (then known as Monte Vista) to the south, Claremont to the west, and Upland to the east. Consistent with patterns of development in peripheral parts of the Inland Empire region at this time, the area consisted of vast expanses of open land, peppered with orange groves and other small-scale agricultural uses, for the first half of the 20th century. The area was included within the city limits of Montclair



The 1948 aerial shows the abundance of orange groves throughout Montclair.

upon its incorporation in 1956.

Compared to other parts of Montclair, most of which had begun the transformation from farmland to suburb by the time of incorporation in 1956, the northern portion of Montclair remained sparsely developed because of its still-relatively-isolated location on the far outskirts of town. However,



The 1957 Base Map shows the street grid and housing tracts at that time.

this began to change with the construction of the San Bernardino Freeway/Interstate 10 in the 1950s. The stretch of Interstate 10 that passes through present-day Montclair opened in 1958.

Because the freeway passed through the northern reaches of Montclair, its completion also had the effect of opening up this area of the city, rendering its swaths of untouched

land rich fodder for new development. The presence of the freeway facilitated the construction of a large, regional shopping mall known as the Montclair Plaza (now known as Montclair Place), which instantly became a regional attraction and put this suburban community squarely on the regional radar. Montclair Plaza was designed by Ernest Hahn in a Mid-Century Modern style. The mall was extremely successful, drawing in shoppers from across the entire San Bernardino and Pomona valleys.

Buoyed by the presence of the mall and the clientele that it drew, other commercial shopping centers were developed on properties in the general vicinity of the plaza,

including several that are located to the east of Central Avenue and fall within the boundaries of AHMUD. In 1967, the cornerstone for the Mid-Century Modern designed Crocker-Citizens Bank was laid at the southeast corner of Central Avenue and Moreno Street. A fire station was constructed at the southeast corner



The Clock Tower and the Birds, sculpted by artist John Svenson, served as a primary gathering place for shoppers.

of Arrow Highway and Monte Vista Avenue, occupying the site of the former Montclair City Corporate Yard.

Despite the proximity to Montclair Plaza, most of the study area remained sparsely developed until later in the 20th century, no doubt a result of its location at the edges of town and its setting between the Interstate 10 freeway and a major rail corridor – areas seen as less than desirable because of their exposure to noise, pollution, and other environmental maladies. One of the earliest instances of development is a single mobile home park located on Moreno Street that dates to the early 1960s. The mobile home park was constructed on the periphery of town,



1950

1956 City incorporation

1958 Interstate 10 opens



The highway enhanced Montclair's connection to the region between downtown Los Angeles and the cities of the San Bernardino Valley, hastening the pace of suburban growth in the area and beyond.

in conformance to land use patterns and attitudes toward modular housing at the time which often relegated them to undesirable areas on the far outskirts of cities.

In the late 1970s, a Grand Prix Raceway was constructed to the east of the mall, near the corner of Moreno Street and Benson Avenue, adjacent to the freeway. The raceway was a popular local attraction, where customers could drive motorized go-carts on a purpose-built race circuit. It remained in operation until the 1990s. The remainder of AHMUD was incrementally developed with light industrial uses between the 1980s and early 2000s. By this time, there remained little developable land in



central Los Angeles, and the cost of acquiring and developing industrial sites in the urban core became increasingly expensive. Industries – especially those associated with warehousing and light manufacturing – turned their attention to the vast swaths of undeveloped land in communities in west San Bernardino County. By virtue of their access to I-10 freeway and other regional transportation corridors, these communities were within convenient reach of key distribution hubs in central Los Angeles and at the ports of San Pedro. The first big surge of

industrial development in the Inland Empire began in the 1980s, and has continued in waves ever since. Montclair rode this wave of industrial development.

By the 1980s, when the region was tapped by industrial developers, much of the city had been built out with suburban housing tracts and commercial centers. One of the last remaining areas of Montclair that had a sufficient amount of large, undeveloped parcels suited for industrial use was in the AHMUD. The same factors that had hindered development in this area in previous years – its peripheral location, and proximity to a major freeway and rail lines – were advantageous to non-residential use. Incrementally, the

undeveloped parcels in this area were improved with a variety of light industrial properties, business parks, and other commercial uses that today are generally concentrated around Arrow Highway.

In 2008, a new police station was constructed at the northwest corner of Arrow Highway and Monte Vista Avenue, near the west end of AHMUD. Designed by Randall Stout and WLC Architects, the building is notable for its unique design comprising sharp angles and jagged planes, which were intended to mimic the contour of the San Gabriel Mountains to the north.



Most of the buildings are functional in design and construction with a similar industrial utilitarian vocabulary: they are bulky, low-slung buildings with sizable footprints that are utilitarian in appearance, apart from a few flourishes on their street-facing façades. The auto-centric development pattern does not encourage transit use or walking and biking.

1960



1968 A 1.3 million square foot regional mall opens in the heart of the City, just off I-10 freeway.

The mall facilitated the construction of other commercial properties and associated types of development in the northern part of the City.

2000

2008 A new police station constructed.



Economy

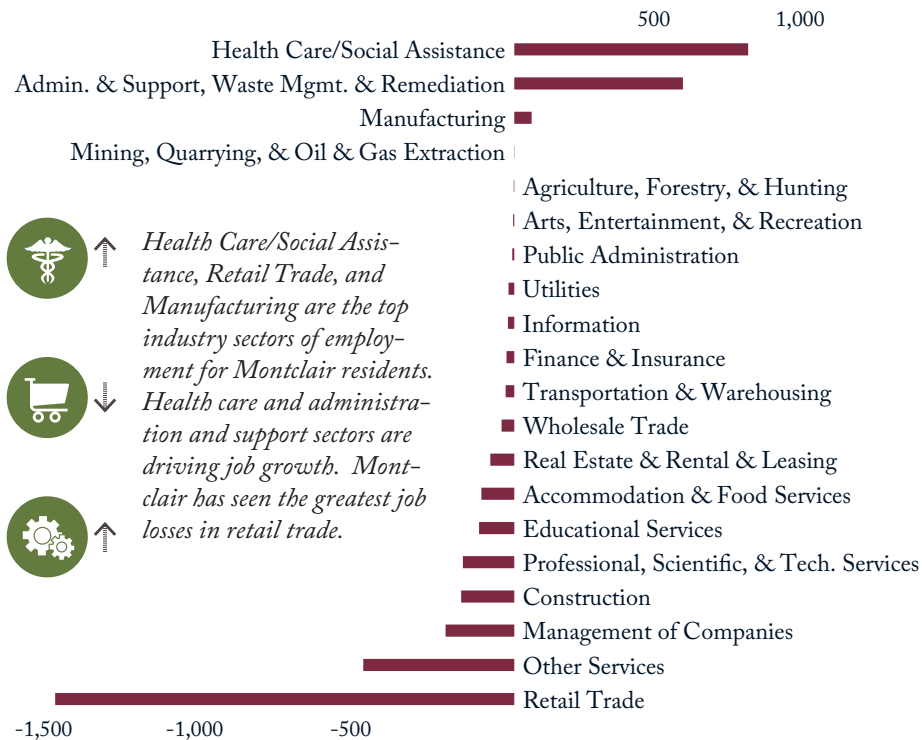


Figure 1.3: Montclair Job Growth Sectors. Source: LEHD.

Montclair has lost approximately 3,500 jobs since 2006 (for a net loss of approximately 1,700), with the greatest losses seen in Retail Trade, Other Services, and Management of Companies. Health Care and Administration and Support Services are driving job growth.

8 Arrow Highway Mixed-Use District **Specific Plan**

The AHMUD area is dominated by industrial and retail land uses, with one mobile home park. The AHMUD area contains 1.9 million square foot of rentable building area, 19percent of the total 10.7 million square foot of rentable building area in Montclair.

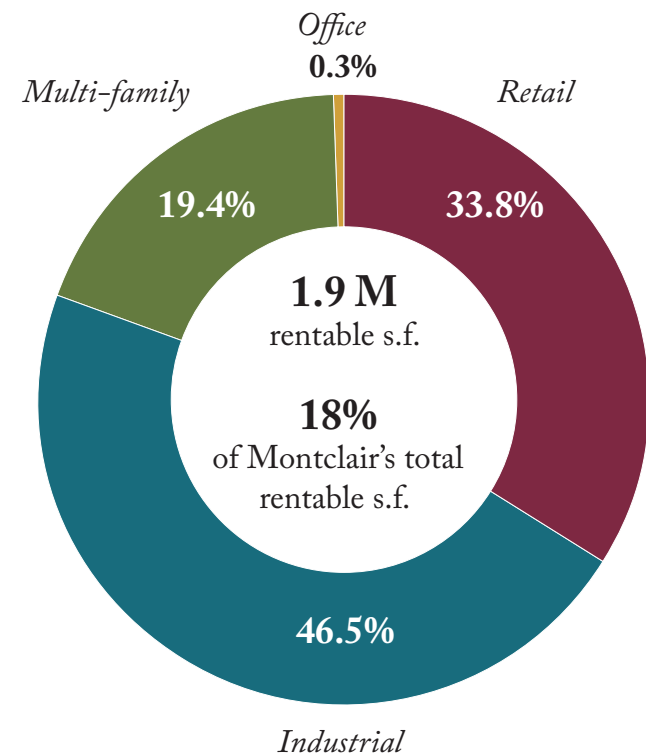
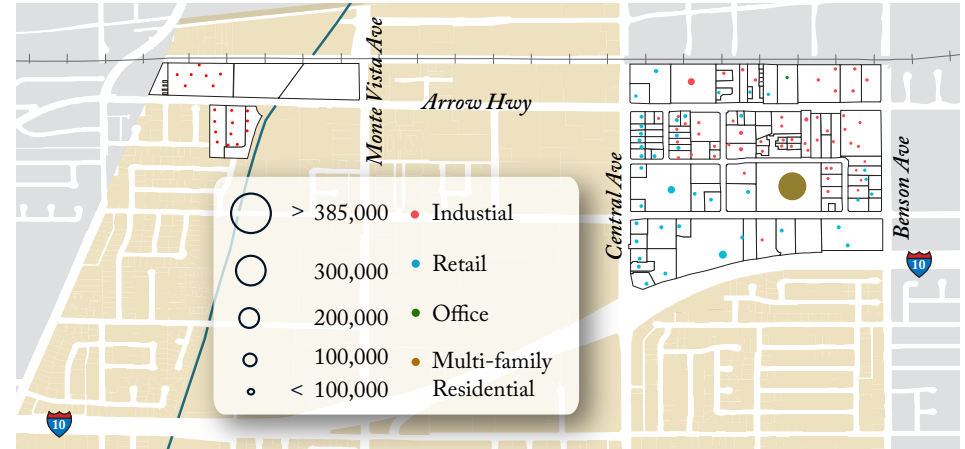


Figure 1.4: Land Use Inventory by square feet.. Source: CoStar, ESRI.

Arts & Culture

Diverse multicultural population, personal and active cultural activities throughout the year, creative industries and businesses that serve the region are all unique features of the Montclair arts and cultural milieu.



Montclair's transportation system includes streets (travel lanes, bicycle lanes, sidewalks, etc.), bus and passenger rail transit, and truck routes.

1. Roads

Within the AHMUD Specific Plan Area, Arrow Highway serves as the primary east/west arterial north of I-10 freeway. Arrow Highway is a five-lane street (two lanes in each direction with a center turn lane and no median). It provides direct access to the Montclair Transcenter and access to a mix of residential and commercial uses. Moreno Street also serves as an east/west connector in the eastern portion of the Specific Plan Area. Moreno Street is a two-lane road between west city limits and Monte Vista Avenue serving the residential areas to the west of San Antonio Creek, and then becomes a four-lane road serving commercial areas between Monte Vista Avenue and Benson Avenue. The corridor connects residential areas, schools, parks, and retail centers, and is within walking distance from Montclair Transcenter.

North/south access is provided by Central Avenue and Monte Vista Avenue. Currently, Central Avenue is a six-lane road between the northern city limits and Benito Street, and a four-lane road between Benito Street and southern city limits with twelve-foot travel lanes. The corridor serves mostly commercial areas north of I-10 but serves a mix of commercial and residential uses south of I-10 freeway. On-street parking and loading zones are provided on both sides of the street throughout the corridor. Central Avenue carries the highest average daily traffic in the City. Monte Vista Avenue is a five-lane street (two in each direction with a center turn lane and no median). In the Specific Plan Area the corridor serves mostly residential uses.

Goods Movement

The City of Montclair has adopted a Truck Route Ordinance, which divides routes into two categories: unrestricted streets and restricted streets. Unrestricted streets are open to vehicles of any size and weight. Restricted are open only for vehicles which do not exceed a gross weight of 10,000 pounds.

The following roadways within the Specific Plan Area are designated as unrestricted streets:

- Arrow Highway, from the westerly City Limits to Benson Avenue;
- Monte Vista Avenue, from Palo Verde Street to the northerly City Limit; and
- Central Avenue, from the northerly City Limits to the southerly City Limit.

Planned Roadway Improvements

The San Bernardino Countywide Transportation Plan (*SBCTA, 2015*) categorizes roadway improvements into a "Baseline" scenario and an "Aggressive" scenario. The Baseline scenario includes projects that can be funded with traditionally available local, Measure I, State, and Federal revenue sources through 2040. The Aggressive Scenario is a needs-based scenario that would require additional sources of revenue.

The following planned roadway improvements are outlined in the SBCTA Plan 2015 under the Aggressive Scenario:

- Widen Central Avenue from Montclair City limit to Chino City limit from 4 to 6 lanes; and

- Widen Monte Vista Avenue from San Bernardino Street to Arrow Highway from 4 to 6 lanes.

The following planned roadway improvements are currently under construction or in project development:

- I-10 Freeway and Monte Vista Avenue Interchange: Construct a new interchange for I-10 freeway at Monte Vista Avenue; and
- I-10 Freeway Corridor Project: Provides approximately 10 miles of express lanes along the I-10 freeway in each direction from the Los Angeles County line to the I-15 freeway.

The North Montclair Downtown Specific Plan (NMDSP) shares boundaries with this Specific Plan. The NMDSP outlines the following planned roadway improvements:

- Arrow Highway Streetscape: Between Central Avenue and Monte Vista Avenue, Arrow Highway will be reconstructed and landscaped. This will include the widening of sidewalks and a reduction in the width of vehicle travel lanes. The number of travel lanes will be maintained as two lanes in each direction. A 14-foot wide median will also be constructed.
- Fremont Avenue Streetscape: Fremont Avenue will be reconstructed and landscaped. Travel lanes will be reduced from two lanes in each direction to one. A 12-foot wide median will be constructed, and 6-foot wide Class II bike lanes will also be introduced on both sides of the roadway.
- Richton Street Streetscape: Richton Street will be



Proposed improvements to the Arrow Highway and Freemont Avenue intersection include new intermittent bulb-outs/tree planters with parallel parking between them and new curbless islands at crosswalks.

reconstructed and landscaped to widen the sidewalks and reduce the width of vehicle travel lanes. This will allow for the accommodation of on-street parking.

- **Neighborhood Streets:** Throughout the Plan Area, neighborhood streets created in the process of subdivision and individual development projects will be constructed and landscaped.

The current Montclair General Plan also recommends a grade separation across the Union Pacific tracks at Benson Avenue.

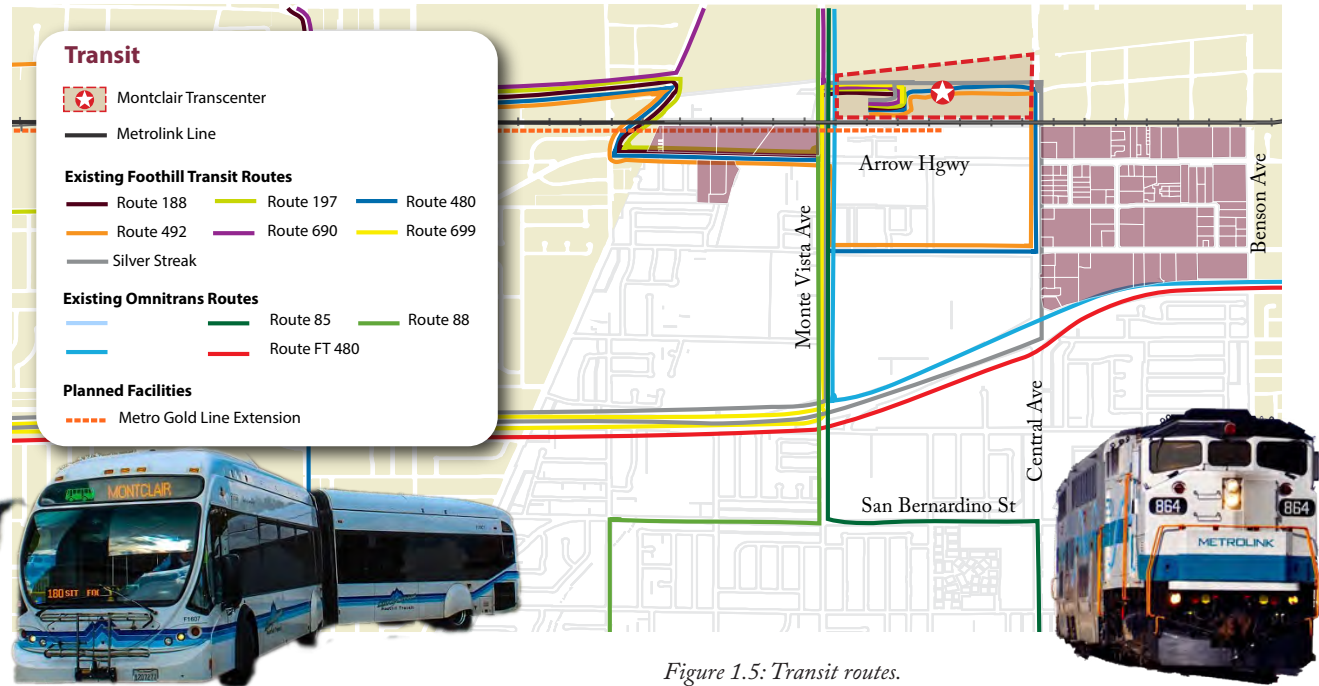


Figure 1.5: Transit routes.

2. Transit

Transit in Montclair includes bus service, ADA paratransit service, and Metrolink commuter rail. These services are described below:

- **Bus service** – Bus services are provided by Omnitrans, Foothill Transit, and the Riverside Transit Agency (RTA).
- **Paratransit Service** – Omnitrans and Foothill Transit operate Access Service, a shared-ride paratransit service for qualified applicants. Access Service is provided within ¾-mile of and during similar hours as fixed-route service. Demand/response transit services to senior citizens and the handicapped are provided by dial-a-ride and medi-van.
- **Metrolink Commuter Rail** – Metrolink is the regional commuter rail service that links Southern California. The City has one Metrolink station.

Average daily Metrolink ridership at Montclair Station is at least 8,000.

Many of these existing bus routes either terminate or stop at Montclair Place and Montclair Transcenter, where riders have access to the Metrolink service. Currently, there are a total of 12 routes connecting to either the Montclair Place or the Montclair Transcenter. Figure 1.6 shows the transit routes within Montclair.

The Montclair Transcenter is located just outside the Specific Plan Area at the corner of Richton Street and Monte Vista Avenue, and serves as an intermodal transit center for the City. The 20-acre transit center is the largest facility of its kind between Union Station in Los Angeles and the San Bernardino station, and conveniently connects the region's fixed route commuter rail, bus service, and rideshare programs in one centrally located area. The Montclair Transcenter is a regional

transportation hub, with a Metrolink station, a park-and-ride facility, and an on-site childcare center to serve the commuting public. It accommodates approximately 1,600 parked vehicles and includes a 1.6-acre site slated for future residential/mixed-use development.

Planned Transit Improvements

The following transit improvements are currently planned within AHMUD:

- Light Rail – Planned improvements to light rail include the Foothill L Line extension and improvements to the Montclair Transcenter.
- The Foothill L Line Extension Line from Glendora to Montclair. Completion of this project will shorten commute time from Montclair to downtown Pasadena to just over 40 minutes and further to Los Angeles will take approximately 75 minutes. The expected opening year for service to Montclair is 2028.
- The North Montclair Downtown Specific Plan also outlines changes to the L Line/Metrolink train platforms, bus platforms, and overall layout of the transcenter.



Village green at the train station. Image Courtesy of Torti Gallas + Partners.



Figure 1.6: Arrow Highway and Fremont Avenue Streetscape Improvements: Arrow Highway will be redesigned as a narrower four-lane thoroughfare, allowing for parallel parking along its curves and establishing space for a central median. By creating a rich landscape of canopy and palm trees, this regional arterial will transfer into a picturesque downtown passage. Fremont Avenue will be narrowed from a four-lane connector to a verdant two-lane street with wide sidewalks and flowering trees. Fremont Avenue is envisioned as a major pedestrian spine connecting the Montclair Place mall to the Transcenter. The street is narrowed to improve walkability, retaining on-street parking on both sides.

3. Pedestrian Facilities

Pedestrian facilities are primarily developed as part of the roadway and trail systems of the city and reflect the interconnected nature of circulation and transportation systems as a whole. Most of the major roadways through AHMUD provide continuous sidewalks on both sides of the road.

Planned Pedestrian Facilities

The North Montclair Downtown Specific Plan envisions a vibrant town center in North Montclair, oriented around residential and retail spaces. The Plan envisions Arrow Highway with two- to four-story mixed-use buildings facing a tree-lined parkway with a wide median and landscaping on the street edge. Fremont Avenue is envisioned as a slow speed, narrow, tree-lined street.

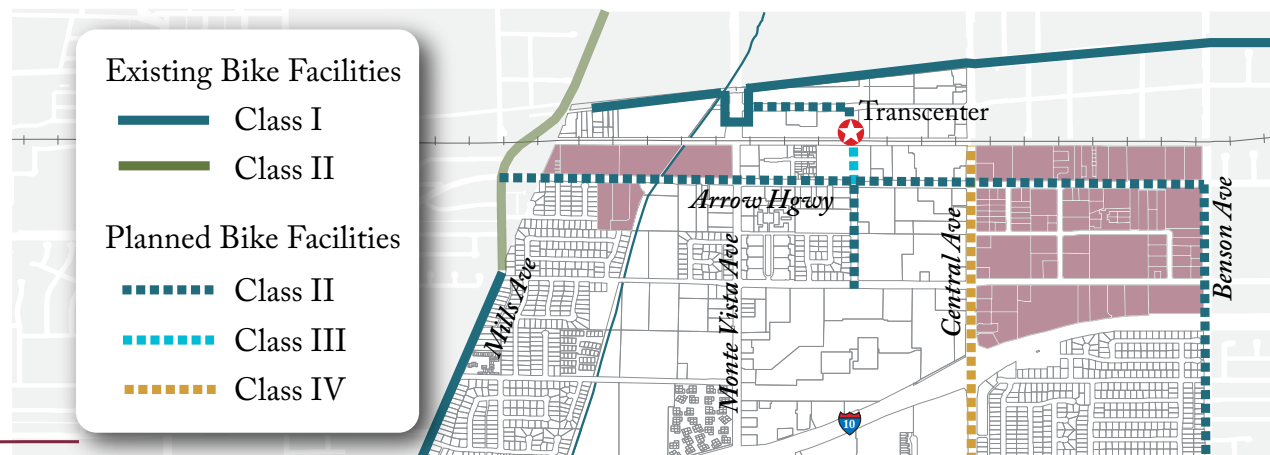


Figure 1.7: Bike Facilities.

4. Bicycle Facilities

Currently, there are limited bike facilities in AHMUD. The Pacific Electric Trail provides a Class I facility just beyond the northern boundary of the Plan Area and intersects Monte Vista Avenue. Connections to the trail are provided through sidewalks on both sides of Monte Vista Avenue. There is also access available through the Montclair Transcenter to the north of Richton Street. The City owns the portion of the trail that extends from the Los Angeles County Line to the City of Upland boundary.

There are many opportunities to improve the quantity and quality of bicycle facilities and the connectivity to key destinations (employment centers, residential areas, transit, and high use activity centers). An improvement in Montclair's bicycle network system could contribute to the overall bicycle mobility in the region.

Planned Bike Facilities:

- The North Montclair Downtown Specific Plan proposes the introduction of bike facilities on Arrow Highway (Class II).
- The SBCTA Points of Interest Pedestrian Plan proposes to install Class IV parking-protected bike lanes on Central Avenue, striped with green paint, and conflict zone striping near intersections; paint "T" perpendicular parking stall markings; and narrow all existing vehicle travel lanes to calm traffic.

The City recently prepared an Active Transportation Plan. This plan takes a detailed look at pedestrian and bicycle facilities in the City.

Bikeway facilities are categorized into four classes:

Class I: Bike or Multi-Use Paths provide a separate right-of-way and are designated for the exclusive use of bicycles and pedestrians with vehicle and pedestrian cross-flow minimized. The recommended pavement width for a two-directional shared use path is 10 feet.

Class II: Bike Lanes provide a restricted right-of-way and are designated for the use of bicycles with a striped lane on a street or highway. Bicycle lanes are generally 5 feet wide. Adjacent vehicle parking and vehicle/pedestrian cross-flow are permitted.

Class III: Bike Route or Signed Shared Roadways provide for a right-of-way designated by signs or shared lane pavement markings, or "sharrows," for shared use with pedestrians or motor vehicles

Class IV: Separated Bikeways provide a restricted right-of-way with separation and are designated for the use of bicycles with a raised barrier such as curbs or bollards. Separated bikeways are generally 5 feet wide with a 3 feet minimum horizontal and vertical separation area. Adjacent vehicle parking is permitted, and vehicle/pedestrian cross-flow is restricted to selected locations (e.g., driveways) indicated by breaks in the barrier and buffer.

Planning Process



Arrow Hwy Mixed-Use Specific Plan

The AHMUD Specific Plan was developed in conjunction with the General Plan Update. This allowed a wider dialogue about linkages and the role of AHMUD within the larger city framework. The planning process for AHMUD Specific Plan was designed around extensive and thoughtful public input.

The project team consisted of a core executive team, an advisory committee, and six focus groups. Focus Groups supported the public process of developing policy options and actions to implement the public's vision. Each Focus Group contributed members and expertise to the combined CORE Group which concurrently addressed the Well Planned and Accessible Community. The Core Focus Group considered AHMUD's approach to growth and preservation, incorporating perspectives from the other six groups.

The Project Team included key City staff members with responsibility to keep the project on schedule and within budget. The Advisory Committee included members from City Council, Planning Commission, and department heads that provided on-going direction throughout the project.

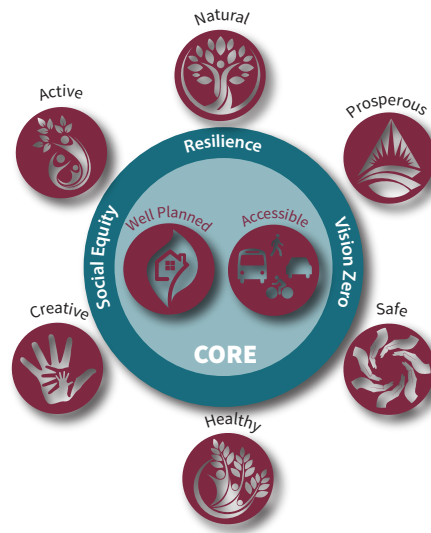
The community engagement approach was designed around five goals:

1. **Inform** — to provide the public with balanced and objective information to assist them in understanding the challenges, alternatives, opportunities, and/or solutions.
2. **Consult** — to obtain public feedback on analysis, alternatives and/or decisions.
3. **Involve** — to work directly with the public throughout the process to ensure that public concerns and aspirations are consistently understood and considered.
4. **Collaborate** — to partner with the public in each aspect of the decision including the development of alternatives and the identification of the preferred solution.
5. **Empower** — to place final decision-making in the hands of the public.

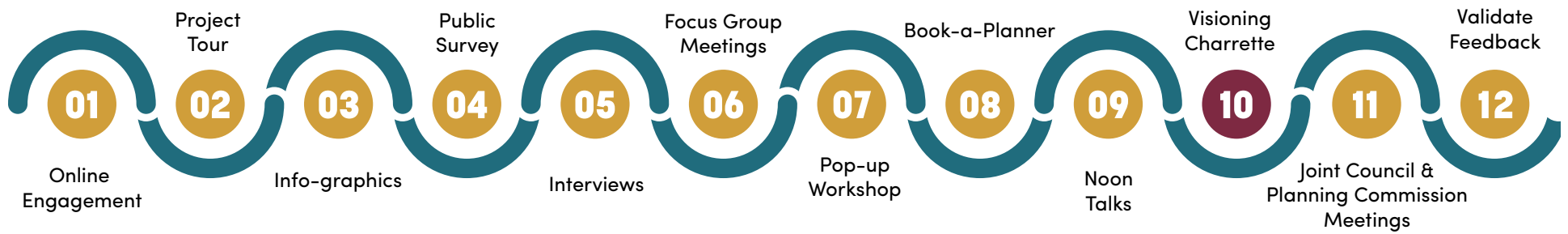
In conjunction with the General Plan Update, the extensive public engagement process combined new and trusted techniques to encourage a diverse group of citizens to contribute to the AHMUD Specific plan, including four-day visioning charrette, over 10 Focus Group meetings, a robust online presence to help guide the process. The Speaker Series during the charrette brought national experts to provide perspective, present

alternatives, and stimulate community dialogue.

Opportunities to participate included large public meetings to small stakeholder roundtables, surveys, project web page updates, social media engagement, email notifications, and Facebook posts. Each method encouraged the public to learn and convey their opinions on what was important for the city to consider over the next 20 years.



Civic Engagement Strategies





2

Vision

The original North Montclair Downtown Specific Plan (NMDSP) created a framework for creating a transit-oriented downtown for Montclair between the Montclair Place (mall) and Montclair Transcenter. The NMDSP was amended in 2017 to introduce transitional zoning for the “big box” properties; accommodate the extension of Metro L Line to Montclair; expand the Specific Plan area boundaries; and update the Illustrative Plan and Regulating Plan to reflect development that had occurred since 2006.

Also in 2020, the Montclair Place District Specific Plan was adopted to enable the transformation of the Montclair Place site into an urban,

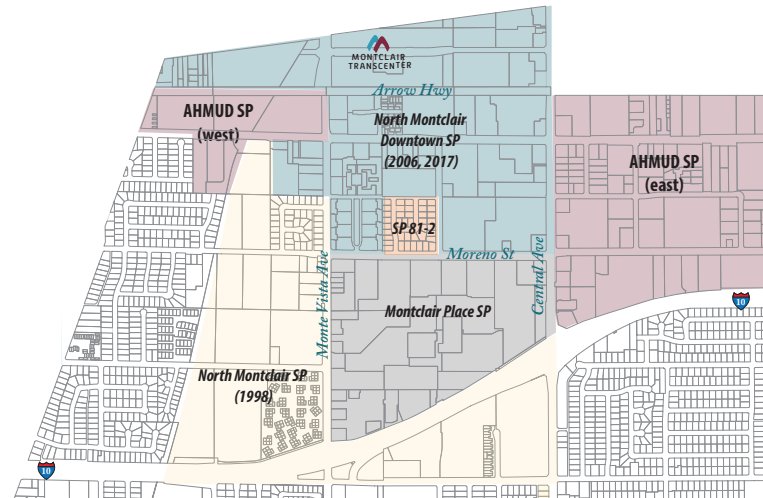


Figure 2.1: Specific Plan map

pedestrian-friendly, transit-oriented downtown core for Montclair. The Specific Plan provided a vision for infilling the parking lots with an interconnected street network, introducing multi-family and mixed-use development, and either keeping the Mall, replacing portions of the Mall, or completely removing the Mall.

This AHMUD Specific Plan builds on these previous versions, and focuses on two areas on either side of the downtown.

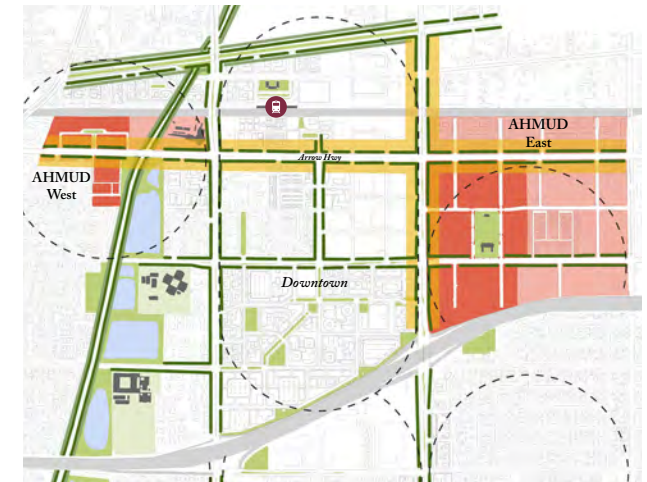


Figure 2.2: Key Concept map. The Plan conceives two great neighborhoods on either side of the new downtown area linked by a network of great streets and open spaces.

1. Key Concept

The plan envisions creating two great neighborhoods on either side of the downtown area and Montclair Transcenter. A reinvigorated public realm of streets and open spaces instills civic pride, brings people together to create a unique and vibrant place that will attract private investment to the area.

The plan weaves together existing and proposed streets into interconnected networks designed to encourage walking, reduce the number and length of automobile trips, and conserve energy. Existing streets such as Arrow Highway, Central Avenue, Moreno Street are reimagined as linear public spaces that serve mobility needs while creating safe, comfortable, interesting and walkable places that support commerce.

The Plan integrates parks, open spaces, greenways, and trails with land use and transportation, transit centers, corridors, and neighborhoods. The proposed Central Green on the east side, the San Antonio Channel Trail, green streets and a variety of smaller parks and open spaces are integrated as Green



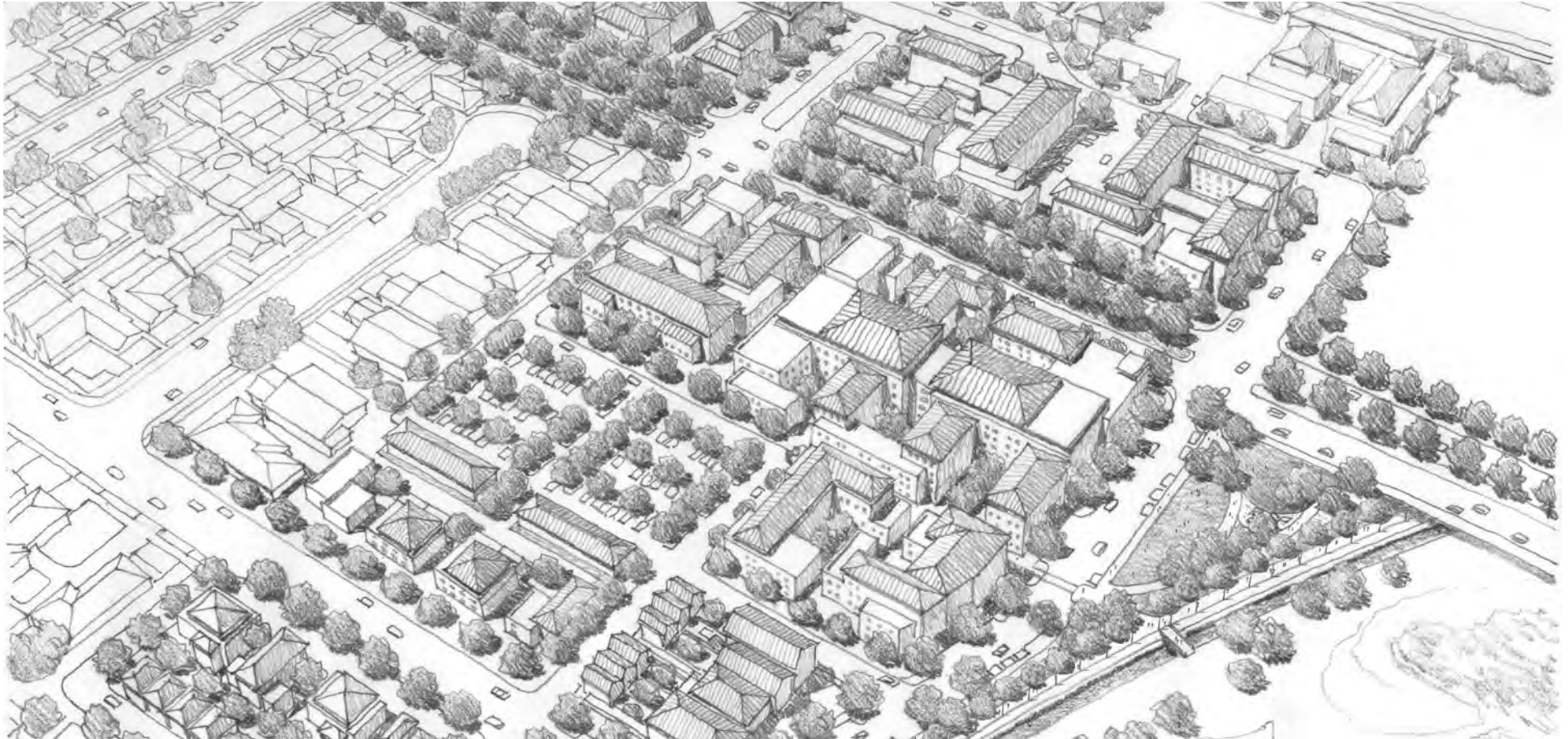


Infrastructure conceived around the central vision for AHMUD neighborhoods to be prosperous, healthy, equitable, and resilient.

The Plan envisions a transformation of the existing auto-centric area dotted with individual commercial and warehouse uses into a cohesive complete neighborhood that builds on existing industrial uses while featuring a traditional fine-grained mix of small-scale manufacturing, with supporting retail and office space, housing types, civic buildings, and recreational facilities. The project emphasizes a rich public realm, walkability, mixed uses, a sense of community, and diversity.

Limit and repurpose existing warehousing with local small-scale manufacturing businesses creates tangible goods that can be replicated and packaged—and often sold retail on the premises. Small-scale manufacturers tie into the “experiential retail.” Such businesses provide a living wage, because they tend to pay higher wages than retail or service industries. Local ownership is essential for long-term success.

Figure 2.3: AHMUD Illustrative map



2. AHMUD WEST

This plan intends to extend the social and economic vitality of a new vibrant pedestrian-friendly downtown to the west, linking it to the Creek. The Creek will be activated by lining it with new infill development that will also create positive faces to Arrow Highway.

The components of this Plan are as follows:

1. **Arrow Highway Enhancement** - Arrow Highway will retain four lanes of traffic, with parallel parking on both sides. However, its current wide configuration will be redesigned to include a median and more consistent street trees along the sidewalks, making it

an iconic east-west arterial north of the freeway.

2. **New Residential Development west of the Creek, south of Arrow Highway** – This development will be two-stories average in height, with portions of the development being three-stories tall, and some others being one-story, giving the entire development a non-homogeneous look. Positive frontages will line Arrow Highway and any street directly facing the San Antonio Creek. This development will only be allowed to have one- and two-story masses toward the southern and western edge to ensure it is compatible with the existing single-family fabric.

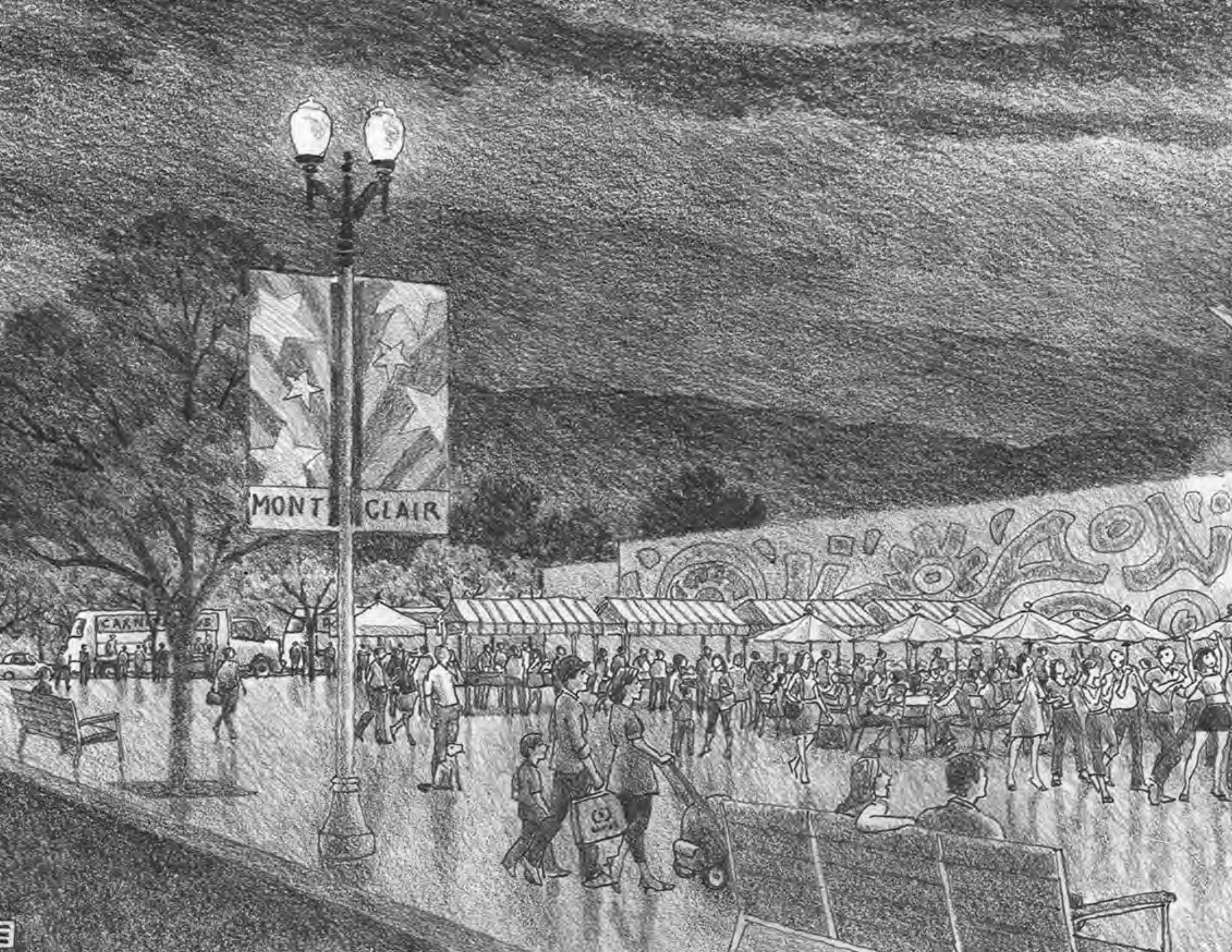
3. **New Residential Development west of the Creek, north of Arrow Highway** – This development will be two-stories average in height, with portions of the development being three-stories tall, and some others being one-story, giving the entire development a non-homogeneous look. It is encouraged that a small private open space be created as part of this development to create a communal amenity for the residents, who do not have direct access to the Creek. Positive frontages will line Arrow Highway.



3. AHMUD EAST

This plan extends the social and economic vitality of the new downtown to the east. It envisions the creation of a new mixed-use district through selective infill within the existing industrial area. The area will create a signature place that is different from the downtown, and also create a transition between the industrial area and the new downtown. The components of this Plan are as follows:

1. **Arrow Highway Enhancement** - Arrow Highway will retain four lanes of traffic, with parallel parking on both sides. However, its current wide configuration will be redesigned to include a median and more consistent street trees along the sidewalks, making it an iconic east-west arterial.
2. **A new public Park** – This Plan proposes a new park along Moreno Street just east of Central to create a new public open space amenity east of downtown. This park will be visible from Moreno Street and lined with positive frontages throughout.
3. **New Development north of Arrow Highway** – This development can be up to three-stories in height. Ground floors along Arrow Highway can be mixed use or residential, with positive frontages to the street. Buildings are also expected to create positive frontages to the degree possible to the train tracks.
4. **New Development facing Central Avenue** – This new mixed-use development would be a maximum of three-stories in height. Ground floors can be flex in use with upper levels given to residential and/or office uses.
5. **New Development south of Arrow Highway** – This development will consist of strategic infill on select parcels within the existing industrial area. Buildings can be a maximum of three-stories tall with flex uses on the ground floor.





4. Public Realm

The public realm consists of streets, lanes, plazas, parks and open spaces of different scales and purposes. These elements of public space are combined to create a distinctive and unique sense of place that invite use and activity. The quality of our public realm is at the heart of how we experience and relate to the surrounding environment. Therefore, it is important that the public realm offer safe, accessible, and enriching opportunity to celebrate everyday humanity and to engage in freedom of expression, public assembly, and critical civic discourse.

The private realm of buildings defines the public realm. The mix and location of land-uses in close proximity activate the public realm, so people can readily access the uses needed for daily life, and are invited to do so due to the quality of the urban setting.

The design of the public realm in AHMUD integrates ecology, mobility, infrastructure, and programming and activation strategies to enrich the lives of their users, and provide lasting value. The AHMUD is envisioned as an area that is clean and pleasant to be in, comfortable to walk through and that provides choices for pedestrians to sit, relax and people-watch where food, information and shopping is readily available. The key components of AHMUD public realm are as follows:

A. Parks and Open Spaces

1. Greenway: The San Antonio Channel is the major north-south stormwater channel that goes through AHMUD west. The proposed promenades, walkways and plantings will transform the creek into a public trail that is both communal and ecologically important. The trail would also link neighborhoods along the channel providing connections to multiple parks, schools, the Pacific Electric Trail, and the Montclair Transcenter. The trail is a key component to the green infrastructure that drains stormwater while promoting active living, healthy environment, and economic reinvestment in adjacent parcels.

2. West Park: The West Park along the San Antonio Creek serves as a destination along the trail and an amenity for the local residents and a respite for travelers

on the regional trails.

3. Central Park: The Central Park pulls together the concentration of economic, physical, and networking assets to create a signature public open space in east AHMUD. The park has formal and informal spaces, a sunken amphitheater with a stage, a multipurpose structure for hosting cultural events such as market, and arts and cultural fair.

4. Private and Public Open Space: New development will provide a range of integrated public and private open spaces that will allow five minute access to parks or open space to 100% of AHMUD residents.

5. Reimagining underutilized/vacant sites: The AHMUD area has many expansive parking lots that while providing space for cars, do very little else. This vision calls for the temporary use of some of the spaces in these lots for community events and gatherings.

Temporary events at an underutilized parking lot along Arrow Highway

1. Green Infrastructure

The green infrastructure network will connect AHMUD to parks, schools, and other amenities within the City. The framework and hierarchy of various types of corridors is a guide for the creation of this network.

Greenways: The San Antonio Creek Channel and the PE Trail are the Greenway corridors that connect multiple parks, schools, and destinations within Montclair and the region.

Parks and Open Space: The trees and vegetation in parks clean the air and water. They support active living and mental health by offering a variety of active and passive recreation options. During rainfall the percolation basins replenish the groundwater for future use -- these basins also serve as large open spaces within Montclair.

Gateway Green Corridors: These Corridors are used to maximize green opportunities along the city's two prominent corridors: Central Avenue and Holt Boulevard. They are characterized by planted medians, double rows of street trees where feasible, and more formal planting.

Neighborhood Connectors: Mission Boulevard, Arrow Highway, and Benito Street connect neighborhoods. Trees along the street and plantings in curb extensions narrow the visual field and encourage drivers to slow down. A variety of native trees and plants provide environmental benefits and visual interest.

Active + Cool Corridors: Street trees are used to maximize shade for comfortable walking and bicycle experience within neighborhoods. The Active + Cool Corridors include Ramona Avenue, Monte Vista Avenue, and Moreno Street.

Quiet Streets/Paseos: Shade strategies focus on increasing the appeal of pedestrian circulation within neighborhoods with a more traditional suburban landscape feel. These corridors facilitate circulation north/south and east/west on quieter streets.

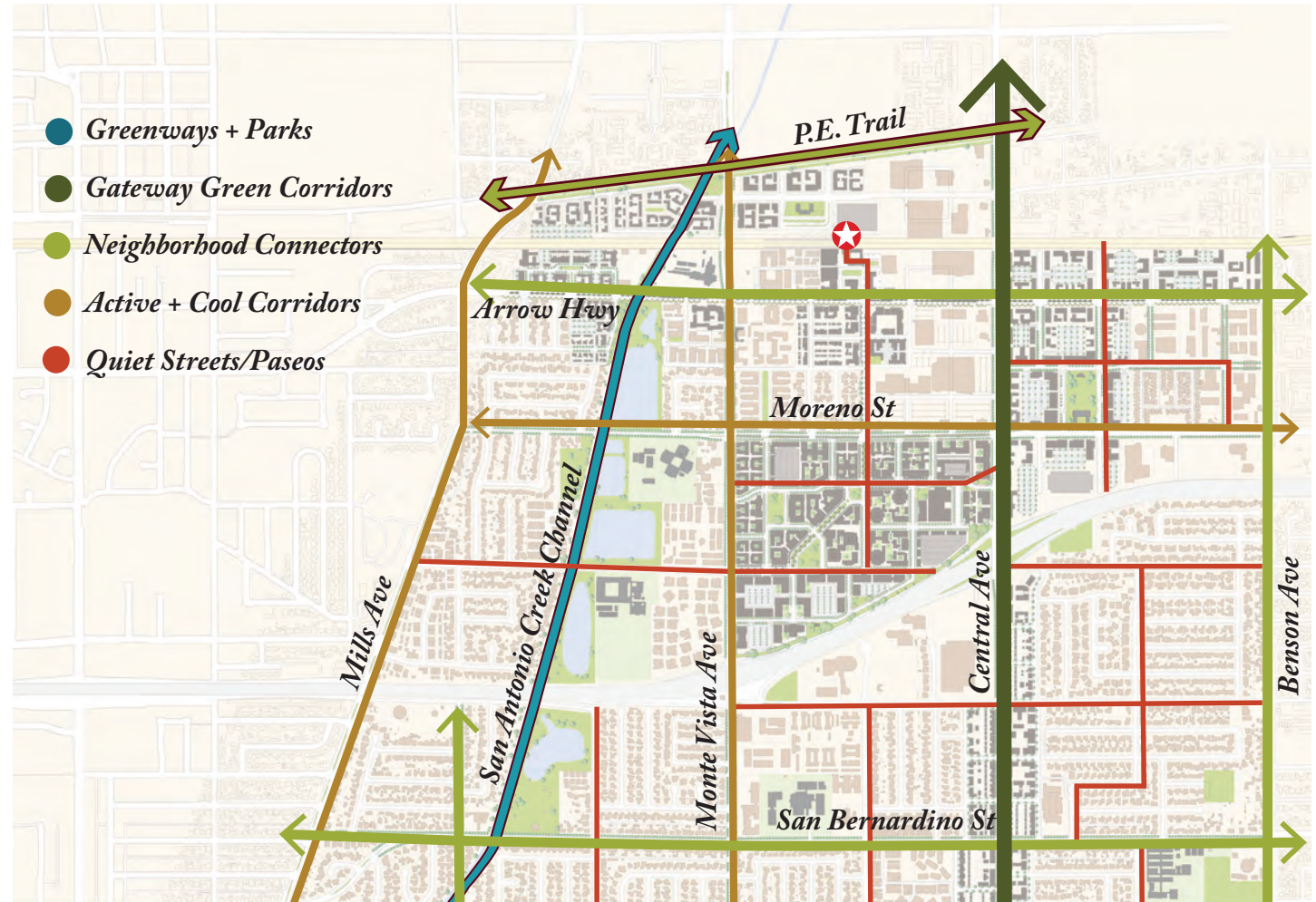
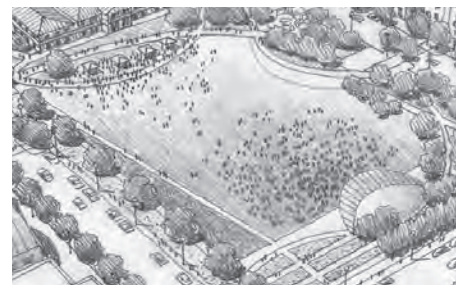


Figure 2.4: Green Infrastructure map.



Greenway+Parks



Central Park, AHMUD East



Arrow Highway, Neighborhood Connector

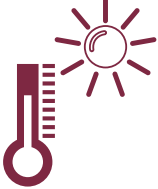















































	Landscape + Material Treatments			Program				
								
	Cooling Strategies	Stormwater Capture Plantings	Bold Accents	Active Program	Passive Program	Part-time Program	Public Art	Traffic Calming
Greenways +Parks								
Gateway Green Corridors								
Neighborhood Connectors								
Active+Cool Corridors								
Quiet Streets/Paseos								

Table 2.1: Greenway Framework, Source: Montclair General Plan



San Antonio Creek Channel, AHMUD West



Quiet Residential Street



Central Avenue, Gateway Green Corridor



Active Cool Corridor



2. West Park: The West Park overlooks the Santa Ana River Watershed percolation ponds and runs adjacent to the stormwater drainage channel. The edge of the channel is envisioned as a regional linear bike and jogging/walking trail which the park will serve as a destination esplanade along the trail and an amenity for the proposed multi-family housing. The park functions as an open space and play area for the local residents and a respite for travelers on the regional trails. The north part of the park is bermed to soften the traffic noise from Arrow Highway for the park users. The sloping “Green Panel” also offers a partial view of the park for the commuters along Arrow Highway. A large green and play area is centrally located for easy access to the housing developments. An esplanade slightly elevated over the channel offers strolling along the edge to the smaller southern park.

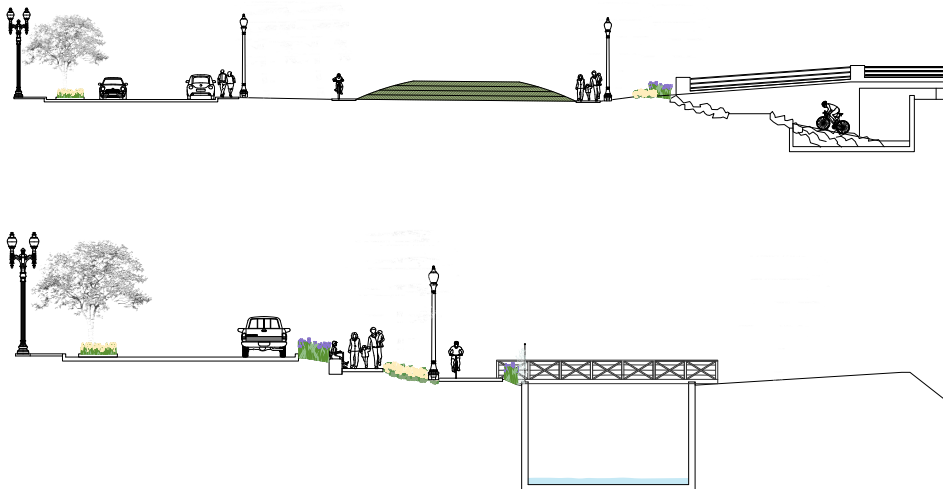


Figure 2.5: Sections through the West Park and San Antonio Creek Channel.

