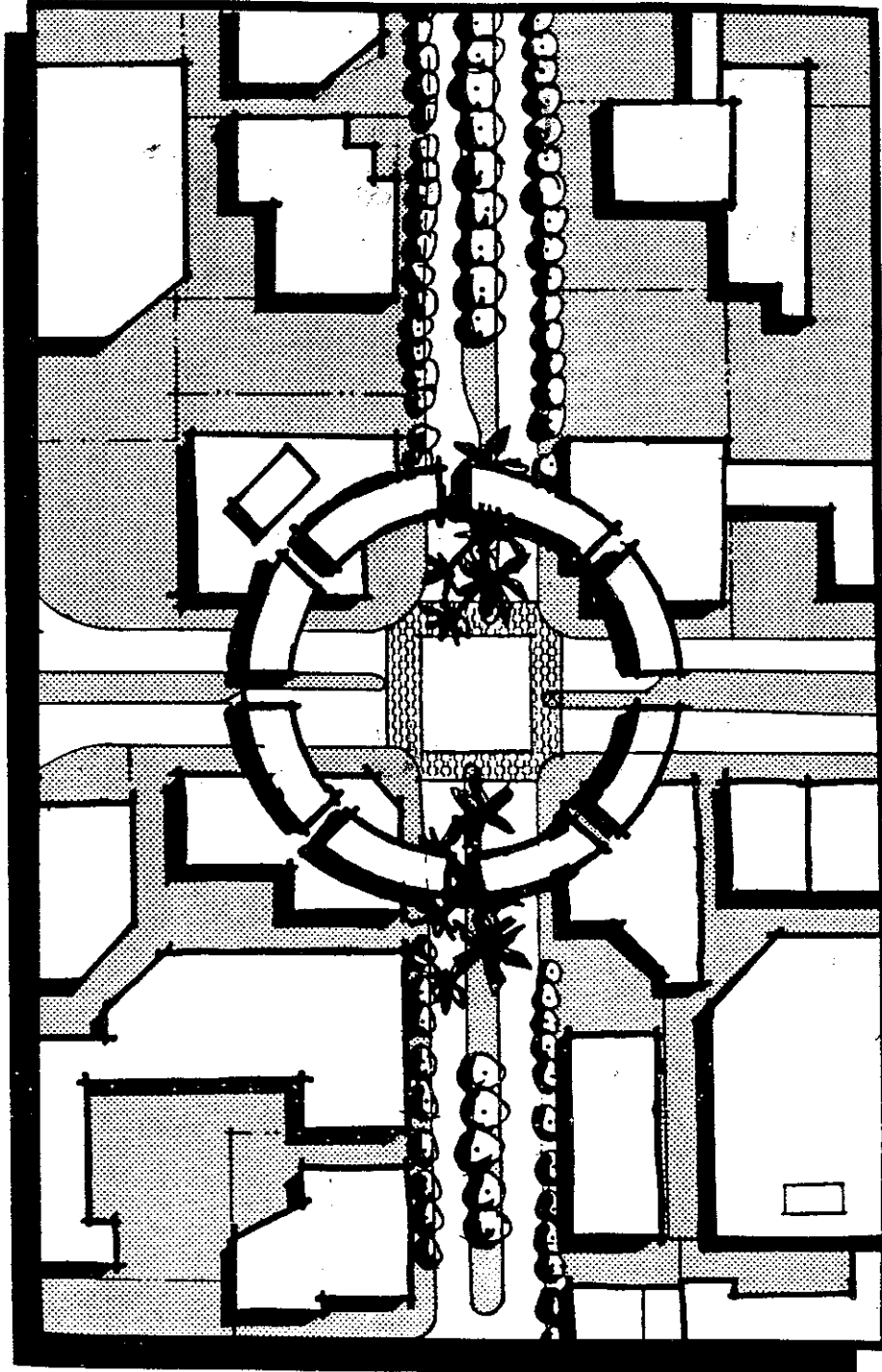


HOLT BOULEVARD SPECIFIC PLAN



CITY OF MONTCLAIR

PREPARED BY:

Planning Network
and
Urban Design Studio

FOR:

Redevelopment Agency of
the City of Montclair

City Council

Larry J. Rhinehart, Mayor
Paul Eaton, Mayor Pro Tem
Walter Hackett, Jr.
Elouise "Dolly" Lewman
Leonard Paulltz

Planning Commission

Luis Flores, Chairman
Maynard Lenhart,
Vice-Chairman
Frank Decoteau
John Dutrey
Bill Ruh

City Administrator

G. Michael Milhiser

Planning Division

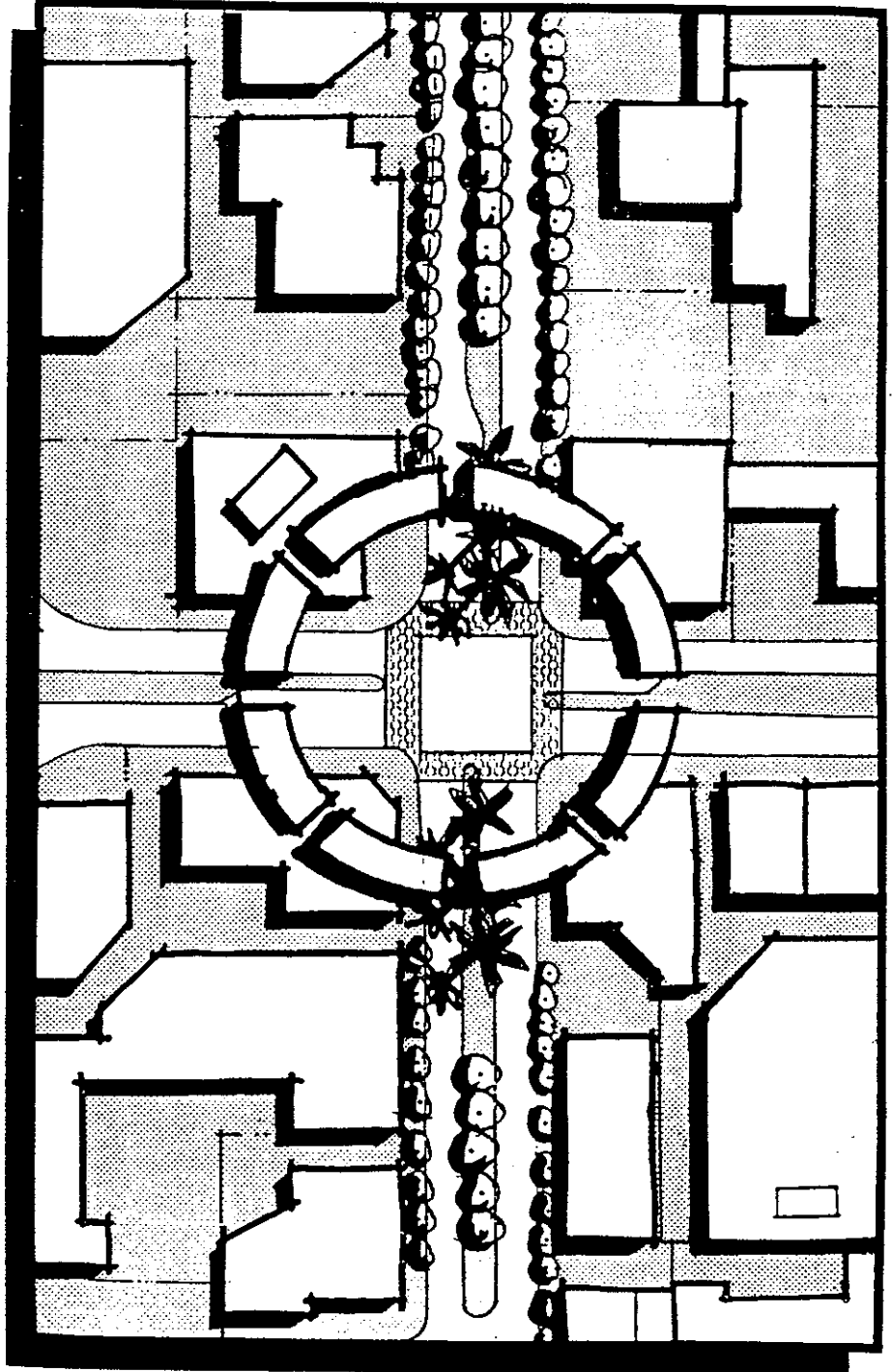
Robert W. Clark, Director of
Community Development

Hal G. Fredericksen,
City Planner

Jim S. Lai,
Associate Planner

Christine Sanchez,
Assistant Planner

HOLT BOULEVARD SPECIFIC PLAN



CITY OF MONTCLAIR

TABLE OF CONTENTS

I.	COMMUNITY BACKGROUND	
A.	INTRODUCTION	I-1
1.	PURPOSE	I-1
2.	PLANNING AREA	I-1
3.	AUTHORITY	I-1
4.	CALIFORNIA ENVIRONMENT QUALITY ACT COMPLIANCE	I-2
5.	PUBLIC PARTICIPATION	I-2
II.	DEVELOPMENT FRAMEWORK	
A.	PLAN ORGANIZATION	II-1
1.	COMMUNITY BACKGROUND	II-1
2.	GOALS, OBJECTIVES AND POLICIES	II-1
3.	CONCEPT PLANS	II-1
4.	DEVELOPMENT REGULATIONS	II-1
5.	IMPLEMENTATION AND ADMINISTRATION	II-1
6.	INITIAL STUDY	II-1
B.	COMMUNITY BACKGROUND	II-2
1.	EXISTING SETTING	II-2
2.	OPPORTUNITIES AND CONSTRAINTS	II-2
C.	GOALS AND OBJECTIVES STATEMENTS	II-11
1.	COMMUNITY DESIGN	II-11
2.	LAND USE/ECONOMIC	II-15
3.	CIRCULATION	II-16
4.	ENVIRONMENTAL	II-17
5.	INFRASTRUCTURE	II-18
6.	CRIME PREVENTION SERVICE	II-18
D.	CONCEPT PLANS	II-20
1.	LAND USE PLAN	II-20
III.	DESIGN GUIDELINES	
A.	INTENT AND PURPOSE OF THE GUIDELINES	III-1
1.	APPLICABILITY AND IMPLEMENTATION OF THE DESIGN GUIDELINES	III-1
2.	PRECISE PLAN OF DESIGN REQUIRED	III-2
3.	ORGANIZATION OF THE DESIGN GUIDELINES	III-3
B.	DESIGN GUIDELINES FOR COMMERCIAL DEVELOPMENT	III-4
1.	GENERAL DESIGN PRINCIPALS	III-4
2.	ARCHITECTURAL DESIGN GUIDELINES	III-5
3.	ROOFS	III-7
4.	AWNINGS AND CANOPIES	III-8
5.	LIGHTING	III-9
6.	CRIME PREVENTION TECHNIQUES	III-10
7.	SITE PLANNING	III-11
8.	PARKING AND CIRCULATION	III-13
9.	LANDSCAPING	III-16
10.	WALLS AND FENCES	III-18
11.	SCREENING	III-19
12.	AUTOMOTIVE DEALERSHIP RENOVATION	III-20

TABLE OF CONTENTS

C.	DEVELOPMENT STANDARDS	IV-27
1.	LANDSCAPING	IV-31
2.	PARKING REQUIREMENTS	IV-32
3.	LOADING REQUIREMENTS	IV-47
4.	OUTDOOR STORAGE	IV-49
5.	PERFORMANCE STANDARDS	IV-49
6.	HAZARDOUS MATERIALS AND WASTES	IV-52
7.	PLANNED DEVELOPMENTS	IV-52
D.	SPECIFIC USE DEVELOPMENT STANDARDS	IV-54
1.	AUTOMOBILE AND LIGHT TRUCK REPAIR FACILITIES	IV-54
2.	AUTOMOBILE DEALERSHIPS	IV-55
3.	AUTOMOBILE RENTAL AGENCIES	IV-55
4.	AUTOMOTIVE SERVICE STATIONS	IV-55
V.	IMPLEMENTATIONS	
A.	PHASING	V-1
1.	LAND DEVELOPMENT PHASING	V-1
B.	REMOVAL OF NON CONFORMING SIGNS	V-1
1.	PURPOSE	V-1
2.	REQUIREMENTS	V-1
3.	INCENTIVE PROGRAM	V-2
C.	FUNDING MECHANISMS	V-3
1.	POTENTIAL FUNDING MECHANISMS	V-3
2.	"DEVELOPER," "SHARED," AND "COMMON" FACILITIES	V-8
3.	RECOMMENDED FUNDING MECHANISMS	V-8
D.	NONFINANCE IMPLEMENTATION	V-11
1.	REGULATORY PROCEDURES	V-11
2.	CITY IMPLEMENTATION RESPONSIBILITIES	V-12

LIST OF TABLES

Table IV-1	Summary of Permitted Uses	IV-24
Table IV-2	Off-Street Parking Space Requirements	IV-41
Table IV-3	Widths for Double Loaded Parking Aisles with One-Way Traffic	IV-45
Table IV-4	Widths for Single Loaded Parking Aisles with One-Way Traffic	IV-45
Table IV-5	Widths for Double Loaded Parking Aisles with Two-Way Traffic	IV-45
Table IV-6	Widths for Single Loaded Parking Aisles with Two-Way Traffic	IV-45
Table V-1	Estimate Construction Costs	V-9
Table V-2	Implementation Responsibility Matrix	V-13



I. COMMUNITY BACKGROUND

I. COMMUNITY BACKGROUND

A. INTRODUCTION

1. PURPOSE

The purpose of this Specific Plan document is to guide land use and community design along the Holt Boulevard Corridor. To fulfill this purpose, the document provides a comprehensive plan for land use, development regulations, design guidelines, development incentives and promotes a comprehensive approach to implementing these actions.

The motivation to undertake this study was; "to establish a high quality, integrated and attractive developments" which will upgrade the City's image, along with creating job opportunities, providing support services and facilities, and upgrading the infrastructure and circulation system".

Upon adoption of this Specific Plan, this plan constitutes the legally established zoning development regulations, standards, and guidelines for properties within the planning area.

2. PLANNING AREA

The planning area extends approximately 2.2 miles along Holt Boulevard from Mills Avenue on the west to Benson Avenue on the east. Major intersecting streets include Monte Vista Avenue and Central Avenue (see Figure I-1).

The primary land uses along the corridor is commercial and industrial, with some office and residential. Holt Boulevard is a major east/west transportation route for the City of Montclair. It is dominated by the automobile with less pedestrian amenities. In the past, Holt Boulevard has proven to be a primary area for auto related uses and auto dealerships. Its location in the southern part of the City could well balance commercial and retail activities around Montclair Plaza, located in the northern part of the City. The improvement of the boulevard's physical appearance could bring commercial, retail, and auto related uses back to this area of the City.

3. AUTHORITY

The Holt Boulevard Specific Plan is established through the authority granted to the City of Montclair, by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (specific plans).

The California Government Code establishes the authority for a legislative body to adopt an ordinance or resolution requiring that a specific plan be prepared when it is in the public interest to do so. As with general plans, the Planning Commission must hold a public hearing before the planning agency can recommend the adoption of a specific plan by ordinance or resolution.

This Specific Plan is regulatory in nature, and serves as a zoning law for the properties involved. Development plans, site plans and tentative tract/parcel maps in this area must be consistent with both this Specific Plan and the City's General Plan. The scope of subjects covered in the Specific Plan is the same as that of the General Plan, to the extent that the subject under consideration involves the Holt Boulevard portion of the City.