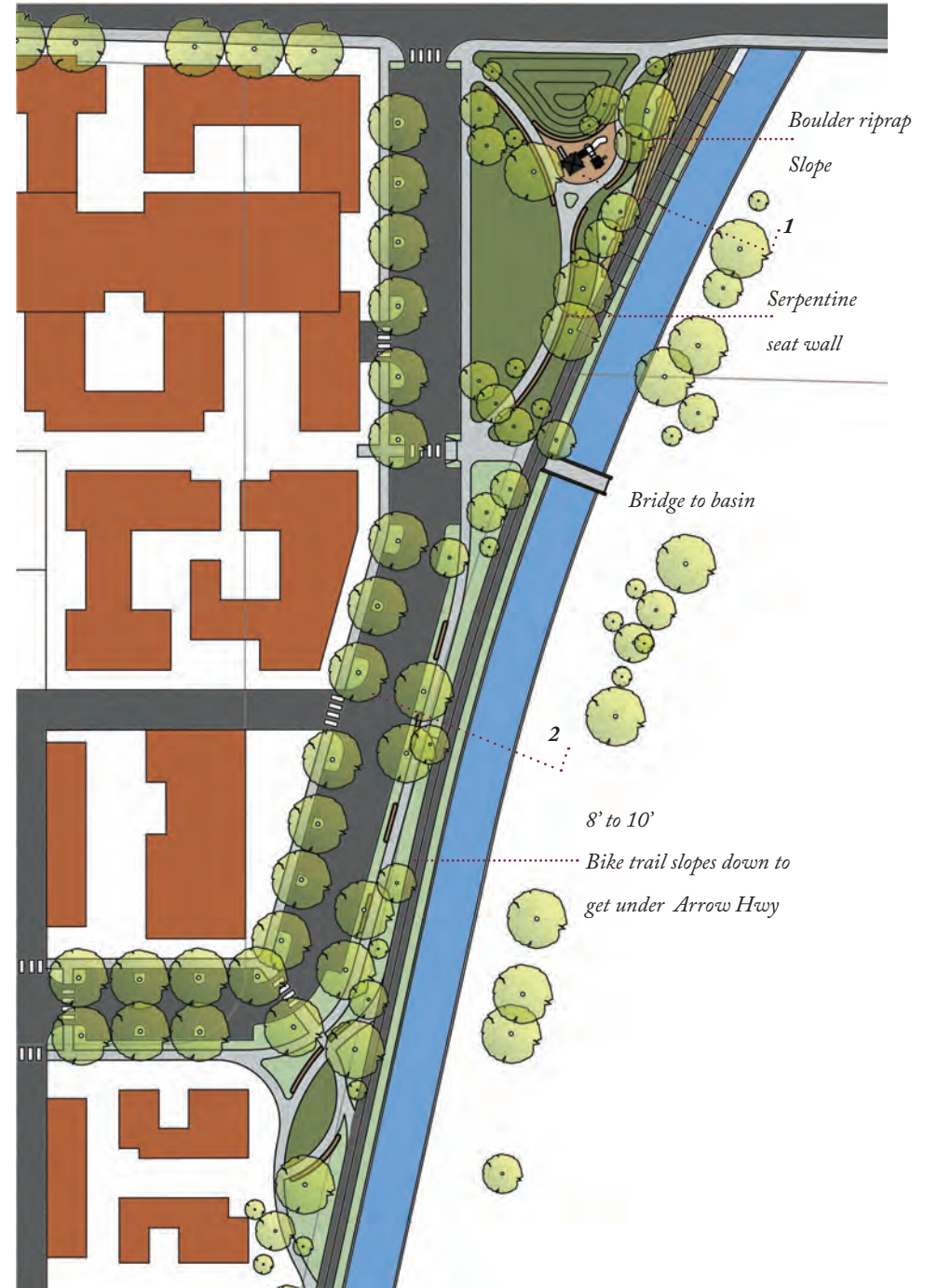
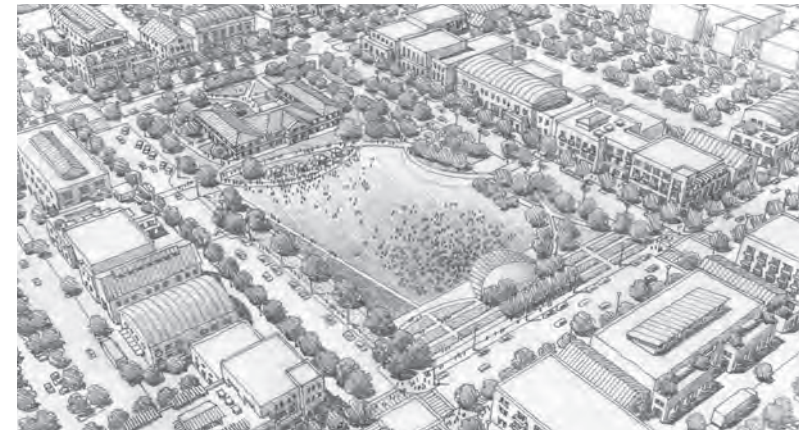


Figure 2.6: West Park Illustrative Plan.



3. Central Park: Central Park is located on the eastern side of the AHMUD. The park has shaded formal gardens along its northern border and a treelined informal garden along its western edge. Within the western garden are play areas, meandering jogging and walking paths and a grassy knoll for shaded viewing of the stage area.

The main park is gently sunken to accommodate an amphitheater seating for small concerts, movie nights and other outdoor events. The park's stage and orientation emphasizes the view of the San Gabriel Mountain range to the north.

The sunken portion also serves as a sponge park to filter storm water from the future urban development surrounding the green space.

The southern part of the park is terminated by public buildings with arcades for shade and a formal courtyard that faces the street incorporating a speakers platform for public speeches and events.

Wide pedestrian walks, diagonal parking and a drop-off area on Moreno Street makes for convenient use for the immediate and local citizens of Montclair.

Figure 2.7: Central Park Illustrative Plan.

5. Mobility

The AHMUD streets have been designed with the needs of drivers and automobile traffic put first. This traffic-centered conception of streets has led to the creation of dysfunctional places. The needs of people who want to use streets in other ways – such as for walking, shopping, cycling, using wheelchairs, or sitting and watching the world go by – have been given relatively little consideration. The social and economic value of streets as places of community interaction, as well as conduits for traffic are being reexamined in AHMUD Specific Plan.

Sidewalks tend to be narrow with limited or no shade, and the distance between pedestrian crossings are long, making walking through the plan area uncomfortable. There are limited bicycle facilities in AHMUD to provide true connectivity for bicyclists. Additionally, high speeds on roadways and narrow sidewalks provide limited on-street or off-street opportunities for cyclists. Bicyclists face the same barrier as pedestrians in long distances between crossings which make traversing Arrow Highway and Moreno Street inconvenient for cyclists.

There is minimal transit service to both AHMUD East and AHMUD West, with stops limited to the major north-south corridors of Central Avenue and Monte Vista Avenue. The AHMUD Specific Plan seeks to improve the quality of travel for employees, residents and visitors by providing a new vision for safe, accessible, and connected mobility in the area.

A. Urban Structure and Street Network

The pattern of blocks, streets, open space, and buildings, and landscape influences the choices people make to walk, cycle, take transit, or drive. A successful mobility framework:

1. Provides the maximum choice for how people will make their journeys;
2. Takes full account of the kinds of mobility a development will generate; and

3. Makes clear connections to existing routes and facilities.

A network of connected streets are not just better for walking, biking, and livability, but also offer far greater vehicle capacity. The existing street network in the Specific Plan Area has many gaps and opportunities for improvements. Figure 2.8 shows the proposed streets that offer the opportunity to complete the street grid network. Completing the street network results in smaller blocks with more street intersections and creates a more walkable and therefore health-promoting environment. People working and living in areas with higher street intersection density tend to drive less and walk and take transit more.

The greatest opportunity for public space exists in reimagining the streets and trails as linear public space corridors that connect a range of new smaller parks and open spaces that provide safe, comfortable, and inviting walking and biking experiences.

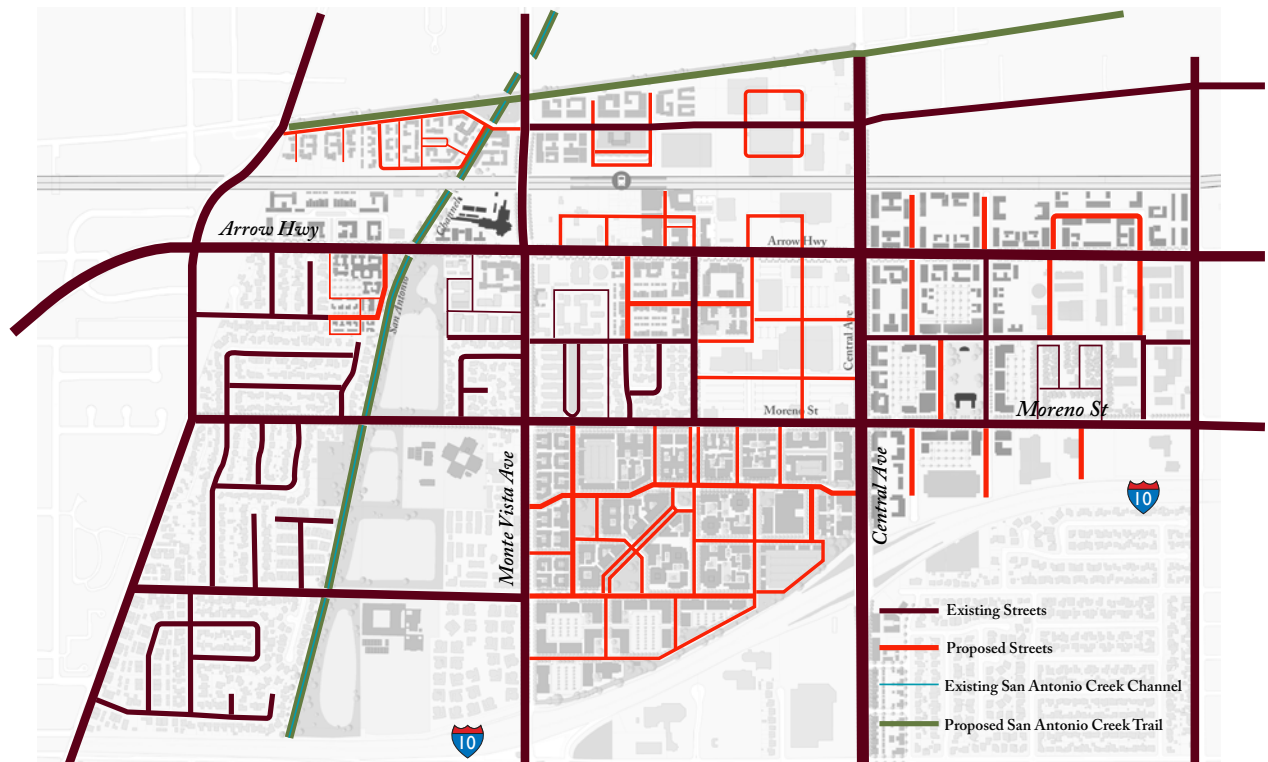


Figure 2.8: Street Network

B. Complete Streets

Instead of prioritizing one mode over another, complete streets provide travel options for all modes of transportation and users of all ages and all abilities; including pedestrians, bicyclists, motorists, and public transportation users. In Montclair, safety is a top concern and innovative complete streets design can help improve safety, access, and mobility for all.

In the Plan Area, modifications to several existing arterial and collector streets (Central Avenue, Moreno Street, and Central Avenue) will be made to make streets more

comfortable for all users. Designs for a series of new internal mixed-use streets will also be introduced within the Plan Area to connect various amenities provided in the area.

Arrow Highway, Moreno Street, and Central Avenue are envisioned to have reduced travel lanes and widened sidewalks to make streets more accessible for all users. In addition, the major streets in the Plan Area will be added to the citywide bicycle network by introducing bike facilities.



Figure 2.9: Arrow Highway, east of Central Avenue



Figure 2.10: Arrow Highway, west of Monte Vista Avenue

1) Arrow Highway: A major east-west five-lane street (two in each direction with a center turn lane and no median) that provides access to a variety of commercial and residential uses. Arrow Highway will be redesigned as a narrower four lane thoroughfare, allowing for parallel parking along its curves and establishing space for a central median. By creating a rich landscape of canopy and palm trees, the regional arterial will be transformed into a picturesque downtown passage. The roadway will include a Class III bike facility, or “sharrows”, to indicate that the street be shared by both motorists and cyclists. The proposed redesign of Arrow Highway Streetscape as a complete street with bike facilities, rich landscape and human scale design features will create a signature outdoor public place where vehicles and pedestrians can co-exist.

Central Avenue from I-10 Freeway to Moreno Street.



Central Avenue Moreno Street to Richton Street.

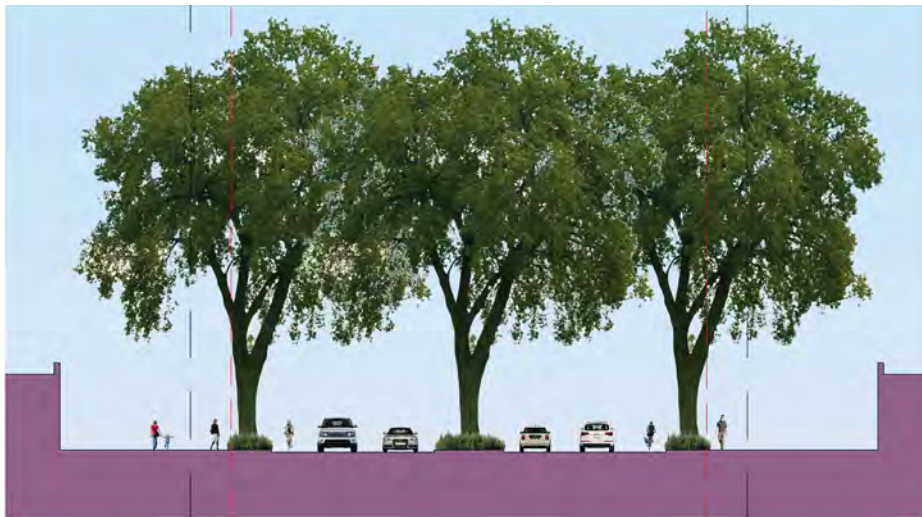


Figure 2.11: Central Avenue

2) Central Avenue: Currently, Central Avenue is a six to eight-lane north-south thoroughfare running through the entire length of the Montclair city limits, continuing into the neighboring communities to the north and the south. A significant point of interest along Central Avenue is its intersection with the I-10 freeway. Current vehicular travel lanes are a typical twelve-foot width. The corridor serves mostly commercial areas north of the I-10. Central Avenue carries the highest average daily traffic in the City. The full right of way varies from min. 115-foot ROW - max. 96-foot ROW, including typical sidewalks of seven-feet, and three travel lanes in each direction with planted medians and tree cover occurring sporadically throughout the corridor. This street also serves as a connector to the Pacific Electric Trail. There are four bus stops in this section of the street, one north of Moreno and one before and after Arrow. With the Montclair Place and other interventions, Central Avenue will become a vibrant north south artery.

Central Avenue from I-10 Freeway to Moreno Street: Our proposal sees Central Avenue south of Moreno as a main arterial north-south connector, but also as a vibrant pedestrian-friendly street. It will become a connector into the new Downtown and I-10 Freeway. The proposed street section envisions Central Avenue as a six-lane gateway green corridor with twenty-foot landscaped median. The typical cross section would accommodate three eleven-foot travel lanes in each direction. We propose to retain all the existing sidewalks and curb locations, but will add a seven-foot wide planted parkway that would create a buffer between the pedestrian and traffic on this major road, encouraging a comfortable walking experience. The twenty-foot wide planted median creates a park-like setting. The layout of sidewalks, parkways, and median creates a corridor that is inviting and accessible to all users.

Central Avenue from Moreno Street to Richton Street: This proposal still envisions Central Avenue as a main arterial north-south connector to the new downtown and the I-10 Freeway, but is significantly less focused on the car. Reducing Central from six lanes to four lanes with a twelve-foot landscaped median creates a vibrant pedestrian-friendly street while maintaining this gateway green corridor. The typical cross section would accommodate two eleven-foot travel lanes in each direction. The proposed eight-foot wide planted parkway would create a buffer between the pedestrian and traffic on this major road, encouraging a comfortable walking experience. A twelve-foot wide planted median is proposed between the vehicular travel lanes. South of Moreno Street, Central Avenue experiences high volumes of traffic; however, north of Moreno Street is less heavily trafficked and able to accommodate bicyclists. A six-foot class II bike lane runs throughout Central in each direction and connects to the proposed class IV bike lane on Moreno and the Pacific Electric Trail at the northern edge of Montclair. The introduction of the bicycle lane is a respond to the recent vision for a new transit-oriented Downtown for Montclair just east of Central between Moreno and the freeway. Only between Olive and Moreno does southbound Central have three travel lanes due to a high volume of cars heading towards the freeway.



Figure 2.12: Moreno Street

3) **Moreno Street:** A two-lane road between west city limits and Monte Vista Avenue serving the residential areas to the west of San Antonio Creek, and a four-lane road between Monte Vista Avenue and Benson Avenue serving the commercial areas on the east side. The proposed street section envisions Moreno Street as a two-lane active and cool corridor (see table 2.1) serving the retail and public facilities along the corridor. A typical cross-section would accommodate eleven-foot wide travel lanes with a 12-foot wide planted median. An 8-foot wide striped parking lanes will serve the adjacent businesses along the corridor and would promote lower travel speeds as a traffic calming strategy. The proposed 6-foot wide bike lanes with vertical buffers will make the bicycling experience more comfortable. Wide sidewalks are proposed along with seating spaces to cater to the needs of visitors and patrons alike.



Figure 2.13: Monte Vista Avenue



Figure 2.14: New Mixed-use Street



Figure 2.15: New street along West Park



Figure 2.16: New Residential Street

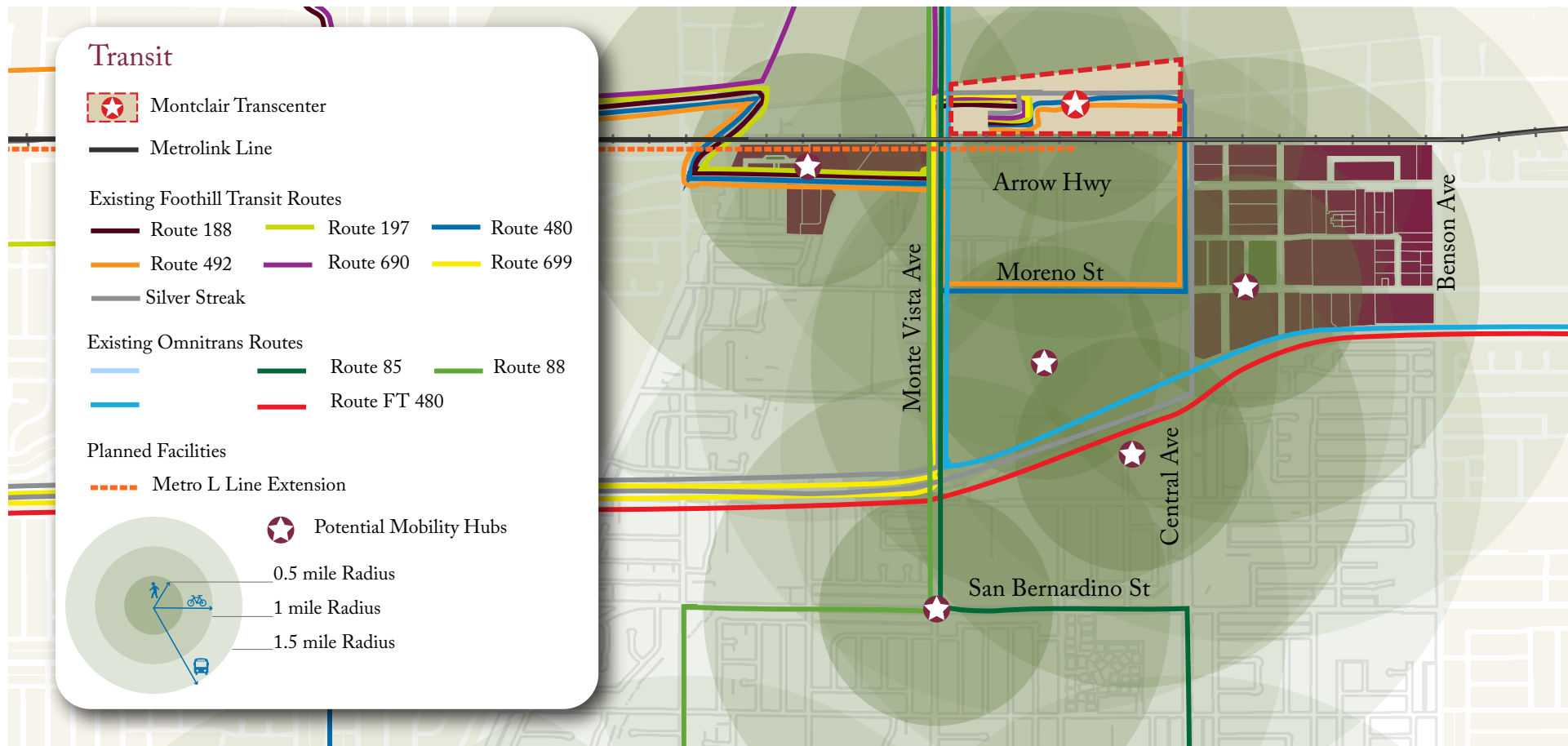


Figure 2.18: Transit Map

C. Transit

An individual may use different modes of transportation to complete a trip—they may walk, bike, take transit, or combine multiple modes. While bus and train services provide their services on fixed routes, most people rely on other modes to reach the transit service facility on their own. This is referred to First Mile/Last Mile, where a person must find transportation options to get to a transit station and from transit to their destination.

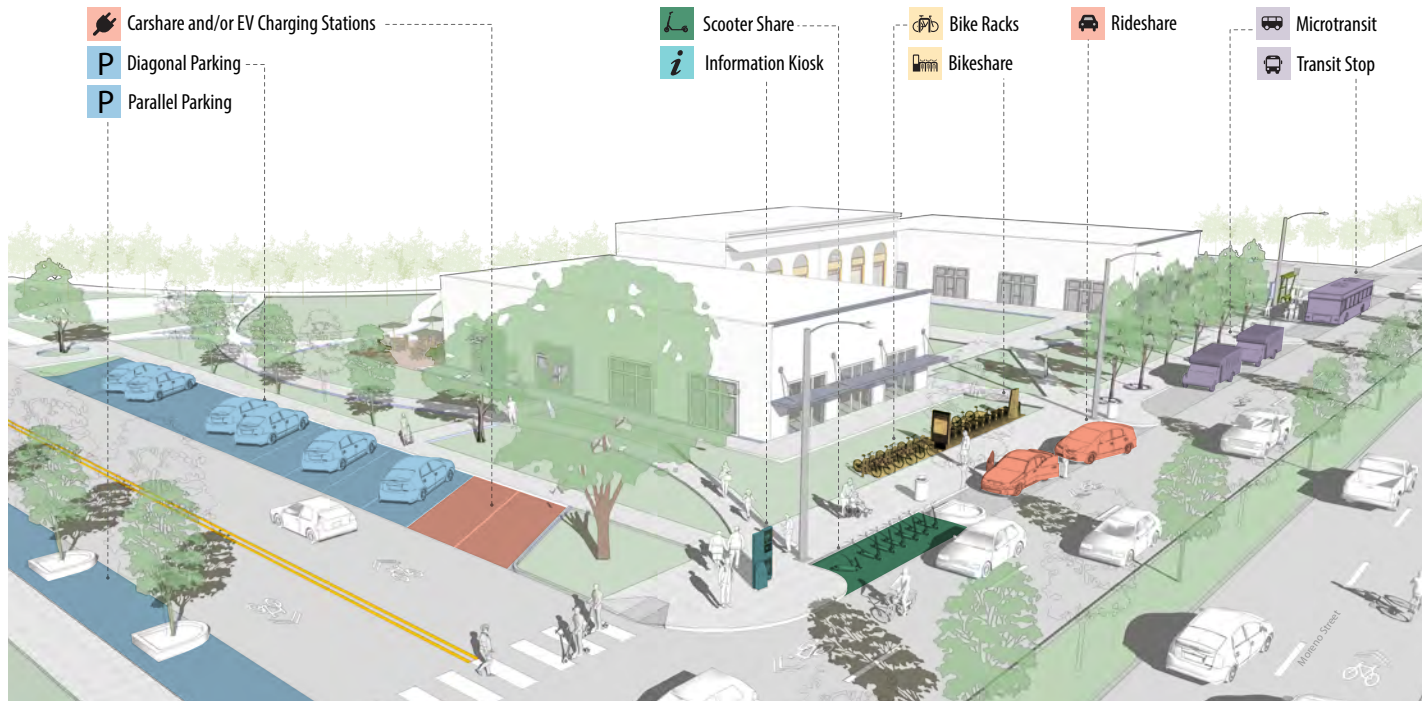
Mobility hubs are places where different modes of travel converge seamlessly. They are best located near high frequency transit and mixed-use developments with a concentration of employment, shopping, housing, and/

32 Arrow Highway Mixed-Use District **Specific Plan**

or recreation. They may include infrastructure for walking and biking, microtransit, park-and-ride, bikes on the bus, signage and wayfinding, etc. Mobility hubs could also include designated space for vehicle pick-up and drop-off, and could also provide specific zones for Transportation Network Companies (TNCs). In addition to the physical facilities, mobility hubs can also provide technologies and programs such as real-time traveler information, real-time ridesharing, and dynamic parking management strategies. Mobility hubs can range in size from a large transit center with stops for multiple transit stops, multi-modal parking, and other services, to a curb-side bus station with space for bike parking and ride-share pick-up.

In AHMUD, a mobility hub will be located at the Transit Center to serve both regional and local riders. As the hub will integrate with the commuter rail and future L Line extension at the Transcenter, riders traveling east and west will have increased transportation choices and decreased dependence on automobiles.

The City should consider opportunities to convert or relocate existing bus stations in the Plan Area to mini-mobility hubs which could provide connections from the Plan Area to the Transcenter.



Mobility Hubs

Mobility Hubs in AHMUD West on Arrow Highway and in AHMUD East at the new Central Park would provide mobility options to residents, employees, and visitors in the Plan Area and provide connectivity to transit. As shown in the conceptual layouts of the mobility hubs (figures 2.19 & 2.20), improvements could include providing bus shelters, informational signage, bike parking, and designated loading/unloading curb-space for TNCs.

Figure 2.19: AHMUD East at the new Central Park would provide mobility options to residents, employees, and visitors in the Plan Area and provide connectivity to transit.



Figure 2.20: AHMUD West intends to extend the social and economic vitality of a new vibrant pedestrian-friendly downtown to the west, providing a potential future connection to the planned San Antonio Creek Channel Trail.

D. Walking and Biking

Multimodal connectivity is an important component of mobility where traffic networks are accessible and interconnected among different modes. This includes automobile, pedestrians, bicycles, and public transit. A well-connected multimodal network allows people to travel from one place to another by providing multiple travel options in different modes. Key components of network connectivity are route completeness, network density, route directness, access to destinations, and network quality.

In AHMUD, the vision for an equilibrium in mobility options would be served by envisioning a transportation network with improved bike and pedestrian connectivity, while still providing automobile capacity on key corridors.

The complete streets and green infrastructure improvements planned in the Plan Area will provide new and enhanced pedestrian and bicycle facilities on Arrow Highway, Central Avenue, Moreno Street, and the new internal connector roadways. Existing sidewalks will be widened, and trees will provide shade for a more comfortable walking environment for users of all ages and abilities. New bicycle lanes on Central Avenue and Moreno Street will provide separation between vehicles and bicyclists will provide a more comfortable experience for drivers and cyclists.

A parklet is a sidewalk extension that provides more space and amenities for people using the street. Parklets and active frontages throughout AHMUD will invite residents, employees, and visitors to walk and bike.

The San Antonio Creek traverses Arrow Highway in AHMUD West and provides an opportunity to connect to a future trail network. The planned trail improvements along the Creek frontages will connect to the Pacific Electric Trail. This could provide an opportunity for another connection to the Transcenter. The location of the trail access would be near and also compliment the proposed Mobility hub on Arrow Highway.

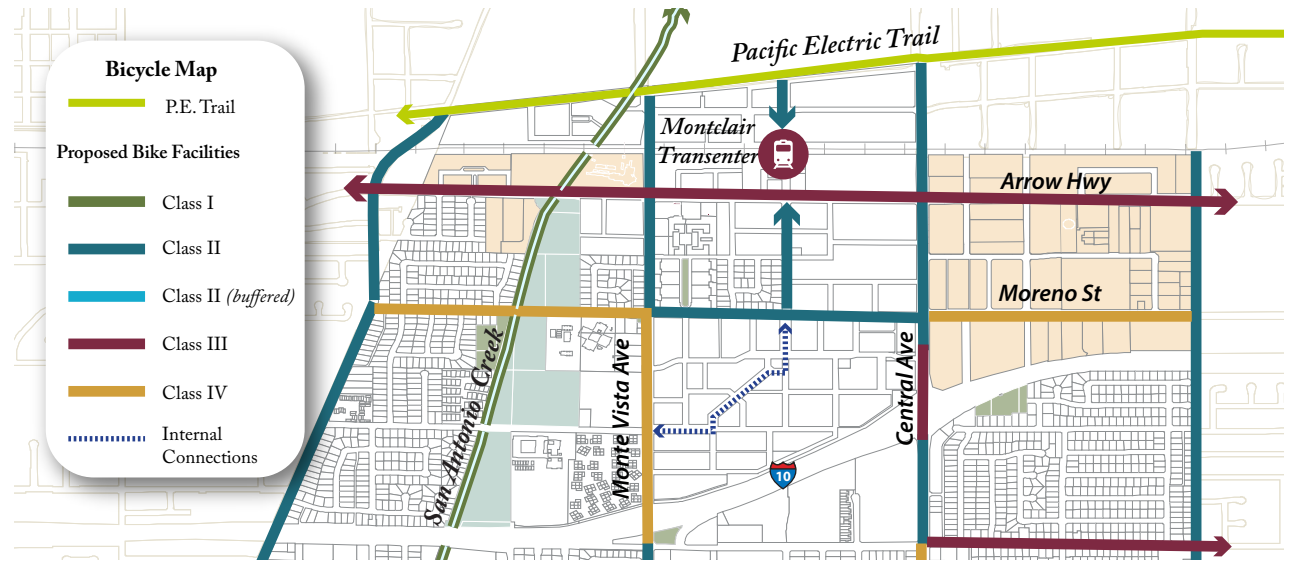


Figure 2.21: Proposed Bicycle Route Map



Enhanced pedestrian and bicycle facilities on Moreno Street

In AHMUD, Arrow Highway and Moreno Street vary in width, but have curb-to-curb widths of approximately 70-75 feet. Both corridors carry large volumes of vehicular traffic and provide east/west connectivity for all modes. The addition of bicycle facilities on Moreno Street and Arrow Highway would provide new connectivity for bikes. Currently, there are no crossing locations for bikes or pedestrians on Arrow Highway between Benson Avenue and Central Avenue in the AHMUD East, on

Arrow Highway between Monte Vista Avenue and the City boundary in the AHMUD West, and on Moreno Street between Vernon Avenue and Benson Avenue in the AHMUD East. Pedestrian and bike crossings on these corridors would improve connectivity for all modes.

Improvements could include striped and signed crosswalks, or enhanced signal activation for pedestrians and cyclist. Signals such as in-pavement flashers, a Pedestrian Hybrid Beacon (PHB), and Rectangular Rapid Flashing Beacon (RRFB) signals should be considered when creating new mid-block crossings. Also, activation buttons for cyclists can be placed at a level that a cyclist can push while on the bike. The City will determine the appropriate locations and characteristics (center medians, decorative pavement crosswalks, decorative painted crosswalks, flashing lights, signage, push button call signals) for any crossings.

E. Future Transportation Considerations

Mobility for everyone is being redefined. The following disruptive trends have changed mobility choices over the past five years and will change our mobility options into the future:

- **Transportation Network Companies (TNCs)**, also called a ride-hailing service, are companies like Uber and Lyft that provide on-demand rides for passengers with mobile apps or websites. TNCs tend to increase demand for curb space but can decrease the demand for parking.
- **Autonomous vehicles (AVs)** are vehicles that are capable of driving with limited or no human involvement. There are six levels of autonomy that range from issuing warnings and momentary interventions with the human driver to a fully automated machine which requires no human involvement to operate.
- **Car Sharing Services** allow consumers access to a vehicle without owning a personal car. Car share services typically charge a monthly or yearly membership fee and an hourly rate for access to its shared vehicle fleet.
- **Bike Sharing Services** operate like car sharing services in that consumers can rent from a shared bicycle fleet. Bike sharing services typically do not charge a monthly membership fee and can be either docked (at set stations where one picks-up and drops-off a bike) or dockless (bikes are picked-up wherever the last user dropped-off).

Curb Space Productivity (Passengers per space-hour)

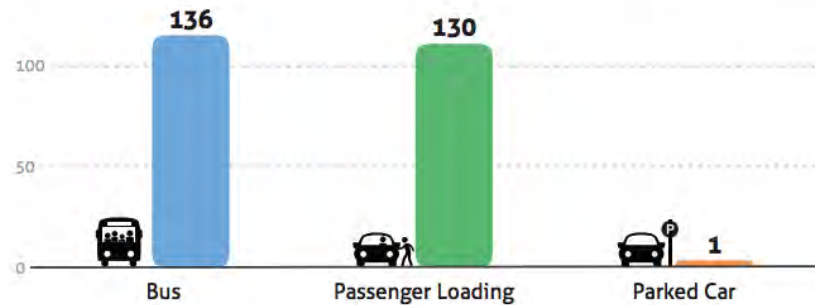
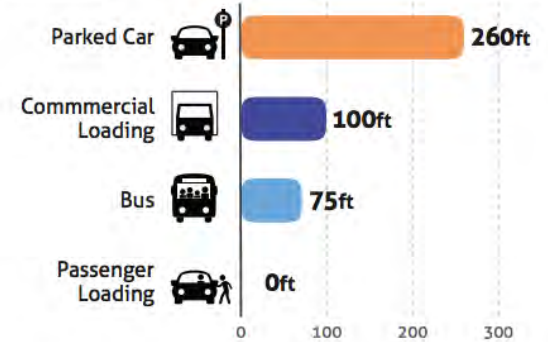


Figure 2.22: Curb Space Productivity and Allocation, The less productive uses claim larger portion of the curbside real estate, and as a result, other users of the road, from commercial delivery to passenger loading, can spill over into traffic and bike lanes, causing delays and unsafe vehicle interactions.

Curb Allocation



Relocate curb spaces to better utilize the curb zone.



Convert curb space to different uses to better utilize the curb zone.



Convert curb spaces to *flexible time of day zones* to meet demand-based uses throughout the day.

Figure 2.23: Curbside Management. With the increasing concern for balancing the needs for all roadway users, and the growth of TNCs like Uber and Lyft, as well as online shopping and associated deliveries, demand for curbside pickups, drop-offs and dwell times is growing dramatically. Curbside Management seeks to inventory, optimize, allocate, and manage curbspaces to maximize mobility and access for the wide variety of curb demands.

- **Electric Scooters and Bikes** are powered by an electric motor to propel riders along streets and up hills. E-bikes can travel up to 20 mph and e-scooters have a top speed of 15 mph. In the United States, e-bikes and e-scooters sharing services are typically dockless and have expanded rapidly since the first launch of e-bike service in 2017, and e-scooter service in 2018.
- **Microtransit** is defined as a privately-operated transit system, which in many cases mirrors the operations of public transit agencies along select routes. Microtransit operators can be highly flexible, tailoring their operations to match short-term or long-term changes in travel behavior.

Studies show that on average, bus and transit trips less than five miles have decreased in recent years more than rail and transit trips greater than five miles. Transit

agencies can be proactive and morph into a mobility agency focused on mobility options instead of providing traditional transit services (that operate at high costs with limited ridership).

In AHMUD, planning for future transportation trends can be incorporated within planned mobility hubs. Design of mobility hubs should consider the inclusion of pick-up/drop-off zones for TNCs, car-share spaces, microtransit, bike share, and e-scooters, as appropriate.

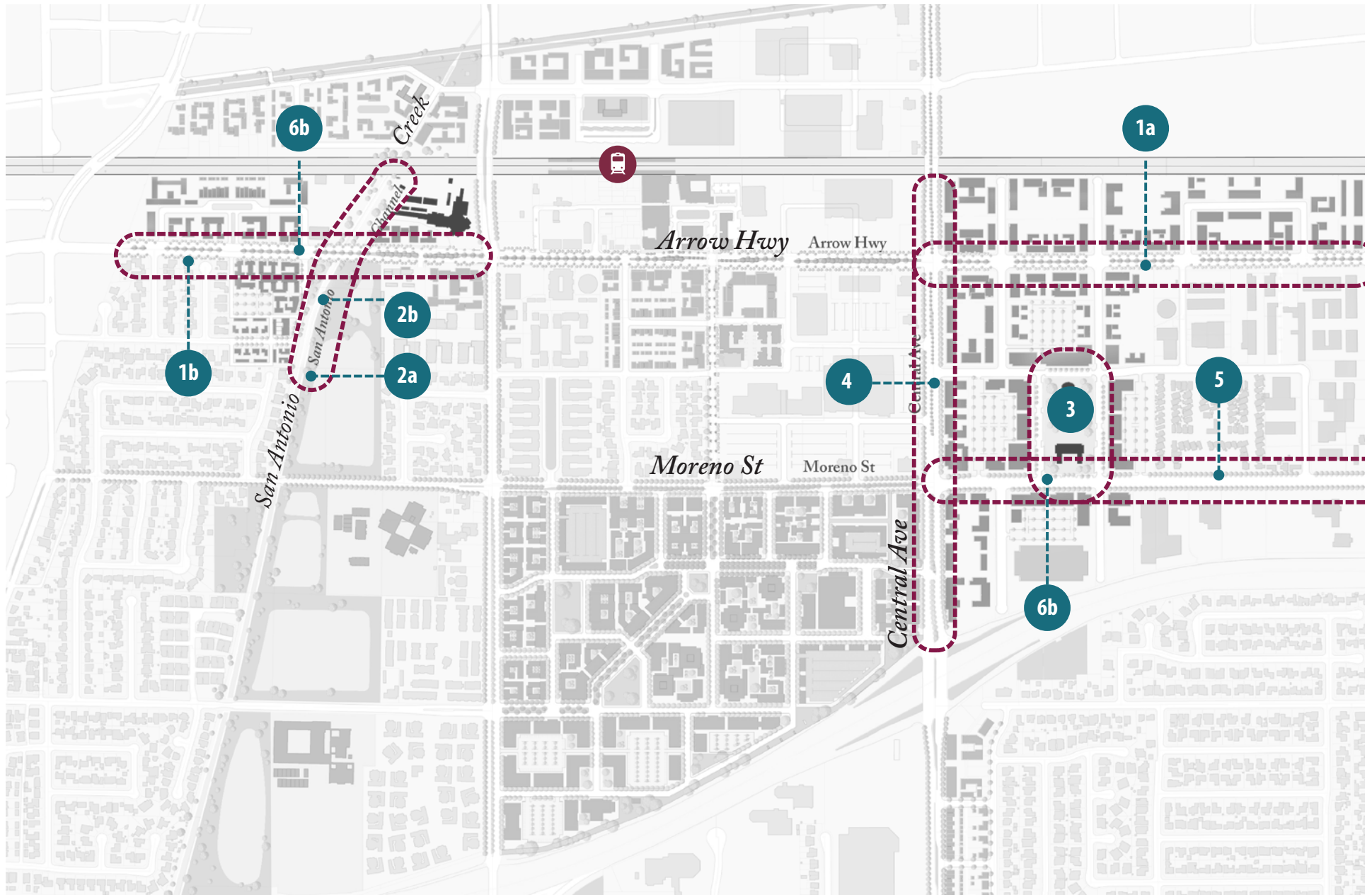


Figure 2.24: Phased Infrastructure Improvements.

6. Phasing of Public Infrastructure Improvements

The preferred approach is to prioritize the funding and development of public realm improvements in areas where private development is proposed. Public funding for public realm improvements could be combined with areawide/private sector funding as might be provided through an assessment district in the area (e.g.: Enhanced Infrastructure Financing District, Business Improvement District, Community Finance District, or Community Benefit District). The public realm projects are prioritized based on their ability to kick-start or facilitate orderly implementation of the overall vision.

1A

1B

Arrow Highway Streetscape Improvements: The City is currently pursuing a streetscape project for Arrow Highway between Monte Vista Avenue and Central Avenue. These improvements should be extended within the two AHMUD areas.

2A

San Antonio Creek Improvements: Coordinate with redevelopment of the adjacent parcels.

2B

West Park: Coordinate with redevelopment of adjacent parcel and San Antonio Creek Improvements.

3

Central Park: The Central Park is a significant opportunity for place-making, economic development, enhancing the environment, improving health, and promoting arts and culture for AHMUD and the City. The Central Park is proposed on parcels currently under private ownership. A combination of development incentives, transfer of development rights, and Park Development Impact (per Quimby Act) fees may possibly be used to acquire and develop the Central Park.

4

Central Avenue Streetscape: Proposed improvements will benefit AHMUD and Montclair Place and facilitate walkable urban development. These improvements should be coordinated with redevelopment of parcels along Central Avenue.

5

Moreno Street Streetscape: The improvements can be coordinated with redevelopment of adjacent parcels.

6A

6B

Mobility Hubs at West Park and Central Park: Creation of Mobility Hubs will be a joint effort between the regional and local transit agencies, ride-share providers, the City, and the private sector. Mobility Hubs should be coordinated with the opening of the San Antonio Creek to people and private redevelopment on the west side. On the east side, the Central Park and private redevelopment would generate demand for a Mobility Hub.



Policies

The AHMUD Specific Plan follows the General Plan organization which features the following eight elements:

01. Our Natural Community

Addresses how AHMUD can thrive in balance with the natural ecosystems.

02. Our Prosperous Community

Addresses how AHMUD can attract and retain high-wage and high value enterprises, and diversify and increase the local tax base.

03. Our Well Planned Community

Directs new growth by reinvesting in key opportunity sites in AHMUD, making great places by insisting on the highest standard in architecture, landscaping, and urban design.

04. Our Accessible Community

Addresses transportation choices by strengthening and balancing pedestrian, bike, and transit connections.

05. Our Healthy Community

Improve physical and mental health and social well being.

06. Our Safe Community

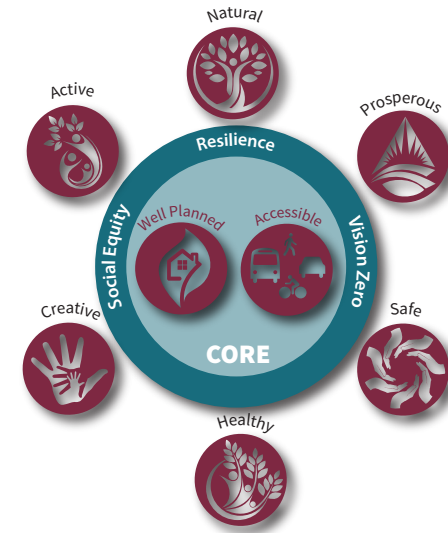
Enhance personal and community safety and security in AHMUD.

07. Our Active Community

Addresses parks and open spaces to provide enriching recreational options for the entire community.

08. Our Creative Community

Weave arts, cultural events, community programs into everyday life.



Three lens filters address specific interest that applies to all eight elements:

1. **Resilience:** Help Montclair better serve residents today and in the long term, while preparing for any future potential adversity.
2. **Social Equity:** Integrates elders, youth, and minority ethnic groups by improving access to jobs, affordable housing, parks, and active inclusion in government decisions.
3. **Vision Zero** Eliminate all traffic fatalities while increasing safe, healthy, and equitable mobility for all.

The eight plan elements features and actions.

Goals — Goals are long-range, broad and comprehensive targets. Goals are not necessarily measurable or achievable; rather, they describe a desired end-state condition for Montclair.

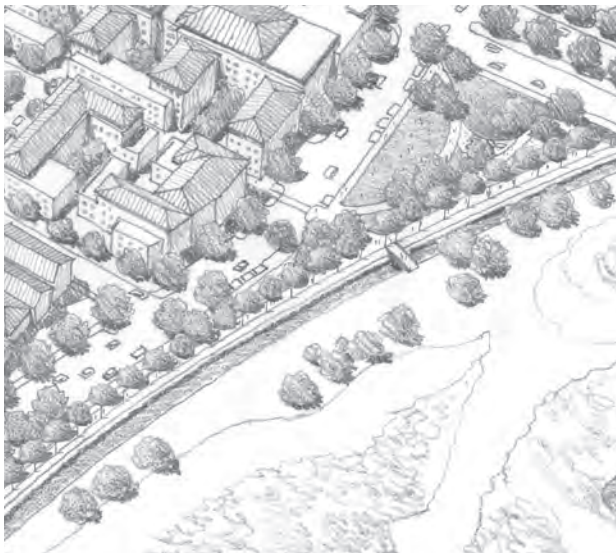
Policies — Policies describe context and rationale of desired outcomes. Policies are focused and specific.

Actions — Actions are specific proactive steps to achieve the goals. They are the critical link between long-range planning and current-decision making.

01 Our Natural Community

A. Green Infrastructure

The General Plan calls for a network of green spaces that protects and enhances the natural ecosystems (e.g., wildlife) and provides associated benefits for people and the economy. Green Infrastructure harnesses the power of nature to provide critical services such as protection against floods or excessive heat, help improve air and water quality, which underpin human and environmental health.



In the AHMUD the Green Infrastructure includes:

- **Public Open Space:** This includes publicly accessible destinations such as the San Antonio Creek, other parks, and parklets. The desire to retain and build on an authentic sense can be reinforced by public art, contextual architecture or materials, commemorative features and signage which express and convey the distinctive personality of AHMUD and the local character of the landscape and heritage.

Integrate the creek, streets, and proposed open spaces into an integrated green infrastructure that improves air quality, reduces storm-water runoff, recharges the aquifer, and increases the tree canopy.

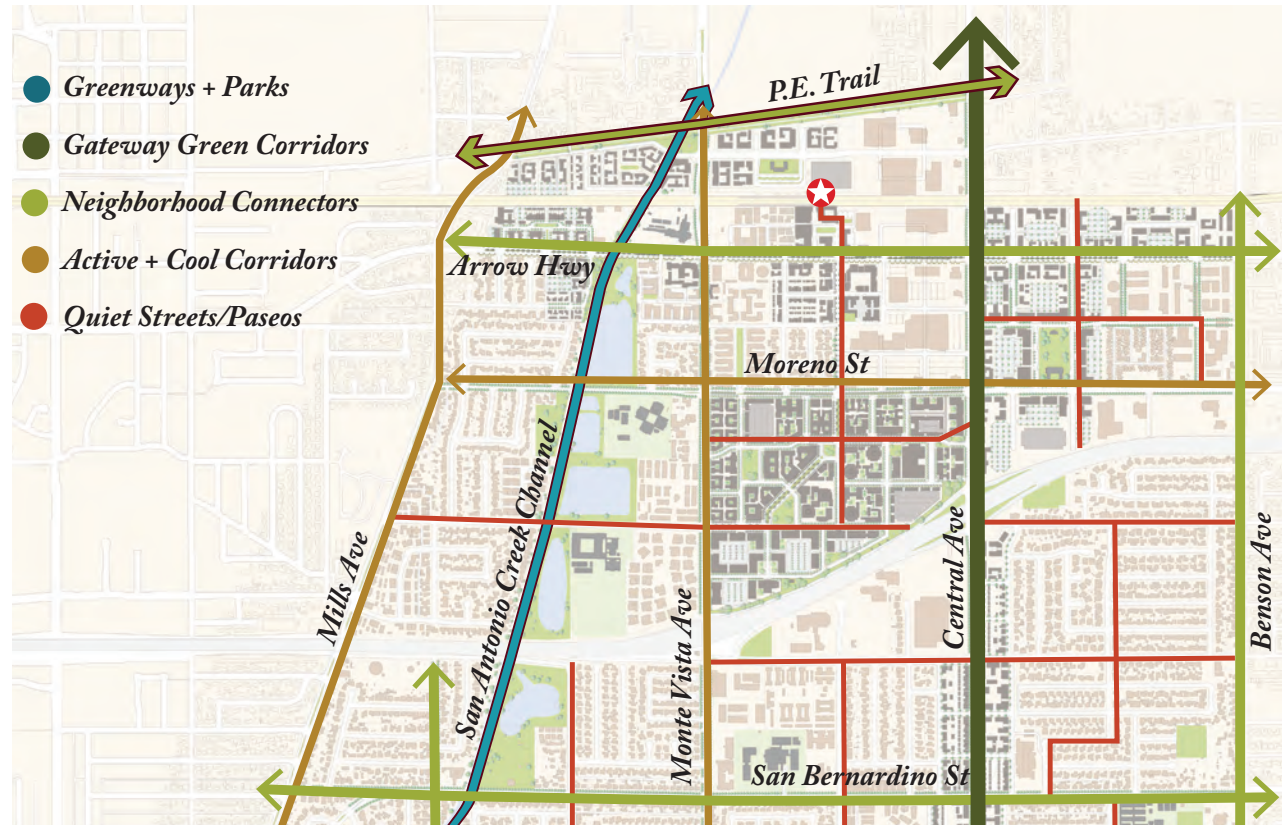
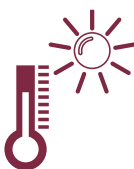

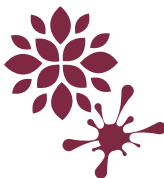

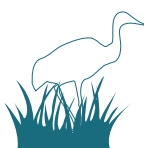













































Figure 3.1: Green Infrastructure Plan



- **Streets:** These are the corridors that link the public open spaces. They are streets such as the enhanced Arrow Highway and Central Avenue, with network of bikeways, and enhanced network of alleys. The goals for connectivity include strengthening pedestrian and multi-modal access, wayfinding, and greenway linkages in a networked system throughout the AHMUD landscape. These corridors also incorporate infiltration, biofiltration, and/or storage to collect, retain, or detain stormwater runoff. Street trees and other plantings provide cooling, shade, wind protection, attractive streetscapes, and other environmental benefits.

	Landscape + Material Treatments			Program				
								
	Cooling Strategies	Stormwater Capture Plantings	Bold Accents	Active Program	Passive Program	Part-time Program	Public Art	Traffic Calming
Greenways+Parks								
Gateway Green Corridors								
Neighborhood Connectors								
Active+Cool Corridors								
Quiet Streets/Paseos								

Examples

<ul style="list-style-type: none"> • Shade Trees • Cool Paving • Shade Structures 	<ul style="list-style-type: none"> • Planted Bulb-out • Rain Garden • Biofiltration Strips 	<ul style="list-style-type: none"> • Flowering Trees + Shrubs • Special Paving Treatments • Decorative Crosswalks • Plazas 	<ul style="list-style-type: none"> • Playgrounds • Outdoor Fitness Equipment • Parklets • Community Garden • Dog Park 	<ul style="list-style-type: none"> • Great Lawn 	<ul style="list-style-type: none"> • Farmer's Market • Play Street • Open Street Event • Outdoor Movie 	<ul style="list-style-type: none"> • Murals • Sculptures • Gateway Signage 	<ul style="list-style-type: none"> • Mid-block Crossings • Removable Planters • Raised Crossings • Bulb-outs
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Table 3.1: Green Infrastructure Framework

B. Air



The two main pollutants of concern are: ozone and suspended particulate matter. In Montclair, reducing air pollution is contingent on reducing the number of vehicles miles traveled (VMT) in the city.

Promoting walkable and mixed-use development served by robust transit systems; and promoting complete street policies that encourage walking and use of public transit, reduces traffic congestion, and improves air quality.

Green Infrastructure in the form of trees and plants absorb certain pollutants from the air through leaf uptake and contact removal. Widely planted street trees and plants also cool the air and slow the temperature-dependent reaction that forms ground-level ozone pollution.

C. Water



By weaving natural processes into the built environment, green infrastructure provides both stormwater treatment and flood mitigation benefits.

Green Streets: Green Streets incorporate infiltration, biofiltration, and/or storage to collect, retain, or detain stormwater runoff as well as a design element that creates attractive streetscapes. New and reconstructed streets and roadway projects and Capital Improvement Projects conducted within public streets which add or disturb at least 10,000 square feet of impervious surface should consider Green Streets best practices.

Low Impact Development (LID): LID is a strategy for improving the quality of runoff by requiring that development projects direct runoff to treatment systems consisting of vegetation and soil. LID best practices should be considered for projects that are required to incorporate stormwater mitigation measures.

Stormwater System Improvement Recommendations

There are a number of rainwater interventions that could alleviate pressures on the storm drain system, enhance the livability, and replenish the aquifer:

Tree Pockets can be installed between parking spaces in the parking lanes on streets. Simply sawcut the tree pocket, scoop out a hole, and fill it with soil and a tree. Leave the existing curb and gutter in place, and leave openings so the gutter rainwater can flow into and fill up the tree pocket, and then flow out at the lower end.

Tree Zippers can be installed in parking lanes by sawcutting the entire strip of parking, scooping it out and replacing with soils, trees, and pervious pavers. Rainwater enters the pervious pavers from the street surface and gutter flow and is absorbed by the trees and soils.

Tree pockets and zippers help to define the outdoor room of the street, help to shade and cool the paving and parked cars and sidewalks, and absorb rainwater for tree evapo-transpiration while allowing for infiltration.

Green Street is a slight modification of an existing street curb and adjacent planting area. A portion of curb can either be replaced with a depressed top-of-curb section, or the curb face can be simply core-drilled. Both modifications allow rainwater to pass to the adjacent planting area, which has been slightly excavated and re-landscaped to accept the rainwater run-off. This area can be planted with indigenous, drought-tolerant landscaping and a permeable surface to allow the rainwater to re-enter the aquifer.

In-Street Detention Storage can be used to detain rainwater runoff and release it slowly after the peak flows to reduce flooding downstream. Some infiltration can also be accomplished. Depending on utility locations, some streets may have room in the center of the street and may be conveniently located for this rainwater treatment.

Designing parking lots as plazas by replacing asphalt with trees, pervious pavers, and other permeable materials that provides filtering, infiltration, and storage of rainwater runoff from the site.

Policies and Actions

- P1.1** Promote alternative transportation modes like walking, biking, and transit that reduce emissions related to vehicular travel.
- A1.1** Redesign Arrow Highway and Central Avenue to promote walking, biking, ridesharing, public transit use, the use of alternative fuel vehicles or other clean engine technologies. VZ SE R
- P1.2** Promote and require the integration of Green Infrastructure into storm water management systems.
- A1.2a** Review and revise development regulations to encourage a green approach in new developments. Minimize impervious areas. Develop new projects and retrofit existing surfaces to reduce runoff through infiltration. SE R
- A1.2b** Incorporate Green Street elements into the redesign of Arrow Highway and Central Avenue. VZ SE R
- A1.2c** Promote the use of green roofs, bio-swales, pervious materials for hardscape, and other stormwater management practices to reduce water pollution. SE R
- A1.2d** Promote the use of captured rainwater, grey water, or recycled water. SE R
- A1.2e** Require landscaping for all new development to use drought tolerant plants. SE R

VZ Vision Zero

SE Social Equity

R Resilience

02 Our Prosperous Community

This section lays out specific economic development initiatives to deploy in the AHMUD. This Chapter is informed by discussions with community members and economic development stakeholders, in particular the ‘Our Prosperous Community’ focus group, to articulate the following economic development policies and actions in a manner that best addresses community needs and aspirations.

This section aligns with the 20-year timeframe of the General Plan and seeks to complement the Specific Plan with initiatives to help bring a unique, and coherent identity to the AHMUD. This section briefly reprises the AHMUD’s economic context, and outlines the following driving focus topics for the District’s economic development strategy: placemaking and district identity, economic diversity, funding, and governance.