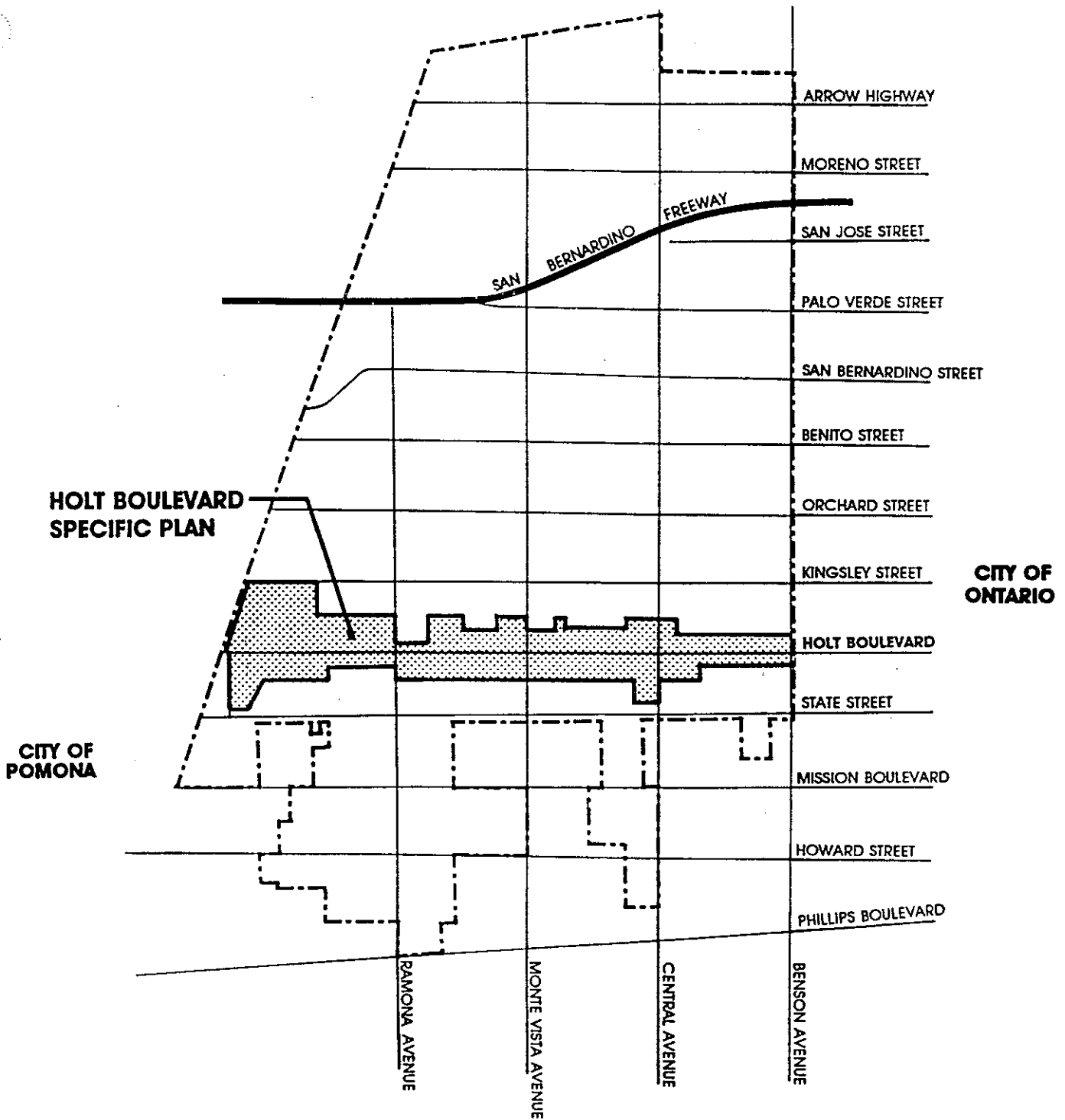
**4. CALIFORNIA
ENVIRONMENT
QUALITY ACT
COMPLIANCE**

In compliance with the requirements of the California Environmental Quality Act, an environmental assessment for the Holt Boulevard Specific Plan has been completed. The environmental assessment determined whether or not potential environmental impacts would be significant to require an Environmental Impact Report, or that the implementation of the Specific Plan would have the same or less impacts than implementation of the General Plan Policies and zoning currently in effect.

**5. PUBLIC
PARTICIPATION**

The Holt Boulevard Specific Plan has incorporated a high level of participation from property owners, residents, business owners, staff members, Planning Commissioners, City Council persons, and developers within the Specific Plan area. Through the process, several joint meetings/workshops were held with City Council and Planning Commission to obtain input and to reconfirm direction. Community input was also received at community workshops and will be received at public hearing on the Specific Plan before the Planning Commission and City Council.

FIGURE 1-1



AREA CONTEXT

HOLT BOULEVARD SPECIFIC PLAN

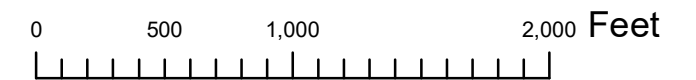
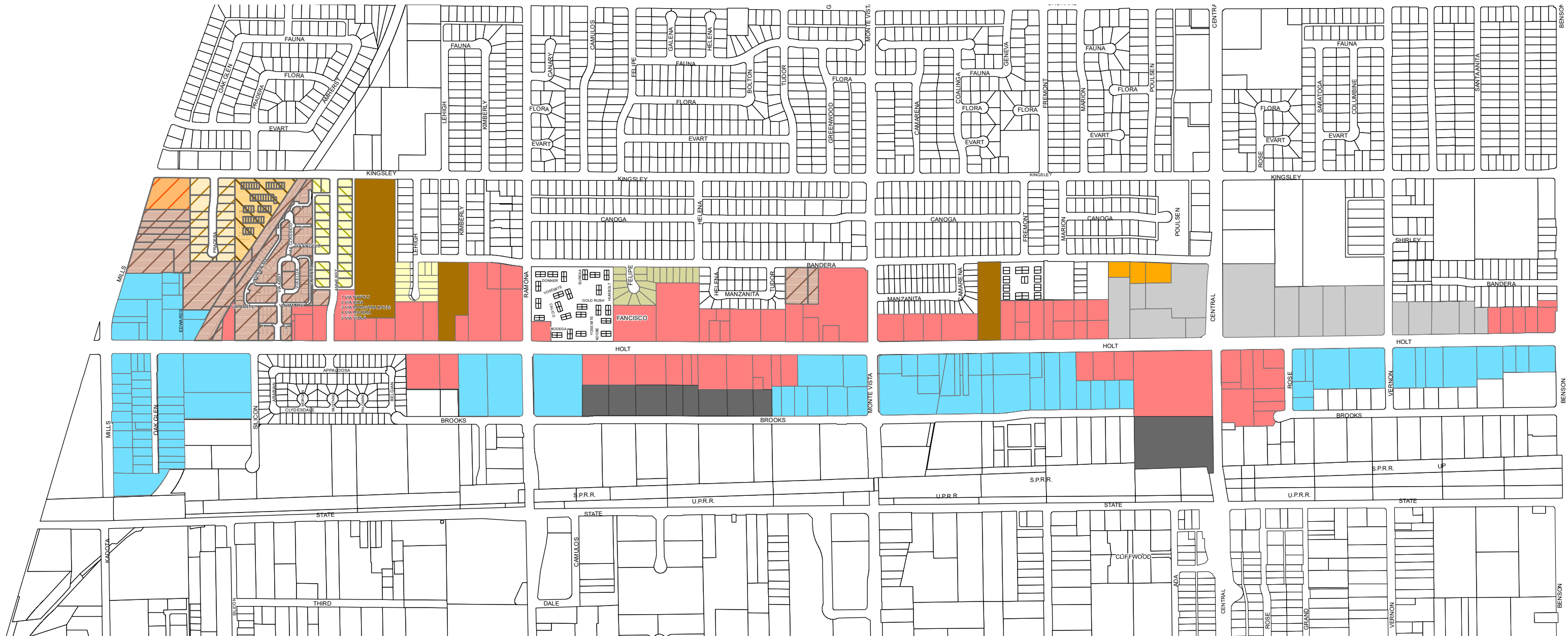
City of Montclair



ILLUSTRATING NETWORK

Holt Boulevard Specific Plan

Land Use Map



Legend

- | | | | | |
|---------------|------------------|---------------|----------------|----------------|
| Auto Mall | Commercial | R-1 (6 du/ac) | R-3 (10 du/ac) | R-3 (18 du/ac) |
| Business Park | Industrial | R-1 (7 du/ac) | R-3 (11 du/ac) | R-3 (24 du/ac) |
| Comm / Office | Mobile Home Park | R-3 | R-3 (14) | |

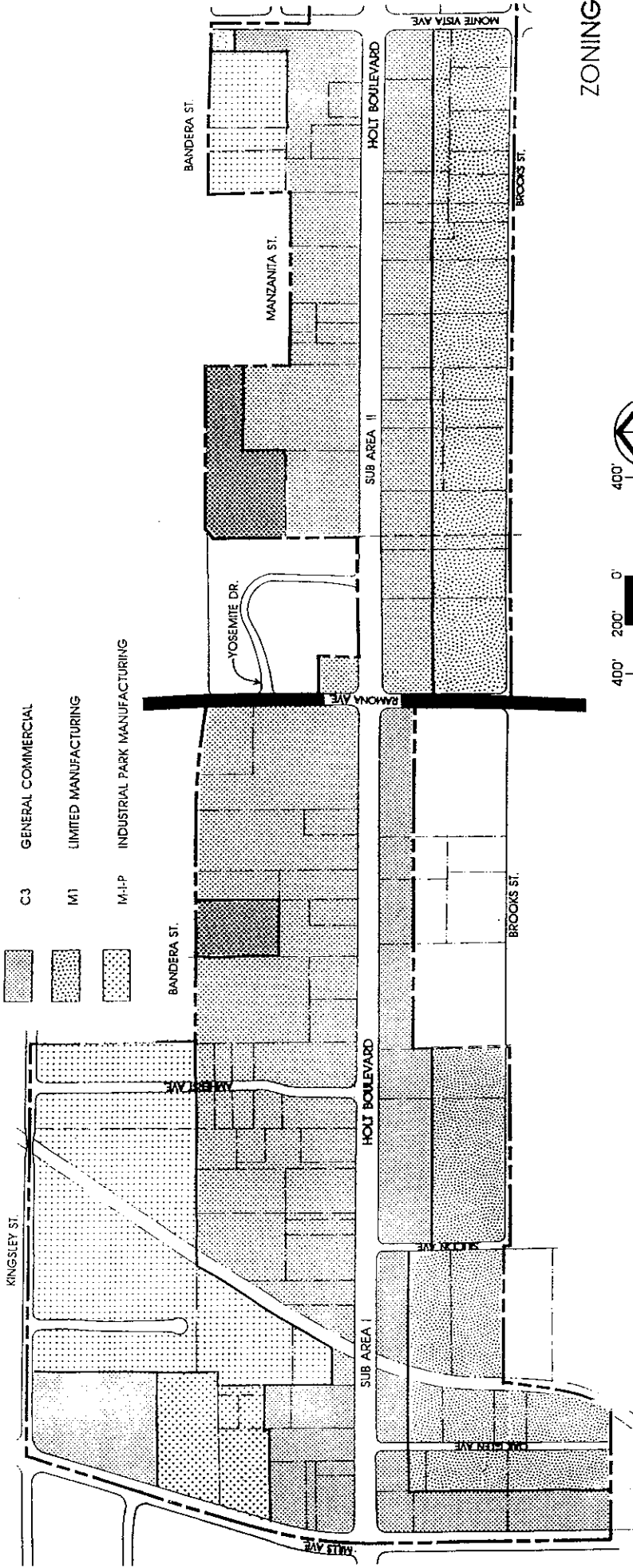
Updated
11/26/18



FIGURE II-5

LEGEND (EXISTING ZONING)

- R-1 SINGLE FAMILY RESIDENTIAL
- R-1 (SL) SINGLE FAMILY RESIDENTIAL SMALL LOTS
- R-2 TWO FAMILY RESIDENTIAL
- R-3 MEDIUM HIGH DENSITY RESIDENTIAL
- C3 GENERAL COMMERCIAL
- M1 LIMITED MANUFACTURING
- M1-P INDUSTRIAL PARK MANUFACTURING



ZONING

HOLT BOULEVARD SPECIFIC PLAN

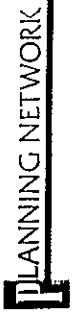
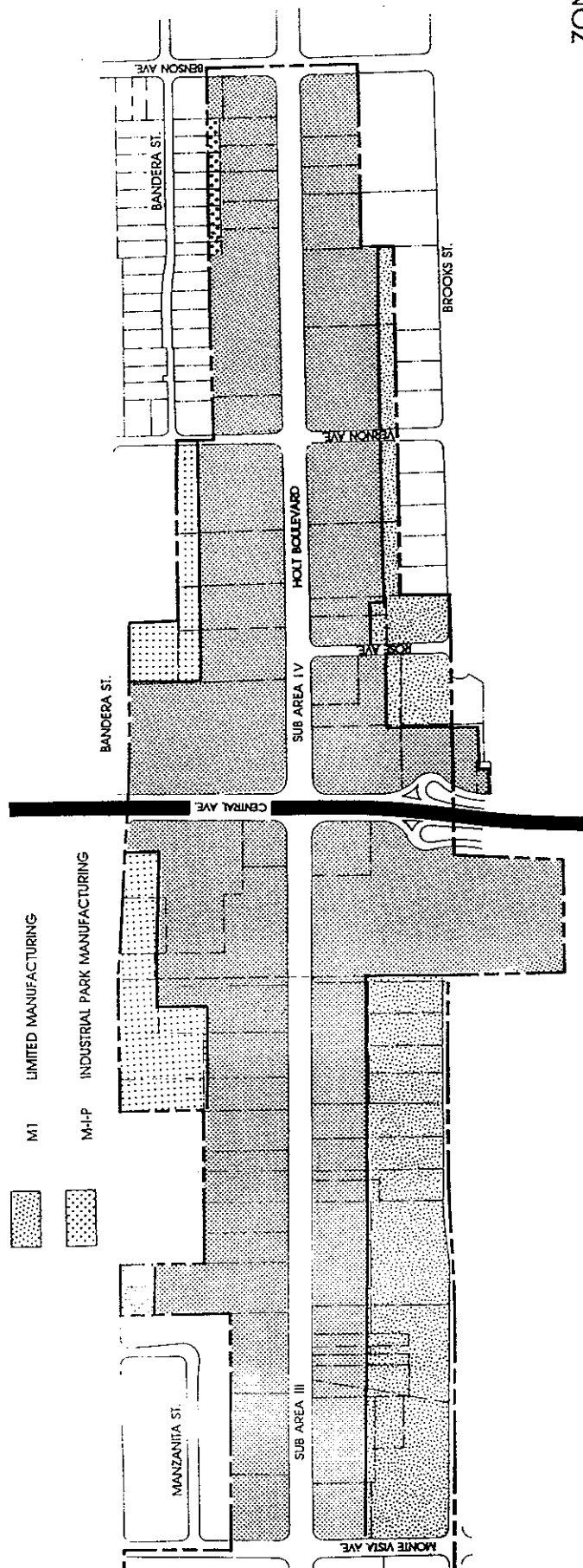


FIGURE 11-6

LEGEND (EXISTING ZONING)

- R-1 SINGLE FAMILY RESIDENTIAL
- R-1 (S1) SINGLE FAMILY RESIDENTIAL SMALL LOTS
- R-2 TWO FAMILY RESIDENTIAL
- R-3 MEDIUM HIGH DENSITY RESIDENTIAL
- C3 GENERAL COMMERCIAL
- M1 LIMITED MANUFACTURING
- M1-P INDUSTRIAL PARK MANUFACTURING