A. Market Context

The District is on either side of the boundaries of the North Montclair Downtown Specific Plan, which has some of the City’s highest allowable densities and was developed to coordinate planning efforts and development around the City’s transit hub. The City should take care to coordinate its implementation of the following initiatives with the Specific Plans for North Montclair Downtown and Montclair Place, as they both share boundaries with AHMUD.

Today, the East AHMUD generally contains a varied mix of commercial and industrial uses, with the most notable exception being the Villa Montclair Mobile Home Park, located between Olive and Moreno Streets. In addition to the mobile home park, the East AHMUD’s retail uses are generally focused on the lots adjacent to Central Avenue, while industrial uses are located to the south of Arrow Highway. The much smaller West AHMUD is primarily composed of two business parks on Arrow Highway across from each other. The southern

Promote an eclectic mix of existing and proposed residential uses and small entrepreneurial and creative businesses.

park contains mostly light industrial uses including auto repair shops, whereas the northern park supports a wide variety of different commercial uses (such as a furniture store, an Omnitrans facility, animal rescue, and others).

Based on twenty-year demand projections, the AHMUD is unlikely to capture significant projected demand for hotel rooms or traditional office space, both of which are more likely to concentrate adjacent to activity centers like Montclair Place and the transit hub. Furthermore, there is moderate projected demand for retail in the City, most of which will locate more proximate to the existing mall site; any new retail in the AHMUD will likely go into existing underperforming retail establishments and/or mixed-use buildings, and will serve the growing resident and employee population.

In the near term, both sides of the AHMUD will be attractive to industrial users (in particular, warehousing and distribution users) and multi-family housing developers, given the high regional demand for both of those land uses. Currently, West AHMUD is better positioned to capture any residential demand in the near term, due to its adjacencies to new housing developments and proximity to the Claremont Colleges, whereas industrial users will gravitate toward the larger parcels and existing industrial stock in the East AHMUD.

Without policy intervention, new industrial development will likely focus on logistical and warehousing uses, which already occupy a substantial amount of land acreage in the City. However, there is an opportunity for the City to re-envision the AHMUD, particularly the East AHMUD, as the locus of mixed-use which fosters small business development and entrepreneurship and creates opportunities for residents to access meaningful jobs within Montclair. The remainder of this document outlines how the City can take steps to turn this economic development vision into reality through policies and prioritized investments.



The recommendations in this section seek to help establish the AHMUD as a distinct employment destination amid the Metro L Line’s expansion to Montclair. A prosperous AHMUD is one that supports a variety of uses, fosters new and creative businesses in high-growth industries such as manufacturing and healthcare, and leverages transit connectivity to establish a unique experiential identity. In line with the General Plan Update, the intended timeframe is the next 20 years, unless otherwise stipulated, and should be considered in coordination with the General Plan Update and existing Specific Plans. The following focus topics, policies, and actions should be periodically reexamined and can evolve with continued input from the community.

B. Placemaking & District Identity

Establishing a distinct sense of place and identity for the AHMUD is imperative to supporting economic development initiatives and future growth in concert with the incoming Gold Line.

Policies and Actions

P2.1 Implement multi-modal connectivity and pedestrian enhancements.

Assuring access from transit to the AHMUD is necessary to assure the District can be used and enjoyed by all.

A2.1a Implement multi-modal access from transit to AHMUD along Arrow Highway. VZ SE R

Providing for multiple kinds of transportation to and from the AHMUD helps maximize the number of people who may use the District, and improves accessibility to the City overall. Improvements recommended in current and prior planning processes for Arrow Highway, as a critical east-west thoroughfare and the connection between the East and West areas of the District, should be implemented to make the street more friendly and efficient for buses, bikes, pedestrians, and other micro-mobility options.

A2.1b Prioritize capital investments in street improvements within the East AHMUD to create a sense of place. VZ SE R

As it exists today, East AHMUD lacks urban character that could distinguish it from any other industrial district in Montclair or the region. To develop a coherent identity for the District that would eventually attract the types of higher-job density businesses envisioned, the City should prioritize investments in basic streetscape improvements such as sidewalks, signage, street furniture, street trees, and other planted vegetation to create the backbone of walkability and enjoyment of the District.

P2.2 Create an innovative and flexible land use framework.

To help cultivate the AHMUD as a distinct mixed-use district, the City will also need to allow for the proliferation of uses and amenities, that in turn support the branding of the district.

A2.2a Establish specific East AHMUD district. R

Multiple cities have successfully repositioned industrial areas to support a wider variety of uses. A key initial step to do this is establishing the district boundaries, and to begin clearly articulating how the City wants to express the district's identity (through architecture, street art, events programming, etc.).

A2.2b Develop a design identity to brand AHMUD. R

The City can cultivate a coherent feel to the District through small design interventions, such as threshold signage, street signs, a distinct color palette, lighting, or public art. The City can also encourage creative uses, while taking care to coordinate such efforts under a cohesive identity.

A2.2c Prepare affordable housing policy in the AHMUD. SE R

The only housing within the AHMUD is the Villa Montclair Mobile Home Park located in the eastern district. The City should study alternatives to providing a range of affordable units within the AHMUD potentially through a density bonus or in-lieu fee policy depending on the maturity of the market.

A2.2d Require a minimum amount of commercial development in mixed-use housing projects. R

To assure its competitiveness as an employment hub, mixed-use housing developments can be required to include a minimum amount of commercial development, ideally also catering to businesses consistent with the district's identity.

A2.2e Discourage warehousing and heavier industrial uses. R

To accommodate a diversity of uses and successfully reposition industrial spaces for an active public realm, the City should discourage warehousing and heavier industrial uses through land use regulations. This can take the form of, for instance, limiting the number of daily truck trips through the District, establishing minimum employment densities, or an outright ban of certain uses not aligned with the vision for the District.

P2.3 Improve District amenities.

A2.3a Establish a strategic central open space within AHMUD. SE R

The City should identify and acquire a parcel central to East AHMUD that will serve as an attractive gathering place. The space should be transit accessible, and support adjacent businesses, ideally by being developed in tandem with an incubator space.

A2.3b Pilot tactical urbanism to “activate” precise AHMUD spaces. SE R

The City can pilot low-cost, temporary interventions that will help bring new uses and activities to the area. This can take a variety of forms, such as food trucks, street fairs, or installation art pieces.

A2.3c Pilot a façade improvement program for businesses. R

Existing businesses in the AHMUD may not occupy industrial or retail structures that abide by the quality standards necessary to supporting a design district. The City can pilot a façade improvement program to help local businesses coordinate and improve the appearance of their businesses, in line with the design principles of the area.

C. Economic Diversity

In the AHMUD, the focus is on providing alternatives to warehousing, and establishing strong connections to high-value industries.

P2.4 Identify high growth and high value-add industries.

Demand for warehousing and logistical uses presents a low-hanging fruit for AHMUD's industrial spaces. However, the City would do best to focus economic development efforts on industries with higher potential for growth and value. The City should further investigate which industries it wishes to target for establishing within the AHMUD. Particular focus should be paid to opportunities in healthcare, advanced manufacturing, and related industries, contingent upon City policies. However, the City should remain flexible and open to alternative industries and businesses, so long as they meet the City's vision for catalyzing entrepreneurship and small business development.

A2.4a Establish Workforce Development Pipeline through Partnerships and Incubator. SE R

The AHMUD can serve as a platform for training new workforces in emerging, high-value sectors, by establishing strategic partnerships with local educational institutions and investing in resources that help emerging businesses thrive.

A2.4b Establish programs to develop a skilled pipeline to target industries. SE R

The City can begin to establish strategic partnerships with local knowledge centers in manufacturing and healthcare industries, attending to both academic institutions, businesses, nonprofits and industry organizations. These partnerships can then be used to pilot workforce development programs in the associated industries. The City can also explore opportunities to locate early workforce programming in the AHMUD, to help build a workforce pipeline.

A2.4c Explore partnership with local universities. SE R

Connections to the Claremont Colleges, California State Polytechnic University, Pomona, California State University, San Bernardino, and local community colleges should be fostered to provide sites for these institutions' programs in disciplines that dovetail with the AHMUD's creative economy focus, such as art, design, engineering, and manufacturing.

A2.4d Establish an incubator space to grow companies in target industries. SE R

In the longer term (more than five years from now), the City can establish an incubator space for businesses in targeted industries to help cement the AHMUD's relationship along these industries' pipelines. The City should take care to consider how it may want to acquire and establish space for an incubator – for instance, it could negotiate community benefits with large scale developers to provide funding (or physical space) for the incubator. Alternatively, it could purchase its own site for the incubator, or launch it in partnership with an existing property owner, before scaling up. The City should also consider how it can ensure space for local start-ups in the incubator, through (for instance) preferential selection to local businesses, or hosting events tailored to local business outreach.

P2.5 Encourage retail uses that support an entrepreneurial ecosystem.

A diversity of creative retail offerings can help activate industrial spaces and is necessary to support business activities in the area. A base amount of retail will also help drive consumer interest to the AHMUD, and help affirm its identity. The selection of retail offerings should cater to the kinds of industries the City wishes to site in the AHMUD.

A2.5 Provide incentives for certain retail businesses to locate in AHMUD. SE R

To entice a diverse array of retail to locate in the AHMUD (such as cafes, breweries and restaurants), the City can offer access to a variety of incentives. These may include (but are not limited to) discounted licensing taxes, waived business fees, or fast-tracked permitting. While they may require additional partnerships, the City could also provide extra business support and technical assistance, or discounted space at the AHMUD incubator (once established).

SE Social Equity

R Resilience

D. Funding and Governance

Success in the AHMUD requires the City's economic development organizational structure to accommodate the management and monitoring of all associated initiatives. The AHMUD Specific Plan presents an opportunity for the City to better understand how such economic development initiatives play out at the smaller, district scale, and could provide helpful observations for assessing citywide initiatives.

P2.6 Identify seed funding for catalytic early actions.

The City can prepare for implementing changes in the AHMUD by identifying and evaluating key sources of funding that can support specific initiatives.

A2.6a Set aside Economic Development Fund resources to jumpstart economic development activities in the AHMUD. SE R

In the short term, the City can prioritize resources to fund programming and placemaking enhancements in the District which catalyze its identity as a center for entrepreneurs. Such initiatives can include façade improvements, public programs (food trucks, street festivals, etc.), and district branding/marketing efforts.

A2.6b Establish an Enhanced Infrastructure Financing District (EIFD)/Community Revitalization & Investment Authorities (CRIA) to fund AHMUD infrastructure investments. SE R

In the longer term, a District-wide EIFD or CRIA can be used to reinvest increases in land values (from future development and a value premium created by transit) into infrastructure improvements in the AHMUD. This should be done in tandem with plans for the other areas, as referenced within the General Plan economic development strategy.

A2.6c Negotiate community benefits for economic development. SE R

The City should leverage community benefit negotiations from developers to provide funding for, among other items, economic development initiatives citywide and in the AHMUD. For instance, these funds could help fund the AHMUD's incubator space and/or programming.

P2.7 Ensure proposed developments are net fiscally positive.

Increase in developmental demand in the AHMUD provides an opportunity for the City to be more selective. In addition to considering how an incoming project may contribute to the aforementioned initiatives, the City should set up its development approval process to assure how a project meets specific fiscal thresholds.

A2.7 Use a fiscal model to assess project revenue potential. SE R

As referred to within the General Plan, the City should assess potential AHMUD development through a clear "scorecard" for fiscal performance and economic development benefit.

P2.8 Gauge participation in a Business Improvement District (BID) to provide high-quality district services.

Local investment in minor and major District improvements, via the establishment of a Business Improvement District, can go a long way in supporting public funding and initiatives in the area. However, to assure support for such a self-imposed tax, local landowners must have an active role in its visioning and be committed to making the investment work.

A2.8a Build a coalition of AHMUD property owners to coordinate with the City's vision. SE R

Without first rallying a set of landowning stakeholders behind a unified vision for the district, a BID is not likely to succeed. In the short term, an external committee can serve as a hyper-local chamber of commerce, partnering with the City to coordinate District-specific economic development initiatives, and assure they align with Citywide strategies. Once the committee is established, the following Action (A2.8b) may be considered.

A2.8b Establish the Business Improvement District (BID) for district maintenance and programming. SE R

The BID can have the authority and responsibility to manage programming of events, maintenance and other identity-building activities within the AHMUD, funded through the BID assessment. The BID can also be responsible for marketing the District to other businesses, through establishing a website or other proactive outreach efforts.

SE Social Equity

R Resilience

The ultimate impact of each tactic depends on careful coordination, implementation and monitoring, and should help inform evaluation methods for economic development strategies Citywide. Under the guiding themes of transit-connected placemaking, diverse uses, and local entrepreneurship, the AHMUD District can leverage economic development in coordination with Citywide agendas.

The Montclair General Plan calls for maintaining and expanding existing inventory of commercial uses and to allow incremental reinvestment on vacant or underperforming sites. The AHMUD Specific Plan identifies improvements to the built and natural environment that implement the community's economic and social goals, such as nurturing local talent, driving economic opportunity, fostering belonging, maintaining and enhancing Montclair's cultural character, encouraging creativity and exchange in the public realm, supporting community health, and creating access to opportunity for all.

A. Urban Form and Land Uses

The plan identifies 100 residential units within the western and 400 residential units within the eastern portions of AHMUD. The Specific Plan encourages the production of diverse affordable housing unit types such as multi-generational housing that supports large family structures, single-room occupancy, shared housing, co-housing, and cooperative-housing.

The Fiscal Analysis prepared with the General Plan noted that hotels, office space, and retail space contribute significant net positive ongoing revenues to the City's General Fund, exceeding those generated by comparable housing space. Development of hotel and supporting commercial space in AHMUD will grow Montclair's tax base and enhance the City's ability to provide services and programming for its residents. Additionally, a hotel will also further strengthen Downtown Montclair and AHMUD as a visitor destination. Within Montclair's Competitive Market Area, the average daily rates for hotels have been rising, and occupancy rates have peaked. The Market Study prepared with the General Plan noted there is demand for up to 300 hotel rooms in Montclair till 2040. Hotel development in Montclair is likely to be the most successful in highly visible, accessible, and amenitized areas of the City, such as in and around the downtown, and the southeast corner of I-10 freeway and Central Avenue in AHMUD which can capture the demand for a 100-room hotel.

The growth in industrial and flex uses over the last two decades has favored sites in AHMUD with very large floorplates dedicated to logistics uses such as warehouse and distribution. These uses do not serve to activate or improve the public realm. Nor is it a reliable source of long-term fiscal revenues or a path to resilient prosperity. The City will explore collaborating with the Claremont Colleges, California State Polytechnic University in Pomona, California State University, San Bernardino, and/or other local educational institutions to provide training and support for start-ups in an incubator focused on smart manufacturing. Smart Manufacturing combines advanced manufacturing capabilities and digital technologies to collaborate and create highly customizable products faster, affordable, better, and greener. Those technologies include

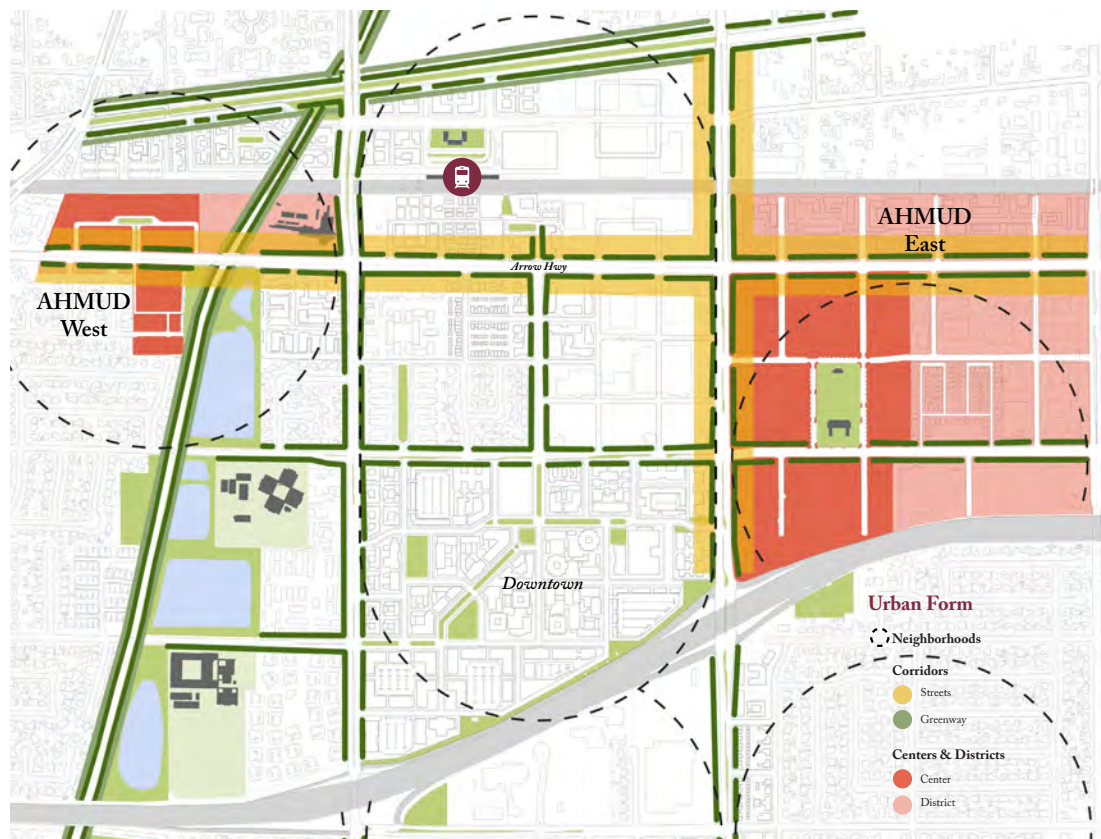


Figure 3.2: The urban form map shows the organizing place types for AHMUD: neighborhood, centers, districts and corridors.

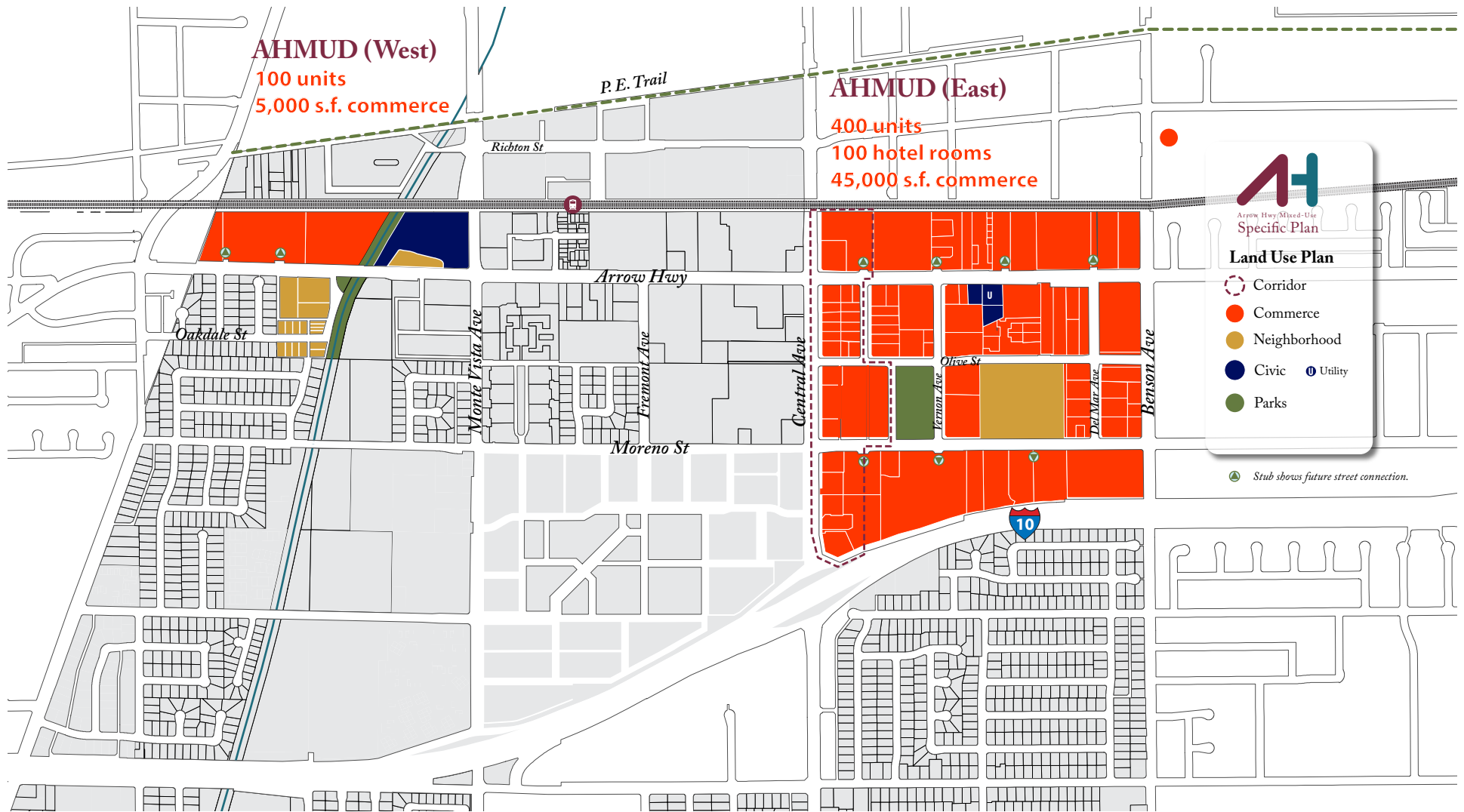


Figure 3.3: AHMUD Land Use Plan

3D printing; cybersecurity; advanced materials; modeling, simulation, visualization and immersion; artificial intelligence; robotics; the industrial internet of things; and cloud computing. The General Plan allocates up to 50,000 square feet of commerce, mostly focused in the eastern AHMUD.

Small, locally-owned businesses should be supported by deploying and focusing available assistance to those businesses, and supporting incubators and accelerators for social enterprise. Provide development incentives to encourage provision and leasing of space serving the needs of local maker and arts/artist activities users on-site in new de-

velopments or repurposing of existing buildings. Zoning should permit flexible ground floor uses that allow light industrial/production and/or office use in addition to retail-related uses, so long as they interact well with the street and collectively add to create a lively, interactive, vibrant and culturally relevant public realm. These local businesses contribute to the economic and cultural vitality of AHMUD and generate tax revenues. The local arts, cultural, retail, dining, and entertainment businesses in AHMUD can draw from and in turn support downtown as a regional, multi-cultural entertainment, dining, and shopping destination.



AHMUD West building types

B. Character & Intensity

In the AHMUD area, there are five areas with distinctive character and intensity:

- **Corridor:** The Central Avenue Corridor Zone permits regional and community serving retail, office, cultural, and residential mixed-uses. Active uses are located at the sidewalk level with commercial uses at the upper levels. New buildings are block scale, up to four floors (50 feet in height). A continuous building frontage along Central Avenue will provide definition and enclosure to the public realm. Taller buildings line up the wide stretch of street frontage with buildings stepping down in the rear. The standards in this zone are intended to promote a walkable, diverse, and well-connected area.
- **Commerce:** The Commerce Zone permits a wide variety of office and large-format retail and commercial activity along with multi-family dwellings. Uses would include flex/light industrial uses including research and development uses, small scale smart manufacturing, professional and medical office, and traditional business park. The Commerce Zone supports continued reinvestment and context sensitive intensification that enhance promotes local businesses and enhances housing choices. Warehouse and industrial uses that have outdoor storage, display, and sales should not be located along highway frontage parcels. Range of open spaces and walkable streets and alleys weave together the various existing and proposed assets.
- **Neighborhood:** Primarily residential with a variety of single- and small multi-family dwellings organized along walkable streetscapes. The basic needs of daily life met by limited neighborhood serving commercial/ retail uses in close proximity. Streets form a connected network, providing alternate routes that help to disperse traffic, and are equitable for vehicles, pedestrians, and bicyclists. Diversity in the type, size, and disposition of buildings, streets, and open spaces creates many options in environments, experiences, functions, uses, prices, and populations.
- **Civic:** Public or quasi-public in operation, civic buildings and spaces are the cornerstone of neighborhoods and a symbolic reflection of Montclair's values and aspirations. The civic buildings may operate in the fields of the arts, culture, education, recreation, government, transit, security, health, safety, and public utility. Since public buildings represent a collective identity, their design should set them apart from more conventional private buildings. Civic building sites should be located within or adjacent to a civic space, or at the terminated vista of a significant thoroughfare. These places are easily accessible by foot or automobile and have formal access points that address the street.
- **Parks & Open Space:** Designate lands to public recreation and leisure and visual resources, and can range from neighborhood tot lots and pocket parks to urban squares and plazas and playgrounds to large regional parks.



AHMUD East building types

Policies and Actions

P3.1	Conserve the commercial character and scale of the AHMUD (East) area, including, while creating places of enduring quality that are uniquely fit to their time and place.	P3.4	Expand the inventory of publicly accessible community gathering spaces so that businesses and residents are within a short walking distance of a park or recreational area.
A3.1a	Develop and adopt a form-based development code that supports existing commercial uses and encourages expansion while permitting infill development with context sensitive architecture, urban design, and landscaping.	A3.4a	New buildings should incorporate public realm improvements described in the AHMUD Vision and integrate such improvements into their existing context in a way that enhances AHMUD's public space network.
A3.1b	Introduce new infill buildings and renovate existing buildings in a manner that enhances AHMUD's walkable urbanism with interconnected streets lined by buildings that engage, frame, and activate the street.	A3.4b	Allow parklets on Arrow Highway to provide visual interest and expand the useable area of the sidewalk.
P3.2	In AHMUD (West), leverage the creek channel to orient the new buildings and their frontages to the Creek Corridor and be respectful of the scale of the existing residential context.	P3.5	Encourage green projects and practices.
A3.2	Develop and adopt a form-based development code that requires the highest standards of context sensitive architecture, urban design, and landscaping.	A3.5	Incentivize sustainable living and business practices, both passive and active, that encourage energy efficiency, improve indoor air quality, and encourage water and resource conservation.
P3.3	Remove regulatory and procedural barriers to good design.	P3.6	Provide high quality housing for current and future residents with a diverse range of income levels.
A3.3	Develop and adopt a Form-Based Code for the Downtown area that emphasizes pedestrian orientation, integration of land uses, treatment of streetscapes as community living space, and offers a streamlined development review process.	A3.6a	Provide for quality housing at a range of income levels and price points, emphasizing housing product that captures the underserved multi-family market.
		A3.6b	Support workforce and market rate units that will expand and diversify AHMUD's housing stock, and support growth in AHMUD employment.
		A3.6c	Provide flexibility in development standards to encourage and facilitate nontraditional housing types and options, including single room occupancy, shared housing, co-housing, and cooperative-housing.

C. Infrastructure

The AHMUD Specific Plan area is built out and is served by well-established utility service providers. Under proposed buildout conditions, land use changes will alter utility demands and may result in increased demands on existing infrastructure.

The AHMUD Specific Plan area relies on the same service providers as the rest of City of Montclair, and all existing and proposed city-wide capacity assessments include the AHMUD area within the analysis. Under current conditions, the Specific Plan area is served by the following providers and major infrastructure:

Utility	Servcice Provider	Infrastructure
Storm Drain	<ul style="list-style-type: none">City of Montclair	<ul style="list-style-type: none">30-87" City Storm Drain
	<ul style="list-style-type: none">San Bernardino County Department of Public Works	<ul style="list-style-type: none">County Flood Control ChannelsSan Antonio Creek Channel
Sewer	<ul style="list-style-type: none">City of Montclair	<ul style="list-style-type: none">6"- 8" Sewer Main
Water	<ul style="list-style-type: none">Monte Vista Water District	<ul style="list-style-type: none">6"-8" Distribution Lines20" and 30" Main Lines
	<ul style="list-style-type: none">Southern California Edison	<ul style="list-style-type: none">Distribution Lines Throughout
Natural Gas	<ul style="list-style-type: none">SoCal Gas	<ul style="list-style-type: none">High Pressure Distribution Lines through both East and West AHMUD
Solid Waste	<ul style="list-style-type: none">Burrtec Waste Industries, Inc.	<ul style="list-style-type: none">Waste transported to offsite facilities

Table 3.2: Current providers and major infrastructure.

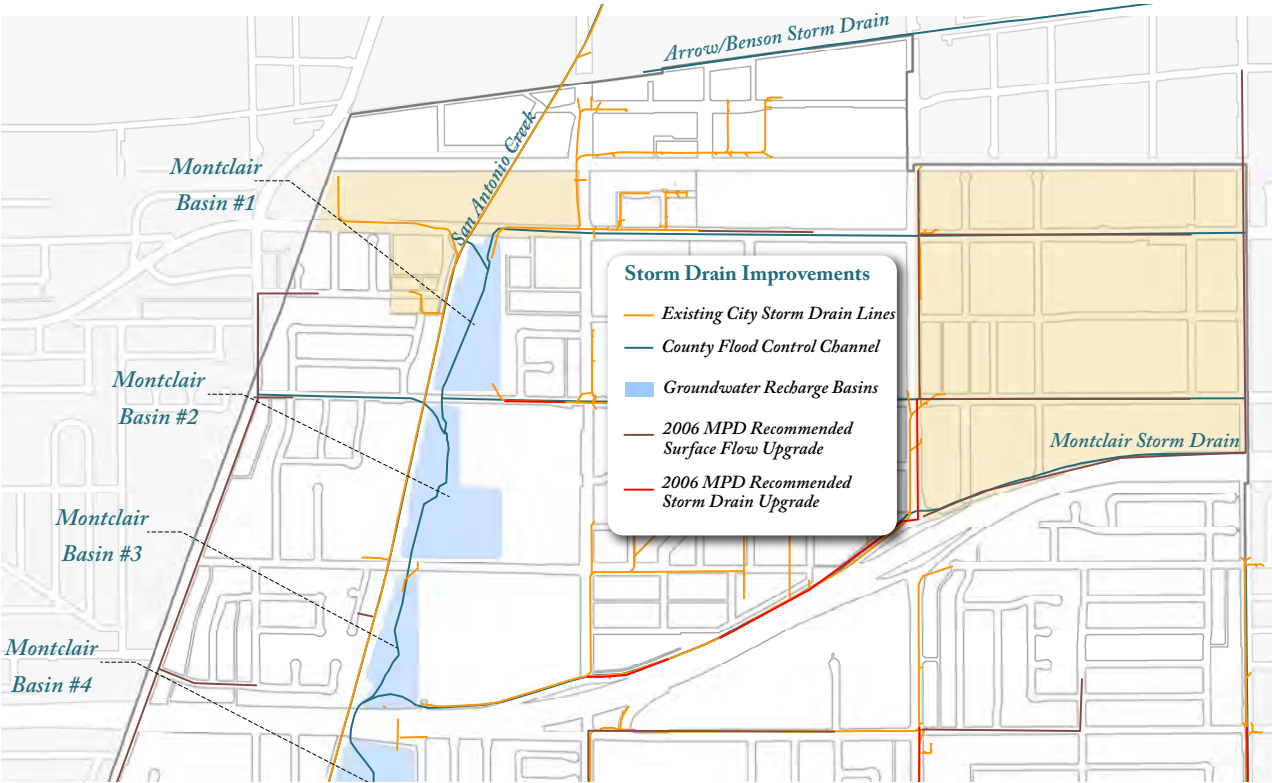


Figure 3.4: Storm Drain Improvements Map

1. Storm Drain

Runoff drains through the AHMUD area through a combination of curb and gutter surface flow, open channel, and subsurface storm drains. The 2006 City of Montclair Master Plan of Drainage (MPD) surveyed storm drain infrastructure throughout the City including infrastructure serving the Specific Plan area. All underground storm drain lines within the AHMUD area were determined to be in acceptable working condition. Portions of the AHMUD East area were marked for improvements of insufficient surface drainage conveyance, which included installation of storm drain pipes from 33" to 54" in diameter. The City maintains a capital improvement program (CIP) and prioritization system to address deficiencies and improvements on a project-by-project basis. New projects within the AHMUD East area will

be required to contribute toward a portion of outstanding improvements based on size, location, and proximity to recommended improvements as identified in the 2006 MPD.

While overall drainage patterns, flow rates, and flow volumes are not anticipated to greatly change under Specific Plan implementation due to the built-out nature of the AHMUD East and West areas, hydromodification requirements and standard flood control requirements for new development and redevelopment will ensure that runoff remains at or below current levels. In addition, due to local, county, and state-level low impact development (LID) requirements, new development and redevelopment within the Specific Plan area will result in reduced pollutant loading to storm drain systems and receiving water bodies. Based on the LID hierarchy and regional

soil characteristics, it is anticipated that infiltration-based Best Management Practices (BMPs) will be implemented for individual projects within the AHMUD Area. These BMPs include dry wells, infiltration trenches, biofiltration basins, permeable pavement, and stormwater landscape planters, and will be sized to effectively treat the “first flush” rainfall event. Per Santa Ana Regional Water Quality Control Board and City of Montclair requirements, new development projects are required to prepare a Water Quality Management Plan (WQMP) that describes the menu of BMPs chosen for the project, as well as operation and maintenance requirements for the site and all structural and treatment control BMPs. Where feasible, opportunities for integrating stormwater treatment and infiltration into the Santa Ana Watershed Project Authority (SAWPA) One Water, One Watershed integrated planning program will be assessed and implemented.

2. Sewer

Proposed land use changes will alter sewer flows within and downstream of the AHMUD Plan area. Existing and proposed flows were modeled based on flow factors from the City of Montclair 2017 Sewer Master Plan.

Under proposed conditions, flows are anticipated to increase by approximately 117,814 gallons per day (gpd), representing an 83 percent increase in flows. Flows will continue to drain south through City infrastructure to regional conveyance infrastructure and ultimately to regional wastewater treatment plants. The projected increase of 0.118 million gallons per day (mgd) falls within the designed total treatment capacity of the Inland Empire Utilities Agency (IEUA) Regional Plant 1 of 44 mgd. Current flows treated by the plant average 28 mgd, indicating adequate capacity.

The most recent sewer capacity assessment prepared for the City, the 2021 Sewer Master Plan Update, did not identify any segments with insufficient capacity within

the AHMUD area under existing conditions or modeled buildout conditions. The City maintains a formal process to ensure functionality of the sewer system and that any priority upgrades, including unforeseen upgrades necessary as part of Specific Plan buildout, are addressed in a time efficient manner. In addition, the increases in sewer flows are anticipated to be spread evenly across the AHMUD East and West subareas, which rely on separate main lines to convey flows to regional sewer infrastructure. The 2017 Master Plan identified future deficiencies in a major trunk line downstream of the Montclair Place Specific Plan located adjacent to AHMUD which may require future improvements associated with the buildout of Montclair Place. Increased flows from the AHMUD area do not drain directly to the trunk line, but connect to it at a point further south in the City. Flows from the AHMUD area will not contribute to local capacity issues generated by implementation of the Montclair Place District Specific Plan.

Number of Dwelling Units	Number of Hotel Rooms	Commercial Square foot	Average Sewer Flows (GPD)
Existing Conditions			
96	0	1,900,000	140,466
Proposed Conditions			
596	100	1,950,000	258,280
Increase in Sewer Flows, Existing to Proposed Conditions			+117,814
Percentage of Flow Increase, Existing to Proposed Conditions			+83%

1. GPD: Gallons per day
2. Sewer demand factors based on City of Montclair 2017 Sewer Master Plan
 - a. All dwelling units modeled as multi-family.
 - b. Multi Family’ demand factor utilized for hotel units.

Table 3.3: Existing and Proposed Sewer Flows

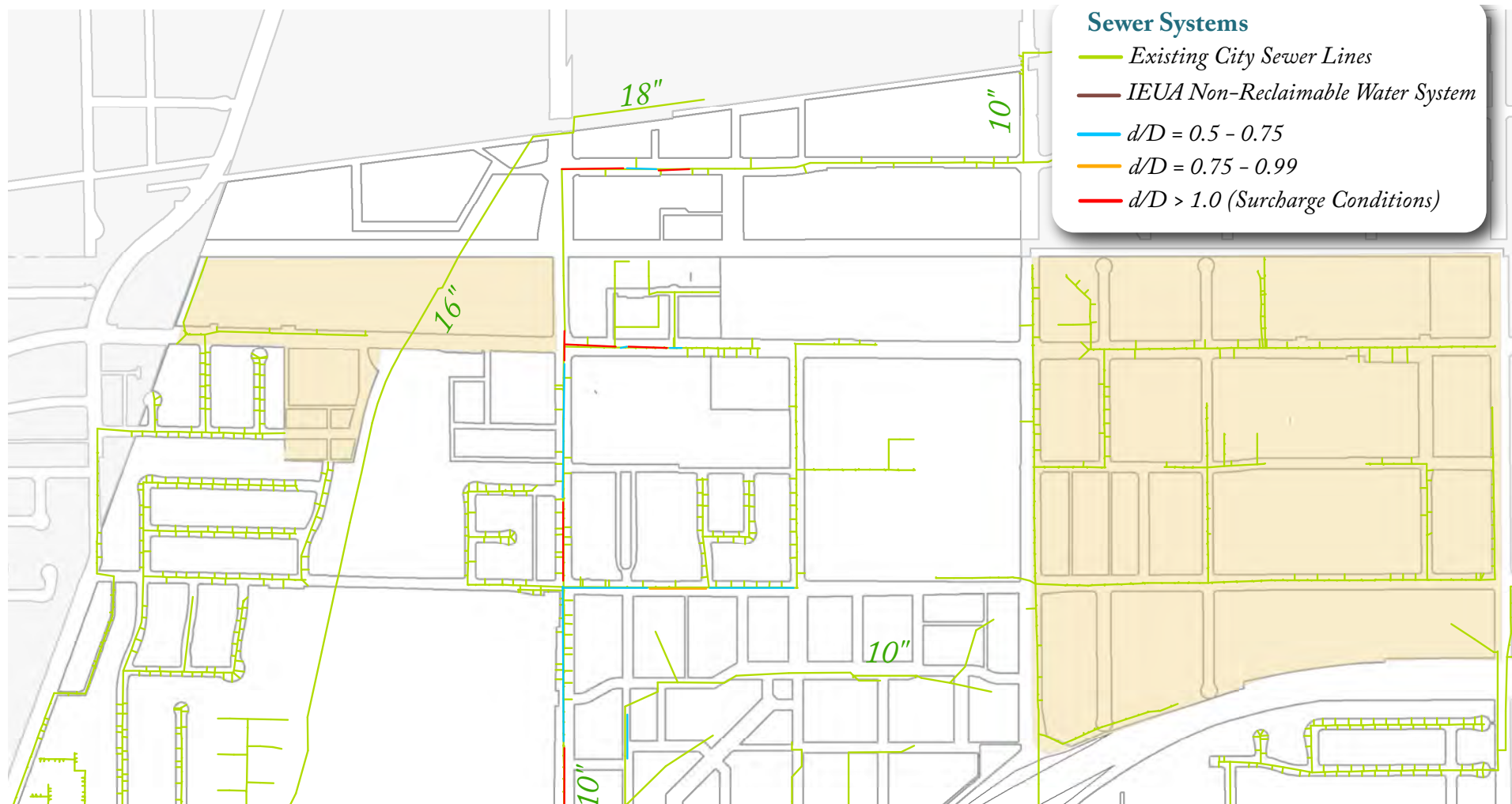


Figure 3.5: Sewer Systems Map

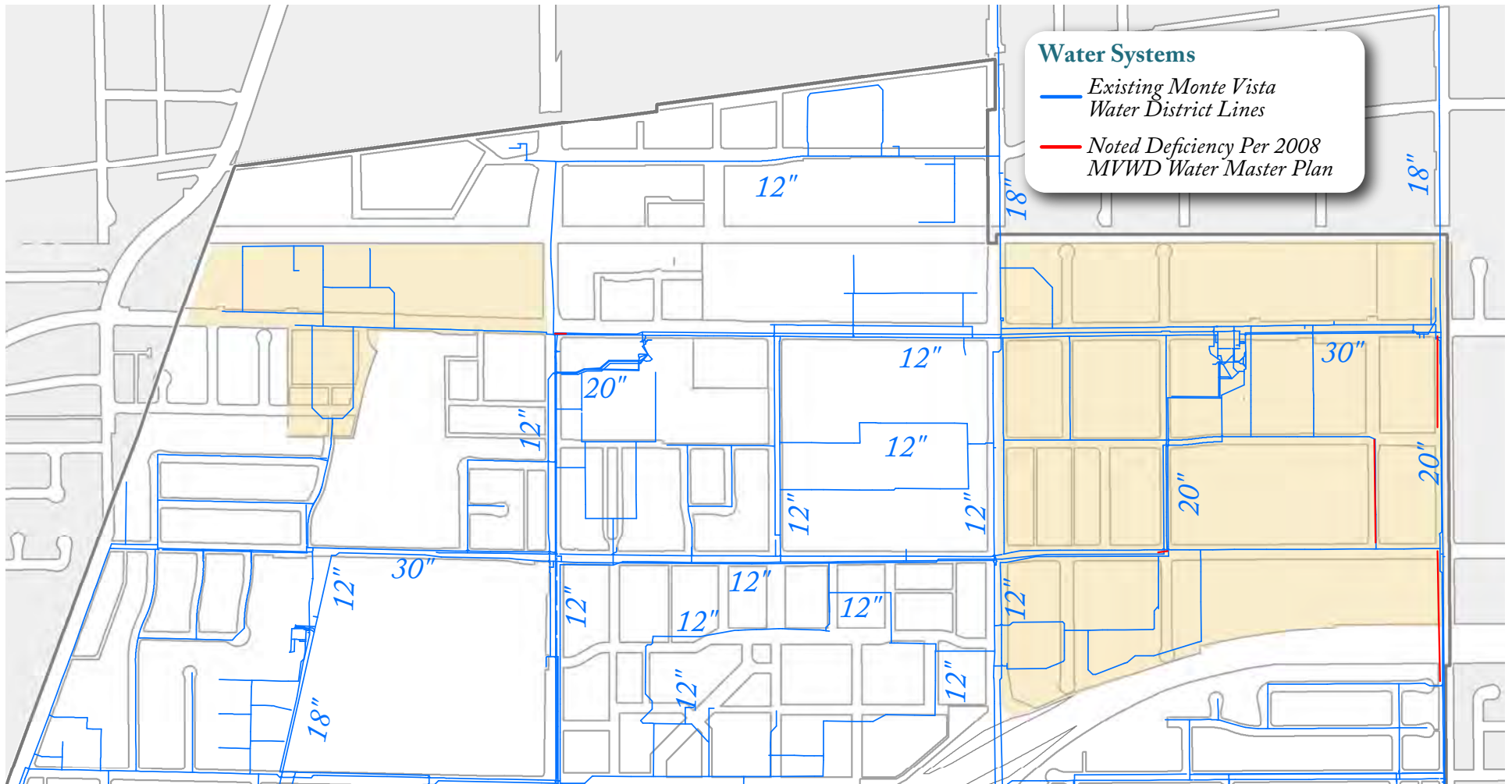


Figure 3.6: Water Systems Map

3. Water

Monte Vista Water District (MVWD) infrastructure will serve the AHMUD area under both existing and proposed conditions. Demand estimates were calculated based on the 2008 Monte Vista Water Master Plan and 2020 Monte Vista Water District Urban Water Management Plan (UWMP).

As shown in Table 3.4, under proposed buildout conditions, water demands will increase by approximately 208,567 GPD, representing a 121 percent increase over existing conditions, with the majority of additional demands stemming from the proposed increase in residential dwelling units.

The 2008 MVWD Water Master Plan included an infrastructure assessment based on both pipe capacity and condition and identified deficient segments throughout the City. There were no deficient segments identified in the AHMUD West area, however three segments of 6” diameter line were identified as deficient within or immediately adjacent to the AHMUD East area. Redevelopment within the area presents the opportunity to upgrade those segments not already replaced or upsized by MVWD. MVWD regularly updates a Financial Master Plan and associated CIP, and prioritizes and determines funding for improvement projects throughout its service area. Proposed projects within the AHMUD East area will be responsible for contributing to outstanding infrastructure improvements to the extent that they impact water lines marked as deficient.

Number of Dwelling Units	Number of Hotel Rooms	Commercial Square foot	Average Sewer Flows (GPD)
Existing Conditions			
96	0	1,900,00	171,981
Proposed Conditions			
596	100	1,950,000	380,458
Increase in Sewer Flows, Existing to Proposed Conditions			+208,567
Percentage of Flow Increase, Existing to Proposed Conditions			+121%

GPD: Gallons per day
Non-residential water demand factors based on Monte Vista Water District 2008 Water Master Plan. Residential water demand factors based on per-acre demand factors from the 2020 Monte Vista Water District UWMP and corresponding Land Use Based Water Demand Technical Memorandum. For single family homes, the 'Residential Low Density' demand factor was utilized, and for multi-family units, the 'Residential High Density' demand factor was utilized.

1. Total dwelling units include single family and multi-family units. Proposed increase in dwelling units modeled as multi-family.
2. Residential High Density demand factor was utilized for hotel units.

Table 3.4: Existing and Proposed Water Demands

4. Electricity

The AHMUD area receives all electrical service from Southern California Edison (SCE). As part of statewide reporting requirements, SCE regularly prepares an Integrated Resource Plan (IRP). The 2018 IRP examines trends in demand and the portfolio of energy supplies that SCE can utilize across the planning period out to 2030. The IRP does not note any existing deficiencies citywide nor in the Specific Plan area under the demand and supply scenarios identified.

Minor increases in electricity demand are anticipated as a result of redevelopment within the AHMUD area. While new on-site infrastructure and connections may be constructed, it is not anticipated any upgrades or changes to regional transmission infrastructure will be required.

5. Natural Gas

Natural gas service to the City is provided by Southern California Gas Company (SoCalGas), which prepares an annual California Gas Report (CA Gas Report) that identifies demand forecasts as well as potential changes in supply. The CA Gas Report covers a 15-year demand and forecast period through 2035. There are currently no existing or projected deficiencies in supplies or infrastructure across the SoCalGas service area given the current demand scenario, which includes the AHMUD area.

Increases in natural gas demands as a result of AHMUD buildout will be adequately served by existing SoCalGas distribution infrastructure.

6. Solid Waste Service

Under AHMUD buildout conditions, residential and commercial units will continue to be served by Burrtec Industries, Inc., with no changes to service anticipated. Commercial and residential solid waste within Montclair is transported to a variety of materials recovery facilities (MRFs) where recyclable materials are sorted out and remaining waste is sent to one of seven landfills serving the City.

While residential waste generation may increase as a result of the proposed increase in dwelling units, local and statewide recycling and sustainability initiatives are anticipated to result in reduced generation at an individual level, and improved collection and disposal methods at a service provider level. A portfolio of landfills utilized shall be maintained to reduce increased loading to individual facilities.

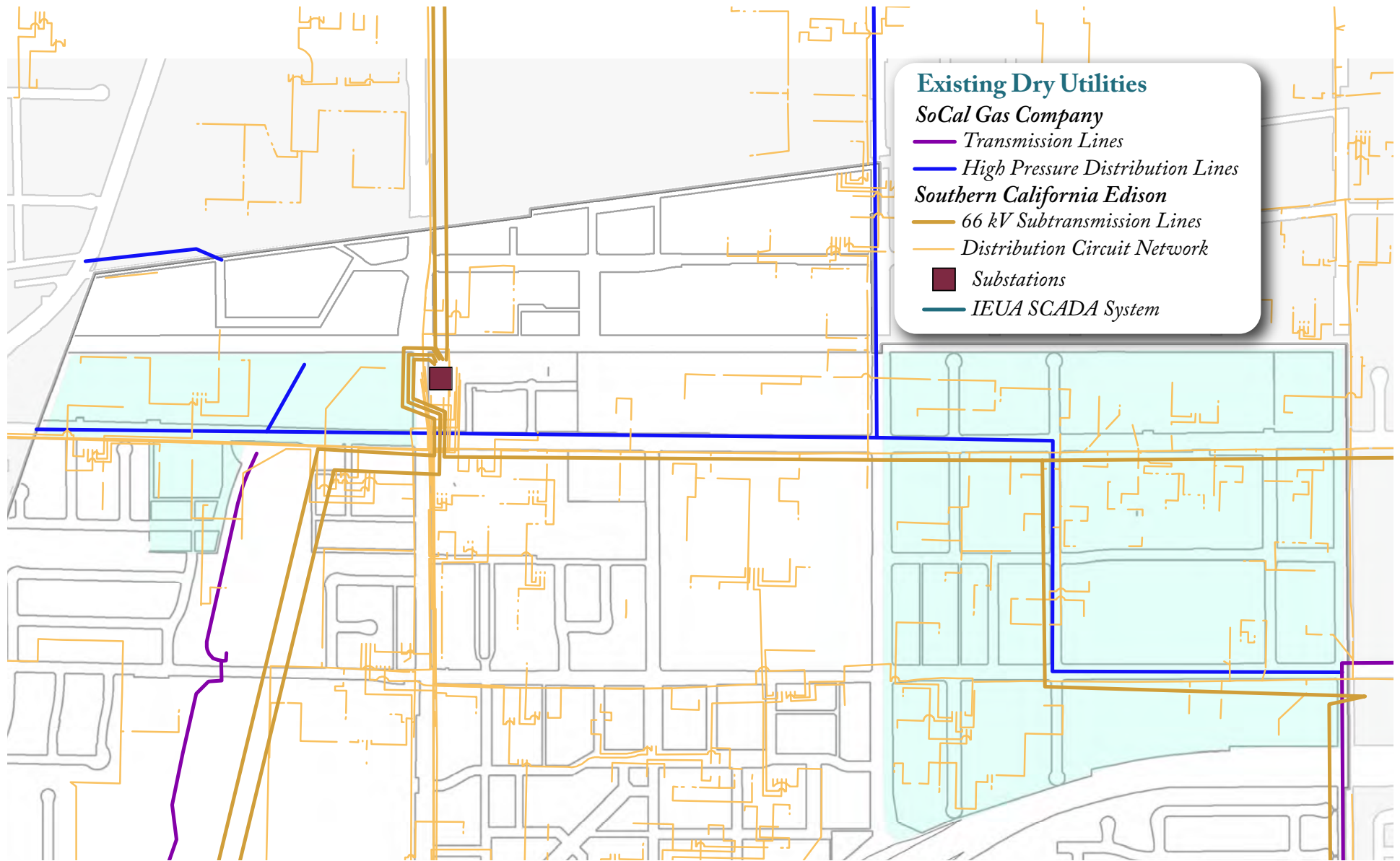






Figure 3.7: Dry Utilities Map



Policies and Actions

The AHMUD Specific Plan policies and actions seek to ensure a reliable system that meets current and future needs and encourages conservation.




A. Stormwater

- P3.7** Utilize and maintain a robust stormwater conveyance system that protects the City from flooding impacts and ensures that storm flows are efficiently routed to regional drainage.
- A3.7a** Continue to regularly update City capital improvement program to ensure effective prioritization, funding, and construction of drainage improvements throughout AHMUD. 
- A3.7b** Enforce hydromodification control requirements on new developments, ensuring that increases in impervious surface do not result in increased peak flows and downstream scour. 
- P3.8** Effectively treat all urban runoff and stormwater and ensure that local groundwater supplies and downstream receiving waters are not degraded.
- A3.8a** Inspect all new developments during both construction and operational phases for compliance with local, regional, and state level water quality regulations. 
- A3.8b** Encourage the implementation of low impact design features for all new development and redevelopment within AHMUD. 


B. Wastewater

- P3.9** Ensure wastewater in the AHMUD area is safely and efficiently conveyed and treated under all demand scenarios, including existing and future average and peak flow sewer flow scenarios.
- A3.9a** Continue to regularly update the City's capital improvement program in order to effectively prepare for sewer flows generated as a result of land use changes and new developments throughout AHMUD. 
- A3.9b** Construct, maintain, and revitalize distribution infrastructure as needed throughout the AHMUD in response to changes in demand, land use patterns, and aging infrastructure. 



C. Water

- P3.10** Maintain, upgrade, and expand water pipeline, storage, and pumping infrastructure to meet projected domestic, commercial, and fire flow demands for all land uses within the AHMUD area.
- A3.10a** Coordinate with MVWD's capital improvement plan in order to effectively prepare for land use changes and new developments within the AHMUD area. 
- A3.10b** Construct, maintain, and revitalize distribution infrastructure as needed throughout the AHMUD area in response to changes in demands and land use patterns. 
- A3.10c** Coordinate with the IEUA to expand range of recycled water infrastructure for efficient irrigation throughout the AHMUD area. 

D. Electricity and Natural Gas

- P3.11** Ensure all AHMUD residents and businesses are safely and affordably supplied with electricity and natural gas throughout all future buildout scenarios.
- A3.11** Coordinate with SCE's and SoCalGas' capital improvement projects to ensure that any necessary connections or upgrades are adequately funded and constructed in a time-efficient manner. 

E. Solid Waste

- P3.12** Provide and maintain adequate and orderly systems for the efficient collection and disposal of solid waste for existing and future development throughout AHMUD.
- A3.12a** Coordinate with solid waste service provider to ensure that waste pickup, recycling, and disposal occurs in the most efficient and sustainable manner possible. 
- A3.12b** Conduct outreach and education to reduce solid waste generation at the household and business level to minimize landfill loading. 

 Resilience

Make AHMUD streets safe, comfortable, inviting, and well connected to the City so that everyone has efficient and reliable access to AHMUD's jobs, services, and amenities.



The City of Montclair is located on the Western border of San Bernardino County. Currently, the transportation network is primarily focused around serving automobiles and is based on development patterns and roadway design standards from the 1950s and 1960s when automobiles were becoming accessible to everyone.

Transit service is provided in Montclair, including bus service and ADA paratransit service, and is anchored by the Metrolink commuter rail and Montclair Transcenter. The Montclair Transcenter is a regional transportation hub, with a Metrolink station, a park-and-ride facility, and an on-site childcare center to serve the commuting public. Additionally, the Metro Gold Line light-rail-transit (LRT) system will be extended to Montclair which will provide additional high capacity transit opportunity to the City.

Prior to the 1950s, most American cities were built based on proximity and access to amenities. This typically resulted in less travel (from a distance traveled perspective) but more congestion. The City of Montclair seeks an equilibrium in the mobility options that would accommodate users of all ages and abilities. In addition, future transportation trends should be considered to prepare for the ever-changing mobility scenario in urban environments. The City of Montclair seeks to be an active community that is well planned and accessible for all. This is supported by the City's community survey that showed 60 percent of the respondents desired ease of getting around without driving; while 85 percent wanted their neighborhoods to be more walkable.

In addition to the overarching vision crafted for the City in the General Plan, the Arrow Highway Mixed-Use District Specific Plan (AHMUD) aims to improve

the quality of travel for employees, residents and visitors by providing a vision for new development in the area.

The mobility strategy for AHMUD includes providing:

- **Network of Streets:** Connect the streets, alleys, and pathways to connect destinations and provide multiple options for people to get around.
- **Safe, Comfortable, and Interesting Streets:** Streets are redesigned to be safe for all modes of transportation including walking and biking. The risk of injury is reduced by regulating traffic speed.
- **Flexible Transit:** Range of options are proposed to connect to transit services and destinations within the City and the region. They include infrastructure for walking and biking, microtransit, multi-modal connections, signage and wayfinding, etc. This Plan recommends mobility hubs at two locations:
 - » On Arrow Highway at the San Antonio Creek Channel on the west side; and
 - » On the southwest corner of the Central Park on the east side.

Policies and Actions

P4.1 Create a transportation system that supports complete streets goals.

A4.1a Design new street connections in the AHMUD to minimize traffic volumes and/or speed, as appropriate, without compromising connectivity for emergency first responders, bicycles, and pedestrians. **VZ** **SE** **R**

This could be accomplished through management and implementation of complete street strategies, short block lengths, narrow streets, and/or traffic calming measures.