ZONING

**HOLT
BOULEVARD
SPECIFIC PLAN**



FIGURE II-7

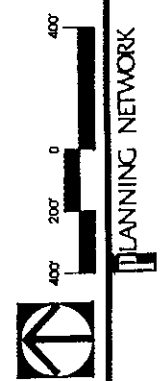
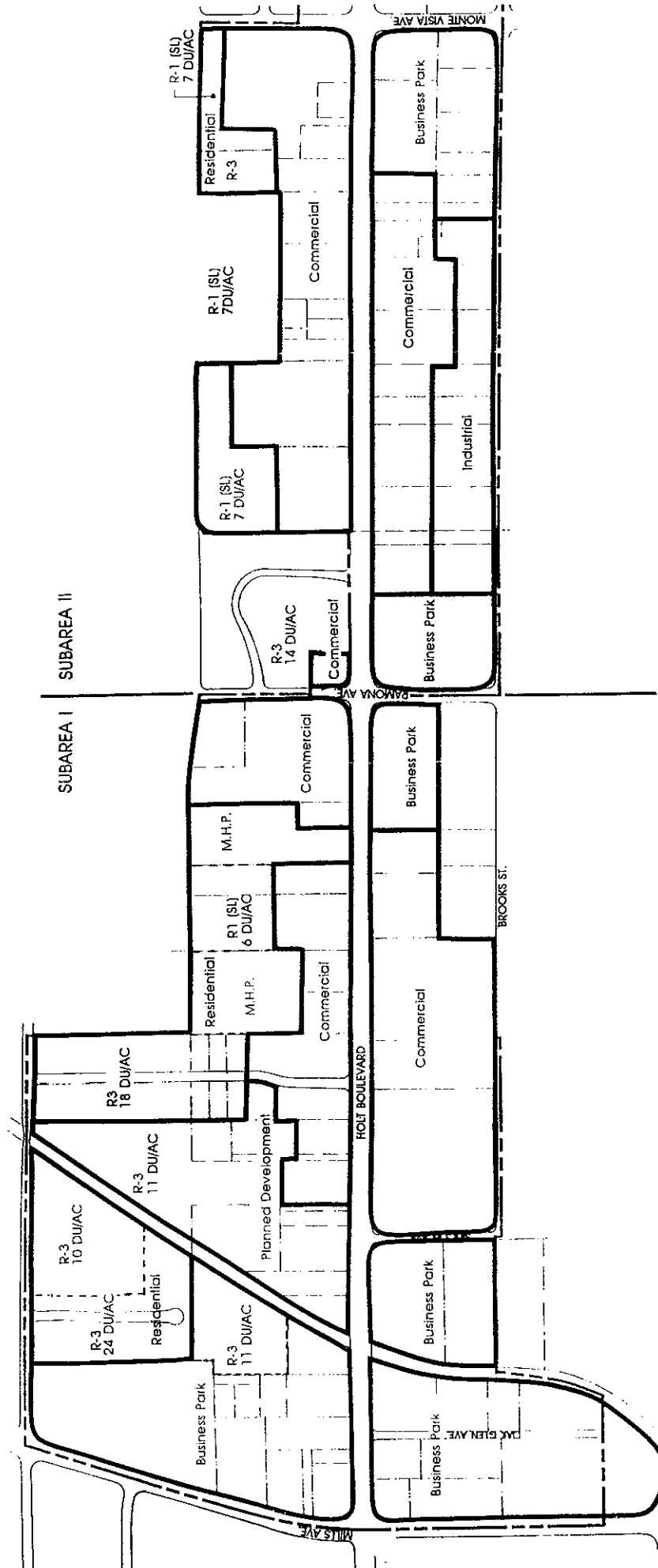
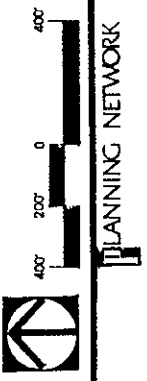
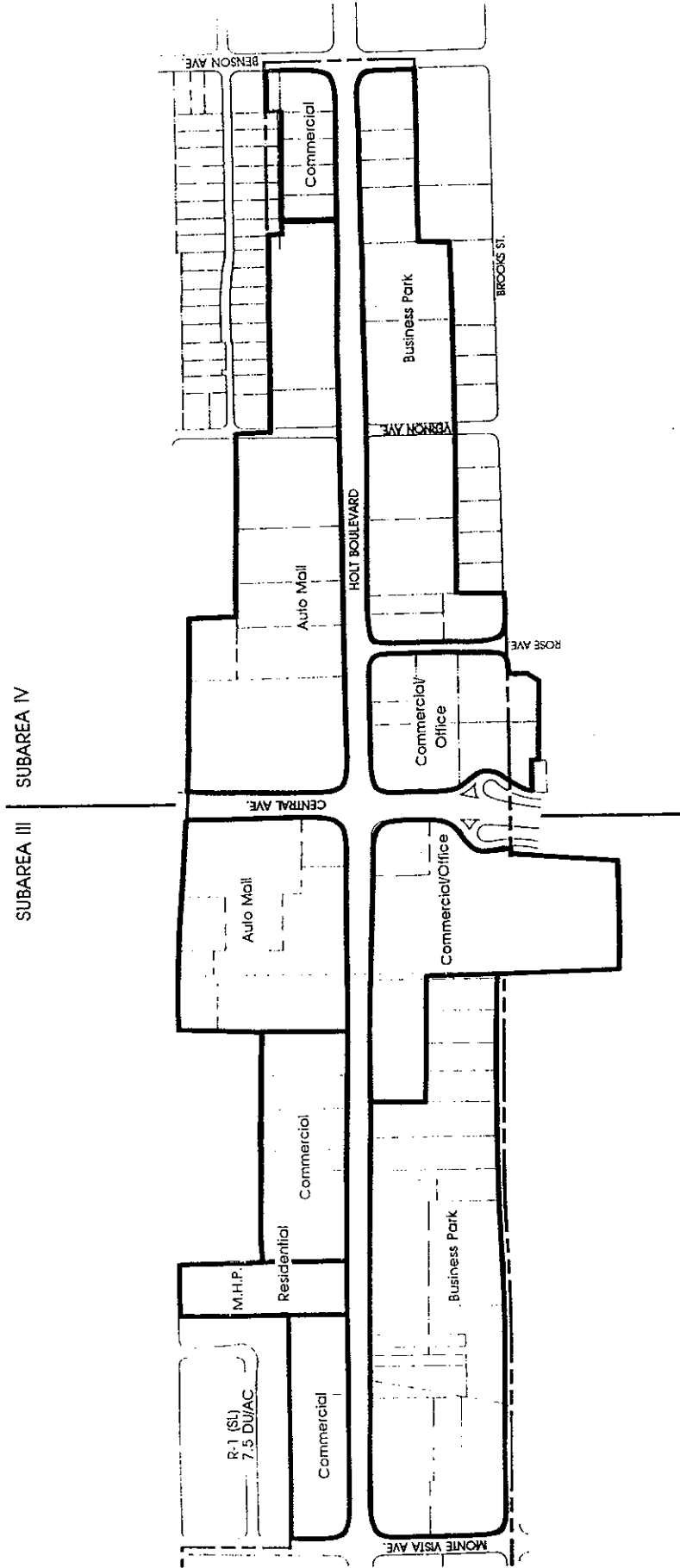


FIGURE II-8





III. DESIGN GUIDELINES

III. DESIGN GUIDELINES

A. INTENT AND PURPOSE OF THE GUIDELINES

This section of the Holt Boulevard Specific Plan sets forth the necessary design guidelines for the consistent promotion of high quality, well-designed developments throughout the Specific Plan area. The design guidelines are composed of written statements and graphic illustrations which articulate the City's goals and basic design philosophy for the Holt Boulevard corridor. The guidelines establish the policies and concepts for good design and quality development.

"Quality development" as used within the context of this document shall mean development which is in accordance with the development standards and design guidelines set forth in this Specific Plan.

The design guidelines contained in this section have been formulated to address the following specific objectives:

- To promote good, innovative design for commercial, office, and business park/industrial developments within the Specific Plan area.
- To increase awareness of design considerations among City officials, developers, architects, and City staff.
- To encourage designers and developers to consider security planning and crime prevention techniques in the physical design of projects.
- To provide a clear understanding of the City's expectations for those embarking on the planning and design of development projects in the Specific Plan.

1. APPLICABILITY AND IMPLEMENTATION OF THE DESIGN GUIDELINES

The design guidelines contained herein are applicable whenever any of the following occur:

- Any new development for which a building permit is required.
- Any addition to or enlargement of an existing structure or use except additions to existing Residential uses.

- Any change of use or reuse where City approval is required and established after a vacancy of 6 months or more.

An applicant who is changing only a portion of an existing building need only comply with the guidelines and standards related to the portion changed and to directly related portions. In the event that proposed modifications affect more than 60 percent of any facade visible to public parking area or public right-of-way, or the Community Development Director determines that the proposed changes are significant, the applicant shall be required to comply with all portions of the design guidelines/standards for the entire facade and all landscaping, parking and signs on the site. In addition, activities not required to apply for a building permit, such as, repairs, are encouraged to follow the guidelines to the greatest degree possible.

Implementation of the design guidelines will be through the City's "Plan Review Process" as established under Chapter 5 of the Municipal Code. In making its design findings on applicable projects, the Planning Commission shall utilize, and make reference to, the guidelines contained herein.

2. **PRECISE PLAN OF DESIGN REQUIRED**

For each applicable project, the applicant shall submit a Precise Plan of Design in accordance with Article 1, Chapter 5 of the Municipal Code.

The Precise Plan of Design shall include:

- The location, size, height, and type of all structures, including signs, walls, fences, trash enclosures, utility cabinets, and transformers.
- The location, size, and dimensions of all yards and setbacks and all spaces between structures.
- The location, dimensions, and method of improvement of all driveways, parking areas, walkways, and means of access ingress, and egress.
- The location, dimension, and method of improvement of all property to be dedicated to the public or to public utilities.
- The general nature of the proposed use.
- A detailed plan of landscaping showing the location, dimension and method of irrigation for all areas to be landscaped.
- The location, dimensions, and layout of all areas to be used for parking, loading, and maneuvering.
- The four elevations of all buildings showing the general design architectural features, building materials, colors, and signs.

**3. ORGANIZATION OF
THE DESIGN
GUIDELINES**

- The location and method of screening of all outdoor equipment (especially roof mounted) and utility service meters and cabinets.

The design guidelines contained in this plan are organized as follows:

a. Design Guidelines for Commercial Development

- General Design Principles
- Architectural Design Guidelines
- Roofs
- Awnings and Canopies
- Lighting
- Crime Prevention Techniques
- Site Planning
- Parking and Circulation
- Landscaping
- Walls and Fences
- Screening
- Automobile Dealership Renovation
- Automobile Dealership Uses and Structures
- Mixed Use

b. Design Guidelines for Business Park/Industrial Developments

- Architectural Design Guidelines
- Roofs
- Lighting
- Site Planning
- Parking and Circulation
- Loading Facilities
- Landscaping
- Walls and Fences
- Screening

c. Design Guidelines for Signs

- General Design Guidelines
- Wall and Fascia Signs
- Freestanding Signs

d. Landscape Guidelines

- Landscape Concept
- Aesthetic Considerations
- General Landscaping Guidelines
- Plant Palette
- Hardscape Guidelines

**B. DESIGN
GUIDELINES FOR
COMMERCIAL
DEVELOPMENT**

**1. GENERAL DESIGN
PRINCIPALS**

- Irrigation Guidelines
- Maintenance

a. *Desirable Elements of Project Design*

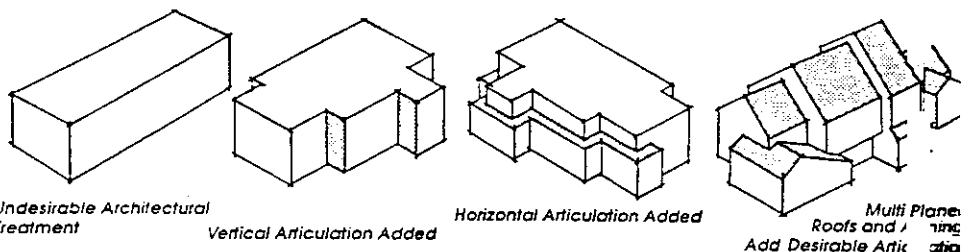
The qualities and design elements for commercial buildings that are most desirable include:

- Richness of surface and texture
- Significant wall articulation (insets, canopies, wing walls, trellises, porches, etc.)
- Multi-planed, pitched roofs
- Roof overhangs, arcades
- Regular or traditional window rhythm
- Articulated mass and bulk
- Significant landscape and hardscape elements
- Prominent access driveways
- Landscaped and screened parking
- Comprehensive signage program

b. *Undesirable Elements*

The elements to avoid or minimize include:

- Large blank, unarticulated stucco wall surfaces
- Unpainted concrete precision block walls
- Highly reflective surfaces
- Metal siding on the main facade
- Plastic siding
- Square "boxlike" buildings
- Mix of unrelated styles (i.e. rustic wood shingles and polished chrome)
- Large, out of scale signs with flashy colors
- Visible outdoor storage, loading, and equipment areas
- Disjointed parking areas and confusing circulation patterns
- Concealed publicly accessible areas (i.e. parking lot)



**2. ARCHITECTURAL
DESIGN
GUIDELINES**

Building heights should relate to open spaces to allow maximum sun and ventilation, protection from prevailing winds, enhance public views of surrounding mountains, and minimize obstruction of view from adjoining structures.

Height and scale of new development should be compatible with that of surrounding development. New development height should "transition" from the height of adjacent development to the maximum height of the proposed building.

Large buildings which give the appearance of "box-like" structures are generally unattractive and detract from the overall scale of most buildings. There are several ways to reduce the appearance of excessive bulk in large buildings.

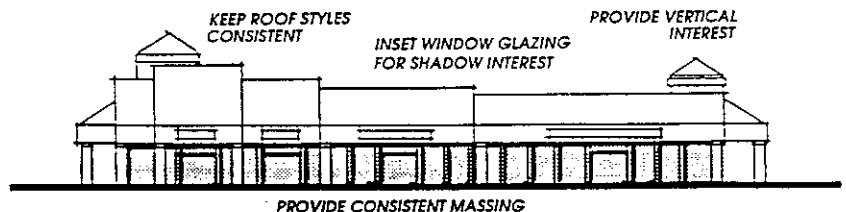
Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for more than fifty (50) feet without an offset.

Vary the height of the buildings so that it appears to be divided into distinct massing elements.

Articulate the different parts of a building's facade by use of color, arrangement of facade elements, or a change in materials.

Use landscaping and architectural detailing at the ground level to lessen the impact of an otherwise bulky building.

Avoid blank walls at the ground floor levels. Utilize windows, trellises, wall articulation, arcades, change in materials, or other features.

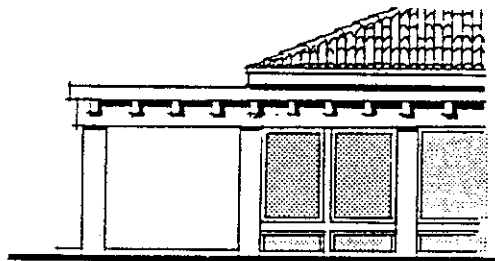


Scale, for purposes here, is the relationship between building size and the size of adjoining permanent structures. It is also how the proposed building's size relates to the size of a human being. Large scale building elements will appear imposing if they are situated in a visual environment of a smaller scale.

Building scale can be reduced through window patterns, structural bays, roof overhangs, siding, awnings, moldings, fixtures, and other details.

The scale of buildings should be carefully related to adjacent pedestrian areas (i.e. plazas, courtyards) and buildings.

Large dominating buildings should be broken up by: 1) creating horizontal emphasis through the use of trim; 2) adding awnings, eaves, windows, or other architectural ornamentation; 3) use of combinations of complementary colors; and 4) landscape materials.



Storefront elements and pedestrian level details provide an intimate scale

Much of the existing color in Montclair is derived from the primary building's finish materials such as brick, wood, stucco, and terra cotta tile. Also dominant are earth tones that match these natural materials.

Large areas of intense white color should be avoided. While subdued colors usually work best as a dominant overall color, a bright trim color can be appropriate.

The color palette chosen for a building should be compatible with the colors of adjacent buildings. An exception is where the colors of adjacent buildings strongly diverge from these design guidelines.

Wherever possible, minimize the number of colors appearing on the building exterior. Small commercial buildings should use no more than three colors.

Primary colors (red, blue, orange) should only be used to accent building elements, such as door and window frames and architectural details.

Architectural detailing should be painted to complement the facade and tie in with adjacent buildings.

3. ROOFS

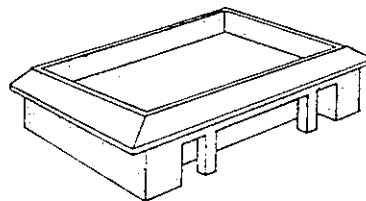
The roofline at the top of the structure should not run in a continuous plane for more than fifty (50) feet without offsetting or joggging the roof plane.

Nearly vertical roofs (A-frames) and piecemeal mansard roofs (used on a portion of the building perimeter only) are prohibited. Mansard roofs, if utilized on commercial structures, shall wrap around the entire building perimeter.

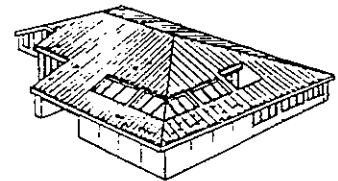
All rooftop equipment shall be screened from public view by screening materials of the same nature as the building's basic materials. Mechanical equipment should be located below the highest vertical element of the building.

The following roof materials are not acceptable:

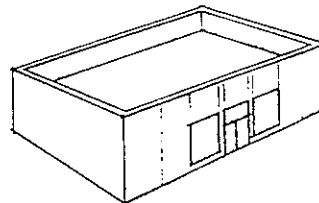
- Corrugated metal
- High contrast or brightly colored glazed tile
- Highly reflective surfaces
- Illuminated roofing



ACCEPTABLE



PREFERRED



NOT ACCEPTABLE

**4. AWNINGS AND
CANOPIES**

General use of awnings along a row of contiguous buildings should be restricted to awnings of the same form and location. Color of the awnings shall be consistent, and a minimum eight foot vertical clearance is required.

Signage on awnings shall be painted on the awnings themselves and be restricted to the awning's flap (valance) or to the end panels of angled, curved, or box awnings. Awnings signs are allowed only as a coordinated program.

Plexiglas, metal, and glossy vinyl illuminated awnings are prohibited. Canvas, treated canvas, matte finish vinyl, and fabric awnings are encouraged.