A4.1b Design new streets and implement improvements on existing streets in the AHMUD to serve users of all ages and abilities, while prioritizing modes and design features based on the context of the street, including the surrounding land use, planned improvements, and collision history. **VZ** **SE** **R**

A4.1c Enhance the bike and pedestrian preferred facilities as part of development, private grants, signing of shared routes, maintenance activities, etc. consistent with the City's Active Transportation Plan, Systemic Safety Analysis Report Program, and General Plan. **VZ** **SE** **R**

P4.2 Establish mini-mobility hubs on Arrow Highway and at the new Central Park consistent with the General Plan approach.

A4.2a Provide signage and curb space for vehicle pick-up/drop-off, car share, microtransit, bike share, and e-scooters, as appropriate. **VZ** **SE** **R**

A4.2b Create safe pedestrian and bicycle access to mobility hubs from major destinations; especially those connecting to the Metrolink station. **VZ** **SE** **R**

A4.2c Provide secure commuter bicycle parking and locker options to businesses and mobility hubs. **VZ** **SE** **R**

A4.2d Convert bus stops into bus shelters. **VZ** **SE** **R**

A4.2e Consider multi-modal links to the Montclair Transcenter. **VZ** **SE** **R**

A4.2f Consider options and opportunities to partner with and support local businesses. **VZ** **SE** **R**

P4.3 Establish amenities and support services for all modes.

A4.3a Create streetscapes and public realms that encourage walking and biking. **VZ** **SE** **R**

A4.3b Incorporate designs that remove barriers to pedestrian and disabled persons from adjoining roadways, including through pathways that do not require passing through parking areas, and that are connected to the sidewalk and transit stops. **VZ** **SE** **R**

A4.3c Consider opportunities for future connections to the planned trail along the San Antonio Creek Channel Frontage. **VZ** **SE** **R**

P4.4 Implement safety enhancements to reduce collisions.

Capitalize on the priorities outlined in the City's SSARP for the AHMUD Plan Area. Safety enhancements at intersections may include high visibility crosswalks and advanced stop bars, implementation of leading pedestrian intervals (LPI) and implementing a semi-exclusive or exclusive pedestrian phase in high foot traffic areas. Implement sidewalk enhancements, curb extensions, protected bikeways, etc. where feasible and appropriate.

A4.4a When performing street maintenance, consider safety improvements at all locations. **VZ** **SE** **R**

A4.4b Use the City's SSARP to identify high injury locations and pursue annual grant funding (such as Highway Safety Improvement Program or Active Transportation Plan) to implement safety improvements at those locations. **VZ** **SE** **R**

VZ Vision Zero

SE Social Equity

R Resilience

05 Our Healthy Community

Consistent with the Montclair General Plan, the AHMUD Plan promotes healthy environments that make it easier for people to adopt healthy behaviors, healthy choices, and prevent the onset of costly chronic diseases. The AHMUD Specific Plan prioritizes projects that have support from various partners and produces multiple health benefits while improving the environment and the economy and strengthening the social ties.

The construction of the I-10 freeway in the 1950s was instrumental in attracting investment into the area, but the pattern of development was largely designed to be accessed by the automobile. The design of streets and buildings in AHMUD do not encourage walking, cycling, or transit use. The design of streets and transportation networks has significant impacts on health and wellbeing. Promoting active travel like walking or cycling helps maintain a healthy weight, reduce road collisions, traffic noise, and the increasingly recognized effects of air pollution. There are also wider impacts on social networks, which are in turn so important for health and wellbeing.

This Specific Plan seeks to improve health by improving the built and natural environment in AHMUD by focusing on the following four areas:

A. Healthy Streets: Healthy Streets put people's needs at the center of decision-making in terms of improving health, enhancing liveability and nurturing community spirit. The elements that make a street good for health are generally the same as those needed to make a street good for the local mobility, economy, community, and environment. The Montclair General Plan and AHMUD Specific Plan recommends existing and proposed streets within AHMUD prioritize safe, comfortable, and active transportation in conformance with the City's Active Transportation Plan (ATP) and Safe Routes to School Plan (SRTS).

B. Green Infrastructure: Proximity to green space can

Promote a network of streets, parks, and open space that encourages active living and well-being.



play a role in how physically active people are likely to be. Montclair's Green Infrastructure trails and green streets will provide many public health benefits, such as psychological relaxation and stress reduction, enhanced physical activity, and mitigation of exposure to air pollution, excessive heat, and noise. The proposed trail along the San Antonio Creek Channel would link the AHMUD West neighborhood to multiple parks, schools, and the Montclair Transcenter.



C. Access to Healthy Food: Eating a healthy diet is difficult without access to nutritious food. When healthy foods are not available, people may settle for foods that are higher in calories and lower in nutritional value. The farmers market can be hosted in the central public park in AHMUD. The central park can provide opportunities for growing food and would attract avid gardeners and foodies as urban residents and attract and support local restaurants and food processing entrepreneurship. Local food, if processed can be kept longer, it can reach customers that do not have the space or time to cook, and it can transform surplus food into new products, hence preventing waste.

D. Noise: Chronic noise from traffic, trains, commercial and entertainment uses, and noisy neighbors can increase stress levels and bring about anxiety and sleeplessness. The predominant source of noise in AHMUD is motor vehicles and trucks on the streets and the Metrolink and freight trains. As AHMUD begins to attract residential uses, difficulties often arise where noise-sensitive land uses such as residential activities move into established commercial areas or close to major infrastructure assets such as transit and freight corridors. Generally, the overriding requirement is for control of noise at source. Usually this means considering the building's location, orientation, design, and construction; and the operational plan of the facility to avoid unreasonable noise. Additionally, specific precautions may include requiring acoustic insulation of new buildings with residential uses.





Policies and Actions

P5.1 Create a multimodal transportation system that encourages active living and healthy lifestyles in all areas of the City across a broad spectrum of ages, interests, and abilities.

A5.1a Design new streets and retrofit existing streets within AHMUD to prioritize active transportation.   


Align the new street design and retrofit of existing streets with the recommendations of the City's Active Transportation Plan (ATP) and Safe Routes to School Plan (SRTS).



A5.1b Make active transportation networks in AHMUD safe and accessible for all ages and abilities.   



A5.1c Continue to implement health programs through the Healthy Montclair Initiative.  

A5.1d Develop the trail along the San Antonio Creek Channel in AHMUD West.   



P5.2 Integrate AHMUD into the citywide healthy and equitable food system.

A5.2a Integrate growing food into large public and private open spaces within AHMUD.  

A5.2b Promote healthy food related events like Farmers Market in the central park area.  

A5.2c Support entrepreneur food processing businesses.  

P5.3 Minimize noise impacts to ensure that noise does not detract from Montclair's quality of life.

A5.3 Require new development projects to implement mitigation measures, where necessary, to reduce exterior and interior noise levels.  

 Vision Zero

 Social Equity

 Resilience

06 Our Safe Community

Montclair is a safe community with low property and violent crime rates. Because the AHMUD area is built-out with limited projected growth, the Specific Plan does not anticipate that additional facilities and/or public safety employees will be required to serve the AHMUD area.

Personal and community safety and security in AHMUD area is a complex issue. The perception of safety or danger does not always relate directly to the actual incidence of crime. We feel comfortable and confident using areas where there is good visibility and effective lighting, where we can be seen and heard by other people. Thoughtful design, good management, and involvement from AHMUD businesses and residents is an important instrument in enhancing sense of well being and making places more user-friendly, easy to understand, and secure. The key design principles that promote personal safety and property security are:

A. Natural Surveillance

The objective is to provide surveillance opportunities from buildings and public streets to discourage anti-social activity. For example: shopfront windows, balconies, and entries allow building occupants and passersby to observe on-site and street activities for the purpose of informal surveillance. Mixed uses, particularly at street level, add vitality at different times of the day and night.

Promote a safer community by making the streets safe for all uses and by making the parks and open spaces more accessible and visible with stronger connections to adjacent uses.



B. Pedestrian and Bicycle Safety

Wide multi-lane streets with larger block lengths in the AHMUD area result in streets that are both difficult to cross and easier to speed on. The community supported vision calls for following pedestrian and bicycle safety enhancements for Arrow Highway, Central Avenue, and Moreno Street:

1. New local streets and mid-block crossings help break the longer block lengths and encourage pedestrian activity along and across the street.
2. A clear and safe alley system could encourage exploration of the AHMUD area, and more enjoyable routes, while allowing a finer grain network that is more comfortable for pedestrians. The alley would become a safe and quiet gathering space as well as a safe pedestrian connection, without getting onto Arrow Highway or Central Avenue.



C. Lighting

To overcome issues such as safety, navigation, and visibility – light installations can effectively be used to welcome people in dark public spaces. Improving street lighting can reduce the prevalence and incidence of crime. AHMUD will attract pedestrians with flattering full-spectrum, low-watt lamps on short poles. Besides street lamps, lighting can be mounted on buildings and where appropriate light the facade, or can be integrated into bollards, feature lights, and streetscape elements.

Lighting levels should be achieved by increasing the number of lights, not their height or wattage. Rather than be everywhere, lighting levels respond to the context. In AHMUD area, street lights should be frequent and support nighttime activity. In residential areas, lighting can be limited with well-lit intersections. Light streaming from storefront windows can make a significant contribution to the feeling of security at night. Mixed-uses in AHMUD, particularly upper level housing as a crime prevention strategy, increases the feeling of safety. Along Arrow Highway and Central Avenue, upper-floor housing provides “eyes on the street” that follow what is going on in the street and deter crime.

“... there must be eyes upon the street, eyes belonging to those we might call the natural proprietors of the street.”

— Jane Jacobs

Policies and Actions

P6.1 Make AHMUD streets safe for pedestrians and bicyclists.

A6.1a Carry out the safety enhancements recommended by the AHMUD Vision for Arrow Highway, Central Avenue, and Moreno Street. **VZ** **SE** **R**

A6.1b Add mid-block crossings and parklets on Arrow Highway and Moreno Street. **VZ** **SE** **R**

A6.1c Amend the development codes to allow context sensitive street types. **VZ** **SE** **R**

P6.2 Employ a range of contextual lighting options to promote safety and security on AHMUD streets.

A6.2a Identify AHMUD public streets and open spaces that are poorly lit and install context sensitive street lights. **VZ** **SE** **R**

A6.2b Install string lights in alleys that provide connections to destinations. **VZ** **SE** **R**

A6.2c Require new development to demonstrate on a lighting plan appropriate level of direct and indirect lighting in the public and private realm. **VZ** **SE** **R**

P6.3 Reduce opportunities for criminal activity through physical design standards, recreation opportunities, educational programs, and counseling services.

A6.3a For new infill development and major rehabilitation, incorporate natural surveillance principles and best practices into development codes and review processes. **VZ** **SE** **R**

Ensure natural surveillance and human presence by:

- *making buildings front on to the public realm;*
- *putting “eyes on streets” and minimizing blank facades; and*
- *mixing uses, particularly at the street level, adding activity at different times of the day and night.*

A6.3b Amend the development codes to require building designs that promote safety by providing active street frontages. **VZ** **SE** **R**

VZ Vision Zero

SE Social Equity

R Resilience



07 Our Active Community



The Central Park in East AHMUD is an event, family, and sponge park. Within the western garden are play areas, meandering jogging and walking paths.



The West Park and the linear bike and jogging/walking trail along the San Antonio Creek channel are the key open spaces in west AHMUD.



The AHMUD vision calls for the temporary use of some of the expansive parking lots for community events and gatherings.

Promote and ensure inclusive and equitable access to a range of opportunities for physical activities including parks, open space, and recreation.

Open spaces and recreational facilities in AHMUD provide an opportunity to exercise, access sunshine, nature and fresh air, and encourage people to walk or bike. The public parks and open space network in AHMUD needs interesting, inviting, and varied places for quiet reflection or the places for physical activities and play. To invite more people to walk, active and attractive ground floor facades, invitations to sit and rest along the streets, pedestrian scale street lighting, and clear organization of the sidewalk space to enable unobstructed walking are needed.

AHMUD is built out. Introduction of residential uses will require a network of parks and open spaces that should be provided with incremental infill opportunities. The greatest opportunity for public space in AHMUD exists in reimagining the streets, the San Antonio Creek, paths, and alleys as linear public space corridors that provide safe, comfortable, and inviting walking and biking experiences.

A. Network of Open Spaces

Open space provides the ‘breathing room’ within the urban AHMUD context and promotes opportunities to engage in outdoor activities. The vision identifies the Central Park and the Creek Corridor as two major open spaces and creates a rich tapestry of proposed parks, plazas, enhanced alleys, parklets and mid-block crossings that will increase the overall amount of open spaces in AHMUD, and provide visitors, workers, and residents with an appealing, thermally comfortable outdoor environment.

B. Recreation

Recreation includes activities that happen both at outdoor open spaces and indoor facilities. Activity is

beneficial by way of being fun, stimulating, refreshing, or relaxing. Active recreation involves physical activity while passive recreation usually involves sitting, lying down, or a leisurely stroll.

Active recreation refers to a mix of possible uses in the Central Public Park that includes: sports fields, buildings or structures for recreational activities, concessions, courses or sport courts, children’s play areas, and recreational pathways.

A passive recreation area refers to a mix of uses along the San Antonio Creek Corridor, which include the landscaped areas, lawn spaces, seating, and picnic areas.




















C. Parklets and Mid-block Crossings

Parklets transform on-street parking spaces into small spaces for people to use and enjoy. They provide pedestrian amenities such as seating, landscaping, and bike parking. To augment pedestrian activity and social interaction along Arrow Highway, and to provide more sidewalk space, a series of parklets are proposed to be distributed throughout the street.

D. Green Network

The street trees and other vegetation in AHMUD will function as a localized green infrastructure which is part of the larger “green infrastructure” framework of the city. Street trees and other vegetation substantially enhance pedestrian activity. All AHMUD streets currently have significant gaps in the tree canopy. The vision calls for thriving and distinctive street trees on all streets.

Policies and Actions

- P7.1** Expand park inventory in AHMUD to ensure the maximum distance between residents' homes and the nearest public park is ½ mile (¼ mile preferred).
- A7.1a Invest in the acquisition of new parkland.  
- A7.1b Develop trail along the San Antonio Creek.   
- A7.1c Amend development code to require new development to provide its fair share of public and private open spaces.  
- A7.1d Expand the parks and recreation system through repurposing public land like excess streetspace, expansive parking lots, and other creative means.  
- P7.2** Promote, expand, and protect a green infrastructure that links the natural habitat.
- A7.2a Use parks as sponges to serve stormwater infrastructure and flood mitigation roles to enhance resiliency, recreational use, and beauty.  
- A7.2b Encourage simple, small, and low-cost demonstration green infrastructure projects both in the public and private realm.  
- P7.3** Identify and remove barriers to access parks. Encourage walking and biking as preferred way to get to and from parks.
- A7.3a Increase the number of entrances to existing parks to expand the number of residents within walking distance of a park.   
- A7.3b Prioritize new access or entry points near the proposed mobility hubs, including sidewalks, bike routes, trails, and transit.   

- P7.4** Explore creative or alternative funding opportunities for programs and capital projects.
- A7.4a Explore establishing a Community Foundation or “Friends of Parks” organization for the purpose of soliciting park land donations; applying for private grants; and for fundraising. 
- A7.4b Explore naming rights, sponsorships and asset management opportunities to create ongoing revenue for maintenance and operations of existing facilities. 
- A7.4c Explore contracts with private business to provide and operate some of the recreational activities which would be financed, constructed, and operated by the private sector with additional compensation paid to the City. 
- A7.4d Explore interlocal agreements between two or more local units of government and/or between a local unit of government and a non-profit organization for the joint usage/development of sports fields, regional parks, or other facilities. 

 Vision Zero

 Social Equity

 Resilience

08 Our Creative Community

A. Festival and Events

Festivals and events are important in developing a memorable destination, and serve as catalysts for other developments. They provide opportunities for community participation; enhance and promote the social, cultural, economic, and environmental fabric of the place; and can attract tourists and visitors from the region.

While the event can be centered in the AHMUD area, it could also include programming throughout the city. The event could be organized around an overarching theme that allows broad community engagement with ideas that have strong resonance in Montclair. The City's arts organizations, artists, library, parks, businesses, restaurants, the school district, churches, and other regional partners can program their participation around the theme and provide shared marketing and promotion for the event.

B. Creative Expression in Public Realm

Creative and welcoming public spaces are vital to fostering more interaction between AHMUD's workers, residents, and visitors, and Montclair's unique history, culture, and artists. The Specific Plan identifies several sites for public space improvements. The proposed Central Park and the San Antonio Creek Channel both have spaces dedicated to public assembly which should be better utilized. Reducing regulatory barriers to outdoor vendors in AHMUD, such as permit costs and business licenses will encourage vibrant and successful public spaces. Temporary pop-ups and vendors can provide "eyes on the street" that enliven and increase safety in shared spaces and plazas. Vendors should be encouraged to fill vacant or underutilized lots that face primary and secondary streets. In addition, vendors in public spaces could help drive foot traffic to nearby retail.

Leverage the creative economy, integrate public art and cultural events and programming within the public realm that attracts and supports creative businesses.

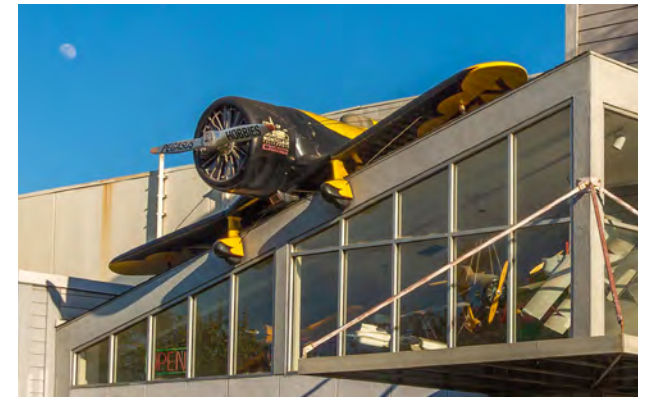


Vacant or underutilized industrial structures offer lots of inexpensive room for artists to practice their art without impacting neighbors.

Leverage underutilized spaces in AHMUD, including vacant ground floor shopfronts and empty parcels, as opportunities for affordable arts, culture, and social enterprise space. The City of Montclair can facilitate the use of vacant or underutilized buildings as temporary affordable art or social enterprise space. Consider establishing a "pop-up" registry program to connect artists, local small businesses, and organizations with building owners.

C. Home to Artists

Many artists find it difficult to take advantage of existing affordable housing programs due to often sporadic or unstable income, making it difficult to officially document for purposes of eligibility for affordable housing initiatives. Additionally, the predominant ranch housing type found in Montclair is less suitable for creative people that prefer high-ceilings, light-filled rooms, at bargain prices. Providing artists with affordable housing in AHMUD where they can live and work can also strengthen the social cohesion and identity of AHMUD. The artists can attract outsiders into AHMUD with events and open houses that showcase their art. They could also take their art-making experiences out into the larger community.



D. Public Art

Public Art plays a remedial role, lending beauty and interest to AHMUD, giving it character and identity.

E. Canvas for Public Art

Murals: Existing blank walls are visually boring and increases the perception of an unsafe environment. Murals are quick and cost-effective forms to create or nurture a sense of culture and creativity in AHMUD's aesthetic. The City and its arts partners could establish a Mural Program, a public art strategy to commission a series of interrelated murals at key points along the San Antonio Creek or throughout AHMUD East, either by a single artist or artists working in collaboration. This approach would encourage the public to explore the area as a creative continuum and glimpsing murals on buildings and walls along the way.

Public Realm: Sidewalks, crosswalks, and plazas offer opportunities for artwork. Existing surfaces can be enlivened via sand-blasted patterns, incorporating concrete stain as well. New construction affords creative use of inset bronze elements, stone and tile mosaic, and aggregate.



Photos show examples of street design elements that provide character and sense of place.

F. Streetscape Design Elements

Streetscape design elements can be used to define and celebrate the character of the place. These features can be consistent throughout AHMUD or change as one moves through individual places within the districts. Outdoor seating, banners, lighting, wayfinding signs, building signs, and other street design elements should be unique to AHMUD to reinforce a strong sense of place. Local artists could be commissioned to design and fabricate some of these features.

Policies and Actions

P8.1 Develop and host an iconic Cultural Event in AHMUD.

Promote AHMUD as an arts and entertainment destination by facilitating an iconic special event/festival. This event should distinguish itself from other offerings in the Los Angeles region by reflecting Montclair's authentic character.

- A8.1a Identify the event promoter with the most promise to serve this role. SE R
- A8.1b Connect available and underutilized venues with those seeking spaces for events. SE R
- A8.1c Leverage parks and public spaces as vital locations to showcase and promote arts and culture in AHMUD. SE R

P8.2 Increase supply of affordable housing for artists.

- A8.2a Create systems and procedures to spur the development of affordable artist housing through the use of innovative tax programs and the maximizing of financial tools. SE R
- A8.2b Allow live-work building type in AHMUD particularly on primary streets where housing may not be desirable on street level. SE R
- A8.2c Partner with nonprofit organizations, for-profit developers, and grassroots groups that have a track record to successfully pull together the partners and resources needed to create affordable live/work spaces for artists. SE R

P8.3 Integrate public art into the development review & capital improvement program.

- A8.3 Identify public art opportunities and engage artists as early as possible in all projects. SE R

P8.4 Promote collaborative process of create powerful murals on blank walls that can transform public spaces, neighborhood identities, and individual lives.

- A8.4 Design and implement high quality temporary outdoor painted murals that reflect the stories and content researched from the larger Montclair community. SE R

The murals are intended to enhance AHMUD's identity, turn ordinary spaces into landmarks, and promote community dialogue, while addressing visual blight and vandalism. Montclair's local artist community can be funded through the percent for public art policy to complete a mural. In return, the selected local artist through community coordination can prepare the wall, and maintain the mural after completion.

SE Social Equity

R Resilience



Inland Pacific Ballet: Inland Pacific Ballet (IPB), located in AHMUD, along Central Avenue is a premier ballet company in the region, enjoying critical acclaim while performing the largest and most varied repertoire in the area. Through creative marketing and an extensive Educational Outreach Program, Inland Pacific Ballet strives to introduce new audiences to the magic of ballet, and to make the experience more available and accessible to all. The Company is also committed to the nurturing of new talent and provides an essential training ground for serious young dancers. IPB's pioneering audience development work has resulted in an exponential increase in annual attendance - from 3,000 in 1995 to a current average of 20,000, making the Company the fastest growing arts organization in the region. For many Montclair residents, the IPB's performances are an introduction to the arts and live theater.



4

Code

4.0 Introduction

Montclair provides a range of living, working, and recreational options to suit diverse needs. The form and character of the different human habitats is conceived as a geography of diverse place types such as neighborhoods, districts, and corridors. The Urban Form Map shows a physical framework of place types that informs and guides the nature, form, and character of the built physical environment.

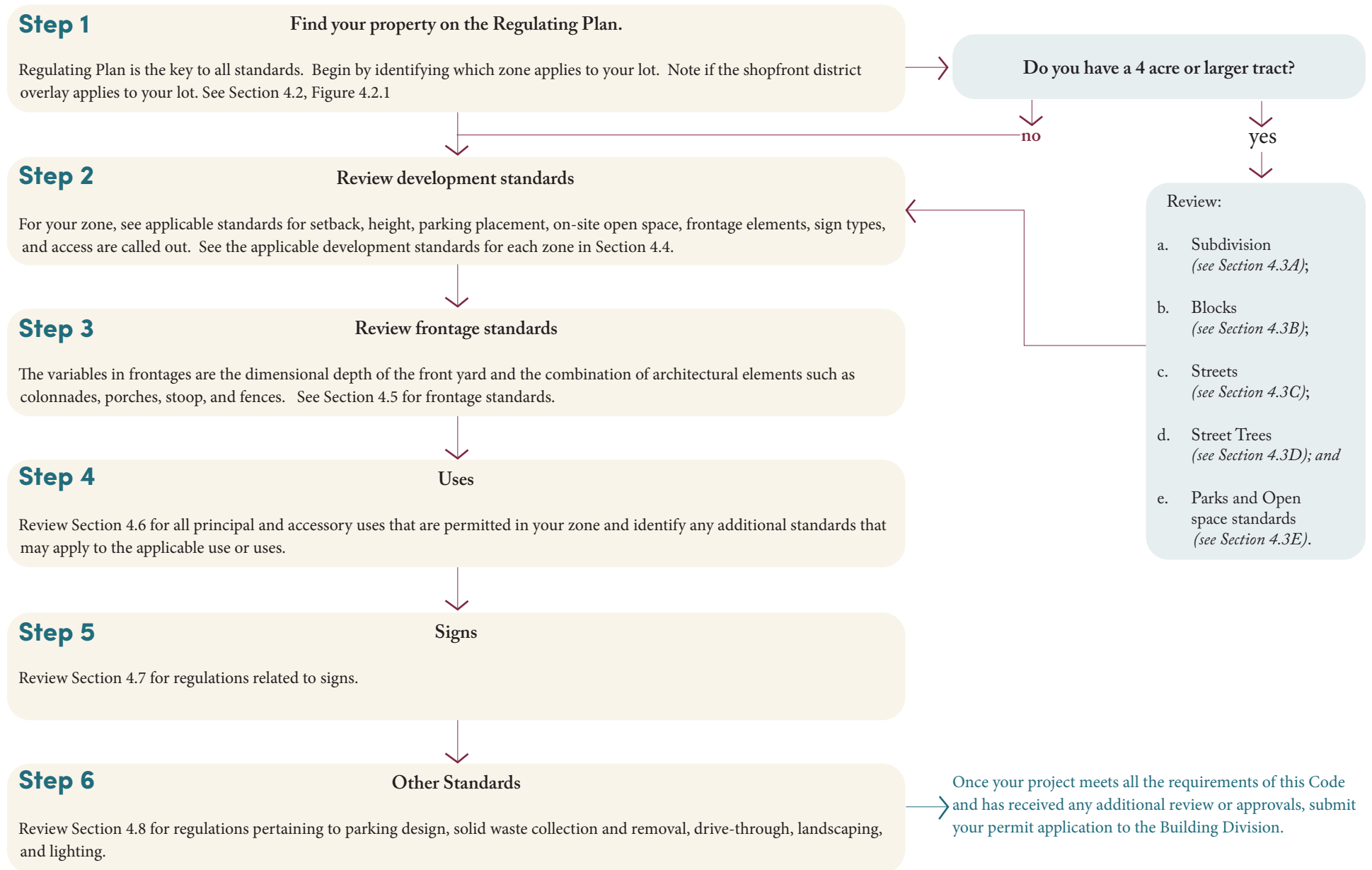
The City is largely built out with stable residential and commercial areas. Some of the vacant and underutilized sites in the AHMUD area offers the greatest potential for preserving commercial development and attracting new growth.

Community preferences and directions shape the corresponding policies and implementation. These statements guide the coding of the built environment — from a broad, citywide scale, to a detailed scale of individual neighborhoods, blocks, buildings and physical character, consistent with the history and desired future of the place.



Figure 4.1: AHMUD Urban form

How to Use this Code?



Glossary: Review Section 10 for definitions of terms found throughout this code.

Understanding Development Standards

The development standards (Section 5.3) for the four zones contained in this Code govern the physical development and form of real estate. The regulations for each zone are presented across two pages with the same general format for each district, as listed below with explanation:

4.4D Commerce

1 Setbacks

Primary Street Setback

- i Ground Floor non-residential 0 ft. min./5 ft. max.
- ii Ground Floor residential 10 ft. min./15 ft. max.

Side Street Setback

- i Ground Floor non-residential 0 ft. min./5 ft. max.
- ii Ground Floor residential and upper floors 10 ft. min./15 ft. max.

Side Yard Setback

- i Non-residential 0 ft.
- ii Residential 8 ft. min.

Yard Setback

- i With Alley 5 ft. min.
- ii Without Alley 15 ft. min.

2 Heights

Top of plate height 3 stories/ 45 ft. max.

Parapet height 4 ft. max.

Patch roof above top of building plate Allowed

Ground Floor above grade

- i Non-residential 0 ft.
- ii Residential 3.5 ft. min.

Ground story floor to floor height

- i Non-residential 15 ft. min.
- ii Residential 12 ft. min.

3 Parking Placement

On-site parking shall be located as indicated below and shown in the diagram on the right:

Primary Street Setback

- i Podium/Surface 25 ft. min.
- ii Subterranean 0 ft.

Side Street Setback

- i Podium/Surface 20 ft. min.
- ii Subterranean 0 ft.

Side Yard Setback

- i Podium/Surface 5 ft. min.
- ii Subterranean 0 ft.

Rear Setback

- i Podium/Surface 5 ft. min.
- ii Subterranean 0 ft.

4 Signs

Allowed Signage Types: A maximum of two of the below sign types are allowed per business. Signs may encroach into Primary Street and Side Street right-of-way as measured from the building facade as identified below:

Sign Type	Encroachment
a. Freestanding Sign	0 ft.
b. Yard Sign	0 ft.
c. Awning Canopy	Within 2 ft. of curb
d. Window Sign	0 ft.
e. Projecting Sign	5 ft.
f. Wall Sign	Sign Thickness
g. Directory	Sign Thickness

5 On-site Open Space

a One (1) or more of the Open Space Types listed below shall be provided on each lot and must be accommodated behind the Primary Street setback line.

Open Space type	Minimum Area	Minimum Dimensions
i Side Garden		
ii Courtyard	10% of total lot area	20 ft. by 20 ft.
iii Back yard		
iv Roof deck		

b Residential projects without a back yard shall provide for each residential unit a private open space in the form of a balcony, door yard, or yard. Private open space shall have a minimum area of 40 square feet with a minimum width of 5 feet.

6 Building Size and Separation

a Buildings shall be designed according to the following building length and building separation standards:

Building length along Primary Street	280 ft. max.
Building length along Side Street	120 ft. max.
Building separation along Primary Street	15 ft.

b Optional Frontage Elements: The following elements may be provided on all Street- and Courtyard facing facades, and may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below:

Element	Encroachment
i Balcony	3 ft. into R.O.W. or setback
ii Bay Window	
iii Awning, Canopy	Within 2 ft. of curb

7 Frontage Elements, Encroachments, and Access

a Required Frontage Elements: Street- and Court-facing building facades shall provide at least one (1) of the frontage elements listed below. These Frontage Elements may encroach into the Primary Street and Side Street setbacks as identified below:

Element	Encroachment
i Arcade, Gallery	To R.O.W. line
ii Shopfront	n/a
iii Stoop	5 ft.
iv Door yard	To R.O.W. line

b Pedestrian Access: All ground floors shall be accessed directly from the sidewalk either directly to each unit or commercial space through an appropriate frontage element, lobby, or open space, such as a court. Upper floor uses may be accessed through an exterior or interior stair, or lobby.

c Vehicular Access: Parking shall be accessed from an alley. Where an alley is not present, parking/service areas may be accessed from primary street. Driveways shall be located as close to side property line as possible. Parking/service areas for corner lots shall be accessed from side street.

Building Placement:
Regulates minimum lot dimensions and where a building is allowed to sit within a lot.

Building Form:
Defines the allowed height and form of buildings on a lot.

Parking Location:
Regulates where parking may be located on the lot.

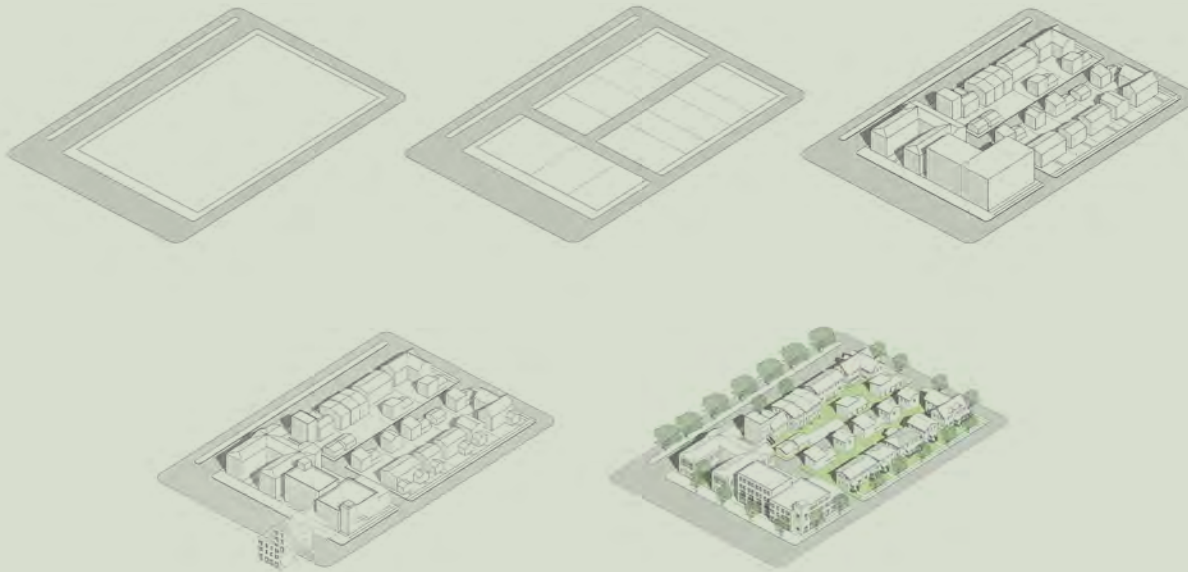
Signs: Regulates the type, size, and location of signs on a lot.

Open Space: Regulates what type of open space is required, the minimum dimensions, and where it may be located.

Building Components:
Regulates attached elements and allowed encroachment into any required setbacks.

Access: Regulates how pedestrians and vehicles access a site.

Form-Based Code



The standards are carefully calibrated for varying context offering a rich public realm and a variety of living and working environments.

The AHMUD Code is a form-based code (FBC). FBCs are an alternative to conventional zoning regulations. FBCs are purposeful place-based regulations with an increased focus on the design of the public realm: the public space defined by the exterior of buildings and the surrounding streets and open space.

FBCs are used to protect and preserve stable areas from incompatible development and to attract appropriate (re)development to transform areas at risk. This FBC reinforces the historical form patterns with the use of streets, frontages, buildings, and open spaces that are appropriate for AHMUD context.

A key difference between conventional use-based and FBCs is that FBCs do not determine entitlements through FAR or floor units per acre. The conventional density controls have failed to produce diversity in living and working arrangements in a contextual manner. Overly restrictive use and density regulations are common barriers to small-scale development.

The focus of this code is on the few but critically important urban standards that shape the public realm. These include design of streets and open spaces, setback, building height, building frontage at street level, parking, and access.

The properties in AHMUD West and East are build-out. The vision for AHMUD calls for preservation of existing commercial development while allowing incremental and contextual infill development.

The regulations in this code focus on creating a unifying public realm and is intended to be lean and less regulatory on the private side. The AHMUD area is distinctively different than neighboring downtown or residential areas. The area will develop incrementally and more organically.

This code intentionally encourages improvisation and innovative infill development that creates a rhythmic patterns of existing and new buildings unified by a distinctive, vibrant, and walkable public realm.

The Development Code consists of the following sections:

4.0 Introduction: What is Form-Based Code?

4.1 Purpose and Applicability: The purpose, use, and applicability of the code.

4.2 Regulating Plan: A zone map that assign standards based on desired character and intensity.

4.3 Public Realm: Standards for subdivision, blocks, streets, and open spaces. Features a Public Realm Plan showing a range of open spaces; and standards for street types, Street Tree Master Plan, street furniture, and outdoor seating areas.

4.4 Development Standards: Setbacks, height, parking standards, and access aimed at generating the individual buildings on a block that collectively with other buildings will shape the form of the public realm.

4.5 Frontage Standards: Standards for private frontages that provide a transition and interface between the street and the building.

4.6 Land Uses: Allowed and restricted land uses.

4.7 Signs: Design standards for signs.

4.8 Other Development Standards: Standards for on-site landscaping, lighting, drive-thru businesses, and solid waste.

4.9 Administration: Responsibility and authority to review and make final decision.

4.10 Glossary: General and land use terms defined.

4.1 Purpose, Design Principles and Application

4.1A Purpose

4.1A.1 Intent

The Arrow Highway Mixed-Use District (AHMUD) Development Code implements the AHMUD's Vision, Goals, and Policies. This Development Code is intended to facilitate the continuing evolution of the Plan Area into a place where:

1. A mixture of land uses including shops, workplaces, and residences are within walking distance of one another and produce an urban form that is inviting, comfortable, safe, and ecologically resilient;
2. Streets and open spaces promote a vibrant, safe, healthy, and prosperous place; and
3. New infill and remodeled buildings work together with existing buildings to define the pedestrian-oriented space of the public streets within the Plan Area, and are harmonious with each other and the desired character of the Plan Area.

4.1A.2 Relationship to the Montclair General Plan

The AHMUD Code is consistent with the Montclair General Plan currently in effect, as required by Government Code 65000, et seq., California Statute. The AHMUD Code implements the General Plan's community supported goal to direct majority of the new growth to districts and corridors in a contextual manner.

The California Government Code Section 65302(c) mandates that each city shall include a Housing Element in its General Plan. The Montclair Housing Element includes an analysis of potential housing sites that could accommodate development of new housing units, to accommodate existing and projected housing needs. All development projects within the AHMUD Code area shall be consistent with the Housing Element Site Analysis.

4.1A.3 Relationship to the Municipal Code

Title 11.14.040 of the Montclair Municipal Code (SDMC) shall be referred to as the "AHMUD Code." The AHMUD Code provides all requirements for development and land use activity within the boundaries identified in Figure 4.1.1. Except as specifically referenced within the AHMUD Code, the Montclair Municipal Code requirements in place are hereby replaced by Title 11.14.040.

4.1B Design Principles

The design principles are a reflection of the City's vision for the AHMUD area as expressed in the Montclair General Plan. They provide guidance for new infill development, encouraging contextual scale, massing, and proportion, while creating an engaging street presence.



The design principles illustrate options, solutions, and techniques to achieve the goal of excellence in infill design. These principles do not supersede zoning regulations or standards in this AHMUD Code or the Municipal Code. Applicants may submit design proposals that are in conflict with these guidelines, but must clearly describe the reasons for the deviation.

4.1B.1 Diverse and Integrated Mix

Complete and diverse neighborhoods accommodate people of every age, race, family dynamic, income, and ability. This requires buildings that support flexible patterns of live-work-play, as opposed to separated and siloed zones. Designs need to respond to the nuances of culture, climate, and place, reinforcing a sense of relevance and identity.

1. Encourage a compatible mix of uses at the neighborhood scale.
2. Identify opportunities for shared uses.



4.1B.2 Curated Street Level Experience

The ground floor provides an opportunity to create visible, eye-level landmarks, making streets more navigable, and reinforcing a sense of place and belonging. A curated ground floor helps to create an attractive environment where people want to live and where businesses want to be.

- a. Privacy, visibility, shelter, shade, or exposure should be appropriately designed for different uses.
- b. A new residential building's formal rooms (living room, dining room, etc.) should help establish its human scale to the street by being placed to face the street.
- c. Buildings should be accessed through prominent public entrances that are visible and directly accessible from the street.
- d. The street level appearance of objects and surfaces and the selection of materials should consider human scale details, craftsmanship, texture, color, and durability. It includes public and private structures and space, street furniture, paving, lighting and signage. Taken together, these elements enhance human comfort, safety and enjoyment of the public or private domain.
- e. Mechanical, electrical, and plumbing systems should be properly integrated within the form of new buildings in a manner that reinforces the building's overall character and quality.
- f. Public utility connection points should be located in a manner that does not interfere with the form of the buildings or their landscape.





4.1B.3 Human-centric Design

New development should be designed to create attractive, comfortable and safe walking environments.

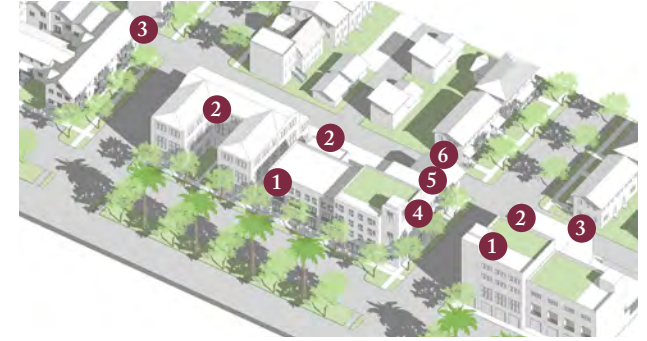
- a. Locate buildings to define street edges and corner.
- b. Buildings should be designed with exterior walls that have windows and doors that are arranged into a definite pattern.
- c. Blank building walls should be avoided.



4.1B.4 Rich Public Realm (streets and open space)

The public realm includes the natural and built environment used by the general public on a day-to-day basis such as streets, plazas, parks, and public infrastructure. Some aspects of privately owned space such as the bulk and scale of buildings, courtyards and entries that are traversed by the public or gardens that are visible from the public realm, can also contribute to the overall sense of place.

- a. Buildings should define and frame adjacent public streets, and/or public or common spaces by matching or complementing adjacent building setbacks, heights, and massing.
- b. The design of commercial and residential front yard landscape should contribute to the creation of a seamless public realm.
- c. New private open spaces should be designed to be inviting, usable, and to properly utilize the space between buildings.



4.1B.5 Context Sensitive Design

The size, shape, and location of buildings as well as the uses contained within them, create "patterns" that define the character of the AHMUD area.

- a. Infill development along the Corridors should be compatible with the pattern of its surrounding context. New infill buildings over two stories in height should respect adjacent buildings by responding to their massing, scale, and need for light, natural ventilation, and views. Typically, this can be accomplished by matching or exceeding existing building heights by only one story. Additional stories should be modulated by setbacks.
- b. Modulating side and rear yard volumes to provide as much distance as possible between the facades of a proposed building and existing buildings in order to preserve the privacy of the outdoor spaces of both.
- c. Orienting the side yards of proposed buildings to face the side yards of adjacent buildings.
- d. Including elements such as porches, galleries, arcades, etc. to relate the scale of facades to those of existing buildings.
- e. Reducing building bulk by introducing dwelling space in attic spaces.
- f. Introducing landscape and/or trees as a screen between existing and new buildings.

4.1C Use and Applicability

4.1C.1 Use of AHMUD Code

Responsibility for Administration: The Montclair Planning Division shall be the primary body responsible for administering the AHMUD Code subject to the provisions of Article 11.

4.1C.2 Applicability to Municipal Code

Property, including structures, land uses and physical improvements such as signs, landscaping, and lighting within the boundaries of the AHMUD Code (Figure 4.1.1) shall comply with all applicable requirements of the AHMUD Code as follows:

- a. **Zoning District(s) Requirements:** All property subject to the AHMUD Code shall comply with the relevant requirements of the applicable zoning district(s).
- b. **Conflicting Requirements:** Where a requirement exists for the same topic, in both the AHMUD Code and other sections of the Montclair Municipal Code, the requirement of the AHMUD Code shall prevail unless otherwise specified.
- c. **Permit Approval Requirements:** Each structure and land use shall be constructed, reconstructed, enlarged, altered, or replaced in compliance with the following requirements:
 - i. **Allowable use:** The land use must be allowed in the zoning district where the site is located (see Table 4.7.1).
 - ii. **Permit and approval requirements:** Any and all permits or other approvals required by the AHMUD Code shall be obtained before the issuance of any required grading, building, or other construction permit, and before the proposed structure is constructed and land use established or otherwise put in operation.
 - iii. **Development standards and conditions of approval:** Each land use and structure shall comply with the applicable development standards of the AHMUD Code for the zoning district in which the site is located.
- d. **Improvements, Modifications, and/or Expansions of Sites or Structures**
 - i. **Improvements to Conforming Structures or Sites:** Applicants that involve an improvement to a conforming site, an existing conforming structure(s), or a modification/expansion of an existing conforming structure(s), sign(s) are subject to the applicable requirements of the AHMUD Code.
 - ii. **Civic Buildings:** Applications that involve a civic building as defined by the AHMUD Code shall comply with Section 4.1C.2g.

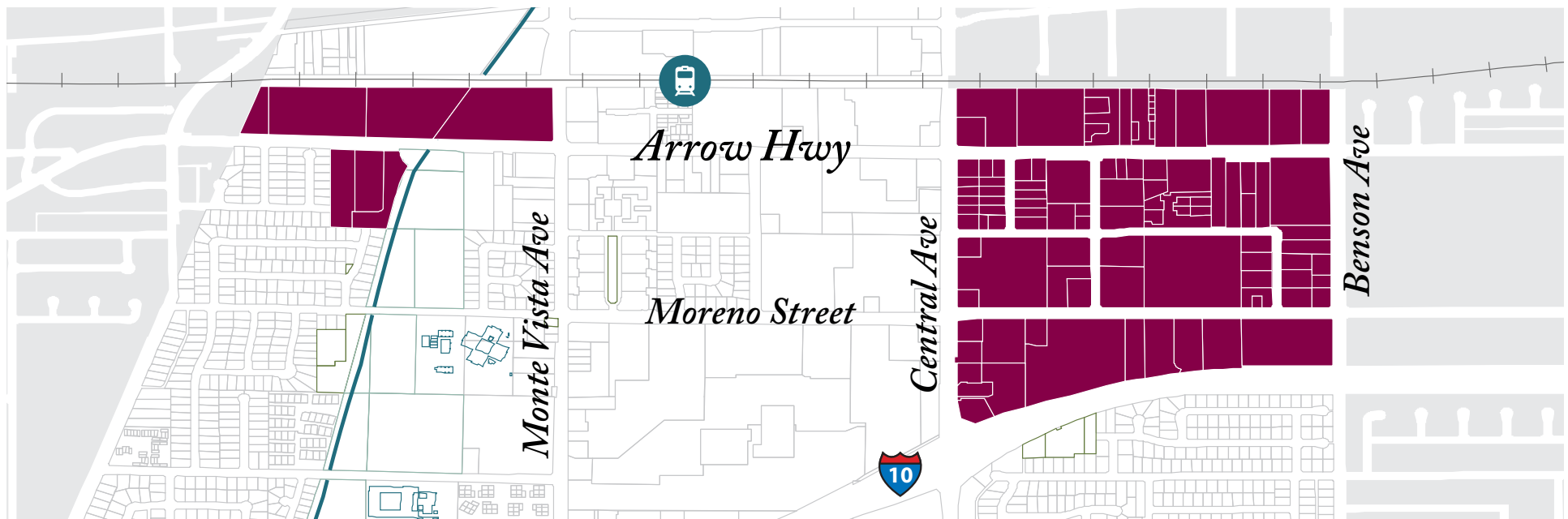


Figure 4.1.1: AHMUD Boundary Map

78 Arrow Highway Mixed-Use District **Specific Plan**

- e. **Modification or Subdivision of Existing Parcel(s):** Applications involving the modification of an existing parcel or the subdivision of an existing parcel shall comply with the applicable requirements of the applicable zoning district(s) and Section 4.3 Public Realm (Subdivision, Streets, and Block Standards).
- f. **Approved Entitlements:** Entitlements approved prior to the adoption of the AHMUD Code that have yet to be constructed are not subject to the AHMUD Code. The entitlement(s) may not be extended beyond the four-year approval period and upon expiration, the owner must demonstrate substantial completion in constructing the approved structure(s) or the property shall then comply with the AHMUD Code. Substantial completion means at least 90 percent completion of the approved structure(s) as determined by the City of Montclair Building Official.
- g. **Civic Buildings:** Applications involving the modification or construction of a building for civic purposes as defined in Section 10 “Glossary,” shall comply with the following:
 - i. Section 4.4 Development Standards and Section 4.5 Frontage Standards are not required of civic buildings.
 - ii. All other sections of the AHMUD Code apply.

4.1C.3 Transitional Lots, Uses, and Structures

- a. **Intent:** The regulations of this section are intended to:
 - i. Recognize the interests of property owners in continuing to use their property for uses that were lawfully established.
 - ii. Promote maintenance, reuse, and rehabilitation of existing buildings.
 - iii. Place reasonable limits on nonconformities that have the potential to adversely affect adjacent properties.
- b. **Transitional Lots:** Lots which are non-conforming as to lot area or lot width may be adjusted or subdivided provided the resulting re-configuration brings the non-conforming lot into, or closer to conformity with the requirements of current applicable regulations.

- c. **Transitional Use:** Transitional uses are those that were legally established but that are not currently listed as permitted or conditionally permitted in the zoning district in which they are located or those that currently require a conditional use permit but at the time of their initiation did not require a conditional use permit. The following provisions shall apply to legal transitional uses:
 - i. No transitional use shall be expanded or moved in whole or part to any portion of the lot or parcel upon which it is located or other structure other than that occupied by such use at the time it was established.
 - ii. No transitional use shall be changed to a different transitional use.
 - iii. If a transitional use is discontinued or abandoned for a continuous period of 180 days or more, such use shall be deemed to have lost its transitional status and shall not be reestablished.
- d. **Transitional Structures:**
 - i. **Additions:** A transitional building or other structure may be added to, provided that an addition of 50 percent or more of the floor area existing as of the adoption of this AHMUD Code shall trigger compliance with all AHMUD Code provisions for the portion of the building or structure comprising the addition.
 - ii. **Repairs and Maintenance:** General non-structural repairs and normal maintenance necessary to keep a nonconformity in sound condition are permitted. Structural repairs to a legal transitional structure may be authorized by the Building Official if it is determined that said repairs are necessary to protect the health and safety of the occupants, public at-large, or adjacent property and the cost does not exceed 50 percent of the replacement cost of the legal transitional structure. Structural improvements required to strengthen unreinforced masonry structures in compliance with seismic safety standards shall be permitted without replacement cost limitations.
 - iii. **Replacement of Transitional Structures:** Any transitional structure that is involuntarily damaged by fire or other catastrophic event may be restored or reconstructed to its original condition provided that the cost of such restoration/reconstruction does not exceed 50 percent of the replacement value of the structure and that the restoration shall commence within one year from the date the damage occurred.

4.2 Zones and Regulating Plan

4.2A Zoning Districts and Overlays

4.2A.1 Purpose and Establishment of Zoning Districts and Overlays

This section establishes the zoning districts and overlays to implement the AHMUD Specific Plan for property and rights-of-way within the AHMUD Code boundaries. Property and rights-of-way subject to the AHMUD Code shall be divided into the following zones and overlays, which shall be applied to all property as shown on Figure 4.2.1.

4.2A.2 Zoning Districts and Overlays

The AHMUD Code is consistent with the Montclair General Plan currently in effect, as required by Government Code 65000, et seq., California Statute. The AHMUD Code implements the General Plan's community supported goal to direct majority of the new growth to districts and corridors in a contextual manner.

4.2B Regulating Plan

4.2B.1 Purpose and Establishment of Regulating Plan

This section establishes the regulating plan, Figure 4.2.1, as the map that identifies and implements the various intentions and principles of the AHMUD Plan. Table 4.2.1 defines the zoning districts, overlays and standards for site development, design and land use through the following:

- a **Zoning Districts:** Each zoning district is allocated standards in the following areas:
 - Building Placement
 - Allowed Development Standards
 - Allowed Frontage Types
 - Building Height and Size
 - Allowed Encroachments into Required Yards
 - Parking Placement and Site Access
 - Required Parking
 - Allowed Land Uses

- b **Shopfront Frontage Overlay:** This overlay requires buildings to have shopfront frontage and a minimum floor-to-ceiling height. This requirement is to accommodate ground floor live-work, commercial, retail or other such non-residential activity on streets where the vision expects active, pedestrian-oriented streetscapes.
- c **Regulating Plan Diagram:** Each zoning district and overlay established by the AHMUD Code is identified on Figure 4.2.1 to show the boundaries of each zoning districts, overlay, and the parcels within each boundary. Figure 4.2.1 is established as the zoning atlas for all property within the AHMUD Code boundaries.
- d **Central Park:** The AHMUD Specific Plan envisions the creation of a new public park (i.e., Central Park) in the eastern portion of the Plan Area. The AHMUD Regulating Plan identifies a centralized location for the park adjacent to and in support of future mixed-use residential development.

While the park location represents an ideal location for the plan, it is not intended to be an permanent fixed location. The AHMUD recognizes the proposed location may be affected by the future unavailability of land, actualized development patterns, and open space requirements in the area. As an alternative, the City may consider options for the development of park area(s) that result in a new location or the creation of smaller-sized public parks distributed throughout the east portion of the Plan Area. New public park locations are intended to be flexible and may be a requirement for future development projects or as standalone municipal improvements so long as the total amount of the smaller park areas is approximately the same size as the Central Park identified in the AHMUD Regulating Plan (approximately 4 acres) and located in the eastern portion of the Plan Area.

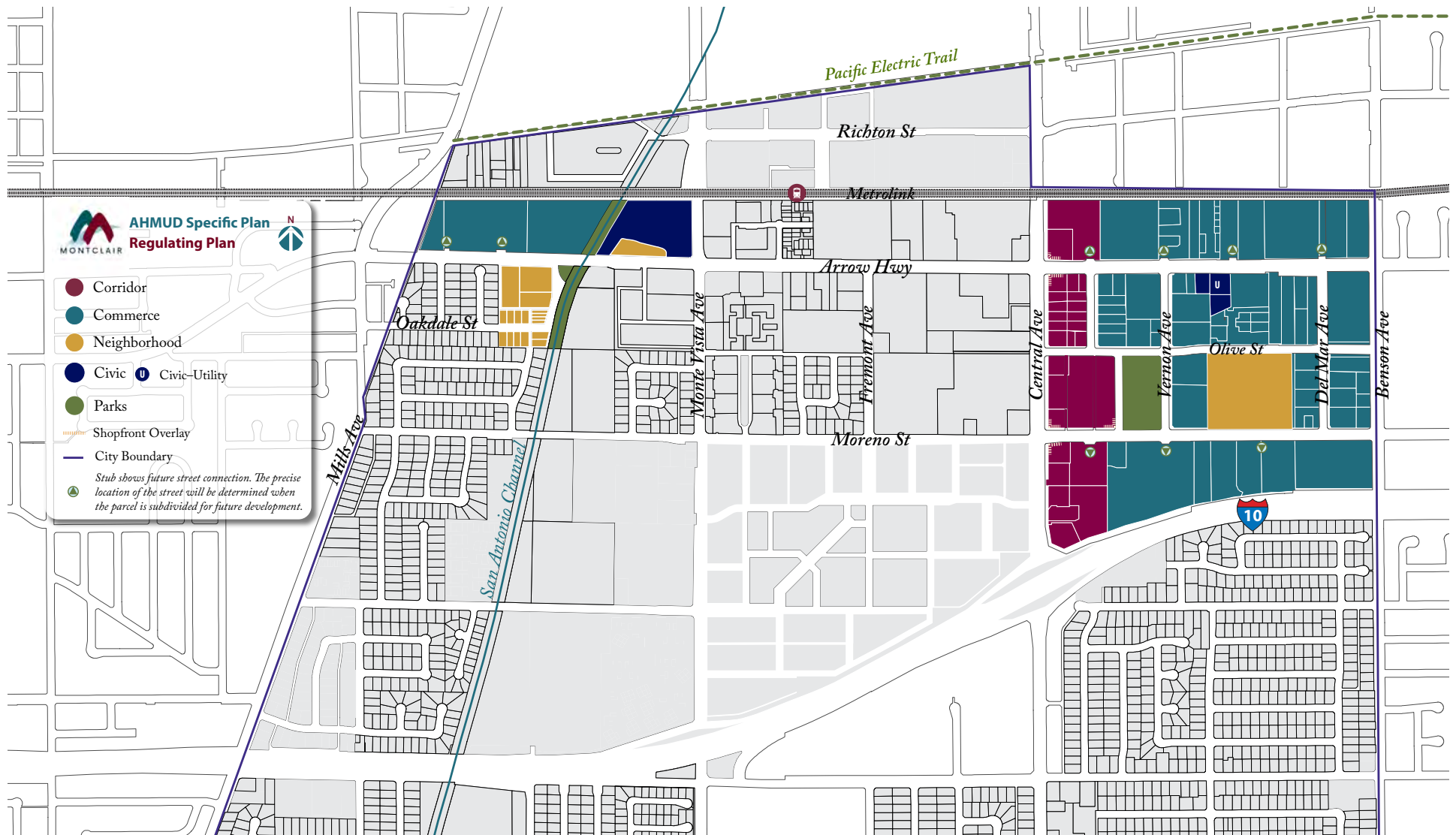


Figure 4.2.1: Regulating Plan




Zoning District Intent	Corridor	Commerce	Neighborhood
Desired Form			
Intensity Streetscape and Public Realm Improvements	<p>New buildings are block scale, up to four floors (50 feet in height), buildings close to property line, and active ground floor commercial activities on key streets.</p> <p>40 to 85 units per acre.</p> <p>Active streetscapes on Central Avenue provide continuity with adjacent areas. Commercial frontages such as shopfronts, arcades, or galleries; wide sidewalks; and street trees encourage interesting, safe, and comfortable walking environment.</p>	<p>New buildings are block scale, up to 3 floors (40 feet in height), buildings close to property line, and active ground floor commercial activities on key streets.</p> <p>40 to 85 units per acre.</p> <p>Arrow Highway is envisioned to be configured as a grand tree-lined street with commercial frontages, wide sidewalks, four travel lanes, two bicycle lanes and parking on both sides.</p>	<p>New buildings are primarily house scale, up to 35 feet in height, buildings close to property line, and active ground floor commercial activities on key streets.</p> <p>10 to 50 units per acre.</p> <p>Active streetscape providing continuity with adjacent areas. Residential frontages and street trees encourage interesting, safe, and comfortable walking environment.</p>
Parking	<p>Parking consists of on-site spaces located either behind buildings or in above- or underground parking structure. On-street public parking spaces are provided. Parking ratios are lower due to available transit and shared parking options.</p>	<p>Parking consists of on-site spaces located either behind buildings or in above- or underground parking structure. On-street public parking spaces are provided. Parking ratios are lower due to available transit and shared parking options.</p>	<p>Parking consists of on-site spaces located either behind buildings or in above- or underground parking structures. On-street public parking spaces are provided.</p>
General Use	<p>Buildings are occupied with ground floor retail, cultural, entertainment, and office activity. Upper floors and the floor area behind shopfronts is flexible for a wide variety of office, lodging, or housing uses.</p>	<p>Buildings are occupied with ground floor commercial, retail, and office activity. Upper floors and the floor area behind shopfronts is flexible for a wide variety of office, lodging, and housing uses.</p>	<p>Buildings are occupied with residential uses with live-work uses if desired along key commerce frontages.</p>

Table 4.2.1: Summary Characteristics of Zoning District.

4.3 Public Realm

The public realm is a vital aspect of the built environment that helps to give the Corridors its identity and where civic interaction occurs. It primarily consists of the streets, parks, and other open spaces.

A well designed public realm balances the mobility and access needs for all users and contributes to the efficient functioning of a corridor and its sense of place. The quality of our public realm is at the heart of how we experience and relate to the surrounding environment. Therefore, it is important that the public realm is safe, sustainable, and enriching.

Subdivision shapes the public realm by creating streets, blocks, and open spaces, and sets the tone for development that will follow because the layout and quality of streets greatly influence the future built environment.

4.3A Subdivision

The placement and design of streets, blocks, and open spaces that are fundamental components of public realm are initially established during the subdivision process.

1. Intent

The intent of this section is to promote development patterns that support the creation of complete neighborhoods with a mix of housing, civic, retail, and service choices within a compact, walkable, and transit friendly environment. This section provides a framework for the subdivision of buildable land that meets the following goals:

- a. Provide clear standards for private development that influences the function and character of AHMUD.
- b. Ensure orderly growth and development through the conservation, protection, and proper use of land through adequate provisions for circulation, utilities, and services.
- c. Ensure street connectivity and safety within walkable corridors having numerous route options.
- d. Provide access to open space.
- e. Promote the health, safety, and general welfare of the City.

2. Applicability

- a. The subdivision, consolidation of lots, or lot line adjustment will comply with regulations specified in this Section of the AHMUD Code.
- b. Where a requirement exists for the same topic, in both the AHMUD Code and other sections of the Montclair Municipal Code, the requirement of the AHMUD Code shall prevail unless otherwise specified.
- c. Where the AHMUD Code is silent or does not provide an explicit provision, the Montclair Municipal Code, Title 11.86 — Subdivisions shall apply.

3. Responsible Authority

As development projects and subdivisions are proposed, the Community Development Director and Public Works Director (or designees) shall jointly determine the type and scope of public improvements that shall be required as conditions of project approval and shall recommend such public improvements to the Development Review Committee (DRC) for consideration and approval. The Planning Commission and City Council, as appropriate, shall have final authority to modify these requirements when it is demonstrated by applicant(s) that the project modification would provide the same or greater level of public improvement(s) as intended by purpose and objectives of this policy.

Types of Improvements

Type of Project	Street Trees	Sidewalk	Curb & Gutter	Street widening/ Resurfacing	Dedication of Rights-of-Way	Sewer Improvements	Street Lights	Drainage Improvements	Fire Hydrants	Trash Enclosures	Others as Mitigation Measures	Median Improvements	Signal Construction/ Modifications	Utility Undergrounding
New Single-family Home(s)	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	—	—	—	✓
Additions to Single-family Home(s)	≈	≈	≈	≈	≈	≈	≈	≈	≈	—	≈	—	—	—
New Multi-family Home Project	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	✓
New Commercial Buildings	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	✓
Additions of 25% or more to all buildings/ uses, except Single-family	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	✓
New Industrial Buildings	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	✓
New Educational Buildings	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	✓
Parcel Maps	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	≈	≈	≈	✓
Tract Maps	✓	✓	✓	✓	✓	✓	✓	✓	✓	≈	✓	≈	≈	✓
Lot line Adjustments/ Parcel Mergers	<i>As may be required and/or allowed by the Subdivision Map Act [Government Code Section 66412(d) and 66499.35 (a) and (b)]</i>													
Permits	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈
Variances	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈	≈

D. Required Improvements:

✓	Required	≈	Potentially Required Based on Specific Project Impacts	—	Not Required
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Table 4.3.1: Types of Improvements.

Table 4.3.1 outlines the typical type of improvements that are required for a given application. Improvements include new infrastructure and may require the repair of existing infrastructure (e.g., sidewalks, curbs, gutter, pavement, etc.). Depending on the extent of trench work for utility connections, street improvements and other roadway/public right-of-way impacts, requirements could include curb, gutter, and sidewalk reconstruction and resurfacing of the existing pavement within and beyond the limits of the development.

4.3B Blocks

4.3B.1 Purpose

The design of blocks influence the type of street network, building types, and the quality of urban space. Smaller blocks produce more intersections, therefore more human scale buildings, greater public realm and superior street network system. All blocks shall be designed per the applicable requirements identified in Table 4.3.2 to ensure that new and modified blocks result in walkable, interconnected streetscapes aligned by appropriately scaled buildings.

4.3B.2 Applicability

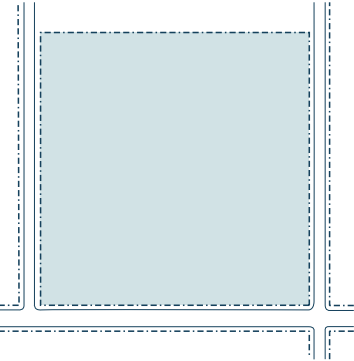
The standards shall apply to all new or modified blocks in the AHMUD area.

4.3B.3 Streetscape Standards

- a New or modified blocks shall front a street (public or private).
- b New or modified blocks shall be subdivided to generate individual lots for buildings or open space as required by the applicable zoning district(s) and overlay(s) and in compliance with Section 4.4 Development Standards.
- c New or modified blocks may be designed in a variety of shapes. Individual block faces and the total block perimeter shall follow the standards established in the Table 4.3.2 below.

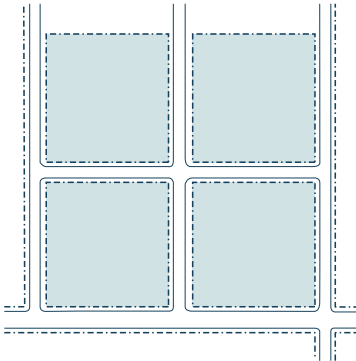
Zone	Face Length	Perimeter Length
Corridor	400 ft. max.	1,600 ft. max.
Commerce	600 ft. max.	1,800 ft. max.
Neighborhood	600 ft. max.	2,000 ft. max.

Table 4.3.2: Block standards.



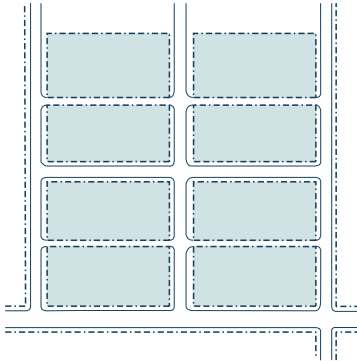
Step 1: Existing Site

Sites that are 4 acres or larger in size or existing blocks with 700 feet of frontage on any side shall be subdivided further to create additional blocks per the requirements of Table 4.3.2.



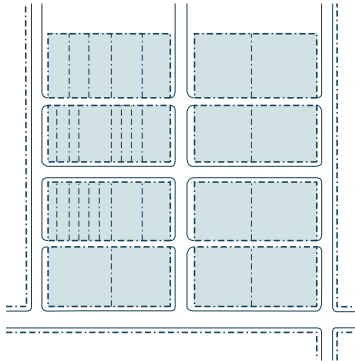
Step 2: Introduce Streets

Sites being subdivided into additional blocks shall introduce streets as allowed in the applicable zoning district(s) and comply with the applicable block-size requirements of Table 4.3.2.



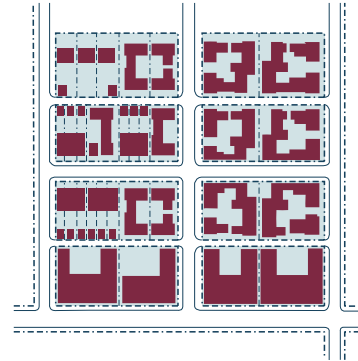
Step 3: Introduce Alleys

Access to new blocks and their individual lots is allowed primarily by alley or, side street, with a certain portion of lots allowed to access the lot via the primary street. The intent of limiting access to the rear or side of lots is to maintain the continuity of the streetscape without interruptions such as driveway access.



Step 4: Introduce Lots

Lots are introduced on each block to correspond with the selected building type(s) allowed by each zoning district.



Step 5: Introduce Projects

Each lot is to receive no more than one main building. The main building is then designed per the requirements of the applicable zoning district along with the allowed frontages and arranged to suit the particular organization of buildings desired for the block.

4.3C Streets

4.3C.1 Intent

The key to transforming the AHMUD into walkable area is the introduction of an interconnected street network with streets that are sized appropriately for intended volume and safe traffic speeds. The principal characteristics and strategies of this kind of network are:

- a. Interconnected streets that serve all modes of travel, including walking, biking, taking transit, or driving.
- b. A diversity of context based street types, each designed according to the adjacent building intensities and uses and each with its own unique character.
- c. Streets that accommodate two-way traffic, facilitating navigation and safe traffic speeds, while eliminating traffic-related fatalities.
- d. On-street parking that provides convenient parking in front of businesses and residences, reduces the amount of required off-street parking, provides a barrier between vehicles in the travel lane and pedestrians on sidewalks, and provide a presence on the street that helps calm vehicular speeds.
- e. Appropriate lane widths, tight curb radii, and narrow street crossings that calm traffic and provide a more comfortable and safe environment for pedestrians, cyclists, and automobiles.
- f. Ample sidewalks, crosswalks, street trees, and generous streetscapes that provide shady, comfortable, and inviting places for pedestrians to walk, while defining the AHMUD's unique and memorable sense of place.
- g. Streets that result in a reduced quantity and improved quality of stormwater runoff, and a diminished heat island effect.
- h. Energy-efficient, pedestrian-oriented lighting that generates an inviting and safe environment for pedestrians, cyclists, and commerce.

4.3C.2 Applicability

- a. The standards in this section apply to all new streets and to the substantial improvement of an entire block length of an existing street, subject to the review and approval of Public Works Director and Fire Chief.
- b. Figure 4.3.1 shows the proposed street and block network applied to the entire Plan Area. Additionally, Figure 4.3.3 shows specific landscape characteristics for each street, differentiating it from other streets within the Plan Area, and contributing to the richness of the Plan Area's sense of place.



Figure 4.3.1: Street Network Plan

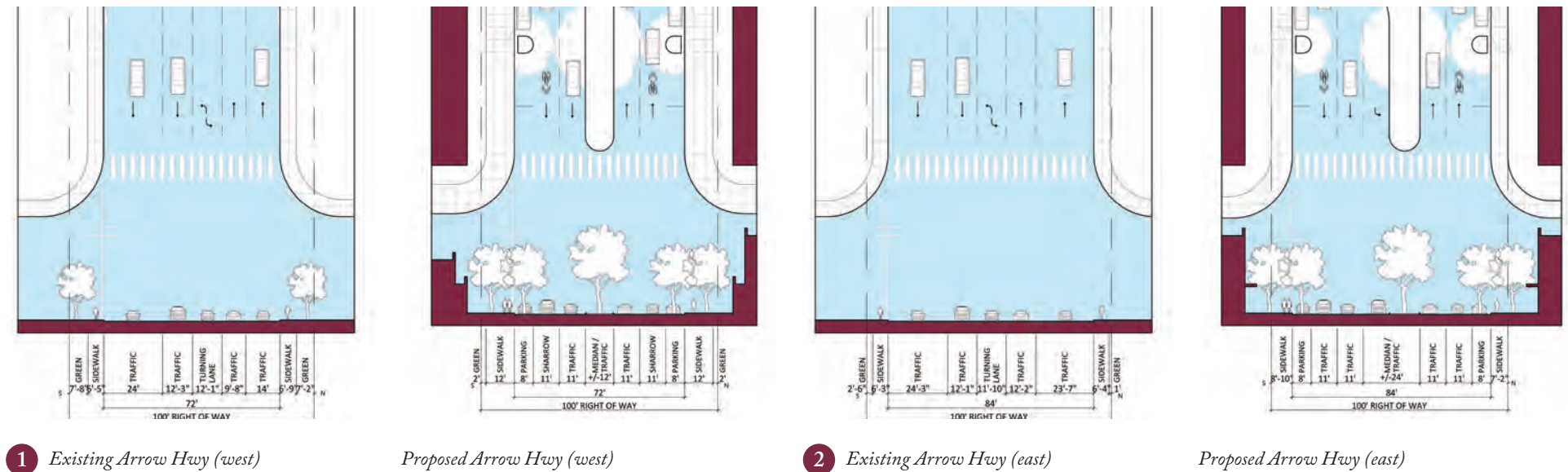
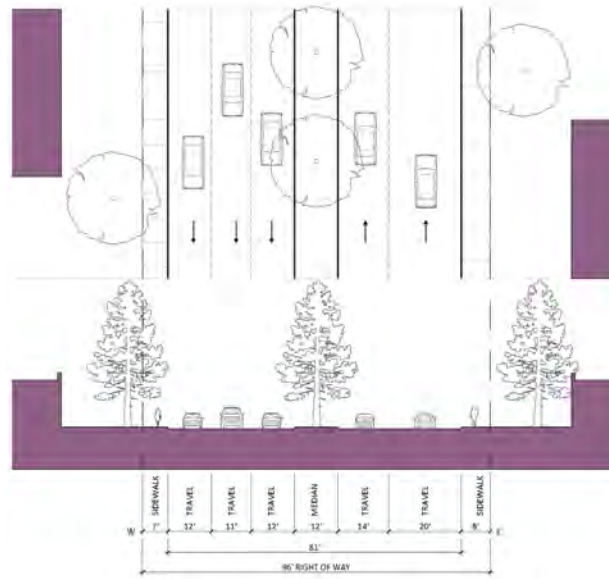
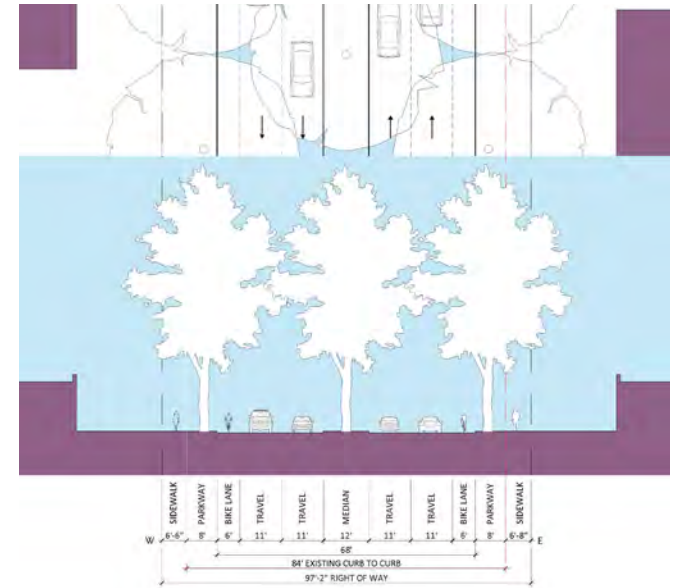


Figure 4.3.2: Existing and Proposed Typical Street Design

North of Moreno Street

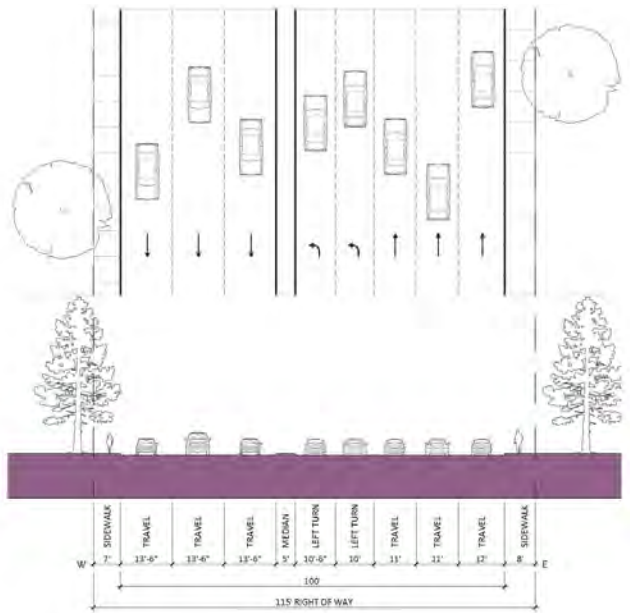


Existing

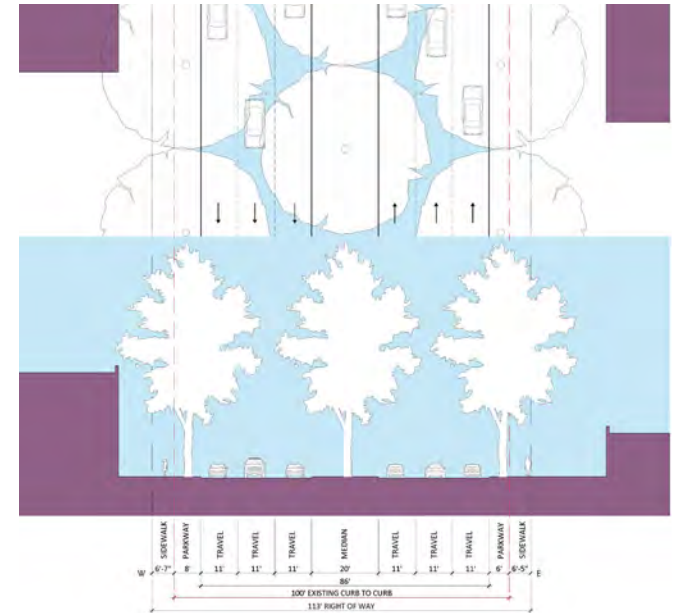


Proposed

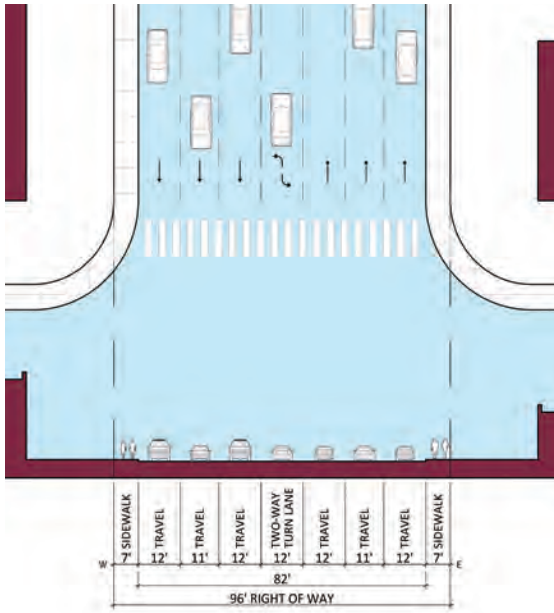
South of Moreno Street



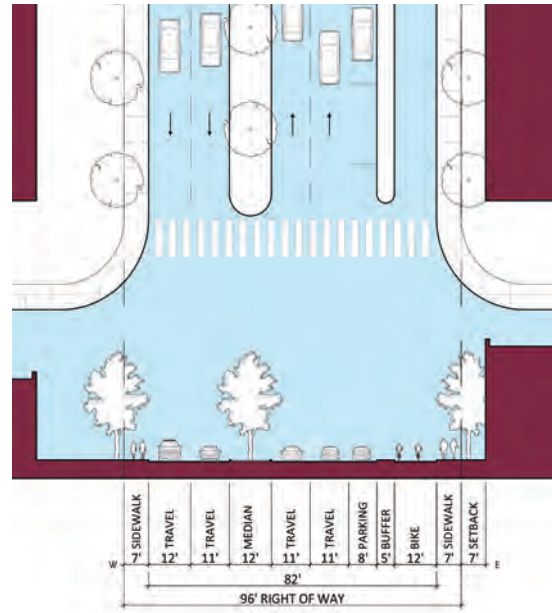
Existing



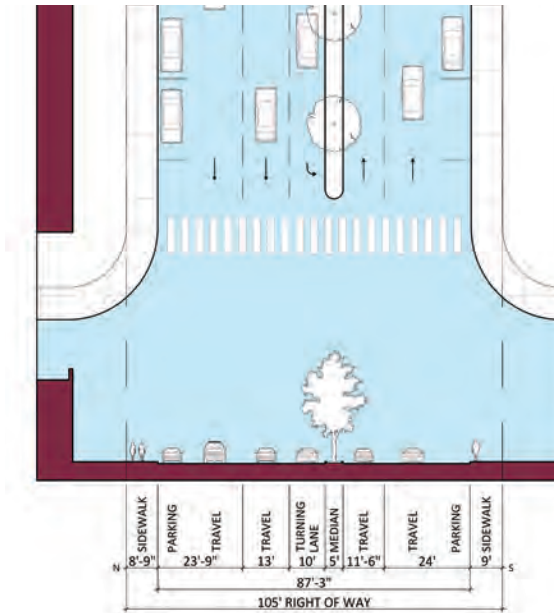
Proposed



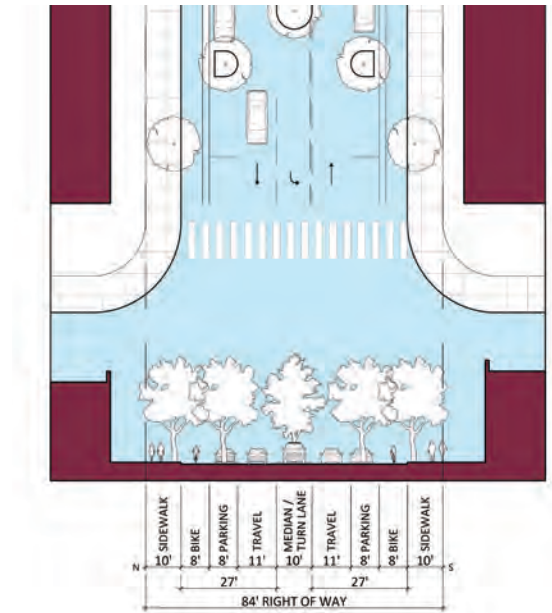
4 Existing Monte Vista Avenue



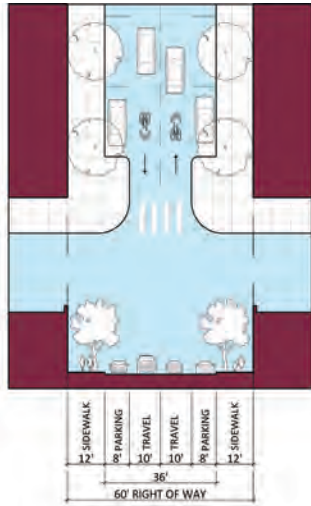
Proposed Monte Vista Avenue



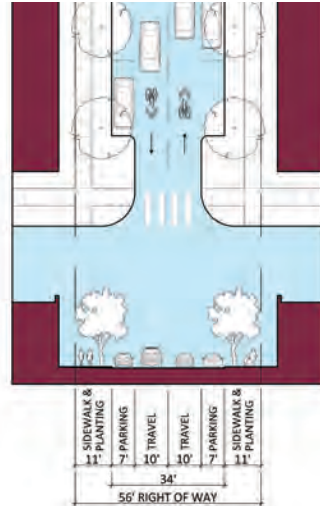
5 Existing Moreno Street



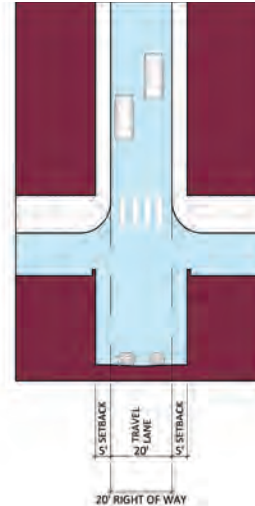
Proposed Moreno Street



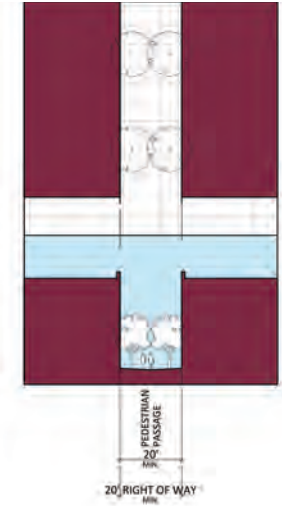
6 Proposed Mixed-Use Retail Street



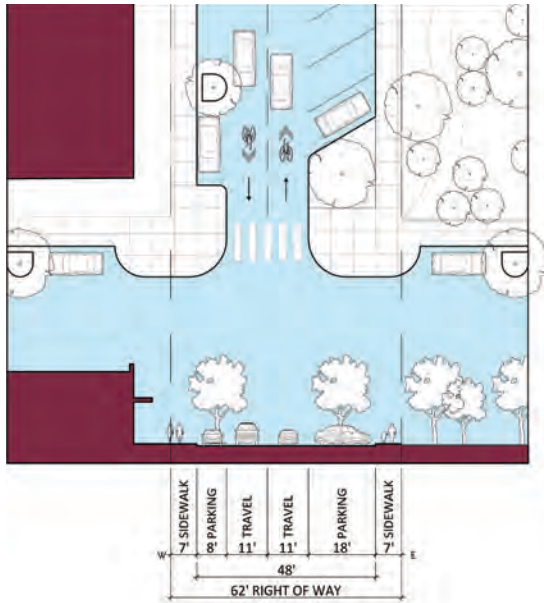
7 Proposed Residential Slow Flow



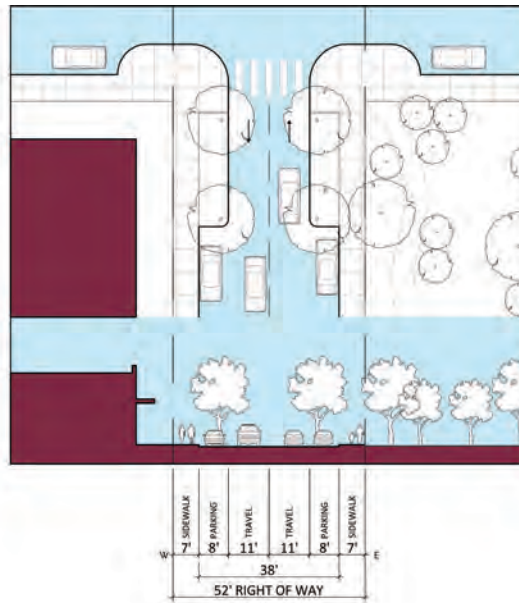
10 Proposed Alley



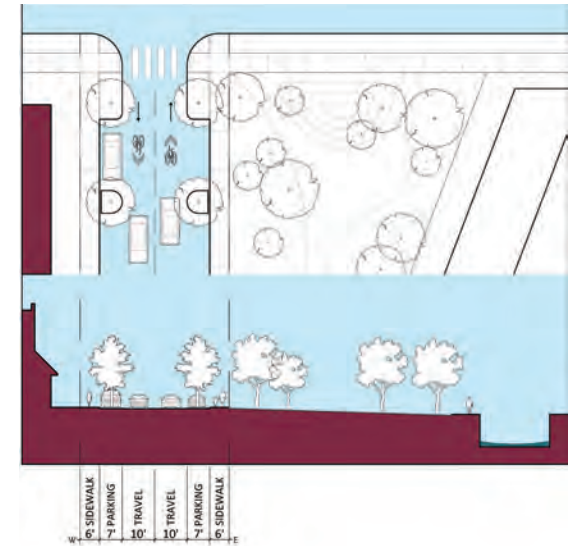
11 Proposed Paseo



8a Proposed Central Park Urban Edge (south)



8b Proposed Central Park Urban Edge (north)



9 Proposed West Park Urban Edge Street

Figure 4.3.2: Existing and Proposed Typical Street Design (continued)

4.3D Street Trees

4.3D.1 Intent

The intent of Street Trees is to provide a comfortable, rich pedestrian experience with shade, planted edges in the public right of way, and increase environmental performance with low impact development strategies that absorb and clean urban runoff.

The Street Tree Master Plan (see Figure 4.3.3) locates specific tree species amongst the public realm's street network, open spaces, and greenways. Tree species are chosen for each type of thoroughfare and open spaces to support its unique aesthetic character, spatial quality, and environmental value. Tree selection considerations included the tree's shape, color, seasonal change, scent, size at maturity, drought tolerance, and resilience.

The numerous qualitative benefits from selecting a diverse street tree palette include shade for pedestrians walking along the sidewalk, a visual rhythm to reduce driving speeds along vehicular streets, and overall balance of the built environment. A street tree's environmental benefits include cooling the immediate area, local air quality improvement through carbon sequestration, and stormwater management which contribute toward pedestrian friendly urban character of Montclair.

The street tree species shown in Figure 4.3.3 (Street Tree Master Plan) are conceptual and may be adjusted subject to Public Works Director and/or Community Development Director approval.

1 Arrow Highway

Median Tree: *Schinus molle*, California Pepper

Parkway "D" Well Planter: *Cercidium* Desert Museum, Palo Verde Desert Museum

Spacing 24 ft Triangular spacing in wide median. For narrow median condition single row at 48 ft, Plant *Bouteloua gracilis* Blonde Ambition below.

Parallel Parking "D" Tree Well. Raised curb planter 6ft X 6ft with 3 ft radius on lane edge curb. Plant with *Cercidium* and decomposed granite under tree. Space at 48 ft on center.



Figure 4.3.3: Street Trees Master Plan

2

Olive Street

Parkway Tree: *Zelkova serrata*, Sawleaf Zelkova

Spacing 30ft on center;
Parkway width 5-6ft



6

Central Avenue

Platanus racemosa,
California Sycamore

Spacing 40 ft on center



3

Moreno Street

Median Tree: *Pinus canariensis*, Canary
Island Pine. Informal
Spacing +/- 30 ft. on
center.

Parkway Tree: *Platanus
racemosa*, California
Sycamore. Formal
Spacing at 40 ft. on
center.



7

**Residential Street at
Linear Park**

Quercus Suber, Cork
Oak

Spacing 30ft



4

Park Street (3 sides)

Jacaranda mimosifolia,
Jacaranda or Mimosa
Tree

Parallel Parking "D"
Tree Well. Raised curb
planter 6ft. by 6 ft. with
3 ft. radius on lane edge
curb. Space at 30 ft on
center.



5

**Typical Street Parkway
Tree**

Lophostemon confertus
(*Tristania conferta*),
Brisbane Box

Afrocarpos falcatus
(*Podocarpus gracilior*),
African Fern Pine

Spacing 30 ft on center



5

**Typical Street Parkway
Tree**

Geijera parvifolia,
Australian Willow

Spacing 30 ft on center.



5

**Typical Street Parkway
Tree**

Koelreuteria bipinnata,
Chinese flame tree

Spacing 30 ft on center.

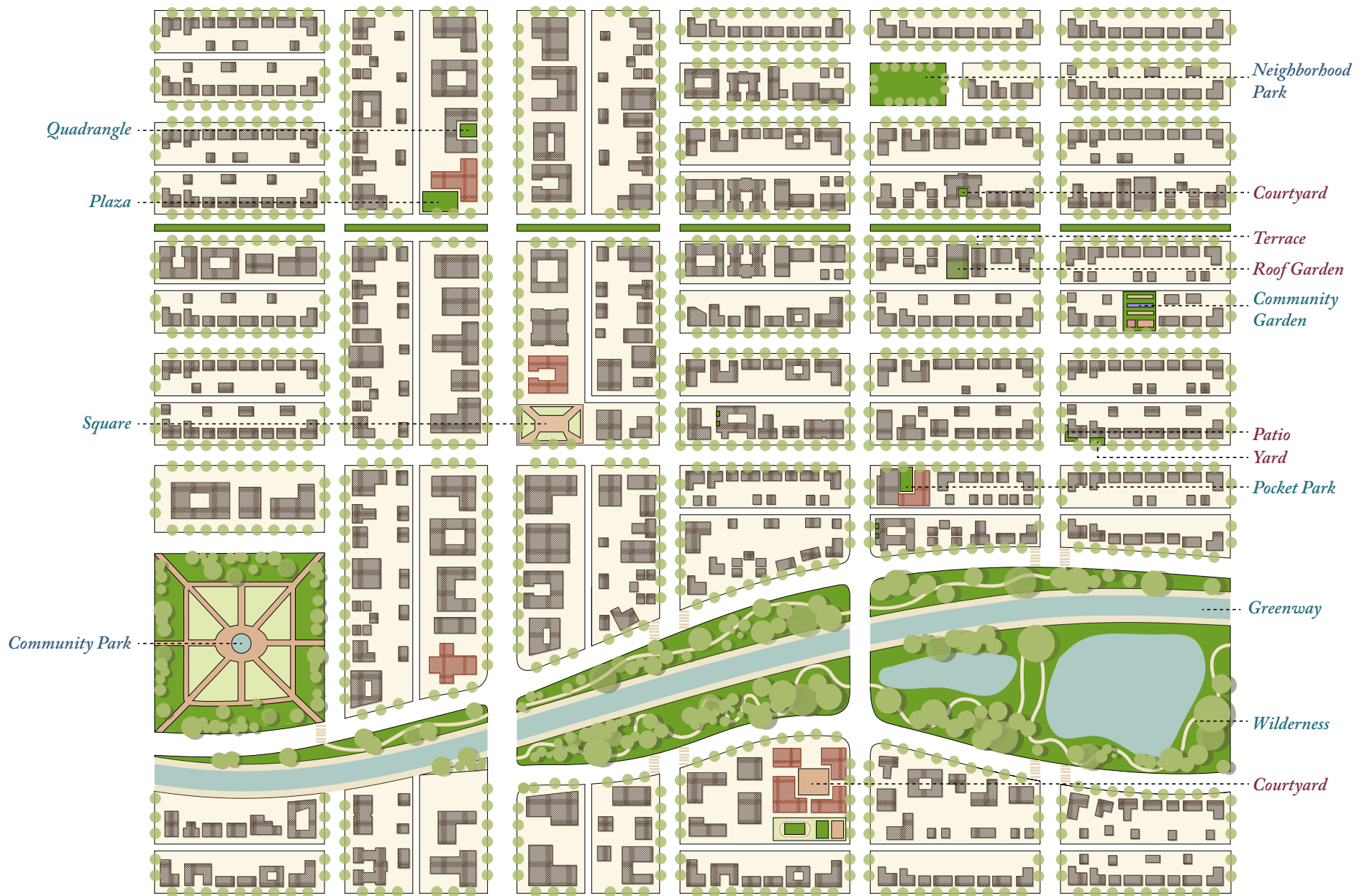


5

**Typical Street Parkway
Tree**

Bauhinia variegata,
Purple Orchid Tree

Spacing 30 ft on center.



The illustrative diagram shows a range of possible parks and open spaces within the public and private realm.

4.3E Parks and Open Space

4.3E.1 Intent

This section establishes a range of open spaces specified based on location and characteristics such as size, landscaping, access, and frontage conditions needed to implement the intended form and character for AHMUD. The Montclair General Plan and AHMUD Specific Plan vision seeks to establish a seamless weave of public and private open spaces at the regional, neighborhood, block, lot, and building level. The Development Standards include private open space requirements for different building types.

The intent of this section is to:

- a. To improve quality of life, instill civic pride, and sense of community.
- b. Provide places for diverse people to meet and interact thereby increasing social capital of the neighborhood.
- c. Increase property values and helps attract and retain knowledge workers and businesses.
- d. Provide venues for many arts and cultural programs which also promotes tourism.
- e. Through informal play and shared experiences with peers, open spaces inspire children to explore, discover, learn and become better stewards of the about natural environment.
- f. Promote walking, biking, and other physical activities essential for fitness and well being and to reduce diabetes, and heart and mental illness.
- g. Reduces the urban heat island effect. Trees in parks and open spaces sequester carbon and other pollutants from the area.

4.3E.2 Applicability

The standards shall apply to all new or modified blocks in the AHMUD area. Additional Public Open Spaces can be integrated into this section as they are approved by the City.





4.3E.3 Park Types

I San Antonio Creek Greenway

a Description

The Greenway is a linear open space intended to provide extensive opportunities for recreation. The Greenway corridor would connect many neighborhoods, existing parks, schools, Pacific Electric Trail and the Metrolink Station. It would also connect the wildlife habitat and reducing the heat island effect.

b Standards

- i **Size:** The greenway would be variable width and span the entire city. In the future, the greenway could become part of a regional trail network.
- ii **Design:**
 - a) The landscape along the greenway shall consist of unobstructed lawns, planting beds, naturalistically arranged trees. Hardscape is minimal in the form of access pathway and peripheral connections.
 - b) Pedestrian amenities such as benches, picnic tables, drinking fountains, exercise equipment, bike racks and playground equipment may be located at appropriate intervals along the greenway.

II Central Park

a Description

Central park incorporates formal and informal areas with focused landscaping and hardscape for civic purposes and commercial activities, spatially defined by building frontages, and fronting Moreno Street. The Central Park spaces are designed and managed to spur interaction, learning, and networking. The Central Park features flexible multi-functional civic structure that can accommodate farmers market, food and craft vendors, etc.; stage for performances for hosting events and festivals; and a mobility hub at the southwest corner provides variety of mobility options. The park is designed to filter storm water from the future urban development surrounding the green space.

b Standards

i Size: 1 to 2 acres

ii Design:

- a) The presence of civic building complements and supports the open spaces, and reinforce the public nature of the space.
- b) A balance of drought-tolerant trees, plants and ground-cover shall provide significant shade.
- c) The Central Park shall be visible from all sides. Pedestrians and motorists alike must be able see through the space to the opposite side.
- d) Furnishings such as benches, chairs, tables and drinking fountains are required.





III Neighborhood Park

a Description

A large green and landscaped area available for unstructured recreation.

b Standards

i Size: 0.5 acre to 1 acre

ii Design:

- a) To encourage maximum use, the open space circulation system shall integrate and connect with the circulation patterns of the neighborhood. The entrances and exits shall be easy to locate from inside and outside the open space. Primary access route through the open space shall be clearly identifiable, well maintained, and well lit.
- b) Visibility is an important design criteria for enhancing people's comfort and security. The open spaces shall be in the line of sight of adjacent land uses and activities to ensure visibility. The frontages shall have active edges such as front doors and windows.
- c) All parking shall be located on-street. The location and design of open space shall facilitate walking and biking to the open spaces.
- d) Dense, visually impenetrable planting creates the opportunity for crime and shall be avoided.
- e) Lighting shall direct movement between destinations.

IV Neighborhood Green

a Description

A small open space within a neighborhood available for passive recreation, spatially defined by building frontages and streets.

b Standards

i Size: 0.25 acre to 0.5 acre

ii Design:

- a) Neighborhood Greens are highly visible and accessible and have as boundaries at least 2 streets. The open spaces shall be in the line of sight of adjacent land uses and activities to ensure visibility. The frontages shall have active edges such as front doors and windows.
- b) All parking shall be located on-street. The location and design of open space shall facilitate walking and biking to the open spaces.
- c) Landscape consists generally of unobstructed lawn, planting beds, and trees arranged naturalistically and providing shade.
- d) Amenities may include pergolas, picnic shelters, flexible gathering and event spaces, benches, chairs, tables (movable recommended) bike racks, playground equipment, and athletic courts and fields.





V Pocket Park

a Description

Pocket parks are urban open spaces on a small-scale and provide a safe and inviting environment for surrounding community members. They also meet a variety of needs and functions, including: small event space, play areas for children, spaces for relaxing or meeting friends, taking lunch breaks, etc.

b Standards

i **Size:** Up to 0.25 acre

ii **Design:**

- a) The parks shall be accessible by both foot and bike.
- b) Pocket parks shall strive to accommodate as many different users as possible, prioritizing the needs of surrounding neighborhoods. Pocket parks may include seating, play or fitness equipment, open lawn space, dog runs, community gardens, ecological/educational displays, and community bulletin boards.
- c) Pocket parks may be designed to serve as stormwater retention and infiltration areas.

VI Private Open Spaces

a Lot Level

Each building type shall prescribe private open space. For example the open space for single dwelling is provided in yards, courtyard buildings includes courts, and flex buildings and towers may provide terraces.

b Within the building

In urban areas where a private yard is not an option, balconies, patios, and roof garden allow residents access to open space, area for small-scale cultivation, recreation, and entertaining.

4.4 Development Standards

4.4A Purpose and Intent

This section establishes standards to ensure that development within the AHMUD area promotes a safe, comfortable, and interesting walkable environment along the street by:

- Placing the buildings in the front and locating the parking behind the building;
- Providing access to adequate public and private parks and open space; and
- Providing contextual building scale and mass, and pedestrian access and visibility that establish a human scale to the street.

4.4B Applicability

All proposed buildings and related improvements shall be designed in compliance with the development standards of this Section for the applicable zone, except for public and institutional buildings, which because of their unique disposition and application are not required to comply with these requirements.

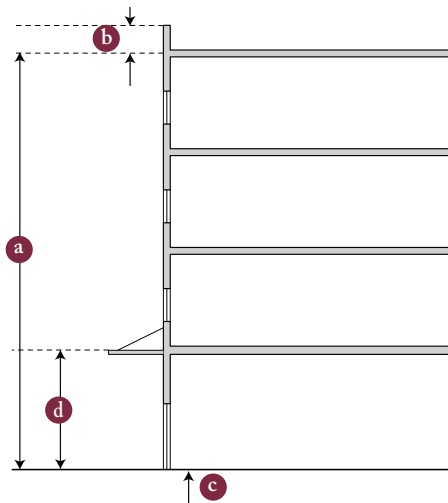
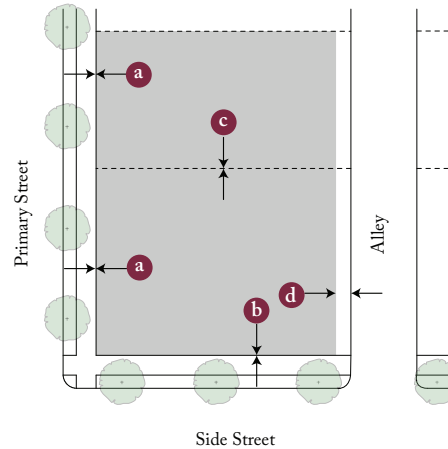
4.4C Corridor

1 Setbacks

- a** Primary Street Setback
 - i Ground Floor non-residential 0 ft. min./5 ft. max.
 - ii Ground Floor residential 5 ft. min./10 ft. max.
- b** Side Street Setback
 - i Ground Floor non-residential 0 ft. min./5 ft. max.
 - ii Ground Floor residential and upper floors 5 ft. min./10 ft. max.
- c** Side Yard Setback
 - i Non-residential 0 ft.
 - ii Residential 8 ft. min.
- d** Rear Setback
 - i With Alley 5 ft. min.
 - ii Without Alley 15 ft. min.

2 Heights

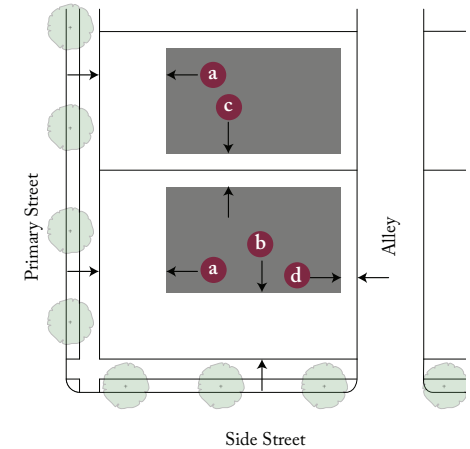
- a** Top of plate height 4 stories/ 55 ft. max.
- b** Parapet height 4 ft. max.
Pitch roof above top of building plate Allowed
- c** Ground Floor above grade
 - i Non-residential 0 ft.
 - ii Residential 3.5 ft. min.
- d** Ground story floor to floor height
 - i Non-residential 15 ft. min.
 - ii Residential 12 ft. min.



3 Parking Placement

On-site parking shall be located as indicated below and shown in the diagram on the right:

- a** Primary Street Setback
 - i Podium/Surface 20 ft. min.
 - ii Subterranean 0 ft.
- b** Side Street Setback
 - i Podium/Surface 20 ft. min.
 - ii Subterranean 0 ft.
- c** Side Yard Setback
 - i Podium/Surface 5 ft. min.
 - ii Subterranean 0 ft.
- d** Rear Setback
 - i Podium/Surface 5 ft. min.
 - ii Subterranean 0 ft.



4 Signs

Allowed Signage Types. A maximum of two of the below sign types are allowed per business. Signs may encroach into Primary Street and Side Street rights-of-way as measured from the building facade as identified below.

Sign Type	Encroachment
a Awning Canopy	Within 2 ft. of curb
b Window Sign	0 ft.
c Projecting Sign	3 ft.
d Wall	Sign Thickness
e Directory/Building ID Sign	Sign Thickness

5 On-site Open Space

- a One (1) or more of the Open Space Types listed below shall be provided on each lot and must be accommodated behind the Primary Street setback line.

Open Space type	Minimum Area	Minimum Dimensions
i Side Garden	15% of total lot area	20 ft. by 20 ft.
ii Courtyard		
iii Back yard		
iv Roof deck		

- b Residential projects without a back yard shall provide for each residential unit a private open space in the form of a balcony, door yard, or yard. Private open space shall have a minimum area of 40 square feet with a minimum width of 5 feet.

6 Building Size and Separation

- a Buildings shall be designed according to the following building length and building separation standards:

i Building length along Primary Street	150 ft. max
ii Building length along Side Street	100 ft. max
iii Building separation along Primary Street	0 ft.

7 Frontage Elements, Encroachments, and Access

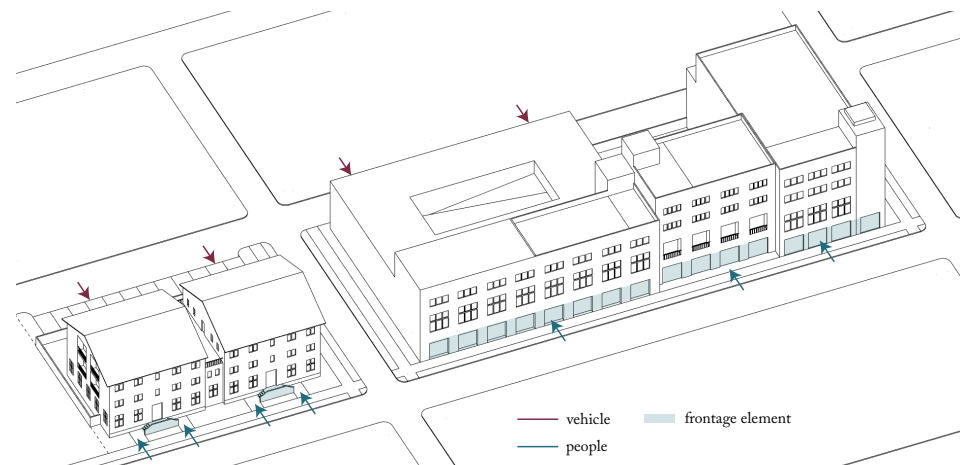
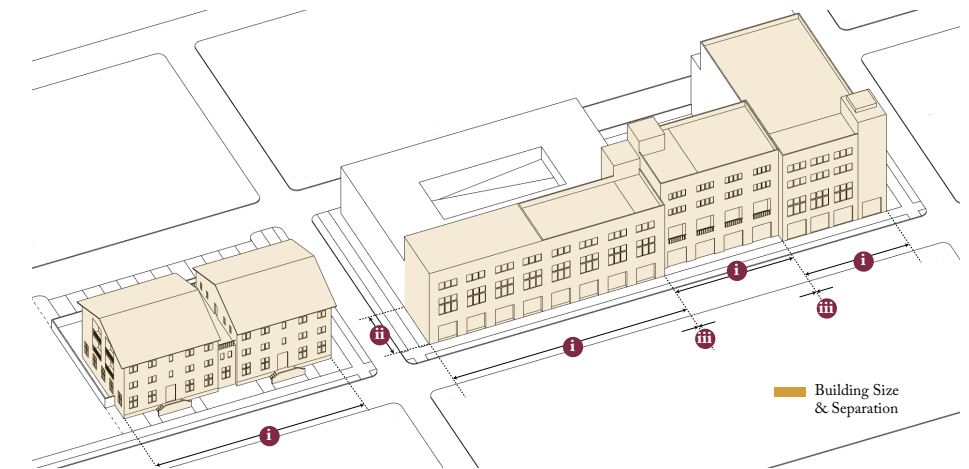
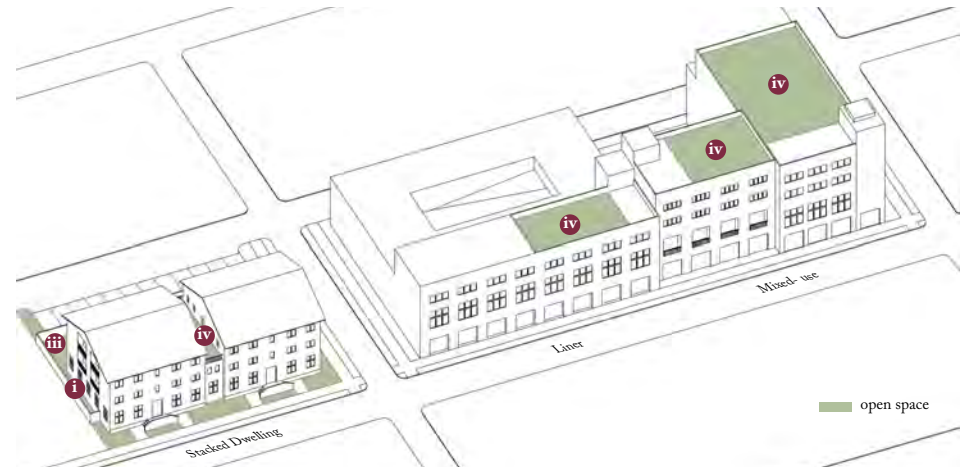
- a Required Frontage Elements: Street- and Court-facing building facades shall provide at least one (1) of the frontage elements listed below. These Frontage Elements may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Arcade, Gallery	To R.O.W. line
ii Shopfront	0 ft.
iii Stoop	5 ft.
iv Door yard	To R.O.W. line

- b Optional Frontage Elements: The following elements may be provided on all Street- and Courtyard-facing facades, and may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Balcony	3 ft. into R.O.W. or setback
ii Bay Window	
iii Awning, Canopy	Within 2 ft. of curb

- c i Pedestrian Access: All ground floors shall be accessed directly from the sidewalk either directly to each unit or commercial space through an appropriate frontage element, lobby, or open space, such as a court. Upper floor uses may be accessed through an exterior or interior stair, or lobby.
- ii Vehicular Access: Parking shall be accessed from an alley. Where an alley is not present, parking/service areas may be accessed from primary street. Driveways shall be located as close to side property line as possible. Parking/service areas for corner lots shall be accessed from side street.



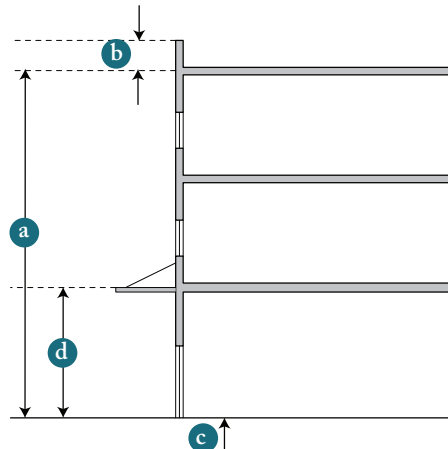
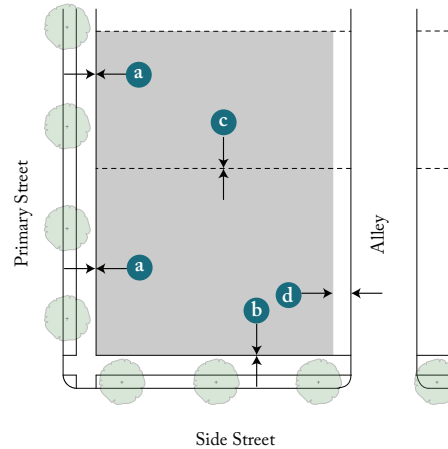
4.4D Commerce

1 Setbacks

- a** Primary Street Setback
 - i Ground Floor non-residential 0 ft. min./5 ft. max.
 - ii Ground Floor residential 10 ft. min./15 ft. max.
- b** Side Street Setback
 - i Ground Floor non-residential 0 ft. min./5 ft. max.
 - ii Ground Floor residential and upper floors 10 ft. min./15 ft. max.
- c** Side Yard Setback
 - i Non-residential 0 ft.
 - ii Residential 8 ft. min.
- d** Rear Setback
 - i With Alley 5 ft. min.
 - ii Without Alley 15 ft. min.