Internally illuminated awnings are not encouraged, however a consistently designed awning program for a center will be reviewed on case by case basis. No signage will be allowed on the awning in this case.



AWNINGS OF THE SAME FORM AND LOCATION ARE REPEATED, WITH THE SIGNAGE ON THE AWNING'S VALANCE.

VARYING ROOF PLANES, SETBACKS AND ARTICULATED FRONT FACADES ADD A PEDESTRIAN SCALE.

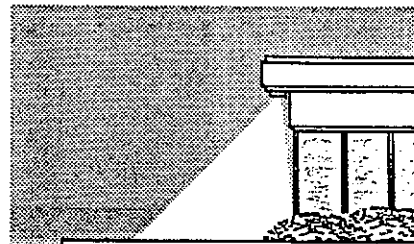
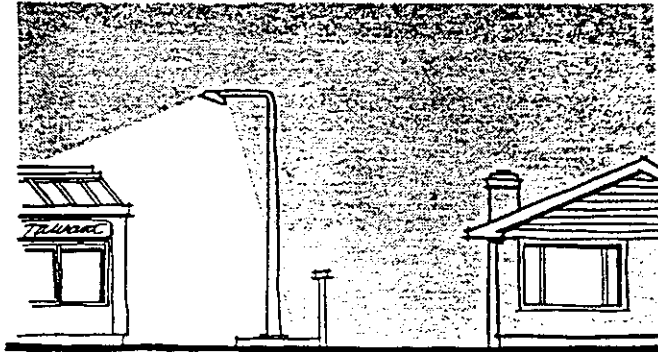
5. LIGHTING

Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and working areas. Average maintained footcandles should be a minimum of 6 footcandles. Higher levels are expected in heavily used pedestrian areas.

The design of light fixtures and their structural support should be architecturally compatible with main buildings on-site. Illuminators should be integrated within the architectural design for the buildings.

As a security device, lighting should be adequate but not overly bright. Tall lights should be avoided. All building entrances should be well lighted.

All lighting must be shielded to confine light spread within the site boundaries.



Confine light spread
to within site boundaries.

6. CRIME PREVENTION TECHNIQUES

The concept of "Crime Prevention Through Environmental Design" was developed in the early 1970's through government funded research. These concepts have been modified to Holt Boulevard's unique crime problems. These design guidelines are statements about the organization and positioning of activity areas and their linkage with one another. Physical design features of buildings and the public areas throughout Holt Boulevard affect crime rates and the perception of crime. While many types of interior design solutions can help alleviate potential problems, these guidelines focus on one major topic, the ability to provide as much "visual penetration" into the building sites as possible.

Use plants and low fencing to direct movement and restrict, symbolically, areas where people should not enter.

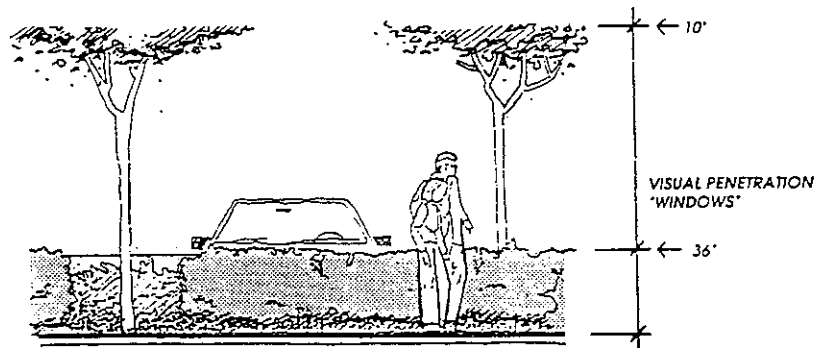
Minimize to the extent possible, the height of parking lot screening (shrubs or low walls) to allow a visible "window" above 36 inches and below ten feet.

Restrict the number of entry points into a development.

Secure areas behind buildings through use of:

- adequate security lighting
- limited access (walls, fences, gates, shrubs)
- surveillance through windows or with cameras

Limit to the extent possible, the depth of the building setback so that visual access into storefronts is possible from Holt Boulevard (i.e., convenience commercial uses can be closer to the right-of-way while furniture sales should be set back further).



Limit to the extent possible, visible security devices such as barbed wire, razor tape, and security bars on windows. These devices give the perception of a high crime area. Utilize more interior security devices, whenever possible.

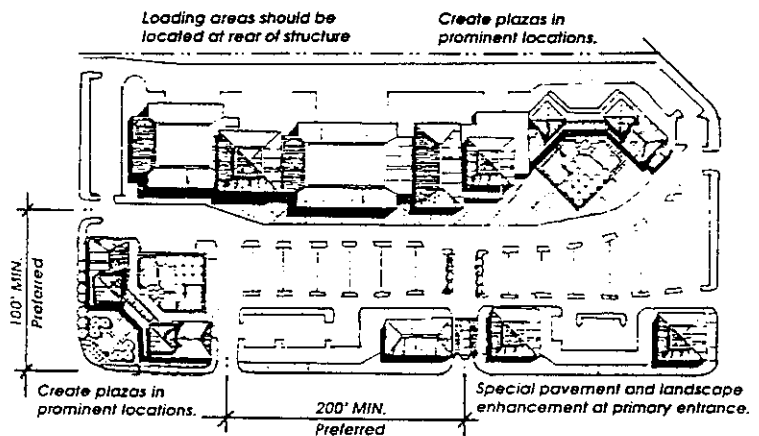
7. SITE PLANNING

Placement of buildings should consider the existing built context of the Holt Boulevard area, the location of any incompatible land uses, the location of major traffic generators, as well as an analysis of a site's characteristics and particular influences.

Buildings should be sited in a manner that will complement adjacent buildings. Building sites should be developed in a coordinated manner to provide order and diversity, and avoid a jumbled, confused development.

Whenever possible, new buildings should be clustered. This provides opportunities to create plazas or pedestrian malls and prevents long "barracks-like" rows of buildings. When clustering is impractical, a visual link between separate buildings should be established. This link can be accomplished through the use of an arcade system, trellis, or other open structure and textured walkways.

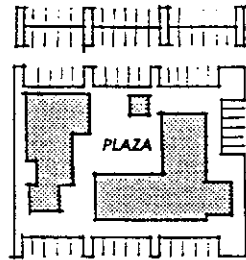
Locate buildings and on-site circulation systems to minimize pedestrian/vehicle conflicts where possible. Link buildings to the public sidewalk where possible with textured paving, landscaping, and trellises.



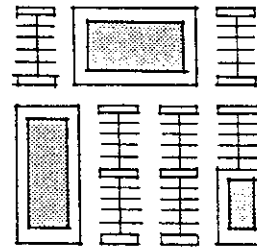
Recognize the importance of spaces between buildings as "outdoor rooms" on the site. Outdoor spaces should have clear, recognizable shapes that reflect careful planning and are not simply "left over" areas between buildings. Such spaces should provide pedestrian amenities such as shade, benches, fountains, etc.

Loading facilities should not be located at the front of commercial buildings where it is impossible to screen them from Holt Boulevard view. Such facilities are more appropriate at the rear of the site where special screening may not be required.

Open space areas should be clustered into larger, predominant, landscape areas rather than equally distributed into areas of low impact such as at building peripheries, behind a structure, or areas of little impact.



CLUSTER BUILDINGS ON THE SITE
TO CREATE PEDESTRIAN AREAS



DO NOT SEPARATE BUILDINGS
WITH PARKING LOTS

8. PARKING AND CIRCULATION

Parking lot design can be a critical factor in the success or failure of a commercial use along Holt Boulevard. In considering the possibilities for developing a new parking area, a developer should analyze the following factors: ingress and egress with consideration to possible conflicts with street traffic; pedestrian and vehicular conflicts; on-site circulation and service vehicle zones; and the overall configuration and appearance of the parking area.

Separate vehicular and pedestrian circulation systems should be provided. Pedestrian linkages between uses in commercial developments should be emphasized, including distinct pedestrian access from parking areas in large commercial developments, such as shopping centers.

Parking aisles should be separated from vehicle circulation routes whenever possible.