2 Heights

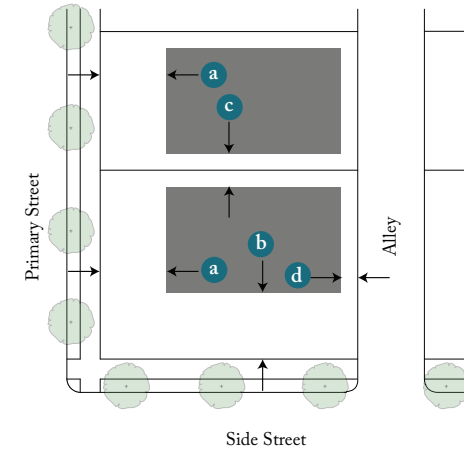
- a** Top of plate height 3 stories/ 45 ft. max.
- b** Parapet height 4 ft. max.
Pitch roof above top of building plate Allowed
- c** Ground Floor above grade
 - i Non-residential 0 ft.
 - ii Residential 3.5 ft. min.
- d** Ground story floor to floor height
 - i Non-residential 15 ft. min.
 - ii Residential 12 ft. min.



3 Parking Placement

On-site parking shall be located as indicated below and shown in the diagram on the right:

- a** Primary Street Setback
 - i Podium/Surface 25 ft. min.
 - ii Subterranean 0 ft.
- b** Side Street Setback
 - i Podium/Surface 20 ft. min.
 - ii Subterranean 0 ft.
- c** Side Yard Setback
 - i Podium/Surface 5 ft. min.
 - ii Subterranean 0 ft.
- d** Rear Setback
 - i Podium/Surface 5 ft. min.
 - ii Subterranean 0 ft.



4 Signs

Allowed Signage Types. A maximum of two of the below sign types are allowed per business. Signs may encroach into Primary Street and Side Street rights-of-way as measured from the building facade as identified below.

Sign Type	Encroachment
a Porch Sign	0 ft.
b Yard Sign	0 ft.
c Awning Canopy	Within 2 ft. of curb
d Window Sign	0 ft.
e Projecting Sign	3 ft.
f Wall	Sign Thickness
g Directory	Sign Thickness

5 On-site Open Space

- a One (1) or more of the Open Space Types listed below shall be provided on each lot and must be accommodated behind the Primary Street setback line.

Open Space type	Minimum Area	Minimum Dimensions
i Side Garden	10% of total lot area	20 ft. by 20 ft.
ii Courtyard		
iii Back yard		
iv Roof deck		

- b Residential projects without a back yard shall provide for each residential unit a private open space in the form of a balcony, door yard, or yard. Private open space shall have a minimum area of 40 square feet with a minimum width of 5 feet.

6 Building Size and Separation

- a Buildings shall be designed according to the following building length and building separation standards:

i Building length along Primary Street	280 ft. max
ii Building length along Side Street	120 ft. max
iii Building separation along Primary Street	15 ft.

7 Frontage Elements, Encroachments, and Access

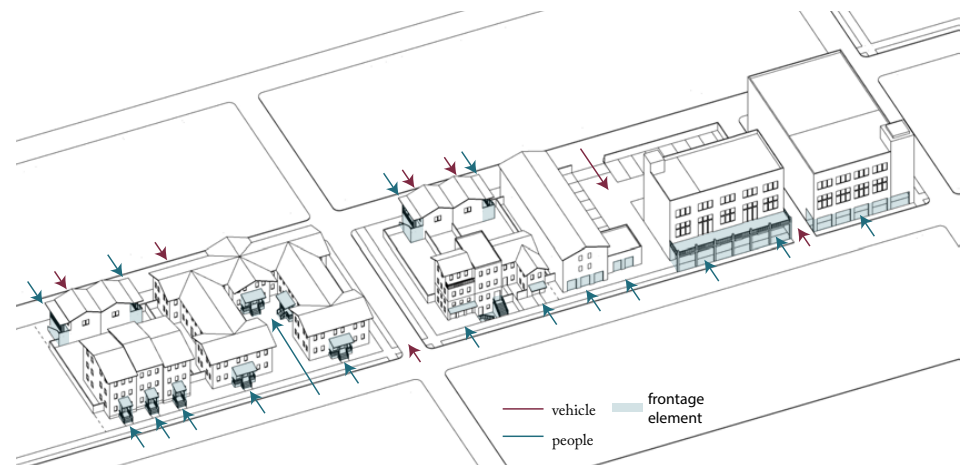
- a Required Frontage Elements: Street- and Court-facing building facades shall provide at least one (1) of the frontage elements listed below. These Frontage Elements may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Arcade, Gallery	To R.O.W. line
ii Shopfront	n/a
iii Stoop	5 ft.
iv Door yard	To R.O.W. line

- b Optional Frontage Elements: The following elements may be provided on all Street- and Courtyard-facing facades, and may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Balcony	3 ft. into R.O.W. or setback
ii Bay Window	
iii Awning, Canopy	Within 2 ft. of curb

- c i Pedestrian Access: All ground floors shall be accessed directly from the sidewalk either directly to each unit or commercial space through an appropriate frontage element, lobby, or open space, such as a court. Upper floor uses may be accessed through an exterior or interior stair, or lobby.
- ii Vehicular Access: Parking shall be accessed from an alley. Where an alley is not present, parking/service areas may be accessed from primary street. Driveways shall be located as close to side property line as possible. Parking/service areas for corner lots shall be accessed from side street.



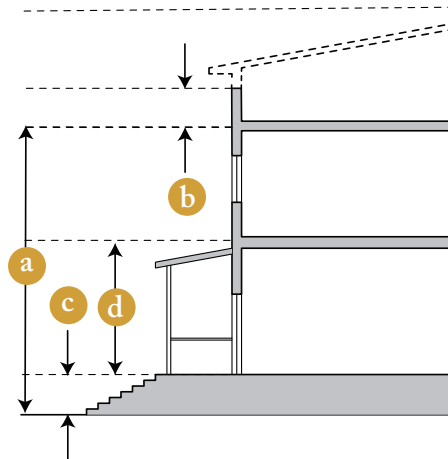
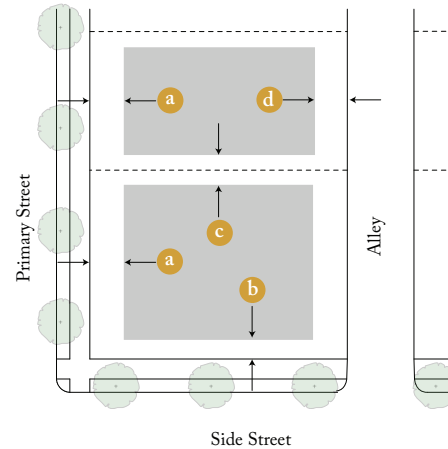
4.4E Neighborhood

1 Setbacks

- a Primary Street Setback**
- i Ground Floor non-residential 10 ft. min./25 ft. max.
 - ii Ground Floor residential 10 ft. min./25 ft. max.
- b Side Street Setback**
- i Ground Floor non-residential 5 ft. min.
 - ii Ground Floor residential and upper floors 10 ft. min.
- c Side Yard Setback**
- i Non-residential 5 ft. min.
 - ii Residential 5 ft. min.
- d Rear Setback**
- i With Alley 10 ft. min.
 - ii Without Alley 15 ft. min.

2 Heights

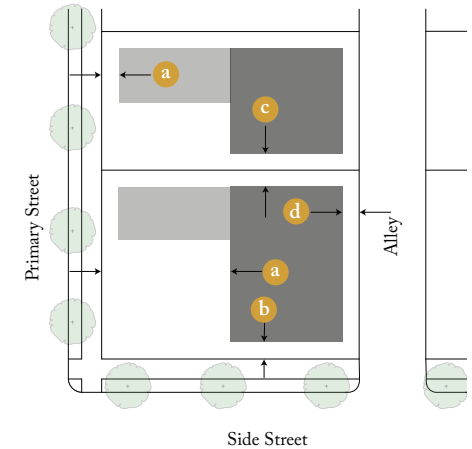
- a Top of plate height** 3 stories/ 40 ft. max.
- b Parapet height** 4 ft. max.
- Pitch roof above top of building plate Allowed
- c Ground Floor above grade**
- i Non-residential 0 ft.
 - ii Residential 3.5 ft. min.
- d Ground story floor to floor height**
- i Non-residential 12 ft. min.
 - ii Residential 10 ft. min.



3 Parking Placement

On-site parking shall be located as indicated below and shown in the diagram on the right:

- a Primary Street Setback**
- i Surface 5 ft. min. for 34% max. of lot width; 50% of lot depth for remaining 66% of lot width; parking prohibited on corners.
 - ii Podium 50% of lot depth
 - iii Subterranean 10 ft.
- b Side Street Setback**
- i Surface 5 ft. min.
 - ii Podium 25 ft. min.
 - iii Subterranean 5 ft. min.
- c Side Yard Setback**
- i Surface 5 ft. min.
 - ii Podium 15 ft. min.
 - ili Subterranean 5 ft. min.
- d Rear Setback**
- i Surface 5 ft. min.
 - ii Podium 20 ft. min.
 - iii Subterranean 5 ft. min.



4 Signs

Allowed Signage Types. A maximum of two of the below sign types are allowed per business. Signs may encroach into Primary Street and Side Street rights-of-way as measured from the building facade as identified below.

Sign Type	Encroachment
a Porch Sign	5 ft. in conjunction with Porch or Stoop
b Yard Sign	Within 3 ft. of R.O.W.
c Awning Canopy	To R.O.W. in conjunction with Awning

5 On-site Open Space

- a One (1) or more of the Open Space Types listed below shall be provided on each lot and must be accommodated behind the Primary Street setback line.

Open Space type	Minimum Area	Minimum Dimensions
i Side Garden		
ii Courtyard	15% of total lot area	20 ft. by 20 ft.
iii Back yard		

- b Residential projects without a back yard shall provide for each residential unit a private open space in the form of a balcony, door yard, or yard. Private open space shall have a minimum area of 40 square feet with a minimum width of 5 feet.

6 Building Size and Separation

- a Buildings shall be designed according to the following building length and building separation standards:

i Building length along Primary Street	60 ft. max.
ii Building length along Side Street	75 ft. max.
iii Building separation must extend entire lot depth or width or lead to Courtyard.	15 ft. min.
iv Building length along rear property line.	150 ft. max.

7 Frontage Elements, Encroachments, and Access

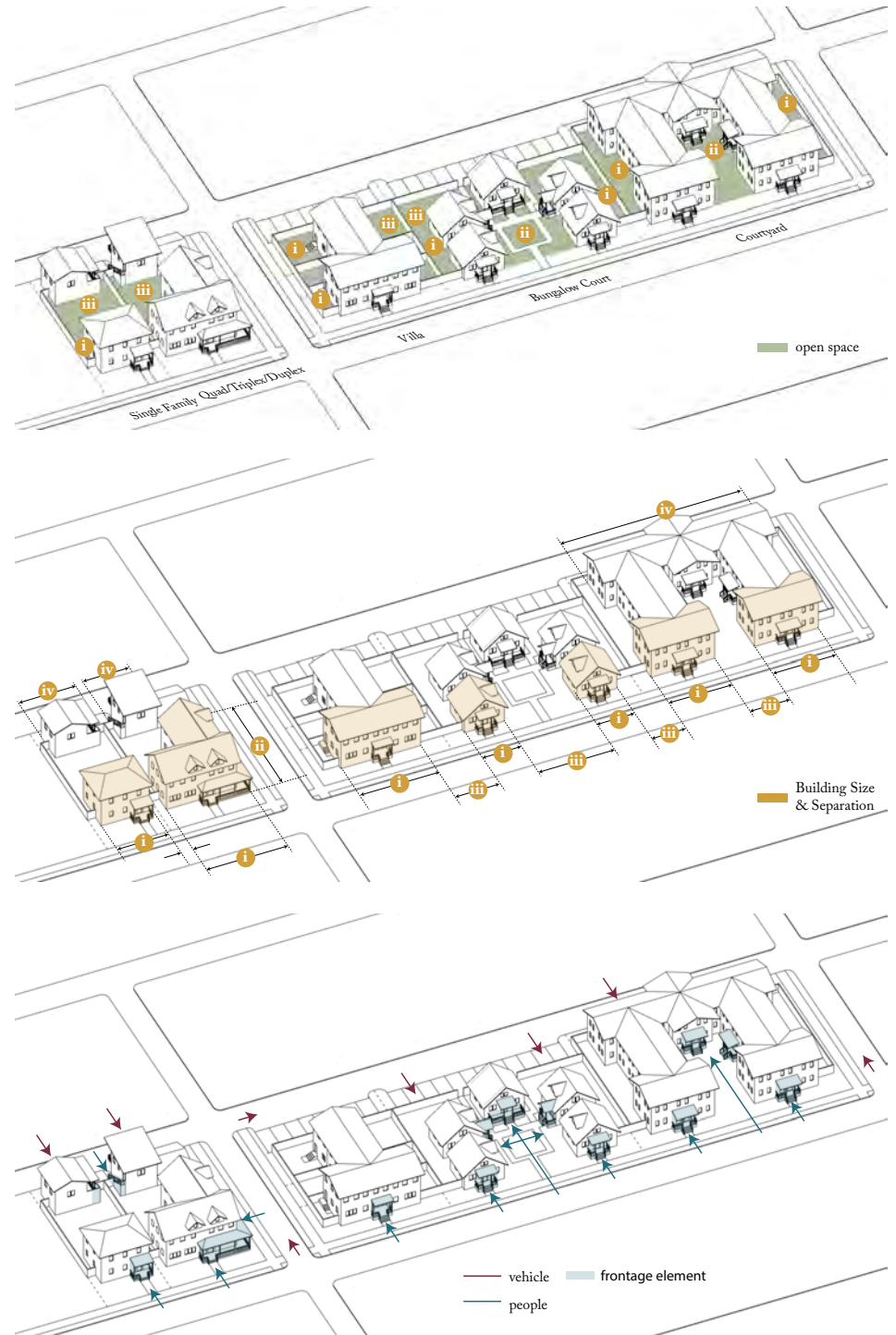
- a Required Frontage Elements: Street- and Court-facing building facades shall provide at least one (1) of the frontage elements listed below. These Frontage Elements may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Shopfront	n/a
ii Stoop	5 ft. max.
iii Door yard	To R.O.W. line
iv Porch	5 ft. max.
i Front yard	n/a

- b Optional Frontage Elements: The following elements may be provided on all Street- and Courtyard-facing facades, and may encroach into the Primary Street and Side Street setbacks or right-of-way as identified below.

i Balcony	3 ft. max.
ii Bay Window	
iii Awning, Canopy	To R.O.W. line

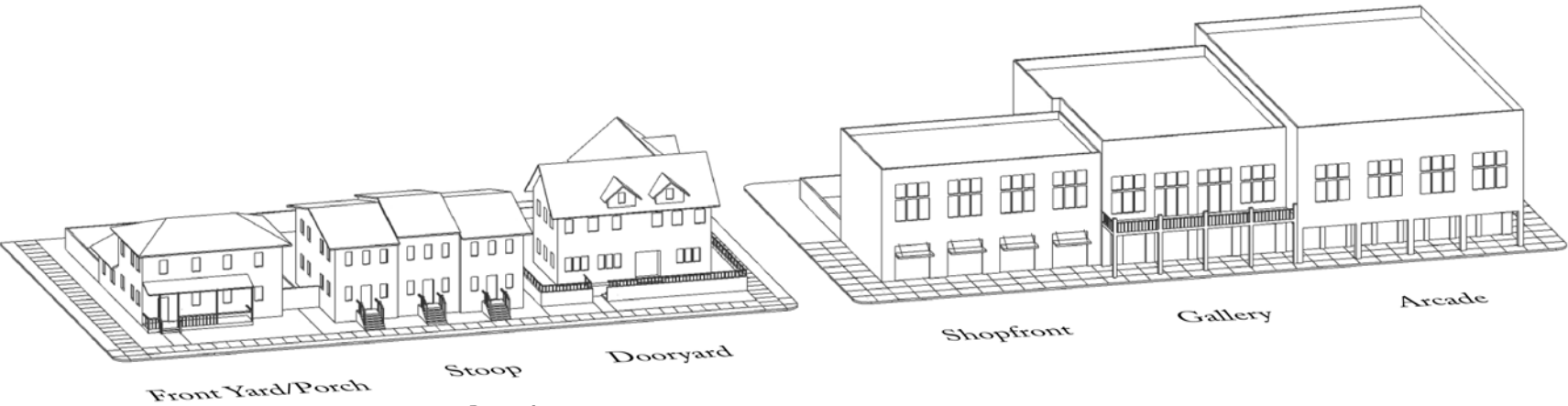
- c i Pedestrian Access: All ground floors shall be accessed directly from the sidewalk either directly to each unit or commercial space through an appropriate frontage element, lobby, or open space, such as a court. Upper floor uses may be accessed through an exterior or interior stair, or lobby.
- ii Vehicular Access: Parking shall be accessed from an alley. Where an alley is not present, parking/service areas may be accessed from primary street. Driveways shall be located as close to side property line as possible. Parking/service areas for corner lots shall be accessed from side street.



4.5 Frontage Standards

4.5A Purpose and Intent

This Section establishes standards to ensure proposed development promotes a walkable environment along the street through the provision of street-facing windows, entries, and appropriately designed frontages; and commercial ground floors that support pedestrian activity through maximum ground floor transparency. Frontages consist of the street facade of the building(s), including any projecting elements, and any landscape, hardscape, walls, and fences between the building facade and the back of sidewalk. They provide a transition between the public environment of the street and the semi-private and private environments of the building interior. They also signal the location of the building entrance and provide a semi-public space within which neighbors and visitors can interact.




4.5B Applicability

The requirements of this Section apply to all new buildings within the AHMUD area.


4.5C Frontage Types

This Section identifies the frontage types allowed within the AHMUD area, and for each type provides a description and design standards to ensure that proposed development is consistent with this Specific Plan’s goals for building form, character, and quality. Frontage types are divided into two categories, Required (Porch, Stoop, Dooryard, Shopfront, Gallery, and Arcade) and Optional (Balcony, Bay Window, Awning, Canopy). The provisions of this Section work in combination with the underlying Zone as identified on the Regulating Plan. Refer to Sections 4.4C, 4.4D, and 4.4E for required and optional frontage types within each zone and allowed encroachments into the setback or right-of-way.


Required




Front Yard




Stoop




Dooryard (non-residential)



Porch




Dooryard (residential)




Shopfront

Optional




Gallery




Balcony




Awning



Arcade



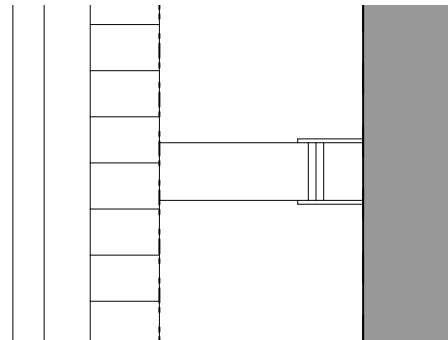
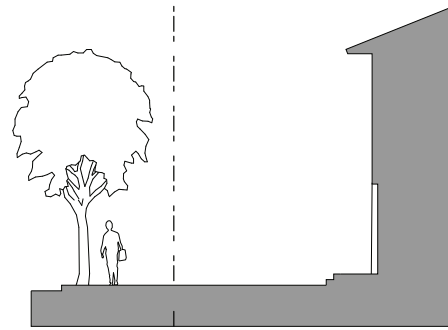
Bay Window



Canopy

4.5Di Front Yard

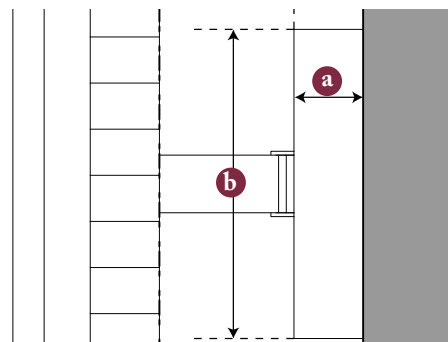
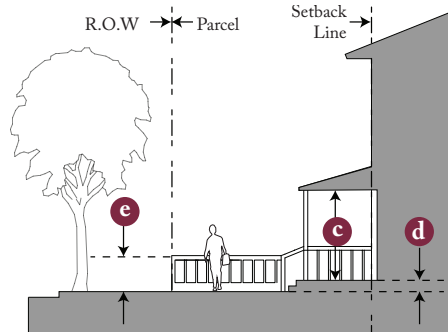
- a) **Description:** The area between the building facade and the property line. Front Yards may be visually continuous with adjacent yards with a common landscape, or enclosed by a low fence, wall, or hedge. On sloping sites, front yards may be raised above the level of the adjacent sidewalk and supported by a low retaining wall at the property line with steps providing access between the sidewalk and the yard. Porches, stoops, balconies, and bay windows may encroach into Front Yards.
- b) **Design Standards**
- Front yards and continuous planters between the curb and sidewalk along the primary and side streets shall be landscaped.
 - Paved areas shall be limited to walks and driveways, where present.
 - Fences defining the front yard shall not exceed four (4) feet as measured from the adjacent sidewalk.

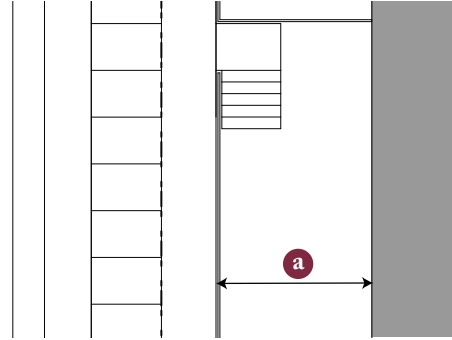


4.5Dii Porch

- a) **Description:** A roofed, unenclosed room attached to the exterior of a building that provides a physical transition between the sidewalk and the building. Porches may be provided on buildings that are set back from the Primary and/or Side Street property lines and may encroach into the front yard and side street yard.
- b) **Design Standards**
- Porches may be enclosed with insect screens if recessed from the exterior wall plane and if visibility is maintained from the sidewalk.
 - Porches shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a) Depth (clear)	6 ft.	-
b) Width (clear)	12 ft.	-
c) Height: ceiling (clear)	8 ft.	12 ft.
d) Height: floor above grade	12 in.	3 ft.
e) Height: fence or hedge	-	3 ft.





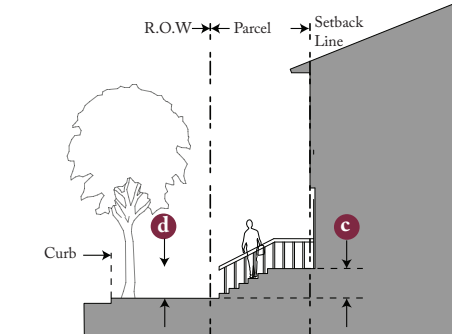
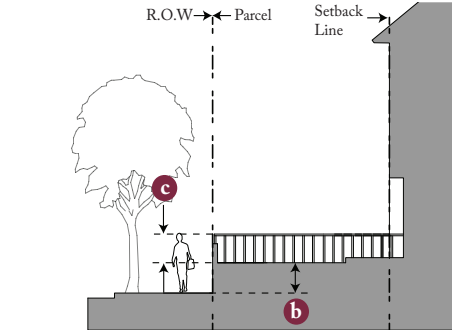
4.5Diii Dooryard

- a) **Description:** An elevated or at-grade garden or terrace that is located in the front yard setback and that is enclosed by a low wall located at or near the property line. For elevated Dooryards, access from the sidewalk to the Dooryard is via a stair or ramp. The Dooryard can accommodate a variety of activities, ranging from dining patios for commercial uses to patios for residential uses. In addition, the interior building spaces are separated from the adjacent sidewalk by the depth of the Dooryard and in the case of raised Dooryards, by the terrace height.

b) Design Standards

- Walls and/or fences shall be consistent with the architectural style of the building in their design, materials, and finishes.
- Dooryards shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a Depth (clear)	7 ft.	-
b Height above grade	0 ft.	3 ft.
c Fence or railings may extend to height required by building code.	-	-



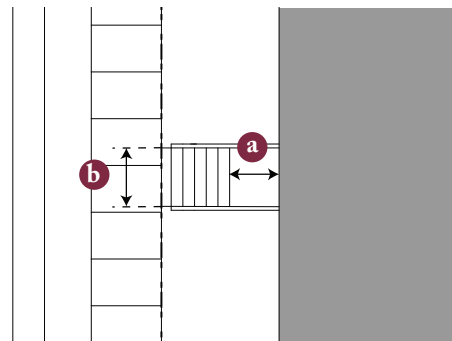
4.5Div Stoop

- a) **Description:** A stair and landing leading directly from the sidewalk to a building entrance. The ground floor of the building is typically raised to provide increased privacy for the rooms facing the public street. Accordingly, stoops are ideal frontage types for ground floor housing that is near the street.

b) Design Standards

- The exterior stairs may be perpendicular or parallel to the adjacent sidewalk. The landing may be covered or uncovered. Landscaping shall be placed on the sides of the stoop.
- Stoops shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a Depth: Landing (clear)	4 ft.	10 ft.
b Width (clear)	4 ft.	10 ft.
c Height: floor above grade	12 in.	3 ft.
d Height: Fence or hedge	-	3 ft.



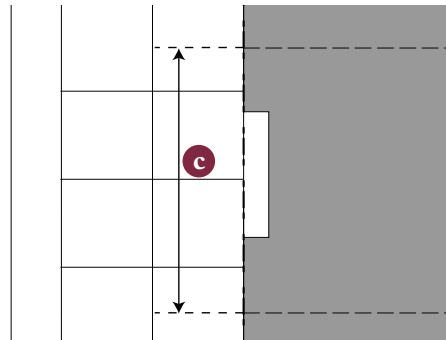
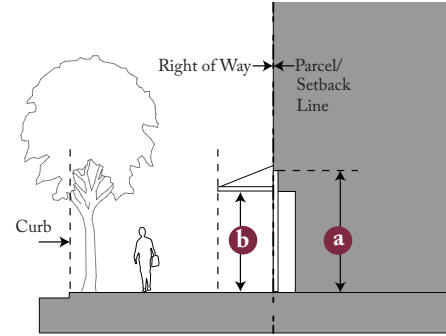
4.5Dv Shopfront

- a) **Description:** Shopfronts are large openings in a building facade that are enclosed with large transparent windows and doors that provide direct, at-grade access to ground floor commercial/retail uses. The basic required architectural elements of shopfronts include large windows, doors with glass, transom windows, and a solid base (bulkhead). Optional elements include awnings, canopies, signage, lighting, and cornices.

b) Design Standards

- Storefront glass shall be clear without reflective coating or dark tinting. Lightly tinted glazing less than 15%, low emissivity, (solar) is acceptable.
- Shopfront shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a Height (clear)	10 ft.	16 ft.
b Distance to bottom of awning	8 ft.	10 ft.
c Width of storefront bay(s)	10 ft.	15 ft.
Glass area % of ground floor wall	70	90



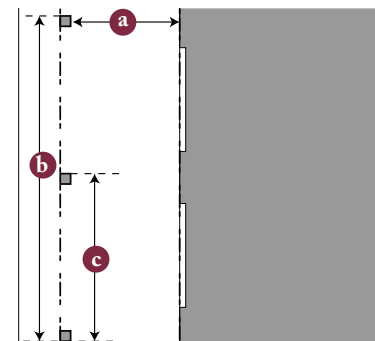
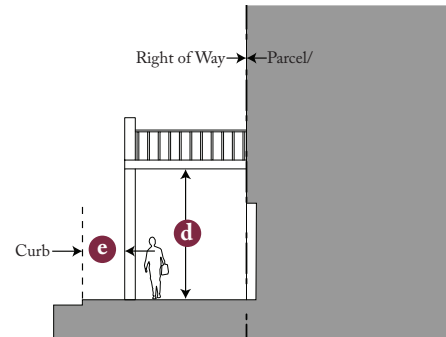
4.5Dvi Gallery

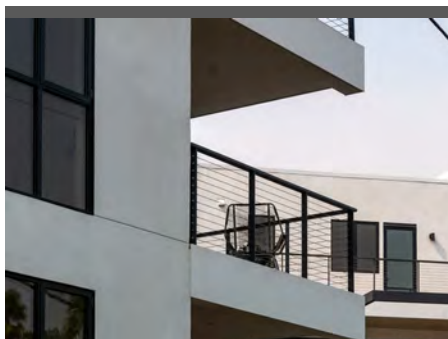
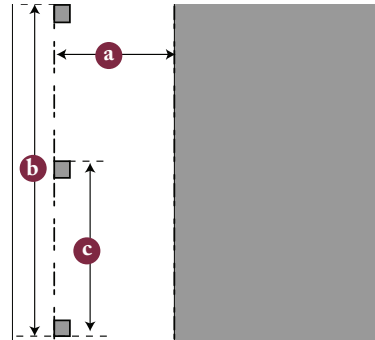
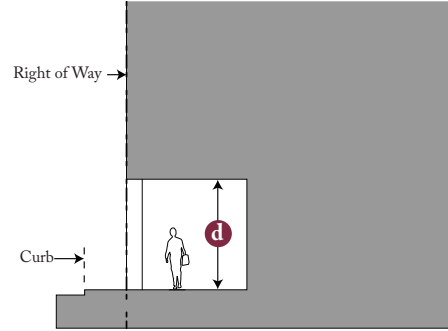
- a) **Description:** Galleries are facades with ground floor colonnades that support a cantilevered shed roof or a deck that covers the sidewalk. Galleries contain ground floor storefronts, making them ideal for retail use. Railing on top of the gallery is only required if the gallery roof is accessible as a deck.

b) Design Standards

- Along primary frontages, the Gallery columns shall correspond to Shopfront openings. Galleries shall be a minimum of two (2) bays wide.
- Galleries shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a Depth (clear)	10 ft.	16 ft.
b Width (clear)	20 ft.	80 ft.
c Width: single bay (clear)	10 ft.	16 ft.
d Height (clear)	12 ft.	16 ft.
e Distance from face of curb (clear)	2 ft.	-





4.5Dvii Arcade

- a) **Description:** Arcades are facades with a ground floor colonnade that supports the upper stories of the building, or for one-story buildings, the roof. Arcades contain ground-floor shopfronts, making them ideal for retail or restaurant use, as the arcade shelters the pedestrian while shading the storefront glass, preventing glare that might obscure views of merchandise.
- b) **Design Standards**
 - i) Along primary frontages, the Arcade shall correspond to Shop-front openings. Arcades shall be a minimum of two (2) bays wide.
 - ii) Arcades shall be designed according to the following dimensional requirements:

Dimensions	Min.	Max.
a Depth (clear)	10 ft.	16 ft.
b Width (clear)	20 ft.	80 ft.
c Width: single bay (clear)	10 ft.	16 ft.
d Height (clear)	12 ft.	16 ft.
e Distance from face of curb (clear)	2 ft.	-

4.5Dviii Balcony

- a) **Description:** A platform enclosed by a wall or balustrade on the outside of a building, with access from an upper-floor door.
- b) **Design Standards**
 - i) Balcony design character, palette of materials, and approach to detailing shall be consistent and compatible with the building's style.
 - ii) Balconies of traditionally styled buildings shall have visible supports in the form of projecting beams or braces. Balcony railings on traditional buildings shall be made of wood, wrought iron, steel bar or tube faithfully simulating true wrought iron. Balconies on modern styled buildings may simply project. Modern buildings may also use galvanized or painted steel, aluminium, and cable railing components. Vinyl substitutes shall be avoided.

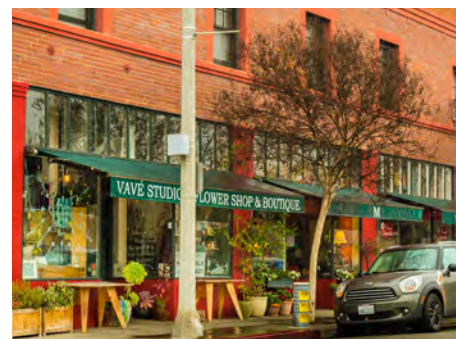
4.5Dix Bay Window

- a) **Description:** A large window or series of windows projecting from the outer wall of a building and forming a recess within.
- b) **Design Standards**
 - i) Bay windows shall be a maximum of ten (10) feet wide and shall have a height that is equal to or greater than their width.
 - ii) Bay windows shall be placed a minimum of two (2) feet from any building corner and a minimum of three (3) from any other bay window.
 - iii) Bay windows shall consist of at least 75% transparent fenestration.



4.5Dx Awning

- a) **Description:** A cloth cover fastened to a building or structure and supported by a frame that is used to protect pedestrians from the sun or rain.
- b) **Design Standards**
 - i) Awnings shall be of a simple shed form, made of code-compliant fire-resistant canvas or materials of similar appearance; materials which are shiny, stiff, and synthetic shall be avoided.
 - ii) Each awning shall correspond to a storefront.
 - iii) Upper floor awnings may be provided. Each individual awning shall cover no more than one Shopfront or window opening. Signage is not permitted on upper floor awnings.
 - iv) Awnings shall be positioned so that signage is not obstructed.
 - v) Awning color(s) and pattern(s) shall be coordinated with the overall design of the building. As appropriate awning colors may vary between adjacent businesses.
 - vi) Internally illuminated and backlit awnings and any associated light diffusers are prohibited.





4.5Dxi Canopy

- a) **Description:** A flat, metal awning that extends perpendicular from the building and parallel to the sidewalk. The canopy's frame is connected to the wall and spanned by a variety of materials, including metal lattice, glass, and metal slats.
- b) **Design Standards**
 - i) Canopies shall be of a simple form, made of metal and/or glass.
 - ii) Canopies shall be supported either from below by metal brackets, or from above by rods, wires, or chains that affix to the wall.
 - iii) Canopies may extend across multiple storefronts.
 - iv) Upper floor awnings may be provided. Each individual canopy shall cover no more than one window opening. Signage is not permitted on upper floor canopies.
 - v) Canopies shall be positioned so that signage is not obstructed.
 - vi) Canopies shall be coordinated with the overall design of the building; all canopies on a single building shall be consistent in their design, color, and material.

4.6 Land Uses

4.6A Allowable Land Uses

A parcel or building within the specific plan area shall be occupied by only the land uses allowed by Table 4.6.1 within the zones specified within the Regulating Plan. Land uses are defined in Section 4.10B.ii (Glossary).

- 1 Use not listed. When a use is not specifically listed in Table 4.6.1, the Director of Community Development or a designee, shall have the authority to determine whether the proposed use is permitted based on the finding that the use is similar to, and no more detrimental than, those permitted in the zone.
- 2 Uses not listed, or otherwise determined by the Director of Community Development or a designee to be permitted, are prohibited.

4.6.B Permit Requirements

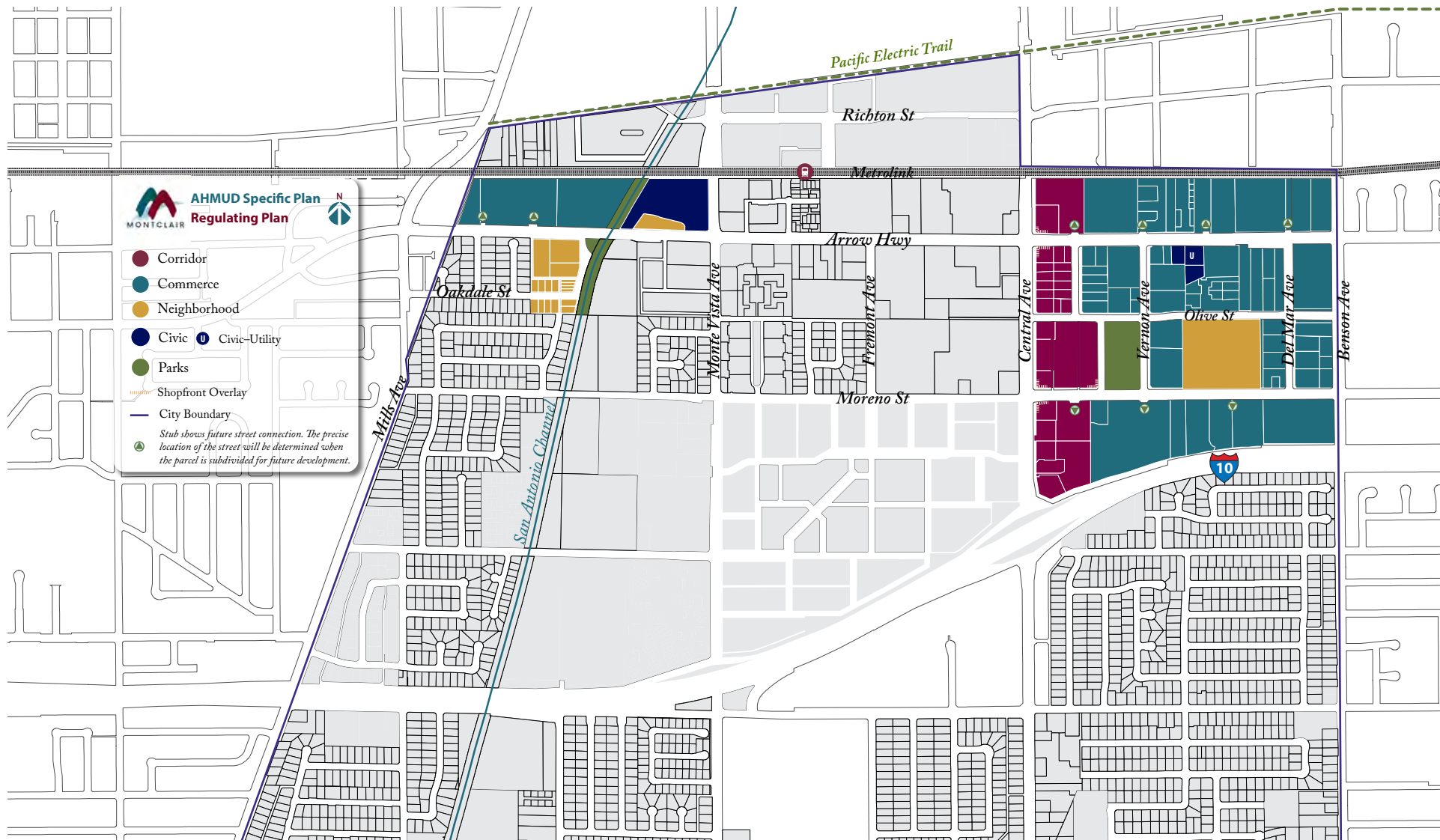
- 1 Permitted subject to compliance with all applicable provisions of this Development Code, and a Precise Plan of Design in compliance with Zoning Code Chapter 11.80. These are shown as “P” uses in the tables;
- 2 Allowed subject to the approval of a Conditional Use Permit in compliance with Zoning Code Chapter 11.78, and shown as “CUP” uses in the tables; and
- 3 Not allowed in particular zones, and shown as a “—” in the tables.

		Corridor	Commerce	Neighborhood
1	Industrial: Manufacturing, Processing, Warehouse			
	Contractor storage yard	—	—	—
	Drop-off dry cleaner	P	P	—
	Dry cleaning and laundry plants	—	—	—
	Laboratory, research	—	P	—
	Laundromat and Cleaners	—	—	—
	Manufacturing - food products (Indoor-only operations. No animal slaughtering)	—	—	—
	Manufacturing - light intensity w/outdoor storage	—	CUP	—
	Manufacturing – handcraft/artisan (indoors only)	P	P	—
	Manufacturing – heavy intensity w/ outdoor storage	—	—	—
	Manufacturing - small-scale artisan (indoors only)	P	P	—
	Manufacturing - small-scale artisan (w/ outdoor storage)	—	CUP	—
	Mini or self-storage	—	CUP	—
	Pallet manufacturing facility and yard	—	—	—
	Recycling Processing Facility	—	—	—
	Recycling Redemption center	—	CUP	—
	Research and development (R&D) - indoors only	—	P	—
	Salvage yards	—	—	—
	Warehouse - cold storage/ancillary use	—	P	—
	Warehouse - cold storage/primary use	—	CUP	—
	Warehouse - fulfillment center	—	—	—
	Warehouse – general (primary use)	—	P	—
	Warehousing – logistics or distribution facility	—	—	—

Legend

P	Permitted
CUP	Conditional Use Permit
—	Not Permitted

Table 4.6.1: Land Use Table



Regulating Plan: Land Use Zones

		Corridor	Commerce	Neighborhood
2	Retail			
	Beer, Wine, or Liquor Stores (Off-Sale) (refer to MMC Section 11:42)	CUP	CUP	—
	Cigar, cigarette, electronic cigarette smoke shops (see definition for specific requirements)	CUP	CUP	—
	Convenience market (with or without alcoholic beverages) (refer to MMC Section 11:42)	CUP	CUP	—
	Neighborhood or specialty food markets (less than 10,000 SF with alcohol sales limited to 10 percent of the floor area)	CUP	CUP	—
	Neighborhood or specialty food markets (less than 10,000 SF with no alcoholic sales)	P	P	—
	Retail – bulk merchandise (see definition)	CUP	CUP	—
	Retail -Garden supply, plant nursery, home improvement center, lumber yard, with outdoor display or storage	CUP	CUP	—
	Retail – general (See Definition)	P	P	—
	Supermarket (over 10,000 SF)	CUP	CUP	—
3	Eating and Drinking Establishments			
	Banquet facility	CUP	CUP	—
	Bar, lounge, nightclub, tavern, cabaret, and similar	CUP	CUP	—
	Cafeterias and buffets (with alcoholic beverages)	CUP	CUP	—
	Cafeterias and buffets (with no alcoholic beverages)	P	P	—
	Coffee shop	P	P	P
	Coffee shop w/drive-thru	CUP	CUP	—
	Food Trucks	Refer to MMC Title 4, Chapter 4.72		
	Microbrewery	CUP	CUP	—
	Retail - Bakery	P	P	P
	Restaurant - fast food (with no drive-thru, no outdoor dining, or alcohol sales*)	P	P	—
	Restaurant - fast food – w/drive-thru	CUP	CUP	—
	Restaurant – general, with alcohol sales*	CUP	CUP	—
	Restaurant – general, with no alcohol sales*	P	P	—
	Outdoor Café or Dining – associated with an adjoining eating establishment	CUP	—	—

Legend

P	Permitted
CUP	Conditional Use Permit
—	Not Permitted

Table 4.6.1: Land Use Table

*Alcohol sales or service - means a business establishment that sells, serves, or gives away alcoholic beverages for consumption on or off the premises and that is applying for or has obtained both: 1) City approval according to Section 11.42 of the Montclair Municipal Code; and 2) a duly issued ABC License(s), for the approved use. The establishment shall include any immediately adjacent area that is owned, leased, rented, or controlled by the licensee. May include “Bars, lounges, and nightclubs,” “Eating and drinking establishments,” or “Alcohol sales, on-sale, accessory only.”

		Corridor	Commerce	Neighborhood
4	Services			
	Adult day care, commercial (under 2,000 SF)	ACUP	ACUP	—
	Adult businesses	Refer to MMC Title 4, Chapter 4.53, and Title 11, Chapter 11.40 for additional information on restricted uses		
	Auction House	—	—	—
	Beauty salon or barber shop (see definition)	P	P	—
	Business support services	P	P	—
	Child care center, commercial	CUP	CUP	—
	Commercial banking, savings institutions, and credit unions	P	P	—
	Laundromat or Coin Laundry	—	—	—
	Massage establishment	Refer to MMC Title 4, Chapter 4.56		
	Pawn Shop	—	—	—
	Postal and Stationary Services	P	P	—
	Professional/Administrative services	P	P	—
	Real estate services office (e.g. real estate agents/brokers, property management, appraisers, escrow and listing services, etc.)	P	P	—
	Services - General	P	P	—
	Services - Restricted	Refer to Definition and MMC Title 4 for additional information on restricted uses		
5	Medical Services			
	Funeral Home (No Embalming or Cremation Services)	—	—	—
	Hospital	CUP	—	—
	Laboratory, Medical and Dental	P	P	—
	Medical Services	P	P	—
	Nursing Home (refer to definition)	—	CUP	—

		Corridor	Commerce	Neighborhood
6	Vehicle Services			
	Car or truck washing and detailing (includes self-serve and automated)	CUP	CUP	—
	Fueling or gas station (with or without convenience store or car wash)	CUP	CUP	—
	Tires – sales, installation, maintenance and repair	CUP	CUP	—
	Tow yard	—	—	—
	Vehicle body repair, paint, or restoration	CUP	CUP	—
	Vehicle parts sales, new (parts only with no installation bays)	P	P	—
	Vehicle parts sales, new (with installation bays)	CUP	—	—
	Vehicle sales (new or used) and rentals with outdoor display	CUP	CUP	—
	Vehicle repair, major	CUP	CUP	—
	Vehicle repair, minor (indoor only)	P	P	—
	Vehicle storage	CUP	—	—
	Vehicle wrecking, dismantling, salvage yard	—	—	—
	* Vehicle Services – means the repair, servicing, alteration, restoration, towing, painting, cleaning, or finishing of automobiles, trucks, recreational vehicles, boats, and other vehicles as a primary use, including the incidental wholesale and retail sale of vehicle parts as an accessory use.			
7	Animal Related Uses			
	Animal Grooming	P	P	—
	Animal boarding, kennel	CUP	CUP	—
	Animal hospital or Veterinary clinic	CUP	CUP	—
	Animal shelter	CUP	CUP	—
	Pet store, retail (domestic pets only and supplies)	P	P	—

		Corridor	Commerce	Neighborhood
8	Residential Uses and Lodging			
	Accessory dwelling unit (per MMC Chapter 11.23)	Refer to MMC Chapter 11.23		
	Caretaker dwelling (refer to definition)	CUP	CUP	—
	Dwelling, multifamily	P	P	P
	Dwelling, single-family	— (1)	—(1)	P
	Emergency Shelter and Single Room Occupancy Lodging Facilities	Refer to Montclair General Plan - Housing Element and Title 11, Section 11.37 of the Montclair Municipal Code for additional information		
	Home occupation permit (per MMC Chapter 11.58)	P	P	P
	Hotel or motel	CUP	CUP	—
	Mixed-use development	See Mixed-Use Development Section 4.9F		
	Mobile home park. Refer to Title 11, Chapter 11.62 of the MMC and California Code of Regulations, Title 25, Chapter 2 commencing with Section 1000.	P(2)	P(2)	P(2)
	Residential care facilities	CUP	CUP	CUP
	Work live studio (with artisanal business)	—	CUP	—

(1) Existing single-family residences located within the AHMUD Specific Plan as of the date of adoption area as of the adoption are permitted to continue.

(2) Existing mobile home parks within the AHMUD Specific Plan as of the date of adoption area as of the adoption are permitted to continue.

		Corridor	Commerce	Neighborhood
9	Recreation, Education, Public Assembly			
	Amusement arcade (over 3 games)	CUP	CUP	—
	Botanical Gardens and Arboretums	P	P	P
	Civic use facility	P	P	—
	Club, lodge, private meeting hall	CUP	CUP	—
	Commercial recreation, indoor – See definition	CUP	—	—
	Commercial recreation, outdoor – See definition	CUP	—	—
	Cultural institutions (libraries, museums, botanic gardens, etc.)	P	P	P
	Health fitness facility (over 2,000 SF)	CUP	CUP	—
	Health fitness facility (under 2,000 SF)	P	P	—
	Public assembly facility	CUP	CUP	—
	Public park	P	P	P
	Religious facility	CUP	CUP	CUP
	School - private or public academic educational institution	CUP	CUP	CUP
	School – specialized education and training	CUP	CUP	—
	Studio fitness – art, dance, martial arts, music (2,000 SF or less)	P	P	—
	Theater, cinema, performing arts	CUP	—	—
10	Utilities			
	Mobility Hub	CUP	CUP	—
	Parking facility, public or commercial	CUP	—	—
	Solar farm or facility	CUP	—	—
	Utility Facility (subject to local control)	CUP	CUP	CUP
	Wireless telecommunication facility	CUP	CUP	—
11	Miscellaneous Uses			
	Seasonal and promotional outdoor sales	Refer to Section 11.68 of the MMC		—
	Temporary structures (refer to definition)	P	P	P
	Temporary uses	CUP	—	—



4.7 Signs

4.7A Sign Standards

4.7A.1 Purpose

These sign regulations are intended to appropriately limit the placement, type, size, and number of signs allowed within the AHMUD area, and to require the proper maintenance of signs. The purposes of these limitations and requirements are to:

- a Avoid traffic safety hazards to motorists, bicyclists, and pedestrians, caused by visual distractions and obstructions;
- b Promote the aesthetic and environmental values of the community by providing for signs that do not impair the attractiveness of AHMUD as a place to live, work, and shop; and
- c Provide for signs as an effective channel of communication, while ensuring that signs are aesthetically designed and proportioned in relation to adjacent structures and the structures to which they are attached.

4.7A.2 Applicability

- a Section 4.7 applies to all signage within the AHMUD Code boundaries except for the following which are subject to the requirements of Chapter 11.72 of the Municipal Code:
 - 1. Real estate signs, special event signs, directional signs; and
 - 2. Billboard or other off-premise sign regulated by Chapter 11.72 of the Municipal Code, and any sign located in the public right-of-way and installed or maintained by the City or by any other public entity having legal authority to maintain signs.
- b Applicability to sign content. The provisions of this Section do not regulate the message content of a sign (sign copy), regardless of whether the message content is commercial or noncommercial.
- c Sign permit requirements. Installation of signs shall require approval of a sign permit:
 - 1. Wall, window, awning/canopy, and sidewalk signs shall be processed ministerially by the Planning Director; and
 - 2. All other sign types shall be subject to the discretionary process requiring site plan approval from the Planning Commission.

4.7B Sign Types

Each proposed sign or existing sign to be modified shall be designed as one of the signage types allowed for the zoning district as identified in Table 4.2.1

Wall			Window			Awning/Canopy			Sidewalk			Marquee			Projecting			Monument			Roof			Pole		
COM	COR		COM	COR	N	COM	COR		COM	COR		COM			COM	COR		COM	COR		COM	COR		COM	COR	
																										
A sign that is painted or applied directly to the wall, typically above the shopfront or more creatively as approved by the City. This type consists of a single externally illuminated panel or individual letters including a logo and does not include cabinet signs. This type of sign is intended for viewing from across the street and along the sidewalk.			A sign that is painted or applied directly to the storefront window(s) and/or door(s). This type consists of individual letters, including a logo with allowances for contrasting background. This type of sign is intended for viewing from across the street and at close range.			A sign that is integral to the awning or canopy above a shopfront. This type of sign may be located either on the valence face of the awning that faces the street or on the valence perpendicular to the building. Also included is a feature area on the main panel. For canopies, this type of sign is located on top of and along the front edge of the canopy. This type of sign is intended for viewing at close range.			A two-sided, non-illuminated, portable sign that is placed outside of the shopfront on the adjacent sidewalk for viewing at close range. The sidewalk sign is intended for use by restaurants, cafes, other food-oriented businesses, theaters and other such activities.			A vertically-oriented two- to three-sided sign that projects from the facade over a sidewalk, public open space or other public right-of-way and may project above the building's parapet. This type of sign is intended for viewing along the sidewalk and down the street from the adjacent block.			A two-sided sign that projects over a sidewalk (public or private) or open space (public or private). This type of sign is intended for viewing at close range.			A sign that is located within front and side yards where the building is setback substantially from the sidewalk and the sign is to be viewed from a distance along the street. Wall-integral: Occurs as part of site-defining landscape, in the form of a low wall that may include a gateway that encloses outdoor dining or open space. Freestanding: Occurs as a freestanding sign subject to location and size limitations aimed at scale and compatibility with pedestrian frontages.			A sign that is for use on block-scale buildings at least 3 stories in height such as hotels, conference centers, and certain civic building. This type of sign is intended for viewing from a distance to help emphasize the identity and presence of a particular area as a whole and therefore, the signage is allowed to be more inventive.			A vertically-oriented sign that is detached from the main building, located along the primary frontage within the front setback, consisting of a structural support with the sign mounted at the top. This type of sign is for use on property that has a certain amount of street frontage and if intended for viewing from a distance to identify a business set back and not readily visible from the street.		

Table 4.7.1 Sign Types



4.7B1 Wall Sign

- | | | | | | | | | |
|-------------------------------------|-----------------|---|--------|-------------|------------------------------|-----|-------------------------------------|------------|
| 1 | Description | A sign that is painted or applied directly to the wall, typically above the shopfront or more creatively as approved by the City. This type consists of a single externally illuminated panel or individual letters including a logo and does not include cabinet signs. This type of sign is intended for viewing from across the street and along the sidewalk. | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height</td> <td>18 in. max.</td> </tr> <tr> <td>Width (as % of facade width)</td> <td>60%</td> </tr> <tr> <td>Features (allowed beyond sign area)</td> <td>6 ft. max.</td> </tr> </table> | Height | 18 in. max. | Width (as % of facade width) | 60% | Features (allowed beyond sign area) | 6 ft. max. |
| Height | 18 in. max. | | | | | | | |
| Width (as % of facade width) | 60% | | | | | | | |
| Features (allowed beyond sign area) | 6 ft. max. | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 wall sign per storefront bay along frontage of building; b. Multi-story buildings qualify for a building identification sign located on the uppermost story; c. Maximum thickness of sign as measured from the wall shall not exceed 4 inches; d. Minimum of 24 inches between sign and any opening or edge of the building facade; e. Minimum of 12 inches between sign and an eave or parapet; and f. If illuminated, individual letters required and shall be either internally illuminated or externally illuminated with decorative lamps mounted to maintain visual integrity of the sign. | | | | | | |



4.7B2 Window Sign

- | | | | | | | | | |
|-------------------------------------|-----------------|---|--------|-------------|----------------------------------|-----|-------------------------------------|------------|
| 1 | Description | A sign that is painted or applied directly to the storefront window(s) and/or door(s). This type consists of individual letters, including a logo with allowances for contrasting background. This type of sign is intended for viewing from across the street and at close range. | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height</td> <td>15 in. max.</td> </tr> <tr> <td>Width (as % of storefront width)</td> <td>40%</td> </tr> <tr> <td>Features (allowed beyond sign area)</td> <td>2 ft. max.</td> </tr> </table> | Height | 15 in. max. | Width (as % of storefront width) | 40% | Features (allowed beyond sign area) | 2 ft. max. |
| Height | 15 in. max. | | | | | | | |
| Width (as % of storefront width) | 40% | | | | | | | |
| Features (allowed beyond sign area) | 2 ft. max. | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 window sign per storefront; b. Minimum of 75% of window sign shall be independent of contrasting background; c. Sign inside the shop-space is not allowed within 4 feet of the window; and d. Storefront width is measured between walls or columns of at least 8 inches in width. | | | | | | |



4.7B3 Awning/Canopy Sign

- | | | | | | | | | | | | | | | |
|---------------------------------------|------------------------|--|-----------------------|------------------------|-----------------------|------------|-------------------------------------|-----|---------------------------------------|----------|-----------------------------------|----------|--------------|----------------------|
| 1 | Description | A sign that is integral to the awning or canopy above a shopfront. This sign may be located either on the valence face of the awning facing the street or on the valence perpendicular to the building. Also included is a feature area on the main panel. For canopies, this type of sign is located on top of and along the front edge of the canopy. This type of sign is intended for viewing at close range. | | | | | | | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height awning valance</td> <td>6 in. min.; 12 in max.</td> </tr> <tr> <td>Height canopy valance</td> <td>2 ft. max.</td> </tr> <tr> <td>Width (as % of awning/canopy width)</td> <td>75%</td> </tr> <tr> <td>Features (logo) for awning main panel</td> <td>2 sq.ft.</td> </tr> <tr> <td>Feature (beyond canopy sign area)</td> <td>3 sq.ft.</td> </tr> <tr> <td>Encroachment</td> <td>within 2 ft. of curb</td> </tr> </table> | Height awning valance | 6 in. min.; 12 in max. | Height canopy valance | 2 ft. max. | Width (as % of awning/canopy width) | 75% | Features (logo) for awning main panel | 2 sq.ft. | Feature (beyond canopy sign area) | 3 sq.ft. | Encroachment | within 2 ft. of curb |
| Height awning valance | 6 in. min.; 12 in max. | | | | | | | | | | | | | |
| Height canopy valance | 2 ft. max. | | | | | | | | | | | | | |
| Width (as % of awning/canopy width) | 75% | | | | | | | | | | | | | |
| Features (logo) for awning main panel | 2 sq.ft. | | | | | | | | | | | | | |
| Feature (beyond canopy sign area) | 3 sq.ft. | | | | | | | | | | | | | |
| Encroachment | within 2 ft. of curb | | | | | | | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 sign per awning / canopy; b. Each awning / canopy shall correspond to a storefront; c. The main panel of awnings shall only be used for a logo feature and shall not be for additional signage area; d. Internal illumination of awning(s) is prohibited; e. Canopy sign may extend up to 24 inches above the top of the canopy; and f. Encroachment permit required. | | | | | | | | | | | | |



4.7B4 Sidewalk Sign

- | | | | |
|---|-----------------|--|-------------------------|
| 1 | Description | A two-sided, non-illuminated, portable sign that is placed outside of the shopfront on the adjacent sidewalk for viewing at close range. The sidewalk sign is intended for use by restaurants, cafes, other food-oriented businesses, theaters and other such activities. | |
| 2 | Size | Height | 18 in. min.; 3 ft. max. |
| | | Width | 18 in. min.; 2 ft. max. |
| | | Thickness on each side | 4 in. max. |
| | | Horizontal clearance from adjacent curb | 18 in. max. |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 sign per business with a minimum distance of 25 feet from the nearest sidewalk sign; b. Sign shall be vertically oriented and have no more than 2 sides; c. Sign shall be placed as close as possible to the business storefront/edge of outdoor seating or along the street edge of the adjacent sidewalk; d. Encroachment permit required within the r.o.w. Adequate pedestrian access shall be maintained; and e. Materials shall not include vinyl or plastic. Lighting shall be from ambient sources. | |



4.7B5 Marquee Sign

- | | | | |
|---|-----------------|---|---------------------------|
| 1 | Description | A vertically-oriented two- to three-sided sign that projects from the facade over a sidewalk, public open space or other public right-of-way and may project above the building's parapet. This type of sign is intended for viewing along the sidewalk and down the street from the adjacent block. | |
| 2 | Size | Height single-story building | max. 35% of facade height |
| | | Height multi-story building | max. 75% of facade height |
| | | Height above eave or parapet | 10 ft. max |
| | | Width of each sign face (up to 3 max.) | 20 ft. max |
| | | Distance from wall | 18 in. |
| | | Thickness | 4 in. max. |
| | | Feature (allowed beyond sign area) | 6 sq. ft. |
| | | Encroachment | within 3 ft. of curb |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 marquee sign per building; b. Sign shall be vertically-oriented and placed within the top half of single-story buildings and on the upper floor(s) of multi-story buildings; and c. Encroachment permit required. | |



4.7B6 Projecting Sign

- | | | | |
|---|-----------------|--|----------------------|
| 1 | Description | A two-sided sign that projects over a sidewalk (public or private) or open space (public or private). This type of sign is intended for viewing at close range. | |
| 2 | Size | Height | 2 ft. max. |
| | | Width | 3 ft. max. |
| | | Thickness | 3 in. max. |
| | | Feature (allowed-beyond sign area) | 1.5 sq.ft. |
| | | Vertical clearance from sidewalk | 8 ft. min. |
| | | Encroachment | within 3 ft. of curb |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 projecting sign per business along frontage; b. If illuminated, shall not be internally illuminated cabinet sign; c. Supporting hardware such as brackets shall be architecturally compatible with the building facade; d. Not allowed under an awning or horizontally within 10 feet of another projecting sign; and e. Encroachment permit required. | |



4.7B7 Monument Sign

- | | | | | | | | | | | | | | | | | |
|--------------------------------------|-------------------------------|--|--------------------------------------|-----------------------|---------------------|-------------|----------------------|------------------------|-----------|--------|------------------------------------|-----------|-----------------------------|------------|----------------------------|-------------------------------|
| 1 | Description | <p>A sign within front and side yards where the building is setback from the sidewalk and the sign is to be viewed from a distance along the street.</p> <p>Wall-integral: Occurs as part of site-defining landscape, in the form of a low wall that may include a gateway that encloses outdoor dining or open space.</p> <p>Freestanding: Occurs as a freestanding sign subject to location and size limitations aimed at scale and compatibility with pedestrian frontages.</p> | | | | | | | | | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height of wall or freestanding panel</td> <td>3 ft. min. 5 ft. max.</td> </tr> <tr> <td>Height of sign area</td> <td>18 in. max.</td> </tr> <tr> <td>Width (freestanding)</td> <td>2 ft. min., 7 ft. max.</td> </tr> <tr> <td>Thickness</td> <td>18 in.</td> </tr> <tr> <td>Feature (allowed beyond sign area)</td> <td>2 sq. ft.</td> </tr> <tr> <td>Distance from adjacent curb</td> <td>5 ft. min.</td> </tr> <tr> <td>Encroachment into setbacks</td> <td>within 3 ft. of property line</td> </tr> </table> | Height of wall or freestanding panel | 3 ft. min. 5 ft. max. | Height of sign area | 18 in. max. | Width (freestanding) | 2 ft. min., 7 ft. max. | Thickness | 18 in. | Feature (allowed beyond sign area) | 2 sq. ft. | Distance from adjacent curb | 5 ft. min. | Encroachment into setbacks | within 3 ft. of property line |
| Height of wall or freestanding panel | 3 ft. min. 5 ft. max. | | | | | | | | | | | | | | | |
| Height of sign area | 18 in. max. | | | | | | | | | | | | | | | |
| Width (freestanding) | 2 ft. min., 7 ft. max. | | | | | | | | | | | | | | | |
| Thickness | 18 in. | | | | | | | | | | | | | | | |
| Feature (allowed beyond sign area) | 2 sq. ft. | | | | | | | | | | | | | | | |
| Distance from adjacent curb | 5 ft. min. | | | | | | | | | | | | | | | |
| Encroachment into setbacks | within 3 ft. of property line | | | | | | | | | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Wall-integral: Maximum 1 sign may be placed at each end of the wall if there is at least 50 feet between each sign; b. Freestanding: Maximum of 1 sign and minimum of 75 feet horizontal separation from another monument sign; c. Not allowed in any required visibility area, right-of-way, or private street; d. Colors and materials shall match the building style and details; and e. Lighting shall be from external or ambient sources. | | | | | | | | | | | | | | |



4.7B8 Roof Sign

- | | | | | | | | | | | |
|-------------------------------------|------------------------------|---|--------|------------------------------|--------|--------------------------|-----------|-------------|-------------------------------------|----------------|
| 1 | Description | <p>A sign that is for use on block-scale buildings at least 3 stories in height such as hotels, conference centers, and certain civic building. This type of sign is intended for viewing from a distance to help emphasize the identity and presence of a particular area as a whole and therefore, the signage is allowed to be more inventive.</p> | | | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height</td> <td>7 ft. min., 15 ft. max. max.</td> </tr> <tr> <td>Length</td> <td>10 ft. min., 30 ft. max.</td> </tr> <tr> <td>Thickness</td> <td>18 in. max.</td> </tr> <tr> <td>Features (allowed beyond sign area)</td> <td>25 sq.ft. max.</td> </tr> </table> | Height | 7 ft. min., 15 ft. max. max. | Length | 10 ft. min., 30 ft. max. | Thickness | 18 in. max. | Features (allowed beyond sign area) | 25 sq.ft. max. |
| Height | 7 ft. min., 15 ft. max. max. | | | | | | | | | |
| Length | 10 ft. min., 30 ft. max. | | | | | | | | | |
| Thickness | 18 in. max. | | | | | | | | | |
| Features (allowed beyond sign area) | 25 sq.ft. max. | | | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 roof sign per building; b. Minimum building height of 3 stories and minimum site frontage along primary street of 150 feet; c. Signs shall not encroach into right-of-way or private street; d. If illuminated, external illumination required and shall be mounted to maintain visual integrity of the sign; and e. Supporting hardware such as brackets shall be architecturally compatible with the main building. | | | | | | | | |



4.7B9 Pole Sign

- | | | | | | | | | | | | | | | |
|------------------------------------|-----------------|---|---------------------------|-------------|---------------|-------------|---------------------|-------------|-----------|-------------|------------------------------------|-----------|----------------------|------------|
| 1 | Description | <p>A vertically-oriented sign that is detached from the main building, located along the primary frontage within the front setback, consisting of a structural support with the sign mounted at the top. This type of sign is for use on property that has a certain amount of street frontage and is intended for viewing from a distance to identify a business set back and not readily visible from the street.</p> | | | | | | | | | | | | |
| 2 | Size | <table border="0"> <tr> <td style="padding-right: 10px;">Height to top of the sign</td> <td>25 ft. max.</td> </tr> <tr> <td>Width of sign</td> <td>15 ft. max.</td> </tr> <tr> <td>Height of sign area</td> <td>10 ft. max.</td> </tr> <tr> <td>Thickness</td> <td>12 in. max.</td> </tr> <tr> <td>Feature (allowed-beyond sign area)</td> <td>25 sq.ft.</td> </tr> <tr> <td>Distance from r.o.w.</td> <td>min. 2 ft.</td> </tr> </table> | Height to top of the sign | 25 ft. max. | Width of sign | 15 ft. max. | Height of sign area | 10 ft. max. | Thickness | 12 in. max. | Feature (allowed-beyond sign area) | 25 sq.ft. | Distance from r.o.w. | min. 2 ft. |
| Height to top of the sign | 25 ft. max. | | | | | | | | | | | | | |
| Width of sign | 15 ft. max. | | | | | | | | | | | | | |
| Height of sign area | 10 ft. max. | | | | | | | | | | | | | |
| Thickness | 12 in. max. | | | | | | | | | | | | | |
| Feature (allowed-beyond sign area) | 25 sq.ft. | | | | | | | | | | | | | |
| Distance from r.o.w. | min. 2 ft. | | | | | | | | | | | | | |
| 3 | Design Standard | <ul style="list-style-type: none"> a. Maximum of 1 pole sign per site; b. If illuminated, illumination shall maintain visual integrity of the sign; and c. Supporting hardware such as brackets shall be architecturally compatible with the architectural style of the main building. | | | | | | | | | | | | |

4.8 Other Development Standards

4.8A Parking Design

4.8A.1 Applicability

- a **Parking required.** Every use, including change or expansion of a use, or structure, shall have appropriately maintained parking and loading spaces or areas in compliance with the regulations identified in this section.
- b **Number of parking spaces**
 - i **Off-Street Parking Spaces.** Off-street parking spaces shall be provided for each land use per the below standards:
 - a) Residential: 1 space per unit (minimum).
 - b) Residential Guest: 1 space for every 4 units (minimum)
 - c) Lodging: 1 space per room (minimum).
 - d) Live/Work: 1 space per unit under 1500 s.f.; if unit is greater than 1500 sq. ft.: 1 space for every 400 gross s.f.
 - e) Office, Retail, Civic: 1 space per 300 gross s.f. of building area.
 - f) Restaurants, Cafes, or similar: 1 space per 200 gross s.f. of building area.
- c **On-Street Parking**
 - i On-street parking located along the development's frontage may be counted towards fulfilling the minimum number of spaces for the commercial component within the development up to 25 percent of the minimum required number of off-street parking spaces.
 - ii In no case shall on-street parking be used for meeting minimum residential parking requirements within a mixed-use development.
 - iii Where street parking spaces are counted toward meeting required on-site parking as allowed by this section, all such on street spaces must remain available for common use and not be exclusively assigned or reserved to any individual use.

4.8A.2 Parking Reduction Requirements

a Shared Parking

Shared parking may be utilized as an effort to promote an overall reduction in parking. The amount of off-street parking required for a mixed use development may be reduced up to 15 percent as determined by the Community Development Director and approved by the City Council when it can be demonstrated through a Parking Demand Study that sufficient parking is or can be met by the subject uses through a shared parking arrangement. Said study should explain the parking demands of the land uses within the development and how business practices or hours of operation impact the demand for parking at different times of the day.

b Waiver of Parking

Any project site where it can be demonstrated that the land use thereon will not utilize the required number of parking spaces due to the nature of the specific land use, or the manner in which the specific land use is conducted, the number of parking spaces required may be reduced pursuant to the following:

- i **Approval Required.** A parking reduction based upon low parking demand may be allowed upon the approval of a Low Demand Parking Reduction Agreement by the City.
- ii **Alternate Parking Plan.** In approving a parking reduction, the City may require the preparation of an Alternate Off-Street Parking Plan, which demonstrates that additional parking spaces can be provided on-site, as necessary, to accommodate future land use changes or intensifications in land use. Furthermore, a restrictive covenant that runs with the land may be required by the City, which restricts the use of the subject property for the duration of the parking reduction.

- iii Parking Management Plan. A Low Demand Parking Reduction Agreement by and between the City, the applicant, and all other affected property owners, shall be executed and recorded with the County Recorder, which, at a minimum, shall:
 - a) provide confirmation that the parking supply proposed will be adequate during periods of maximum demand,
 - b) confirm that the parking demand is provided within a reasonable walking distance to the use it serves, and
 - c) identify parking management strategies that are necessary to ensure the availability of the necessary number of parking spaces for the duration of the current use and future users of the project site. The Low Demand Parking Reduction Agreement shall be subject to City Attorney review and approval as to form and content.
- iv Parking Analysis. The City may require the applicant to submit a parking analysis, prepared by a person/firm experienced in preparing such analyses, to assist the City in determining the appropriate reduction.

4.8A.3 Parking Design and Development Standards

Required parking areas shall be designed and constructed as follows. The standards of this Section may be modified by the review authority where it determines that alternative parking designs and standards will more appropriately relate to the operating characteristics of the proposed development or new land use, while being equally effective in providing parking areas that are safe, convenient, use land efficiently, and are aesthetically attractive.

- a Access to Parking
 - i Within the Specific Plan area, no new parking shall be placed between a building and a street, except where allowed adjacent to a side street in the Neighborhood zone.
 - ii Parking, including parking garages, shall be accessed from an alley or secondary frontage when possible. The opening of a parking lot or garage on a frontage shall not exceed two lanes in width.

- b Tandem Parking. Tandem parking may be allowed at the discretion of the City:
 - i Non-Residential Uses.
 - a) The use of tandem parking for non-residential uses shall require that the operator of the parking facility provide a valet or attendant at all times that the parking is accessible to users, except where the City determines that the nature of the use and its operation will not require attended parking.
 - b) Tandem parking shall be a maximum of two (2) parking spaces in depth.
 - c) Vehicle movements necessary to move cars parked in a tandem configuration shall not take place on any public street.
 - ii Residential Development. Tandem parking is allowed within multi-family projects and the residential component of mixed-use projects subject to the following standards:
 - a) Up to 30 percent of the total required off-street parking spaces may be provided in a tandem configuration. The maximum number of spaces permitted in a tandem configuration refers to the total individual spaces, not the total number of tandem spaces. For example, if twenty (20) total residential spaces are required, three (3) tandem space with two (2) cars in the tandem space for a total of six (6) individual spaces may be provided.
 - b) Tandem parking shall be a maximum of two (2) parking spaces in depth.
 - c) Both tandem spaces shall be assigned to the same dwelling unit.
 - d) Vehicle movements necessary to move cars parked in a tandem configuration shall not take place on any public street.
 - e) Guest parking spaces shall not be provided in a tandem configuration.
 - f) When tandem parking spaces are used in private garages or other parking facilities with more than two (2) parking spaces, they shall be assigned and marked for resident use.
- c Disabled Parking Spaces. Parking spaces for the disabled shall be provided in compliance with the requirements of State Law (California Administrative Code [Title 24, Part 2, Chapters 2B71]. Disabled spaces required by this Section shall count toward fulfilling off-street parking requirements.

d Loading Spaces

- i A minimum of one (1) off-street loading space shall be provided for every non-residential or mixed use building over 3,000 sf in gross area.
- ii Loading spaces shall be generally located at the rear of a building or along the alley when present in a fashion that allows for adequate maneuvering, loading, or unloading of vehicles without interfering with the orderly movement of traffic or pedestrians on the street.
- iii Off-street loading spaces may be striped “loading zone” by the City of Montclair as approved by the Community Development Director.

e Parking space and lot dimensions. Parking lots and stalls shall be designed with the following minimum dimensions.

Angle of Parking	Length of Parking Stall	Width of Parking Stall	Aisle Width
60° or less (one-way)	18 ft	8.5 ft	18 ft
90° (one-way)			
90° (two-way)	18 ft	8.5 ft	24 ft
90° (tandem)	36 ft	9 ft	24 ft

f Landscaping. Parking areas shall be landscaped in compliance with the following requirements.

- i Surface parking areas shall be planted with shade trees at a minimum ratio of one tree for every four spaces in an orchard planting arrangement.
- ii Parking areas adjacent to a street or sidewalk shall be screened with landscaping and a decorative wall between 36 and 48 inches in height as approved by the review authority.

g Lighting. Parking lot lighting shall comply with the following requirements:

- i Outdoor light fixtures shall be limited to a maximum height of 15 feet or the height of the nearest building, whichever is less.
- ii Lighting shall be energy-efficient, and shielded or recessed. Each light fixture shall be directed downward and away from adjoining properties and public rights-of-way.

h Surfacing. The City desires to reduce stormwater run-off and water pollution, and to allow for the replenishment of groundwater. For parking areas, the goal is to reduce the amount of run-off generating surface area. Therefore, permeable surfaces for parking and maneuvering areas are encouraged. Acceptable permeable surfaces include:

- i Pervious pavers (products such as Unipaver, Eco-stone and SF Rima or an approved equivalent).
 - ii Gravel, bark or grass when reinforced to provide adequate load-bearing (including geotechnical structures such as Invisible Structures’ Grasspave and Gravelpave products, or an approved equivalent).
 - iii Other permeable surfaces may be approved by the Public Works Director.
- i Passenger Loading and Unloading Zones. On-street, curb-side passenger loading and unloading zones, including for transit network companies (such as Uber and Lyft) are permitted as approved by the Community Development Director.

4.8B Solid Waste and Recycling

4.8B.1 Purpose

A planned solid waste collection and removal strategy is a necessary and integral component of all new development projects, or projects for which a new land use entitlement is granted. A successful solid waste collection and removal strategy will include appropriate accommodations for the collection, storage, access, and removal of solid waste and recyclable materials from the project site.

4.8B.2 Developer Responsibility

The applicant shall be responsible for completing the following steps:

- a Contact the City's Solid Waste hauler to ascertain all necessary information and/or specifications regarding the collection, storage, and removal of solid waste and recyclable materials associated with a proposed development project. The solid waste haulers will be able to assist in determining the types, sizes, and numbers of containers needed and other services provided. Written proof of consultation and acceptance of the Solid Waste Disposal and Recycling Plan (SWDRP) from the solid waste hauler shall be required at the time of formal application submittal for the project to the City; and
- b Develop and submit a written SWDRP describing the solid waste and recycling strategy for the project. Final review and approval of a SWDRP shall be made by the City.

4.8B.3 Location, Access and Design Criteria

The location of all solid waste collection and/or storage spaces shall meet the following requirements:

- a Collection/storage locations shall be located on the lot of the structure it serves and as close as possible to the intended users and/or convenience of residents, employees, and waste hauler personnel.
- b The staging of trash bins on a public or private street for pick-up by the waste hauler shall not be permitted.
- c The collection/storage space shall be located to minimize visual, noise, and odor impacts to adjacent building occupants both on the subject lot and on adjacent lots.
- d Outdoor trash enclosures or collection rooms within a structure shall not be located on a street-facing facade of the structure. Outdoor trash enclosures shall not be located between the building facade and the street.
- e Avoid locating large trash enclosures or collection/storage rooms in underground parking areas or within parking structures as access by waste collection vehicles maybe be impeded.
- f The collection/storage space shall not block or impede any fire exits, any public rights-of-way, or any pedestrian or vehicular access ways.
- g Multi-level buildings shall incorporate trash chutes or other similar means for conveying solid waste and recyclables to approved storage area(s).
- h The use of compactors as part of a solid waste and recycling plan is highly encouraged to save space and reduce collection trips to the site.

4.8B.4 Design Standards

The design and layout of all solid waste and/or storage areas shall meet the following requirements:

- a The design of outdoor trash enclosures and exterior facing elements of a collection/storage area attached to, or within a building/structure, shall complement the architectural features of the building or development that they serve. The incorporation of landscaping around trash enclosures is encouraged as part of a successful design.
- b All outdoor trash enclosures shall include a solid roof to minimize or prevent spilled or trash contaminants from entering the storm water system.
- c Trash enclosures shall be designed in an architecturally appropriate manner to secure and prevent unauthorized access or dumping to the greatest extent possible.
- d All required or proposed exterior light fixtures affixed to, or installed, to illuminate a trash enclosure shall be fully shielded and located in a manner that minimizes light and glare impacts to the street or adjacent uses.
- e The floor of an outdoor collection/storage space and approach apron shall be constructed of black-colored concrete to minimize the appearance of unsightly stains.
- f Collection/storage spaces including outdoor trash enclosures shall be for solid waste and recyclable materials storage and access only.
- g The storage of used fats, oils, or grease (FOG) is prohibited within any collection/storage area including outdoor trash enclosures. Food/restaurant uses shall plan to store any used fats, oils, or grease in an approved container within their respective tenant spaces and contract with the appropriate recycler for regular removal from the premises.
- h A minimum of two feet of interior clearance shall be provided between the top of an opened container lid and a finished ceiling for interior storage spaces, or from the bottom of the roof for outdoor trash enclosures.

4.8B.5 Maintenance

The property owner or manager shall routinely inspect waste collection/storage areas and outdoor trash enclosures to verify:

- a The appearance and condition of these areas are in good order and properly functioning.
- b Proper maintenance of both the interior spaces and surrounding exterior areas are clean and free of graffiti, trash, residue, debris, and leaks. When deficiencies are discovered, the property owner or manager shall take immediate action to correct the noncompliance.

4.8C Drive-Through Standards

4.8C.1 Development Standards for All Drive-Thru Uses

The following development guidelines shall apply to drive-through uses constructed in the AHMUD Specific Plan:

- a To define and support the street edge, buildings with drive-through facilities should be located at the Front Street and/or Side Street setback lines.
- b The main building entrance should be accessed directly from the public sidewalk. On corner lots, the main entrance should be accessed from the Front Street.
- c Uses/activities that support the street along the public sidewalk (such as restaurant seating or offices) should face the street(s).
- d Building walls along the street frontage and visible from the street should be transparent with windows and doors, maximizing views in and out of the building, emphasizing the relationship between interior and exterior, and supporting and animating the public street and sidewalk.
- e Driveways and stacking lanes.
 - i Driveways and stacking lanes should be located perpendicular to the street.
 - ii Stacking lanes should not be located between the building and the street.
 - iii Multiple windows servicing a single stacking lane (e.g. order window, payment window, pickup window) are encouraged to promote reduced idling.
 - iv Multiple stacking lanes are discouraged.
 - v Stacking lanes that are visible from the street should be screened with low walls, hedges, and canopy trees.
- f Sufficient signage should be provided to indicate direction of vehicular travel, no entrance areas, etc.

4.8C Landscape, Lighting, and Utility Standards

- 4.8C.1 Landscape Materials:** Context appropriate landscaping shall be provided for each project and designed to achieve an attractive “urban” landscape setting. Landscaping is also instrumental in creating and delineating public and private spaces. Appropriate landscaping will depend on factors such as the setbacks, shape, size, and orientation of the building.
- a Proposed developments shall incorporate site amenities such as linear planting beds or seat walls, street furniture, enhanced paving surfaces, public art, lighting, etc, where appropriate. These features should be architecturally compatible with the styles, materials and colors of the principal building on the lot and those in the immediate area.
 - b Plant species should be selected based on their suitability for an urban environment, for their cleanliness, and for their ability to survive in a healthy state despite constrained growing conditions. Ease of maintenance should also be considered.
 - c The majority of plant materials shall consist of drought tolerant and/or native plants with minimum water requirements.
 - d The use of canopy trees for shading and cooling is encouraged where appropriate, particularly in publicly accessible open spaces and plazas, to mitigate the urban heat island effect. Where canopy trees are used, site design should provide sufficiently sized tree pits or planting beds and appropriate planting medium to provide for healthy tree growth.
 - e Decorative water features shall use re-circulating water and, where available, shall use recycled water.
- 4.8C.2 Underground Utilities:** All electric and communications lines serving new development shall be placed underground.

- 4.8C.3 Sustainable Storm Water Management:** The intent of this AHMUD Specific Plan is to create an urban walkable community based on urbanist and form-based code principles. When fully implemented, the development of the AHMUD planning area will result in an urban setting characterized by commercial, residential, and mixed use buildings, and structured or underground parking on sites with minimal to no landscape areas available at grade. In some cases, buildings could approach 100 percent coverage of the site. As part of the development review process, developers and their landscape architects will need to focus on tailoring each project to provide stormwater treatment facilities that are functional and visually attractive and that avoid the use of surface infiltration basins, which are not conducive to an urban setting. In preparing mandated Water Quality Management Plans (WQMPs) the following design guidelines shall be considered and implemented to the greatest extent possible:
- a Consider the full range of Best Management Practices (BMP) alternatives for addressing drainage, infiltration, and stormwater quality for the site, including drainage from roofs, plazas, courtyards, and any surface parking areas.
 - b Utilize Low Impact Development (LID) design principles and practices to address infiltration of runoff through the use of:
 - i Pervious surfaces in lieu of impermeable surfaces in plazas, courtyards, and surface parking areas where technically feasible. Acceptable pervious surfaces include: landscaping; pervious pavers including pervious interlocking pavers and permeable concrete; etc.; as approved by the Community Development Director and Public Works Director.
 - ii Landscaped areas integrated into the drainage design where runoff can be directed into planters with subsurface features to further enhance storage or towards permeable surfaces where technically feasible. Amended soils shall be utilized where technically feasible to enhance infiltration, to provide water quality improvement, and to maintain healthy plant life.
 - c Surface infiltration basins are not allowed, unless:
 - i They are accommodated within an active or passive park or open space that can be closed during flood events.
 - ii They are not visible from a street or public open space and they are in addition to the required on-site open space.

4.8C.4 Site Lighting

- a Site lighting should be shielded by permanent attachments to light fixtures so that light sources are not visible from a public way and to prevent off-site glare.

4.8D Warehouse and Mini Storage

4.8D.1 Development Standards for Industrial Building and Warehouse

The development of a new industrial and/or warehouse building within the City shall comply with the following minimum requirements. For specified types of warehouse developments, the issuance of a Conditional Use Permit (CUP) is required. In addition to applicable development standards (e.g., setbacks, building height, on-site parking, etc.), all warehouse facilities shall also comply with the following site plan requirements:

- a Adequate queuing space shall be provided on-site to prevent vehicles from using adjacent streets for maneuvering. Use of public streets or alleys for queuing or maneuvering shall be prohibited.
- b All parking and storage areas for trailers must be provided on-site and must not encroach into any area providing required parking.
- c The exposure of any adjacent residential use to noise, light, exhaust emissions, and other similar nuisances shall be minimized through building orientation and facility design.
- d Loading areas shall be located away, and screened from direct view to the street, subject to Director of Community Development or designee review and approval.
- e Office operations for any warehouse facility shall be limited to 15 percent of the gross building floor area. Required parking facilities shall be calculated separately for any office use.
- f Off-site improvements shall be provided as required by the Director of Public Works or designee to ensure safe and adequate access and to minimize impact on public infrastructure.
- g Security fencing shall be decorative and compatible with building architecture.
- h If a residential unit (one only) is provided for the caretaker or manager, the unit must be attractively integrated into the building design and shall not be a mobile, modular, or trailer home.
- i A minimum of one enclosed and covered trash enclosure must be provided to service the intended use of the site. The design of the trash enclosure shall complement the design of the project, be screened by a minimum six-foot-high solid masonry wall, and comply with all other adopted local and State development standards and ordinances regarding solid waste collection and separation. Such collection areas shall be located and designed to allow easy access by business staff and trash collection workers. Consultation with the City's contracted solid waste hauler, in conjunction with the City, shall be required before the approval of a final site plan, trash enclosure design, and solid waste disposal and recycling management plan.

- j A security plan, incorporating the use of monitored video surveillance cameras, shall be required for each facility. The security plan is subject to the review and approval of the Chief of Police or designee.

4.8D.2 Development Standards for Mini- or Self-Storage Operation

- a The storage spaces shall not be used for manufacturing, retail or wholesale selling, compounding, office functions, other business or service uses, or human habitation.
- b No water, sanitary facilities, or electricity, except lighting fixtures, shall be provided in individual storage spaces unless otherwise approved by the Planning Commission.
- c Prefabricated shipping containers shall not be used as mini-warehouse facilities.
- d The following materials shall not be stored in mini-warehouses:
 - Flammable or explosive matter or materials;
 - Material that creates obnoxious dust, odor, or fumes; or
 - Hazardous or extremely hazardous waste.
- e A security plan, incorporating the use of monitored video surveillance cameras, shall be required for each facility. The security plan is subject to the review and approval of the Chief of Police or designee.

4.8E Fences

4.8E.1 Purpose

Fences, garden walls, retaining walls, and hedges can define the edge between the public street and private yards. They also define the street face where buildings are absent.

4.8E.2 Standards

- a A clear cross-visibility area shall be maintained at the intersection of the public rights-of-way, unobstructed by any fence or wall taller than 3 feet above the street grade.
- b Fence or low walls up to 3 feet in tall may be used to divide space, create a variety in landscaping and to define site edges.
- c Front yard fencing shall only be allowed for the stoop, dooryard, and porch and front yard frontage types. The fence shall be located per the frontage requirements in Section 4.5 of the AHMUD Code.
- d Fences or garden walls may have a hardy species of hedge or climbing vine planted along their length which softens and adds richness to their look.
- e Fences should be made of ornamental iron, steel, wood pickets or a synthetic wood product (such as Wood-filled Recycled Plastic Lumber) and may have stucco or masonry piers. Metal fences should be black or green.

4.8F Mixed-Use Standards

4.8F.1 Introduction: The AHMUD Specific Plan provides the opportunity for mixed-use developments that combine residential living uses and non-residential uses into a single building, or property with complementary retail, service, cultural, and public uses at or close to the ground floor. Private uses such as housing or offices would be located on upper floors. Common features of mixed-use development generally include outdoor spaces such as plazas and corridors between buildings and sidewalks to encourage people to access places of work or shopping through public transit as well as walking and biking. The AHMUD also allows for the integration of residential living in conjunction with artisan manufacturing uses in the Corridor and Commerce zoning districts, and the development of workforce housing for nearby industrial uses.

When planned well, mixed-use developments can infuse commercial and industrial areas with residential living offering various benefits such as greater housing affordability, walkability between housing and workplaces, and stronger neighborhoods. Mixed-use developments have particular challenges as they relate to combining different land uses and require careful planning to prevent or mitigate potential conflicts. For example, commercial properties have different requirements than residential ones, particularly when it comes to access, privacy, parking, noise/sound attenuation, etc.

4.8F.2 Mixed-Use Locations: The AHMUD Specific Plan encourages mixed-use development but does not require it for all properties. The following table indicates the locations where mixed-use projects may occur when all applicable requirements of the Specific Plan are met and City approval is granted.

	Corridor	Commerce	Neighborhood
Mixed-Use Development			
Allowed on key streets only:			
<ul style="list-style-type: none">Corridor District – Central Avenue, Arrow Highway, and Moreno Street	p ¹	p ¹	p ¹
<ul style="list-style-type: none">Commerce District – Arrow Highway, Moreno Street, and Benson Avenue			
<ul style="list-style-type: none">Neighborhood District – Arrow Highway			

¹ Requires Planning Commission review and approval of a Precise Plan of Design (PPD). See Section XXX

Allowed Uses: Commercial or other non-residential land uses may be allowed per the underlying zoning district in which the site is located, except as restricted below.

Restricted Uses: A mixed-use project shall not include any of the following commercial activities:

- Adult entertainment activities/businesses.
- Bars, cabarets, and nightclubs.
- Auto related uses, such as vehicle repair, tire sales/service, vehicle sales or leasing, car washes, service stations, or other similar services, including similar uses related to boats, motorcycles, trucks, or recreational vehicles.
- Liquor or tobacco stores where the sale of alcoholic beverages or tobacco products (including E-cigarettes) is the primary business activity or any business that stocks and/or displays tobacco or alcoholic beverages on more than 15 percent of the gross floor area (including refrigerators or cabinets) of the subject lease space.
- Industrial uses including welding or machine shops, including uses with interior or exterior storage of flammable or hazardous materials.
- Uses that require outdoor storage, including the use of sea containers or similar storage structures.
- Animal kennels, pet grooming, pet shops, veterinary offices/hospitals.
- Funeral parlors, mortuaries, crematories, columbaria.
- Any activity or use determined by the Director of Community Development or designee to be incompatible with residential uses and/or to have the possibility of adversely affecting the health or safety of residents within, or adjacent to, a mixed-use project, such as the potential for excessive dust, glare, heat, noise, noxious gasses, odor, smoke, traffic, vibration, etc. because of materials, processes, products, or generated wastes.

4.8F.3 Mixed-Use Design and Development Standards

a Minimum lot size for new Mixed-Use Projects

Property Development Standards for New Mixed-Use Development*

Standard	Minimum Size/Dimensions
Lot Size	15,000 square feet
Lot Width	100 feet
Lot Depth	100 feet
Lot Coverage	Coverage is subject to parking, landscaping, open space, and setback requirements.
Private Open Space	Per Unit - 40 square feet with a minimum width of 5 feet
Common Open Space	Minimum area of 15 percent of the lot area with a minimum width of 20 feet. Common space shall be a well defined, coherent landscape open space that is an essential component of the project space (not merely space left over after the building is placed).

* The above standards apply to new development projects and not projects utilizing existing development.

b Ground Floor Commercial Lease Spaces

Ground floor commercial spaces shall be:

- i. Limited to ground floor locations only and shall face the primary and secondary street frontages.
- ii. A minimum height of 15 feet for the ground floor between the finished floor and the finished ceiling for new mixed-use developments. The Director of Community Development or Designee shall determine the feasibility of creating ground floor commercial lease spaces in existing buildings not having the minimum 15-foot height requirement on a case-by-case basis in conjunction with the overall objectives of the proposed project.
- iii. Primarily transparent with a minimum of 50 percent of the ground floor façade consisting of pedestrian entrances and display windows with views to interior spaces. Spandrel, opaque, or reflecting glass shall not be used.
- iv. Residential Access. Any residential uses shall not share a common entrance or share common entrance hallways or balconies.

c Location of Residential Units

Residential units may be located on any floor, behind the first 40 feet of the ground floor area measured back from the building face adjacent to any primary street frontage. The restriction against the residential use of this 40-foot area does not apply to entryways, access corridors, or stairs. In addition, Residential units must occupy a minimum of 50% percent of the total floor area of a mixed-use project.

d Access to Utilities

The site shall have access to all utilities including sewer and water (with adequate pressure) to serve the intended uses and provide for fire suppression requirements as determined by the Fire Marshal and Building Official.

e Conversion of Existing Commercial Buildings to Mixed-Use

The conversion of existing commercial buildings to mixed-use is subject to compliance with the applicable requirements of this section. However, applicants may request relief from a maximum of two of the following mixed-use standards, to account for existing non-conforming site conditions that make it infeasible to physically comply with the following standards:

- i. Existing setbacks of commercial buildings facing the street conflict with residential setbacks standards of this section;
- ii. Loading facilities;
- iii. Height limits, if such buildings have legally nonconforming heights;
- iv. Outdoor common space for residential units; and
- v. Parking requirements if existing on-site parking does not meet minimum requirements and it is not possible to add sufficient parking.

f Minimum Dwelling Unit Size

The following minimum unit sizes shall apply to housing units in mixed-use developments:

Mixed-Use Minimum Residential Unit Sizes*

Unit Type	Minimum Size
Studio	300 square feet
1 Bedroom Unit	500 square feet
2+ Bedroom Unit	Unit 800 square feet

*Individual residential units in a mixed-use development shall not exceed 1,000 square feet.

g Density Bonus

The City defers to the State Density Bonus regulations for projects that seek to provide affordable housing units and take advantage of the benefits provided under the program. Please refer to the State Density Bonus law regarding current regulations.

h Inclusionary Housing

Residential projects maybe subject to the requirements of the City's Inclusionary Housing Ordinance. Affordable units in a Density Bonus project may be used to satisfy the City's inclusionary housing requirements.

i Setbacks and Massing

Mixed-use developments shall have the same setbacks as established in the underlying zone, except no setback shall be required at the property line between two zones if the project (not buildings) is proposed across those property lines.

j Parking

Projects shall comply with the parking standards outlined in Section 4.8A (Parking Design) for each residential and nonresidential use included in the project. Residential parking may be uncovered.

k Loading and Refuse/Recycling Areas

Commercial loading areas, recycling areas, and refuse storage facilities for commercial and other nonresidential uses shall be located away from residential units and substantially screened from view from the residential portion of the project. Where appropriate, the project may provide for the shared use of recycling and refuse storage facilities.

l Noise

The project design shall ensure noise generated at the site complies with the applicable noise standards contained in the Montclair General Plan.

m Landscaping

New mixed-use projects or a mixed-use conversion project adjacent to residentially zoned land shall provide and maintain a landscaping screen or buffer between the mixed-use projects and adjacent residentially-zoned properties to the extent feasible in light of building setbacks, existing and required parking, and whether there is land available along the property boundary.

4.8F.4 Parking for Mixed-Use Development

a Off-Street Parking

- i The number of off-street parking and loading spaces to be provided shall be equal to the sum of the requirements identified for each individual use, unless a shared parking arrangement has been approved by the City. A Parking Management Plan must be developed to indicate where parking will occur.
- ii The number of off-street parking and loading spaces to be provided shall be equal to the sum of the requirements identified for each individual use, unless a shared parking arrangement has been approved by the City. A Parking Management Plan must be developed to indicate where parking will occur.

b On-Street Parking

- i On-street parking located along the development's frontage may be counted towards fulfilling the minimum number of spaces for the commercial component within the development up to twenty-five percent (25%) of the minimum required number of off-street parking spaces.
- ii In no case shall on-street parking be used for meeting minimum residential parking requirements within a mixed-use development.
- iii Where street parking spaces are counted toward meeting required on-site parking as allowed by this section, all such on street spaces must remain available for common use and not be exclusively assigned or reserved to any individual use.
- iv Similar: 1 space/200 gross sq. ft. of building area Guest: 1 space for every 4 units min.

Parking Mixed-Use Development

Use	Requirements
Residential	1 Bedroom Unit – 1 space (Minimum)
	2-3 Bedroom Unit – 1.5 spaces (Minimum)
	Guest - per 4 units
Lodging (Motel or Hotel)	1 space per room
Live/Work	Unit under 1500 sq. ft. = 1 space per unit;
	Unit greater than 1500 sq. ft. = 1 space/400 gross sq. ft.
Civic, Retail, Office	1 space per 300 square feet gross floor area
Food Uses (Restaurant, Café, or similar)	1 space per 200 square feet gross floor area

4.8F.5 Additional Submittal Requirements for Mixed-Use Development

The intent for additional submittal criteria listed below is to provide a framework to assist the City in evaluating mixed-use developments that encourage a high quality of living now and in the future.

a Vision Statement for the Proposed Project

A clear vision and set of goals to guide the design, financing, marketing, and management strategies needed to ensure a strong and sustainable project. What is the main purpose of the proposed mixed-use project? Who are the primary and secondary users? What are the desired outcomes and impacts of your project?

b Location and Market Analysis Statement

- i Mixed-use projects are expected to have a positive impact on the surrounding neighborhood from both an economic and quality-of-life standpoint. As such, not all properties are ideal options for considering mixed-use development, and not all areas are suited to draw quality commercial uses to support residents, or are in an area that can support increased foot traffic, or be served adequately by public transportation.
- ii Describe how the project meets the identified need, its anticipated future performance, and sustainability. Market research is crucial for mixed-use development projects in a given area. Identify how the proposed mixed-use project meets local demand or supply, and how the project is not merely a risky speculative construction scheme.

c Project Design

- i High-quality architectural design is essential for creating an attractive streetscape and pedestrian-friendly environment. Plans for a mixed-use development must consider the physical, functional, and aesthetic aspects of the project's design, such as the scale, density, height, orientation, circulation, accessibility, privacy, architectural appearance, and fit into the surrounding context and community. How buildings relate to each other, streets and public spaces can determine the effectiveness and value of a mixed-use development.

d Property Management

- i Since mixed-use developments consist of a blend of residential, commercial (e.g., retail, office, etc.), and industrial uses, a property management company is required to address the diverse needs of commercial tenants and residents to eliminate or minimize conflicts (e.g. noise, excessive foot traffic, parking, etc.).
- ii Any agreement, resolution, or other document establishing a joint owner's association, a landscape assessment district, or a maintenance agreement shall include the City as a third-party beneficiary with the right, but not the obligation, to enforce said agreement, resolution, or other document. The agreement, resolution, or other document shall be subject to review and approval by the City Attorney.

e Design Review Approval Required

The City will consider each mixed-use proposal on a case-by-case basis to ensure, to the greatest extent possible, that the project will be compatible with surrounding land uses. All new mixed-use projects, or changes/additions to existing mixed-use projects, are subject to a Precise Plan of Design (PPD) reviewed for approval by the Planning Commission.

- i Public Notice is required following the City's public notice guidelines.
- ii The Planning Commission shall have authority through conditions imposed on a proposed development plan to prohibit uses not specified herein, on a use-by-use basis, if the Planning Commission finds that the proposed uses will have a significant adverse impact on adjacent uses, or will not promote harmonious development of the area.
- iii Any decision made by the Planning Commission may be appealed to the City Council in the manner provided in Title 1, chapter 4, of the MMC.

f Findings

The Planning Commission shall review the project and ensure that each of the following findings have been made to approve the proposed mixed-use project:

- i The mixed-use project complies with the standards and development criteria outlined in this section, and the underlying zoning district.
- ii The mixed-use project design addresses and mitigates potential noise, light, hazardous materials, and other land use compatibility issues between residential and commercial uses on the site as well as on adjacent properties.
- iii The architectural design of the mixed-use project is attractive as viewed from the street and neighboring properties and is of a size and scale compatible with surrounding properties.
- iv The mixed-use project does not rely on, or expect, public improvements, utilities, and additional services to be provided and/or constructed by the City at taxpayer expense.

4.9 Administration

4.9A Review Authority

The standards and other requirements of this Development Code shall be administered and enforced by the Planning Division of the Montclair Community Development Department, the Planning Commission, and City Council in the same manner as the provisions of the City's Zoning Code.

- a Chapter 11.04 of the Municipal Code specifies standards for Development Plan Review application. The following types of applications shall be reviewed and acted upon by the identified body or individual:
 - i. Ministerial Applications: The Planning Director shall be the Review Authority for ministerial applications as listed in Table 4.9.1.
 - ii. Discretionary Applications: Discretionary application shall be reviewed as listed in Table 4.9.1.

4.9.B Minor Deviations

- a A Minor Deviation permits a design solution that is not consistent with a specific provision of this Development Code, but is justified by its ability to fulfill this Specific Plan's intent while not compromising its vision and intent.
- b All development standards within this Development Code may deviate up to a maximum of ten percent (10%).
- c Minor Deviations are subject to Community Development Director review prior to Planning Commission consideration.

4.9.C Compatibility and Architectural Review

For the purposes of maintaining consistency with this Specific Plan; establishing a high standard of architectural quality and design variety; and maintaining compatibility with adjoining properties, all development applications within the AHMUD Specific Plan area will be subject to an external peer review. Such a review will be conducted by an architect, urban designer, or planner in private practice, as chosen by the review authority (City). The review authority shall establish fees and procedures for such review.

	Planning Director	Planning Commission	City Council
Ministerial			
Zoning clearance	Approve		
Outdoor dining in r.o.w.	Approve		
Outdoor display	Approve		
Discretionary			
Site plan		Final Decision	Review Appeal
Conditional use permit (CUP)		Final Decision	Review Appeal
Zoning change		Recommend	Final Decision
Subdivision		Final Decision	
Variance		Final Decision	Review Appeal
Specific Plan Amendment		Recommend	Final Decision

Table 4.9.1 Review Authority

4.10 Glossary

4.10Bi Definitions of General Terms

- 4.10A

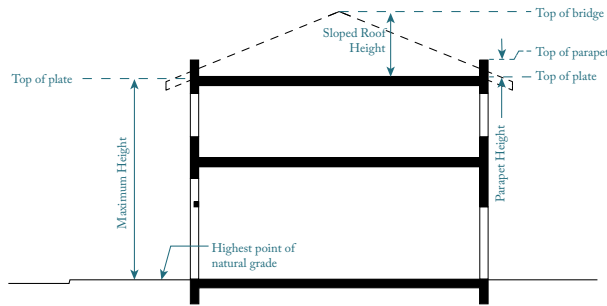
Purpose

This Section provides definitions of terms and phrases used in this Development Code that are technical or specialized, or that may not reflect common usage. If any of the definitions in this Section conflict with definitions in the Zoning Code or other provisions of the Municipal Code, these definitions shall control for the purposes of this Development Code. If a word is not defined in this Section, or in other provisions of the Montclair Municipal Code, the Community Development Director shall determine the correct definition.
- 4.10B

Definitions of Specialized Terms and Phrases

As used in this Development Code, each of the following terms and phrases shall have the meaning ascribed to them in this Section, unless the context in which they are used clearly requires otherwise.

Access, Building	The physical point(s) and type of pedestrian access to a building from the public sidewalk. Also see building access.
Access, Site	The physical point(s) of vehicular access from a street or other right-of-way to a site.
Access, Lot	The physical point(s) of access to an individual lot for vehicles and pedestrians from a street, other right-of-way or sidewalk.
Accessory Structure	A subordinate building such as garages, carports, storage sheds, shelters, pools or similar structures, the use of which is incidental to that of the main building (residential, commercial, or industrial) on the same lot and/or building site.
Alley	A low capacity thoroughfare with one shared lane and no parking lanes, designed and intended for service and/or secondary access purposes.
Block	All property fronting on one side of a street between intersecting and intercepting streets, or between a street and a right-of-way, wash, end of an existing cul-de-sac, or city boundary.
Block-scale Building	The physical appearance when a building by its physical length and overall size is massed and organized to be larger than a house in its footprint.
Building Access	<p>Buildings are accessed in either of two ways: walk-up or point access.</p> <p>Walk-up access means when an individual suite or dwelling is accessed by the pedestrian directly from a public sidewalk or private open space directly accessed by the public sidewalk. House-scale buildings are accessed only by walk-up access.</p> <p>Point access means when individual suites or dwellings are accessed by the pedestrian through a common lobby. Block-scale buildings are accessed by either walk-up or point-access or, through a combination of the two.</p>
Building Accessory	Building(s) which are in support of the main building on the lot and subject to the building placement standards for accessory buildings.



Building Height

The vertical extent of a building measured in stories to the eave of the highest story, not including a raised basement or a habitable attic. Height limits do not apply to masts, belfries, clock towers, chimney flues, and similar structures. Building height shall be measured from the average grade of the fronting thoroughfare.

Building Type

A structure defined by the combination of configuration, placement and function.

- **Mixed-Use Block.** A building designed for occupancy by retail, service, and/ or office uses on the ground floor, with upper floors also configured for service, office, and/or residential uses as allowed by each zone's use requirements. The upper floor may be accessed directly from the street via a stair or through a street-level lobby.
- **Court.** A group of attached dwelling units arranged to share one or more common courtyards, with pedestrian visitor access taken from the courtyard, Front Street, and/or Side Street. The courtyard is intended to be a semi-public outdoor area that is visible from and accessed from the Primary Street. Courtyard buildings may accommodate housing as well as ground floor commercial uses in either a live-work configuration or as commercial/retail space that directly faces the Primary Street, if and as allowed by the zone.
- **Liner.** A building that conceals a larger building such as a public garage, theater, or "big box" store that is designed for occupancy by retail, service, and/or office uses on the ground floor, with upper floors also configured for those uses or for residences.
- **Rowhouse.** Two or more detached two- or three-story dwellings arrayed side by side with zero lot line side setbacks. The building is located at the front of the lot, with the garage at the rear of the lot, separated from the primary building by a rear yard. An integrated housing unit and working space, occupied and utilized by a single household in a structure, either single-family or multi-family, that has been designed or structurally modified to accommodate joint residential occupancy and work activity.
- **Stacked Dwelling.** A building comprised typically of single-floor dwellings usually accessed from a central lobby that provides access to individual units via corridors. Ground floor dwellings facing the street may additionally take direct access via stoops or semi-private dooryards.
- **Flex Shed.** A building designed for occupancy by light manufacturing, workshop, and warehouse uses.
- **Live-work.** An integrated housing unit and work space, occupied and utilized by a single household in a structure, either single family or multi-family, that has been designed to accommodate ground floor work uses and ground floor and/or upper floor residential uses.
- **Quad/Triplex/Duplex.** Duplexes, triplexes, and quadplexes are multiple dwelling forms that are architecturally presented as large single family houses. Each dwelling unit is accessed directly from the street.
- **Single-family residence.** A structure occupied by one primary residence that also accommodates commercial uses as allowed.
- **Carriage House.** A Carriage House is an attached or detached residence, also known as an accessory dwelling, which provides complete independent living facilities and which is located or established on the same lot on which a single-family residence is located. Such dwellings may contain permanent provisions for living, sleeping, eating, cooking, and sanitation.

Civic	The term defining not-for-profit organizations dedicated to the arts, culture, education, government, transit and municipal parking facilities.	Paseo	A public place or path designed for walking that is often within a block, between buildings or along the edge of a public open space.
Civic Building	A structure operated by governmental or not-for-profit organizations and limited to civic uses.	Pedestrian Shed	An area centered on a major destination. Its size is limited by an average distance that may be traversed at an easy walking pace in a given amount of time from its center to its edge. Pedestrian sheds are useful for planning walkable areas.
Civic Space	An outdoor area dedicated for civic activities.	Podium	A continuous projecting base or pedestal under a building.
Courtyard	An unroofed area that is completely or partially enclosed by walls or buildings.	Setback	The area of a lot measured from a lot line to a building facade or elevation that must be maintained clear of permanent structures excepting galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces and decks (that align with the first floor level) which are permitted to encroach into the setback.
Driveway	A vehicular lane within a lot, or shared between two lots, usually leading to a garage, other parking or loading area.	Setback line, front yard (frontage zone).	The line which defines the depth of the required front yard for the lot in order to generate and support the intended public realm of the zoning district.
Encroachment	Any architectural feature or structural element, such as a fence, garden wall, porch, stoop, balcony, bay window, terrace or deck, that breaks the plane of a vertical or horizontal regulatory limit extending into a setback, beyond the build-to-line, into the public frontage, or above a height limit.	Setback line, rear yard or side yard.	The line which defines the width or depth of the required rear or side yard. Said setback line shall be parallel with the property line, removed therefrom by the perpendicular distance prescribed for the yard in the zoning district.
Frontage	A strip or extent of land abutting a thoroughfare, civic space or other public right-of-way. a. Frontage, Private. The area between the building facade and the shared lot line between the public right-of-way and the lot. b. Frontage, Public. The area between the curb of the vehicular lanes and the edge of the right-of-way.	Story	A habitable level within a building from finished floor to finished ceiling.
House-scale	The physical appearance when a building is massed and organized similar to a single-family house.	Transitional Buildings	A building or portion thereof which was lawful when established but which does not conform to subsequently established zoning or zoning regulations.
Infill	A site developed within existing urban fabric, balancing, completing and/or repairing the surrounding area.	Transitional Use	A land use lawful when established but which does not conform to subsequently established zoning regulations.
Lot	A parcel of real property shown on a plat recorded in the office of the county recorder.	Yard	The area between the Buildable Area on a lot and a lot line. a. Yard, Front. The area between the Buildable Area on a lot and the front lot line. b. Yard, Side. The area bounded by the Building Area on a lot, a side lot line, the front lot line and the rear lot line. c. Yard, Rear. The area between the Buildable Area on a lot and the rear lot line.
Massing	The overall shape or arrangement of the bulk or volume of buildings and structures.		
Overlay	The method for addressing additional topics such as certain types of commercial frontage.		
Parapet	A low wall along the edge of a roof or the portion of a wall that extends above the roof line.		
Parkway (planting strip)	The landscaped area between the sidewalk and the curb which is reserved for trees which are intended to shade the sidewalk and provide spatial definition to the streetscape.		

4.10B.ii Definitions Of Land Use Terms

Adult Day Program	Means a community-based facility or program that provides care to persons 18 years of age or older in need of personal care services, supervision, or assistance essential for sustaining the activities of daily living or for the protection of these individuals on less than a 24-hour basis	Bar, Lounge, Tavern, Cabaret, or Nightclub	Means an establishment where the main use is to sell or serve alcoholic beverages for consumption on the premises, which are not part of a larger restaurant, and where persons under twenty-one (21) years of age are not permitted to enter and remain on the premises. The operator is required to obtain City approval and hold a duly issued license from the California State Department of Alcoholic Beverage Control (ABC). Food may or may not be served.
Adult Business	See Chapter 11.40 of the MMC.	Beauty Salon/Barber Shop	An establishment in which hairdressing, makeup, and similar cosmetic treatments are carried out professionally.
Animal Boarding, Kennels	Means a commercial facility (i.e. Kennel) for the grooming, keeping, boarding, or maintaining domestic pets not owned by the owner or operator on a 24 hour basis on a short term basis (generally 3 months or less). Refer to Title 5, Section 5.08.100 of the MMC.	Business Support Services	Means an establishment within a building that provides services to other businesses. Examples of these services include: <ul style="list-style-type: none">• Blueprinting• Computer-related Services (rental, repair)• Copying and Quick Printing Services• Film Processing and Photo Finishing (Retail)• Protective Services• Security Systems Services.
Amusement Arcade or Amusement Game Arcade	Means any establishment containing four or more amusement devices or an establishment with less than four amusement devices when such devices occupy more than five percent of the total floor area of the business. For this title, a typical device and playing area requires 10 square feet.	Caretaker Dwelling	Means a permanent residence that is secondary or accessory to the primary use of the property and used for housing a caretaker employed on the site of any nonresidential use where needed for security purposes. A mobile or motor home does not substitute a caretaker unit.
Animal Hospital, Veterinary Clinic	Means an office and indoor medical treatment facilities used by veterinarians, including large and small animal veterinary clinics, and animal hospitals.	Child Care Center	Means a State licensed commercial child care facility (not a family day care home) such as infant care centers, preschools, extended day care facilities, and school-age child care centers that provide non-medical care and supervision for minor children for periods of less than 24 hours and is licensed by the California State Department of Social Services.
Animal Shelter	Means a place where stray, lost, abandoned, or surrendered animals – mostly dogs and cats – are housed.		
Auction House	Is a business establishment that facilitates the buying and selling of assets, such as works of art and collectibles.		
Botanical Gardens and Arboretums	Means public or private facilities for the demonstration and observation of the cultivation of flowers, fruits, vegetables, or ornamental plants.		
Banquet Facility	Means a building, or portion thereof, for rent by the public which has a principal intended use of holding events, including but not limited to, assembly, meeting, reception, banquet, dinner, lunch, breakfast, dance, concert, or seminar. Alcoholic beverages with meals may be provided for events as part of the overall approval of the facility subject to the approval of a separate ABC license.		

Cigar, Cigarette, Electronic Cigarette, Smoke Shops

Means a business establishment that either exclusively sells or has more than five percent of their floor area dedicated to the sale, display, distribution, delivery, or marketing of tobacco, tobacco products, and smoking paraphernalia but does not provide for on-premises use of tobacco products. Tobacco shall mean the preparation of the nicotine-rich leaves of the tobacco plant for smoking, vaping, heating, inhaling, chewing, absorbing, dissolving, or ingesting by any other means. Tobacco products may include, but are not limited to cigarettes, cigars, e-cigarettes, vaporizers or similar electronic devices that deliver nicotine to the person inhaling from the device, hookahs, pipes, bongos, and related devices for holding burning material.” Such facilities shall also comply with the following:

- Such establishments shall be located a minimum of 1,000 feet, measured property line to property line, from any public or private school serving students in grades Pre-K through 12; and
- Such establishments shall be located a minimum of 1,000 feet, measured property line to property line, from one another.
- No person who is younger than the minimum age of 18 years or as established by State law shall be permitted to enter the premises to purchase or barter, sell, display, market, trade, or exchange any combination of tobacco, tobacco products, or smoking or exchange of tobacco paraphernalia, including electronic smoking devices and accessories.

Civic Facility or Governmental

Means a structure owned, operated, or occupied by the City or other governmental agency to provide a governmental service to the public (e.g., City Hall, community recreation center, post office, library, police or fire station, etc.).

Club, Lodge, Private Meeting Hall

means a permanent meeting facility for organizations operating on a membership basis for the promotion of the interests of the members, including facilities for:

- | | |
|--|---|
| • Business Association | • Political Organizations |
| • Civic, Social, and Fraternal Organizations | • Professional Membership Organizations |
| • Labor Union and Similar Organizations | • Other Membership Organizations |

Commercial Recreation, Indoor

Means an establishment providing indoor amusement and entertainment services for a fee or admission charge, including:

- | | |
|-------------------------------------|---|
| • Bowling alley | • Amusement arcade (video games, pinball, or similar use) |
| • Ice skating and roller skating | • Pool and billiard rooms as primary uses |
| • Dance halls, clubs, and ballrooms | • Batting cages |

This use does not include adult-oriented businesses, which are separately defined in the MMC. Four or more electronic games or coin-operated amusements in any establishment, or premises where 50 percent or more of the floor area is occupied by amusement devices, are considered an electronic game arcade as described above; three or fewer machines are not considered a land use separate from the primary use of the site.

Commercial Recreation, Outdoor

Means facilities for various outdoor recreational activities, where a fee is charged for use. Examples include:

- | | |
|-----------------------------|-------------------------|
| • Amusement and theme parks | • Miniature golf course |
| • Go-cart tracks | • Water slides |
| • Golf driving range | • Batting cages |

May also include commercial facilities customarily associated with the above outdoor commercial recreational uses, including bars and restaurants, video game arcades, etc.

Contractor’s Storage Yard

Means a parcel of land, a minimum of 15,000 square feet in size, with permitted structures and site improvements, for the storage of equipment and materials used in the construction industry including, but not limited to, trucks or trailers, vans, bulldozers, cranes, mixers, backhoes and other similar equipment and/or stockpiles of construction supplies or materials, such as concrete, gravel, woodchips, logs, plant stock, metal, masonry, plumbing or electrical supplies, and other similar materials. This use also includes businesses involved in landscaping construction and maintenance services.

Convenience Market	Means a retail establishment that is less than 3,000 SF in size and sells prepackaged food items, beverages, and small convenience items (e.g., periodicals, tobacco, miscellaneous household and pharmaceutical goods, etc.) primarily for off-site consumption. Convenience markets typically have late hours of operation and are located along heavily traveled streets. Includes an establishment located within or associated with another use (e.g., fuel stations).	Funeral Home	Means a facility where funeral memorial services or receptions are conducted. Cremation and embalming services are prohibited.
Cultural Institution	Means a public or private institution that displays or preserves objects of community or cultural interest in one or more of the arts or sciences. Illustrative examples of these uses include libraries, museums, botanic gardens, registered historic properties, etc.	Fueling Station or Gas Station	Means a retail business selling gasoline and/or other motor vehicle fuels, and related products. A fueling or gas station may also include a convenience store, car wash, and restaurant facilities.
Dry Cleaning and Laundry Plants	A place where clothes or fabrics are dry cleaned, dyed and/or laundered as part of a commercial business, whether or not such clothes were deposited by a customer at that location, or transported from another location, as part of a service. This use includes all establishments subject to Section 19233 of the State of California Business and Professions Code, regulating Dry Cleaning Plants, but excludes laundromats and cleaners as defined in this chapter.	Health/Fitness Facility	A fitness center, gymnasium, health, and athletic club, in a space greater than 2,000 SF, which may include any of the following activities: sauna, spa or hot tub facilities; indoor tennis, handball, racquetball, archery, and shooting ranges and other indoor sports activities. Does not include adult entertainment businesses.
Drop off dry cleaner	Means a business where customers drop off clothing to be washed, dried, and pressed at an offsite location. Clothing is then returned to the customer at a later date and time. Seamstress and tailoring services may be allowed as an ancillary use.	Home Occupation	Means a commercial activity conducted in a dwelling located in a Residential, or Mixed-Use zoning district, which is incidental and secondary to the use of the dwelling for residential purposes and following the provisions of Section Chapter 11.58 of the Montclair Municipal Code.
Emergency shelter	Shall have the same meaning as that term defined in California Health and Safety Code Section 50801, which currently is defined to mean housing with minimal supportive services for homeless persons that is limited to occupancy of six months or less by a homeless person. No individual or household may be denied emergency shelter because of an inability to pay.	Hospital	Means an institution for the diagnosis, care, and treatment of human illness. These establishments have an organized medical staff, inpatient beds, and equipment and facilities to provide complete health care. May include on-site accessory clinics and laboratories, accessory retail uses, and emergency heliports.
		Hotel or Motel	Means a facility with guest rooms or suites, with or without kitchens, rented to the general public for transient lodging. Hotels typically include a variety of services in addition to lodging; including restaurants, meeting facilities, personal services, etc. Also includes accessory guest amenities such as swimming pools, tennis courts, indoor athletic facilities, accessory retail uses, etc.

**Laboratory; Medical,
Dental or Clinical
Laboratory, Research**

An establishment for scientific analysis of blood, tissue or other human or animal components.
means a business for scientific research, investigation, testing or experimentation, but not for manufacturing or sales of products.

Laundromat and Cleaners

A business which offers self-service laundry and/or dry-cleaning machines and dryers, which are coin, token or otherwise fee operated. This use excludes dry cleaning plants.

**Laundromat, Coin
Laundry**

Means an establishment equipped with self-service washing machines and dryers, usually coin-operated

**Manufacturing, Food
Products**

Means the preparing and packaging of food products derived from items such as milled grains and oilseeds, sugar and confectionery products, fruit and vegetable preserving, and specialty dairy, seafood, and precooked meat items. Bread and tortilla baking and production of snack foods, roasted nuts and peanut butter, coffee, and tea, flavoring syrup and concentrate, seasoning and dressing, spice and extract, and all other miscellaneous food manufacturing. No animal slaughtering businesses are allowed in any zone.

**Manufacturing, Light
Intensity**

Manufacturing practices occurring entirely within an enclosed building and that do not utilize intensive machinery or production equipment. Light intensity manufacturing activities include and are similar to the following uses:

- Assembly plants
- Small-scale construction shops (e.g., cabinet shops, furniture, etc.)
- Factories making food products and/or canning of consumer goods (not including slaughterhouses)
- Garment Manufacturing
- Sign Fabrication (indoors)
- Electronic or computer equipment development and/or research
- Machine Shops
- Metal Ironwork and fabrication.

**Manufacturing, Heavy
Intensity**

Medium to large-scale businesses that utilize raw products and energy-intensive machinery to produce large and complex products, and likely to result in noise, fumes, odors, or other potentially adverse environmental impacts. Unlike light manufacturing activities, the products made in heavy industrial zones are less likely to be used by consumers but are for use by other businesses or governments that may or may not eventually use them in consumer products. Heavy intensity manufacturing uses include but are not limited to:

- Manufacturing of construction equipment, infrastructure products, energy-related products, heavy-duty tools, or machinery
- Chemical Processing, refineries
- Transportation-related manufacturing or equipment for auto, train, aircraft, buses, or public transport
- Foundries (producing metal castings) and metal fabrication
- Rendering or Refining of Fats and Oils
- Recycling/Transfer/Collection or Material Recovery Facilities
- Research Development and Testing of products with or without outdoor operation.

Manufacturing, Small-Scale Artisan

Means a small-scale arts and crafts business such as, but not limited to, the production of goods crafted from textiles, wood, metal, gemstones, food or beverage items, or other products created in, and sold from the same building. Industrial activities intended for mass production and utilizing energy-intensive machinery to produce large and complex products are not considered small-scale artisan manufacturing. Examples of these products include:

- Candles, soaps, incense
- Pottery, ceramics and mosaics
- Jewelry and costume novelties
- Musical instruments
- Sewing, needlework, quilting
- Small products made of leather, wood, glass, metal art, etc.
- The sale of artisan products directly to the public from the premises may be allowed, provided the floor area devoted to display and sales is less than 20 percent of the floor area of the building or lease space used for craft making.

Massage Establishment

Means any enterprise or establishment having a fixed place of business where any person engages in, conducts, carries on, or permits to be engaged in, conducted, or carried on, any of the activities outlined in the definition of massage and requirements of Chapter 4.56 of the Montclair Municipal Code.

Medical Services

Means a facility other than a hospital where medical, dental, mental health, ophthalmology, optometry, audiology, surgical, and/or other personal health care services are provided on an outpatient basis by licensed individuals within a facility duly licensed by the State of California. Includes chiropractic and acupuncture services, physical therapy services, physiatrist care, and urgent care facilities.

Microbrewery

Means a small-scale brewery facility making specialty beer in limited quantities. These facilities may also include an on-site tasting room and off sale of beverages limited to beers brewed on site. Microbreweries are subject to separate approval of an ABC license.

Mini or Self Storage

A structure or group of structures divided into storage units, stalls or lockers of no more than five hundred (500) square feet in area each and which are offered to the public for a fee for the storage of goods.

Regulations and Conditions:

1. The area of the proposed site shall be at least one (1) acre.
2. The use shall be established and maintained in accordance with all applicable Local, State and Federal laws.
3. All storage shall be inside an enclosed building; no outdoor storage shall be permitted.
4. Each storage unit shall have an individual door to the outdoors and shall be accessible by the owner of the storage items in accordance with hours of operation approved by the Planning Commission. Such hours of operation shall be posted at the entrance to the facility.
5. The storage of perishable, flammable, toxic, or hazardous substances and the use of the facility to store goods or products for commercial or industrial purposes shall be prohibited.
6. No activities except for rental of storage units, and pick-up and deposit of storage shall be permitted.
7. Parking shall be provided as needed for the office.
8. All parking, maneuvering and drive lane areas shall be provided with a paved surface and all drive aisles shall be twenty-five (25) feet in width. The Planning Commission shall approve the circulation pattern within the site, which shall be clearly marked.
9. All exterior lighting shall be in accordance with MMC 10.16.
10. All signs shall be in compliance with the provisions of MMC 11.72.
11. Landscaping shall be provided in accordance with MMC 11.60.
12. Security for the facility shall comply with Building Security Requirements of MMC 10.16.

Mobility Hub	Means a physical location that provides a point in the transportation network that seamlessly integrates different modes of transportation, multi-modal supportive infrastructure, and place-making strategies to create activity centers that maximize first-mile last-mile connectivity.	Pawn Shops	Means a shop or business (also called a pawnshop or pawnbroker) who loans money to people who bring in valuable items which they leave with the pawnbroker. The valuable items that people leave are called the “collateral.”
Neighborhood or Specialty Food Stores	Means stores with a gross floor area of less than 10,000 SF selling a range of food products, confectionery, baked goods, dairy products, ice cream, meat (butcher), seafood, produce (except farmers markets and certified farmers’ markets), ethnic food items, soft drinks, tea and coffee drinks, purified water, and other specialty foods. The term includes delicatessens. On or off-sale of alcoholic beverages on the premises are subject to the approval of a Conditional Use Permit.	Pet Store	Means an establishment engaged in the retail sale of domestic pets and associated supplies (e.g., food, cages, apparel, equipment, and items for their care and comfort.
Nursing Home	Means a lodging and care facility for convalescents, or aged persons, not including persons suffering from contagious or mental diseases, alcoholism, or drug addiction, and in which surgery is not performed and primary treatment, as given in hospitals or sanitariums, is not provided. The terms “Nursing Home,” “Convalescent Home,” “Convalescent Hospital,” “Rest Home,” or “Rehab Facility” may be used interchangeably.	Postal and Stationary Services	Means a business establishment that offers one or more of the following services such as mailbox rental, notary service, Fed Ex, UPS, USPS, stamps, rubber stamps, fax, packing, shipping, packing supplies, office supplies, key cutting, laminating, copying, business cards, passport photos, document shredding, gift items, and greeting cards.
Parking Facility, Public or Commercial	Means both day use and long-term public and commercial garages, parking lots, and structures, except when accessory to a primary use. All primary uses are expected to include any customer or public use off-street parking required by the Corridors Plan. Facilities constructed by the City or other public agency and intended for public use are exempt from the conditional use permit requirement, but remain subject to City design review and approval.	Professional/Administrative	Means an office-type facility occupied by businesses that provide direct professional services and/or are engaged in the production of intellectual property. Examples of these uses include, but are not limited to: <ul style="list-style-type: none"> • Accounting, auditing, and bookkeeping services • Advertising agencies • Attorneys • Commercial art and design services • Construction contractors (Office facilities only) • Photographers and photography studios • General office services • Financial management and investment counseling • Landscape architecture, urban planning • Management and public relations services • Design services including architecture, engineering • Educational, scientific, and research organizations • Postal and stationery services.
		Public Assembly	Means publicly or privately owned or operated facilities for major public assembly, recreation, sports, amusements, or entertainment, including civic or community auditoriums, sports stadiums, convention facilities, and exhibition halls.

Real Estate Services Office

Means businesses specializing in real estate sales (agents and brokers), property managers and appraisers, mortgage companies, escrow and listing services, etc.

Recycling Processing Facility

Means any variety of facilities involved with the collection, sorting, and processing of recyclable materials, including green waste. Recyclable materials include reusable domestic containers and other materials that can be reconstituted, re-manufactured, or reused in an altered form, including glass, metals, paper, and plastic. Recyclable material does not include refuse or hazardous materials.

Recycling Redemption Center

Means a facility, use, or structure for the collection of recyclable goods, including metal or aluminum beverage containers and newspapers.

Religious Facility

Means a facility operated by a religious organization to provide a place for worship or the promotion of religious activities. This use includes churches, mosques, synagogues, temples, etc., and their accessory uses on the same site, such as living quarters for staff, fund-raising sales, bazaars, dinners, parties, or other outdoor events on the same site. May include full-time educational institutions such as seminary, full-time parochial education, and fields.

Research and Development (R&D)

Means a business engaged in the following activities: 1) industrial, biological or scientific research; and/or 2) product or process design, development, prototyping, or testing. This may include labs, offices, warehousing, and light manufacturing functions as part of the overall Research and Development use.

Residential Care Facilities

Means facilities that are licensed by the State of California, designed to provide permanent living accommodations and 24-hour primarily nonmedical care and supervision for persons in need of personal services, supervision, protection, or assistance for sustaining the activities of daily living. Living accommodations are shared living quarters with or without separate kitchen or bathroom facilities for each room or unit. This classification includes facilities that are operated for profit as well as those operated by public or not-for-profit institutions, including hospices, nursing homes, convalescent facilities, assisted living facilities, and group homes for minors, persons with disabilities, and people in recovery from alcohol or drug addictions. This use classification excludes transitional housing and social service facilities located in existing single-family residences licensed separately by the State of California.

Retail Bakery

Any establishment whose principal business is the sale of baked products such as breads, cakes, cookies, donuts, pastries, bagels, rolls, and similar products. These products are then baked and sold mainly at retail on the premises for consumption off-premises. The sale of any beverages shall be incidental to the primary business. The public floor plan, including indoor customer seating, shall not be more than 20% of the total premises. The total premise shall not exceed 2000 square feet.

Restaurant, General

Means an establishment (includes a café or coffee shop) for selling prepared foods and/or drinks for on-premises consumption as the principal use of the establishment. This definition does not include a snack bar or refreshment stand at a public or private recreational facility which is operated solely by the agency or group operating the recreational facility for the convenience of the patrons of the facility.

Restaurant, Fast Food

Means franchised or independently-operated restaurants where customers are served prepared food and non-alcoholic drinks (i.e., coffee) from an ordering counter or drive-through window, for either on- or off-premises consumption. This use does not include a mobile food vendor.

Restaurant, Buffet, or Cafeteria

Means a food business where a variety of dishes are presented in a line or food station, from which diners in a restaurant serve themselves, usually at a fixed price; or a restaurant in which patrons wait on themselves, carrying their food to tables from counters where it is displayed and served.

Retail, Bulk Merchandise

Means retail establishments engaged in selling goods or merchandise in bulk as differentiated from general retail by either of the following characteristics:

1. A high volume of sales of related and/or unrelated products in a warehouse setting (i.e., “big box” retail).
2. The sale of goods or merchandise that require a large amount of floor space and that are both warehoused and retailed at the same location. Does not include marine sales, warehousing, wholesaling, or vehicle sales.
3. Membership requirements.

Retail, General

Retail establishments, completely within an enclosed building, engaged in selling new goods or merchandise to the general public. Examples of these establishments and lines of merchandise include:

- Antiques (not thrift stores)
- Apparel, shoes, and related accessories
- Appliances, electronic equipment, or furniture
- Art gallery
- Artist and hobby supplies, office supplies, and stationery
- Automotive parts and accessories (indoor sales only – no service bays)
- Bicycle sales and rentals
- Books, toys and games shops
- Cameras and photographic supplies
- Carpeting and floor covering
- Drug and discount stores
- Fabrics and sewing supplies
- Florists and houseplant stores (indoor sales only).
- Furniture (new)
- Jewelry, gift, collectibles items shops
- Hardware, housewares, and kitchen utensils
- Luggage and leather goods
- Medical supplies and equipment
- Musical instruments, parts, and accessories
- Optical/Auditory products
- Cosmetics and beauty supplies
- Sporting goods and equipment
- Feed and tackle supplies (indoors only).

Salvage Yards

Means a business operation engaged in disassembling, sorting, and the temporary storage and distribution of recyclable or reusable scrap and waste materials, including auto wreckers engaged in dismantling automobiles for scrap, and obtaining parts, metal, or materials for re-use or resale or the storage and keeping of such dismantled parts.

School	Means a private or public academic educational institution, including: <ul style="list-style-type: none"> • Boarding school • Community college, college, or university • Elementary, middle, and junior high schools • High school • Military academy 	Services, General	Means establishments providing non-medical services to individuals as a primary use. These uses may also include accessory retail sales of products related to the services provided. Examples of these uses include, but are not limited to, the following: <ul style="list-style-type: none"> • Barber and Beauty Shops (with no Massage) • Clothing Rental • Facial and Skin Care • Home Electronics and Small Appliance Repair • Tailoring (Clothes) • Locksmiths • Nail Shops • Waxing and Skin Care • Eyebrow Threading, Eye Lashes, and Micro Blading
School – specialized education and training	Means a private academic educational institution, in lease spaces greater than 2,000 SF in area, providing specialized education/training. Examples include the following: <ul style="list-style-type: none"> • Art school • Ballet and other dance schools • Business, secretarial and vocational • Computers and electronics school • Drama school • Driver education school • Trade school (plumbing, electrical, HVAC, etc.) • Language school • Music school • Professional school (law, medicine, etc.) • Seminaries/religious ministry training facility 	Services, Restricted	Means personal services that may have a potentially negative adverse impact upon surrounding areas (see Chapter 4 of the Montclair Municipal Code for additional information on restricted uses). Examples of restricted uses include the following: <ul style="list-style-type: none"> • Check cashing or gold exchange stores • Day Spas, saunas, hot tubs, and tanning as single-use facilities. Does not include adult-oriented businesses as per Chapter 11.40 of the MMC. • Massage Service (See Chapter 4.56 of the Montclair Municipal Code) • Tattoo, derma graphics and body piercing services • Psychics, palm reading, and fortune telling • Card Rooms • Hypnosis • Pawnshops • Adult-oriented businesses per Chapter 11.40 of the MMC.
Seasonal Product Sales	Means the sale of products for a limited duration of time, usually associated with a seasonal holiday, including but not limited to, sales of Christmas trees and pumpkins.		

Studio, Instructional - art, dance, martial arts, music, etc.

Means small-scale facilities, occupying less than 2,000 square feet, typically accommodating one group of students at a time, in no more than one instructional space. Larger facilities are considered “Schools” or “Fitness/Health Facilities” depending on the type of activity. Examples of these facilities include individual and group instruction and training in the arts; production rehearsal; photography, and the processing of photographs produced only by users of the studio facilities; martial arts training studios; gymnastics, yoga, and similar instruction; and aerobics and gymnastics studios with no other fitness facilities or equipment.

Supermarket

Means a retail establishment that generally maintains a minimum gross floor area of 10,000 square feet or greater and carries a broad range of food products (e.g., fresh fruits; fresh vegetables; baked goods, meat, poultry, and/or fish products; frozen foods; processed and prepackaged foods, etc.). May include accessory uses (coffee kiosks, ATMs, banks, etc.). Does not include a “Convenience market.”

Temporary Structures

Means a structure (e.g., classroom, office, tent, or similar portable structure, including a manufactured or mobile unit—does not apply to mobile home parks) installed or erected for a limited time, or in conjunction with a permitted event, use, or construction project. Temporary structures shall be removed at the end or completion of the approved event, project, or use.

Temporary Uses

Means uses of limited duration.

- Use, Primary – means the main purpose for which a site is developed and occupied, including the activities that are conducted on the site a majority of the hours during which activities occur.
- Use, Temporary – means a use of a building, property or land area, that is limited in duration of time (e.g., interim, nonpermanent, and/or seasonal), and does not permanently change the character or physical facilities of the premises or property and is in keeping with the purposes listed in the district where it is located.

Utility Facility

Means a permanent structure or facility subject to local control and includes energy generating plants, electrical substations, above-ground electrical transmission lines, lone switching structures, refuse collection, transfer recycling or disposal facilities, water reservoirs, flood control or drainage facilities, water or wastewater treatment plants, transportation or communications utilities, and similar facilities of public agencies or public utilities (includes electrical distribution lines, underground water lines, underground sewer lines, etc).

Vehicle Parts Sales, New	Means stores that sell new automobile parts and accessories. Establishments that provide installation services are considered Vehicle Repair, Minor. Businesses dealing exclusively in used parts are included under Vehicle Wrecking/Dismantling/Salvage.	Warehouse, Cold Storage/ Ancillary Use	Means a warehouse building equipped with refrigerated rooms or areas designed for storing temperature-sensitive products, often-perishable goods like fresh produce, frozen foods or beverages, pharmaceutical products, or similar, occupying less than 25 percent of the gross floor area of the building.
Vehicle Repair, Major	Means establishments that perform collision repair and other bodywork, painting services; tire recapping, and engine overhauling, particularly for medium and heavy-duty trucks, trailers, and construction equipment, with a gross vehicle weight rating (GVWR) over 8,500 pounds.	Warehouse, Cold Storage/ Primary Use	Means a warehouse building equipped with refrigerated rooms or areas designed for storing temperature-sensitive products, often-perishable goods like fresh produce, frozen foods or beverages, pharmaceutical products, or similar, occupying more than 25 percent of the gross floor area of the building. Applications for Cold Storage/Primary Use Warehouse shall require the submittal of a Truck Traffic Analysis/Study.
Vehicle Repair, Minor	Means small facilities providing limited repair and maintenance services. Examples include car stereo and alarm installers; detailing services; muffler and radiator shops; quick-lube services; tire (not including recapping) and battery sales and installation, and smog checks.	Warehouse, Fulfillment, or Distribution Center	Is a warehouse building, also referred to as a high-cube warehouse (HCW), primarily used for temporary storage and/or consolidation of manufactured goods before the direct distribution of products to end users. Fulfillment centers often require outdoor delivery vehicle parking and generate a significant number of vehicle trips. Applications for a Warehouse, Fulfillment, or Distribution Center Warehouse shall require the submittal of a Truck Traffic Analysis/Study.
Vehicle Sales and Rental	Means a retail or wholesale establishment selling and/or renting automobiles, trucks and vans, recreational vehicles, trailers, and motorcycles. May also include repair shops and the sales of parts and accessories, incidental to vehicle dealerships. Does not include the sale of mobile homes or boats.	Warehouse, General	An enclosed building used for storing goods in a secure location, which can sometimes include receiving and processing inventory, picking and packing orders, and transporting purchases to stores or customers. This definition also includes the incidental storage of equipment, goods, and stock associated with and used by an approved business operation for the subject site.
Vehicle Storage	Means a facility or site used for the long-term storage of operative cars, trucks, buses, recreational vehicles, and other motor vehicles, for clients. Does not include a dismantling yard or tow yard.	Work-live Studio	An incidental residential living space associated only with an approved artisanal business, occupying one (1) or more rooms or floors in a building primarily designed and used for industrial or commercial occupancy. Living space shall mean that portion of a work/live studio used for residential purposes including, but not limited to, a sleeping area, a food preparation area with reasonable workspace, and a full bathroom including bathing and sanitary facilities meeting applicable codes.
Vehicle Wrecking, Dismantling, Salvage Yard	Means a business operation engaged in disassembling, sorting, and the temporary storage and distribution of recyclable or reusable scrap and waste materials, including auto wreckers engaged in dismantling automobiles for scrap, and obtaining parts, metal, or materials for re-use or resale or the storage and keeping of such dismantled parts.		





5

Implementation

This Section describes the steps and actions to implement the Arrow Highway Mixed-Use District (AHMUD) Specific Plan based on collaboration with community members, City decision makers, and City Staff.

The AHMUD Specific Plan is designed to be implemented over the next 14 years (2021—2035) by residents, businesses, property owners, non-profit organizations, community groups, city and county agencies, and elected and appointed officials. Some actions are straightforward and relatively easy to achieve, others will demand significant investment of time and resources and will require steadfast commitment on numerous levels.

The City will need to develop robust partnerships with local businesses, residents, and other public agencies to fully implement the vision outlined for AHMUD. These partnerships will be crucial to ensuring the most important strategies are being implemented, and the most pressing community needs are being addressed.

Time frames: Each action includes a time frame, within which the action should be carried out. These are intended to provide a general sense of how long it will take to implement the action.

Ongoing	Some actions require continuous monitoring or effort. These are identified as ongoing actions.
Immediate	Begin work immediately.
Near term	Begin work within 1 to 3 years.
Mid term	Begin work within 4 to 7 years.
Long term	Begin work within 7 to 10 years.

Implementers: Agencies and partners most likely to carry out the action. Most actions include one or more City departments. In some cases, however, the action is entirely within the private or non-profit sector.

Funding Sources: Potential funding sources are identified for each action item.

Funding Sources

To carry out the actions recommended in this Plan, a variety of Federal, State, regional, local, and private funding sources have been identified:

Federal

Community Development Block Grant

This fund accounts for activities of the Community Development Block Grant received from the U.S. Department of Housing and Urban Development, including monies received from this agency as part of the federal stimulus program.

National Endowment for the Arts Grants for Arts Projects

The NEA Grants fund institutions whose projects are vehicles for any of the following: the portfolio of American Art is expanded, Americans throughout the nation experience art, and Americans of all ages acquire knowledge or skills in the arts, and American communities are strengthened through the arts.

State

Enhanced Infrastructure Financing District (EIFD)

An EIFD is a governmental entity established by a city that carries out a plan within a defined area (boundaries of which do not need to be contiguous) to construct, improve and rehabilitate infrastructure; construct housing, libraries, and parks; remediate brownfields, etc.

Active Transportation Program (ATP)

ATP taps both state and federal funds for bike and pedestrian projects across California. The program allows cities to compete for grants to build bicycle/pedestrian paths, install bike racks, and other projects or programs that make walking or biking easier, safer and more convenient.

Caltrans Transportation Planning Grant

The Caltrans Sustainable Transportation Planning Grants seek to fund projects that ensure consideration of sustainability, preservation, mobility, safety, innovation, economy, health, and equity in transportation planning.

California Strategic Growth Council (CSGC)

The CSGC provides grants to cities to promote sustainable community planning and natural resource conservation. The grant program supports development, adoption, and implementation of various planning elements in three focus areas: Local Sustainable Planning, Regional SB 375 Plus, and Regional Planning Activities with Multiple Partners.

California Arts Council (CAC)

CAC offers grants as well as provides a great resource to search for other grants by applicant type and field.

Proposition 68

Authorizes \$4 billion in general obligation bonds for: creation and rehabilitation of state and local parks, natural resources protection projects, climate adaptation

projects, water quality and supply projects, and flood protection.

Proposition 41

Authorizes \$600 million in general obligation bonds for affordable multifamily supportive housing to relieve homelessness, affordable transitional housing, affordable rental housing, or related facilities for veterans and their families.

Highway Users Tax Account (HUTA)

The HUTA, also known as the Gas Tax, can be used for any street or road purpose including new construction, bikeways, curbs, landscaping, signs, signals etc.

Regional

AQMD Program

The AB 2766 Motor Vehicle Subvention Program is a funding source for cities to encourage the development of measures or projects that result in the reduction of motor vehicle emissions. Projects include alternate fuels/electric vehicles, vehicle emissions abatement, land use strategies that encourage people to walk, bike or use public transit, traffic management, transportation demand management, effective bike expenditures, PM reduction strategies, and public education.

SCAG Sustainable Planning Grant

The Southern California Association of Government (SCAG) offers direct funding of innovative planning initiatives for member cities through the Sustainability Planning Grants program. The Sustainability Planning Grants Program

provides direct technical assistance to SCAG member jurisdictions to complete planning and policy efforts that enable implementation of the regional SCS. Grants are available in the following three categories:

Integrated Land Use – Sustainable Land Use Planning, Transit Oriented Development (TOD) and Land Use & Transportation Integration;

Active Transportation – Bicycle, Pedestrian and Safe Routes to School Plans; and

Green Region – Natural Resource Plans, Climate Action Plans (CAPs) and Green House Gas (GHG) Reduction programs.

Measure I

San Bernardino County voters approved passage of Measure I authorizing the San Bernardino County Transportation Authority to impose a one-half cents sales tax to finance transportation improvement and traffic management programs. Cities are apportioned the “local share” of Measure I monies on a per capita basis and in accordance with a transportation plan and capital improvement program adopted by the local jurisdiction.

Cities may also receive funding for regional projects pursuant to San Bernardino County Transportation Authority’s (SBCTA) five-year and twenty-year plans. In the past, Montclair was allocated regional monies for the Montclair Multi-Modal Transit Center and the Monte Vista Avenue Extension projects.

Local

Capital Improvement Program (CIP)

The CIP identifies all of the major projects to be undertaken to improve facilities and infrastructure within the city. During the fiscal year, a separate CIP document that reflects the current year program and proposes a program of prioritized projects for the next four to five years is prepared. City Departments submit all proposed projects in the foreseeable future, along with their best cost-estimate. The request includes the year a project will commence, any funding sources that may be available with either future sources or ones which might have been previously designated, justification for the project, and on-going costs expected to occur after the project has been completed. The CIP budget team then compiles the information and presents a draft CIP program to the City Council. Projects are prioritized, based on City Council and staff input.

General Fund

The General Fund is the City's largest single fund type used to account for basic City services such as police, fire, design and development, community resources, and general administration. The three major sources of revenue are property, sales, and transient occupancy taxes. For the 20-21 budget, the breakdown is as follows:

Total Revenue \$27,448,646.

Sales Tax \$11,081,810 or 40.4% of the total.

Property Tax \$6,668,558 or 24.3% of the total.

TOT \$50,000 or 0.2% of the total.

Property Business Improvement District

A Property and Business Improvement District (PBID) is a mechanism of funding improvements through assessments to businesses and real property within the established PBID boundaries. Under the Property and Business Improvement District Law of 1994, revenues from PBID assessments may be used to fund capital improvements and maintenance costs for projects such as parking facilities, street furniture, public restrooms, art, parks, street and streetscape enhancements, and plazas. A PBID formation petition, which is initiated by property owners, requires the signature of more than 50 percent of the property owners, weighted by assessment liability.

Park Dedication Fees

The City receives fees from developers to fund recreation facilities. These funds are used for qualified recreational purposes throughout the City.

Various Grant Funds

Various Federal, State, and regional grant programs distribute funding for public improvements. Because grant programs are typically competitive, grant funds are an unpredictable funding source.

Developer Contributions

Development Impact Fee

The City charges one-time impact fees on new private development to offset the cost of improving or expanding City facilities to accommodate the project. Impact fees are used to help fund the construction or expansion of needed capital

improvements.

Development Agreements

Development agreements are contracts negotiated between project proponents and public agencies that govern the land uses that may be allowed in a particular project. Development agreements provide a developer with assurances for a specified length of time that the proposed project may proceed as originally approved, and not be affected by future changes in land use regulations. In exchange for this assurance, the landowner/developer may agree to public improvements, land dedications, or in-lieu fees, as negotiated with the City, as a condition of the agreement.

1. Our Natural Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P1.1	Promote alternative transportation modes like walking, biking, and transit that reduce emissions related to vehicular travel.				
A1.1	Redesign Arrow Highway and Central Avenue to promote walking, biking, ridesharing, public transit use, the use of alternative fuel vehicles or other clean engine technologies.	Short-term	PW, CD, AQMD, HS	\$\$	Grants, General Funds, Developers
P1.2	Promote and require the integration of Green Infrastructure into storm water management systems.				
A1.2a	Review and revise development regulations to encourage a green approach in new developments. Minimize impervious areas. Develop new projects and retrofit existing surfaces to reduce runoff through infiltration.	Short-term	CD, PW	\$	General Fund
A1.2b	Incorporate Green Street elements into the re-design of Arrow Highway and Central Avenue.	On-going	PW, CD, CBWCD	\$	General Fund
A1.2c	Promote the use of green roofs, bio-swales, pervious materials for hardscape, and other stormwater management practices to reduce water pollution.	Short-term	PW, CD	\$	Grants, General Fund
A1.2d	Promote the use of captured rainwater, grey water, or recycled water.	Short-term	PW, CD	\$	General Fund
A1.2e	Require landscaping for all new development to use drought tolerant plants.	On-going	CD	\$	Developers

2. Our Prosperous Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
Placemaking & District Identity					
P2.1	Implement multi-modal connectivity and pedestrian enhancements				
A2.1a	Implement multi-modal access from transit to AHMUD along Arrow Highway.	Short-term	PW, Foothill Transit, Omnitrans, Riverside Transit Agency	\$\$\$	Grants
A2.2b	Prioritize capital investments in street improvements within the East AHMUD to create a sense of place.	Short-term	PW, CD	\$\$\$	CIP, Grants
P2.2	Create an innovative and flexible land use framework.				
A2.2a	Establish specific East AHMUD district.	Short-term	CD	\$	General Funds
A2.2b	Prepare affordable housing policy in the AHMUD.	Short-term	CD	\$	General Funds
A2.2c	Require a minimum amount of commercial development in mixed-use housing projects.	On-going	CD, ED	\$	General Funds
A2.2d	Discourage warehousing and heavier industrial uses.	On-going	CD, ED	\$	General Funds
P2.3	Improve District amenities.				
A2.3a	Establish a strategic central open space within AHMUD.	Mid-term	CM, CD, ED, PW, CS	\$\$\$\$	Developers, Quimby Fees, Impact Fees, Grants
A2.3b	Pilot tactical urbanism to “activate” precise AHMUD spaces.	Short-term	CM, CD, ED, PW, CS	\$	Developers, Quimby Fees, Impact Fees, Grants
A2.3c	Pilot a façade improvement program for businesses.	Short-term	ED, CD	\$\$	CDBG, Grants

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
Economic Diversity					
P2.4	Identify high growth and high value-add industries.				
A2.4a	Establish Workforce Development Pipeline through Partnerships and Incubator.	Short-term	ED	\$	General Funds
A2.4b	Establish programs to develop a skilled pipeline to target industries.	Short-term	ED	\$	General Funds
A2.4c	Explore partnership with local universities.	Short-term	CM, ED	\$	General Funds
A2.4d	Establish an incubator space to grow companies in target industries.	Mid-term	CM, ED	\$\$\$\$	Grants, Property Owners
P2.5	Encourage retail uses that support an entrepreneurial ecosystem.				
A2.5	Provide incentives for certain retail businesses to locate in AHMUD.	Short-term	CM, ED	\$\$	General Funds, Grants

Funding and Governance

P2.6	Identify seed funding for catalytic early actions.				
A2.6a	Set aside Economic Development Fund resources to jumpstart economic development activities in the AHMUD.	Short-term	CM, ED	\$\$	General Funds
A2.6b	Establish an EIFD/CRIA to fund AHMUD infrastructure investments.	Short-term	CM, ED	\$	General Funds
A2.6c	Negotiate community benefits for economic development.	On-going	CM, CD	\$	General Funds
P2.7	Ensure proposed developments are net fiscally positive.				
A2.7	Use a fiscal model to assess project revenue potential.	Immediate	CM, CD	\$	General Funds
P2.8	Gauge participation in a Business Improvement District (BID) to provide high-quality district services.				
A2.8a	Build a coalition of AHMUD property owners to coordinate with the City's vision.	Mid-term	CM, ED	\$	General Funds
A2.8b	Establish the Business Improvement District (BID) for district maintenance and programming.	Mid-term	CM, ED	\$\$	General Funds

3. Our Well Planned Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P3.1	Conserve the commercial character and scale of the AHMUD (East) area, including, while creating places of enduring quality that are uniquely fit to their time and place.				
A3.1a	Develop and adopt a form-based development code that supports existing commercial uses and encourages expansion while permitting infill development with context sensitive architecture, urban design, and landscaping.	Immediate	CD	\$\$	General Funds, Grants
A3.1b	Introduce new infill buildings and renovate existing buildings in a manner that enhances AHMUD's walkable urbanism with interconnected streets lined by buildings that engage, frame, and activate the street.	On-going	CD, Developers	\$-\$\$\$	General Funds, CDBG, Grants
P3.2	In AHMUD (West), leverage the creek channel to orient the new buildings and their front-ages to the Creek Corridor and be respectful of the scale of the existing residential context.				
A3.2	Develop and adopt a form-based development code that requires the highest standards of context sensitive architecture, urban design, and landscaping.	Immediate	CD	\$\$	General Funds, Grants
P3.3	Remove regulatory and procedural barriers to good design.				
A3.3	Develop and adopt a Form-Based Code for the Downtown area that emphasizes pedestrian orientation, integration of land uses, treatment of streetscapes as community living space, and offers a streamlined development review process.	Immediate	CD	\$\$	General Funds, Grants
P3.4	Expand the inventory of publicly accessible community gathering spaces so that businesses and residents are within a short walking distance of a park or recreational area.				
A3.4a	New buildings should incorporate public realm improvements described in the AHMUD Vision and integrate such improvements into their existing context in a way that enhances AHMUD's public space network.	On-going	CD, PW, Developers	\$	General Funds
A3.4b	Allow parklets on Arrow Highway to provide visual interest and expand the useable area of the sidewalk.	On-going	CD, PW, Property Owners, PBID	\$\$	Grants
P3.5	Encourage green projects and practices.				
A3.5	Incentivize sustainable living and business practices, both passive and active, that encourage energy efficiency, improve indoor air quality, and encourage water and resource conservation.	On-going	CD, PW, Businesses, Property Owners	\$-\$\$	Grants

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P3.6	Provide high quality housing for current and future residents with a diverse range of income levels.				
A3.6a	Provide for quality housing at a range of income levels and price points, emphasizing housing product that captures the underserved multi-family market.	On-going	CD, Developers	\$\$\$	Grants, Developers
A3.6b	Support workforce and market rate units that will expand and diversify AHMUD's housing stock, and support growth in AHMUD employment.	On-going	CD	\$	General Funds
A3.6c	Provide flexibility in development standards to encourage and facilitate nontraditional housing types and options, including single room occupancy, shared housing, co-housing, and cooperative-housing.	Immediate	CD	\$	General Funds

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
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Stormwater

P3.7	Utilize and maintain a robust stormwater conveyance system that protects the City from flooding impacts and ensures that storm flows are efficiently routed to regional drainage.				
A3.7a	Continue to regularly update City capital improvement program to ensure effective prioritization, funding, and construction of drainage improvements throughout AHMUD.	Ongoing	PW, CD	\$\$\$	General Fund, Grants
A3.7b	Enforce hydromodification control requirements on new developments, ensuring that increases in impervious surface do not result in increased peak flows and downstream scour.	Ongoing	PW, CD, Developers	\$	General Funds, Developers
P3.8	Effectively treat all urban runoff and stormwater and ensure that local groundwater supplies and downstream receiving waters are not degraded.				
A3.8a	Inspect all new developments during both construction and operational phases for compliance with local, regional, and state level water quality regulations.	On-going	PW	\$	General Funds
A3.8b	Encourage the implementation of low impact design features for all new development and redevelopment within AHMUD.	On-going	PW, CD, Developers	\$	General Funds, Developers

Wastewater

P3.9	Ensure wastewater in the AHMUD area is safely and efficiently conveyed and treated under all demand scenarios, including existing and future average and peak flow sewer flow scenarios.				
A3.9a	Continue to regularly update the City's capital improvement program in order to effectively prepare for sewer flows generated as a result of land use changes and new developments throughout AHMUD.	On-going	PW, IEUA	\$	General Fund, Grants
A3.9b	Construct, maintain, and revitalize distribution infrastructure as needed throughout the AHMUD in response to changes in demand, land use patterns, and aging infrastructure.	On-going	PW, IEUA	\$\$	General Fund, Grants

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
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Water

P3.10	Maintain, upgrade, and expand water pipeline, storage, and pumping infrastructure to meet projected domestic, commercial, and fire flow demands for all land uses within the AHMUD area.				
A3.10a	Coordinate with MVWD's capital improvement plan in order to effectively prepare for land use changes and new developments within the AHMUD area.	On-going	PW, MVWD	\$	General Funds, Grants
A3.10b	Construct, maintain, and revitalize distribution infrastructure as needed throughout the AHMUD area in response to changes in demands and land use patterns.	On-going	MVWD, Developers	\$\$	General Funds, Grants, Developers
A3.10c	Coordinate with the IEUA to expand range of recycled water infrastructure for efficient irrigation throughout the AHMUD area.	On-going	PW, IEUA, MVWD Developers	\$\$\$	General Funds, Developers

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
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Electricity and Natural Gas

P3.11	Ensure all AHMUD residents and businesses are safely and affordably supplied with electricity and natural gas throughout all future buildout scenarios.				
A3.11	Coordinate with SCE's and SoCalGas' capital improvement projects to ensure that any necessary connections or upgrades are adequately funded and constructed in a time-efficient manner.	On-going	PW, SoCal-Gas, SCE	\$	General Fund, Grants

Solid Waste

P3.12	Provide and maintain adequate and orderly systems for the efficient collection and disposal of solid waste for existing and future development throughout AHMUD.				
A3.12a	Coordinate with solid waste service provider to ensure that waste pickup, recycling, and disposal occurs in the most efficient and sustainable manner possible.	On-going	PW, Burrtec Waste Industries	\$	General Fund, Grants
A3.12b	Conduct outreach and education to reduce solid waste generation at the household and business level to minimize landfill loading.	On-going	PW, Burttec	\$\$	General Fund, Grants

4. Our Accessible Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P4.1	Create a transportation system that supports complete streets goals.				
A4.1a	Design new street connections in the AHMUD to minimize traffic volumes and/or speed, as appropriate, without compromising connectivity for emergency first responders, bicycles, and pedestrians.	On-going	PW, CD, SBCTA, OmniTrans, CalTrans, Developers	\$	Grants, Developers, General Fund
A4.1b	Design new streets and implement improvements on existing streets in the AHMUD to serve users of all ages and abilities, while prioritizing modes and design features based on the context of the street, including the surrounding land use, planned improvements, and collision history.	On-going	PW, CD, SBCTA, OmniTrans, CalTrans, Developers	\$	Grants, Developers, General Fund
A4.1c	Enhance the bike and pedestrian preferred facilities as part of development, private grants, signing of shared routes, maintenance activities, etc. consistent with the City's Active Transportation Plan, Systemic Safety Analysis Report Program and General Plan.	On-going	PW, CD, ED, Developers	\$\$\$	General Funds, Grants
P4.2	Establish mini-mobility hubs on Arrow Highway and at the new Central Park consistent with the General Plan approach.				
A4.2a	Provide signage and curb space for vehicle pick-up/drop-off, car share, microtransit, bike share, and e-scooters, as appropriate.	On-going	PW, Foothill Transit, OmniTrans, SBTCA, RTA	\$\$\$	General Funds, Grants
A4.2b	Create safe pedestrian and bicycle access to mobility hubs from major destinations; especially those connecting to the Metrolink station.	On-going	PW, Foothill Transit, OmniTrans, SBTCA, RTA	\$\$\$	General Funds, Grants
A4.2c	Provide secure commuter bicycle parking and locker options to businesses and mobility hubs.	Long-term	PW, CD, SBCTA, Developers	\$\$\$	General Funds, Grants, Developers
A4.2d	Convert bus stops into bus shelters.	On-going	PW, OmiTrans, SBCTA, CD, Developers	\$	Grants, Developers, General Funds
A4.2e	Consider multi-modal links to the Montclair Transcenter.	Mid-term	PW, OmiTrans, SBCTA, CD, Developers	\$\$\$	Grants, Developers, General Funds
A4.2f	Consider options and opportunities to partner with and support local businesses.	Short-term	PW, Chamber of Commerce	\$	Grants, General Funds

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P4.3	Establish amenities and support services for all modes.				
A4.3a	Create streetscapes and public realms that encourage walking and biking.	On-going	PW, CD, Developers	\$-\$\$\$	Grants, Developers, General Funds
A4.3b	Incorporate designs that remove barriers to pedestrian and disabled persons from adjoining roadways, including through pathways that do not require passing through parking areas, and that are connected to the sidewalk and transit stops.	Immediate	PW, CD, Developers	\$-\$\$\$	Grants, Developers, General Funds
A4.3c	Consider opportunities for future connections to the planned trail along the San Antonio Creek Channel Frontage.	Mid-term	PW, CD, Developers	\$-\$\$\$	Grants, Developers, General Funds
P4.4	Implement safety enhancements to reduce collisions.				
A4.4a	When performing street maintenance, consider safety improvements at all locations.	On-going	PW, PD, CD	\$-\$\$\$	Grants, General Funds
A4.4b	Use the City's Systemic Safety Analysis Report Program to identify high injury locations and pursue annual grant funding (such as Highway Safety Improvement Program or Active Transportation Plan) to implement safety improvements at those locations.	On-going	PW, HS	\$-\$\$	Grants, General Funds

5. Our Healthy Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P5.1	Create a multimodal transportation system that encourages active living and healthy life-styles in all areas of the City across a broad spectrum of ages, interests, and abilities.				
A5.1a	Design new streets and retrofit existing streets within AHMUD to prioritize active transportation.	Short-term	PW, CD, Developer	\$\$\$\$	Developers, General Funds, Grants
A5.1b	Make active transportation networks in AHMUD safe and accessible for all ages and abilities.	On-going	PW, CD, HS, Developer	\$	Developers, General Funds, Grants
A5.1c	Continue to implement health programs through the Healthy Montclair Initiative.	On-going	HS, PW, Community Groups	\$	Developers, General Funds, Grants
A5.1d	Develop the trail along the San Antonio Creek Channel in AHMUD West.	Mid-term	PW, HS, CBWCD, USACE, Developers	\$\$\$\$	Grants (ATP), General Fund
P5.2	Integrate AHMUD into the citywide healthy and equitable food system.				
A5.2a	Integrate growing food into large public and private open spaces within AHMUD.	On-going	CD, HS, Developers, Property Owners	\$	Developers, General Funds, Grants
A5.2b	Promote healthy food related events like Farmers Market in the central park area.	On-going	HS, ED, AS, CD, Community Groups, Chamber of Commerce, Local Businesses	\$	General Funds, Grants
A5.2c	Support entrepreneur food processing businesses.	On-going	ED, HS, AS, CD, Community Groups, Chamber of Commerce, Local Businesses	\$	General Funds, Grants
A5.2d	Develop the trail along the San Antonio Creek in AHMUD West.	Mid-term	PW, HS, CBWCD, USACE, Developers	\$\$\$\$	Grants (ATP), General Fund
P5.3	Minimize noise impacts to ensure that noise does not detract from Montclair's quality of life.				
A5.3	Require new development projects to implement mitigation measures, where necessary, to reduce exterior and interior noise levels.	On-going	CD, Developers	\$	General Funds, Developers

6. Our Safe Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P6.1	Make AHMUD streets safe for pedestrians and bicyclists.				
A6.1a	Carry out the safety enhancements recommended by the AHMUD Vision for Arrow Highway, Central Avenue, and Moreno Street.	Short-term	PW, PD	\$\$\$\$	Grants
A6.1b	Add mid-block crossings and parklets on Arrow Highway and Moreno Street.	Mid-term	PD, FD, PW, CD	\$\$\$\$	CIP, Grants, Developer
A6.1c	Amend the development codes to allow context sensitive street types.	Immediate	CD, PD, FD, PW	\$	General Funds
P6.2	Employ a range of contextual lighting options to promote safety and security on AHMUD streets.				
A6.2a	Identify AHMUD public streets and open spaces that are poorly lit and install context sensitive street lights.	Short-term	PW, CD, PD	\$	General Funds
A6.2b	Install string lights in alleys that provide connections to destinations.	Short-term	PW, CD, PD	\$	Grants, CIP
A6.2c	Require new development to demonstrate on a lighting plan appropriate level of direct and indirect lighting in the public and private realm.	On-going	CD, PD	\$	General Fund, Developer
P6.3	Reduce opportunities for criminal activity through physical design standards, recreation opportunities, educational programs, and counseling services.				
A6.3a	For new infill development and major rehabilitation, incorporate natural surveillance principles and best practices into development codes and review processes.	On-going	CD, PD	\$	General Fund, Developer
A6.3b	Amend the development codes to require building designs that promote safety by providing active street frontages.	On-going	CD, PD	\$	General Fund, Developer

7. Our Active Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P7.1	Expand park inventory in AHMUD to ensure the maximum distance between residents' homes and the nearest public park is 1/2 mile (1/4 mile preferred).				
A7.1a	Invest in the acquisition of new parkland.	On-going	CD, CM, PW, HS	\$	Quimby Fees, Developers, Grants
A7.1b	Develop trail along the San Antonio Creek Channel.	Mid-term	PW, HS, CBWCD, US-ACE, SBCFC, Developers	\$\$\$\$	Grants, General Fund
A7.1c	Amend development code to require new development to provide its fair share of public and private open spaces.	Immediate	CD	\$	General Fund, Developers
A7.1d	Expand the parks and recreation system through repurposing public land like excess streetspace, expansive parking lots, and other creative means.	On-going	PW, HS, CD	\$	General Fund, Grants
P7.2	Promote, expand, and protect a green infrastructure that links the natural habitat.				
A7.2a	Use parks as sponges to serve stormwater infrastructure and flood mitigation roles to enhance resiliency, recreational use, and beauty.	On-going	PW, CD, CBWCD, IEUA	\$	General Fund, Grants
A7.2b	Encourage simple, small, and low-cost demonstration green infrastructure projects both in the public and private realm.	On-going	PW, CD, CBWCD, IEUA, Developers, Property Owners	\$\$	General Fund, Grants
P7.3	Identify and remove barriers to access parks. Encourage walking & biking as preferred way to get to and from parks.				
A7.3a	Increase the number of entrances to existing parks to expand the number of residents within walking distance of a park.	On-going	PW, HS, CD, NR&PO	\$\$	General Fund
A7.3b	Prioritize new access or entry points near the proposed mobility hubs, including sidewalks, bike routes, trails, and transit.	On-going	PW, HS, CD, NR&PO	\$	General Fund

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P7.4	Explore creative or alternative funding opportunities for programs and capital projects.				
A7.4a	Explore establishing a Community Foundation or "Friends of Parks" organization for the purpose of soliciting park land donations; applying for private grants; and for fundraising.	On-going	HS, CM, CD	\$	General Funds
A7.4b	Explore naming rights, sponsorships and asset management opportunities to create ongoing revenue for maintenance and operations of existing facilities.	On-going	HS, CM, Developer	\$	General Funds
A7.4c	Explore contracts with private business to provide and operate some of the recreational activities which would be financed, constructed, and operated by the private sector with additional compensation paid to the City.	On-going	HS, Chamber of Commerce	\$	General Funds
A7.4d	Explore interlocal agreements between two or more local units of government and/or between a local unit of government and a non-profit organization for the joint usage/development of sports fields, regional parks, or other facilities.	Short-term	HS, PW, School Districts, Community Groups, County	\$	Grants, General Funds

8. Our Creative Community

	Policies/Actions	Time-frame	Responsible Party	Cost	Funding Source
P8.1	Develop and host an iconic Cultural Event in AHMUD.				
A8.1a	Identify the event promoter with the most promise to serve this role.	Mid-term	HS, ED	\$	General Funds, Grants
A8.1b	Connect available and underutilized venues with those seeking spaces for events.	On-going	ED, CD, Property Owners	\$	General Funds
A8.1c	Leverage parks and public spaces as vital locations to showcase and promote arts and culture in AHMUD.	On-going	PW, CD, HS, Arts and Cultural Providers	\$	General Funds, Grants
P8.2	Increase supply of affordable housing for artists.				
A8.2a	Create systems and procedures to spur the development of affordable artist housing through the use of innovative tax programs and the maximizing of financial tools.	Short-term	CD, ED	\$-\$	General Funds, Grants
A8.2b	Allow live-work building type in AHMUD particularly on primary streets where housing may not be desirable on street level.	Immediate	CD, ED	\$	General Funds, Grants
A8.2c	Partner with nonprofit organizations, for-profit developers, and grassroot groups that have a track record to successfully pull together the partners and resources needed to create affordable live/work spaces for artists.	On-going	CD, ED, HS, Arts and Cultural Providers	\$	General Funds, Grants
P8.3	Integrate public art into the development review & capital improvement program.				
A8.3	Identify public art opportunities and engage artists as early as possible in all projects.	On-going	CD, PW, HS, Arts and Cultural Groups, Developers	\$	General Funds, Art Funds, Grants, Developers
P8.4	Promote collaborative process of create powerful murals on blank walls that can transform public spaces, neighborhood identities, and individual lives.				
A8.4	Design and implement high quality temporary outdoor painted murals that reflect the stories and content researched from the larger Montclair community.	Mid-term	CD, ED, HS, Property Owners, Arts and Cultural Groups	\$-\$	General Funds, Art Funds, Grants

AS	Administrative Services
ATP	Active Transportation Program
BID	Business Improvement District
CBWCD	Chino Basin Water Conservation District
CD	Community Development
CFD	Community Facilities Districts
CIP	Capital Improvement Program
CTPG	Caltrans Planning Grant
CM	City Manager
DIF	Development Impact Fee
ED	Economic Development
F	Finance
FD	Fire Department
HS	Human Services
IEUA	Inland Empire Utilities Agency
IT	Information Technology
MVWD	Monte Vista Water District
PD	Police Department
PW	Public Works
SAWPA	Santa Ana Watershed Project Authority
SBCTA	San Bernardino County Transportation Authority
Costs	
\$	under \$100,000
\$	\$100,000 to \$250,000
\$	\$250,000 to \$1,00,000
\$	Over \$1,00,000