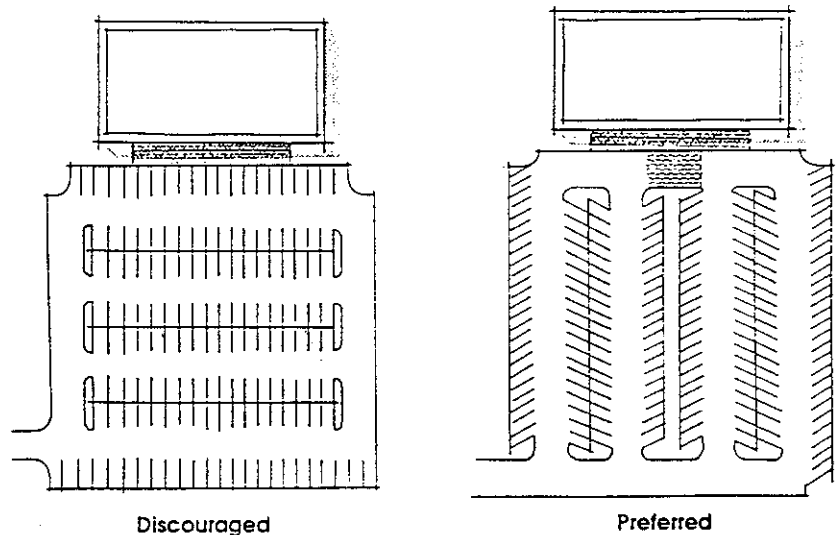
To encourage the use of long term and compact spaces by employees, employee parking will be located adjacent to exclusive employee entrances.

Common driveways which provide vehicular access to more than one site are encouraged.

Angled parking is preferred over 90 degree parking, specifically for uses with rapid parking space turnover (3/hour).

Parking areas shall be landscaped, receiving interior as well as perimeter treatment in accordance with the requirements of this Specific Plan and the Zoning Code.

PARKING AISLE ARRANGEMENT



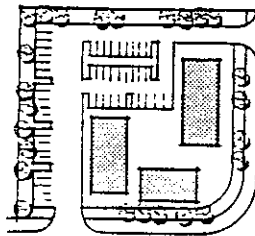
Parking areas should be separated from buildings by either a raised concrete walkway or landscaped strip, preferably both. Situations where parking spaces directly abut the buildings shall be avoided.

Shared parking between adjacent businesses and/or developments is highly encouraged whenever practical.

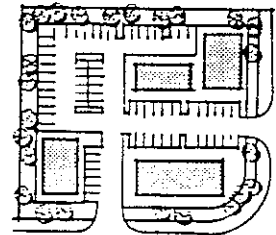
Where parking areas are connected, interior circulation should allow for a similar direction of travel and parking spaces in all areas to reduce conflict at points of connection.

Whenever possible, locate site entries on side streets in order to minimize vehicular conflicts. When this is not possible, design the front site entry with appropriately patterned concrete or pavers to differentiate from the sidewalks.

Parking access points, whether located on front or side streets should be located as far as possible from street intersections so that adequate stacking room is provided. The number of access points should be strictly limited to the minimum amount necessary to provide adequate circulation. Excessive driveways are a conducive element to certain criminal activities.



DO THIS



DON'T DO THIS

For security purposes, parking areas and pedestrian walkways should be visible from buildings to the greatest degree possible.

Design parking areas so that pedestrians walk parallel to moving cars. Minimize the need for the pedestrian to walk between parked cars, or to cross parking aisles and landscape areas.

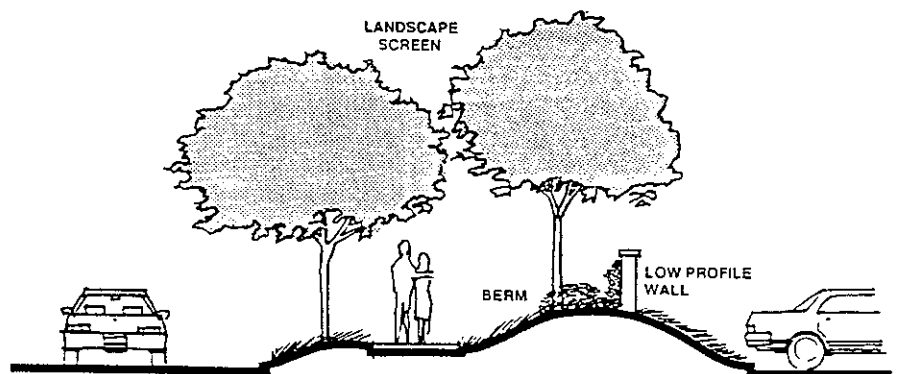
The parking area should be designed in a manner which links the building to street sidewalk system as an extension of the pedestrian environment. This can be accomplished by using design features such as walkways with enhanced paving, trellis structures, and/or landscaping treatment.

Parking areas which accommodate a significant number of vehicles should be divided into a series of connected smaller lots. Landscaping and offsetting portions of the lot are effective in reducing the visual impact of large parking areas.

The first parking stall which is perpendicular to a driveway or first aisle juncture, should be at least 40 feet back from the curb. With larger commercial centers, significantly more setback area may be required for vehicle stacking.

Utilize a 36" high (maximum from adjacent sidewalk elevation) opaque wall or landscaping to screen any parking at the street periphery. A combination of walls, berms, and landscape material is highly recommended. Where practical, lowering the grade of the parking lot from existing elevations may aid in obscuring views of automobiles while promoting views of architectural elements and interior activities (security).

Vehicles should not be required to enter the street in order to move from one parking area to another parking area on the same site. Reciprocal access agreements will be required of most development proposals.

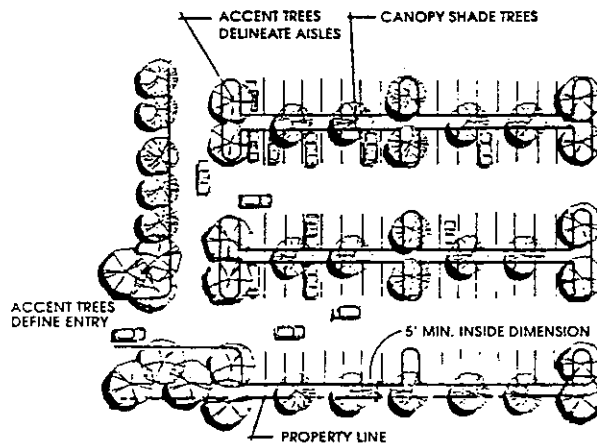


9. LANDSCAPING

Landscaping for commercial uses should be used to define specific areas by helping to focus on entrances to buildings, parking lots, defining the edges of various land uses, providing transition between neighboring properties (buffering), and providing screening for loading and equipment areas.

Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.

Landscaping around the entire base of buildings is recommended to soften the edge between the parking lot and the structure. This should be accented at entrances to provide focus.



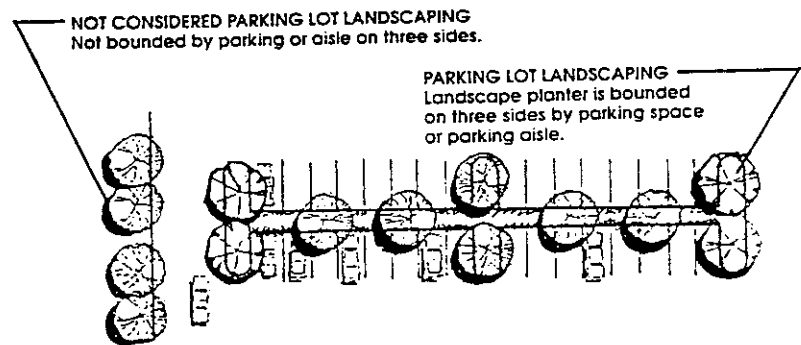
Trees should be located throughout the parking lot and not simply at the ends of parking aisles or in setback areas. In order to be considered within the parking lots, trees must be located in planters that are bounded on at least 3 sides by parking area paving. Only trees located in landscaped "fingers" or "islands" can count towards parking lot landscaping.

Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs. Concrete mow-strips separating turf and shrub areas are required per development regulations.

Vines and climbing plants integrated upon buildings, trellises, and perimeter garden walls are strongly encouraged. A few plants to consider for this purpose are: bougainvillea, grape ivy, and wisteria vines.

Use boxed and tubbed plants in clay, glasscrete, masonry, or wood containers, especially for enhancement of sidewalk shops.

At maturity, trees should be able to be trimmed ten (10) feet above ground and shrubs should be maintained at a maximum height of three (3) feet to provide visual penetration by law enforcement entities.



10. WALLS AND FENCES

Where walls are used at property frontages, or screen walls are used to conceal storage and equipment areas, they must be designed to blend with the site's architecture. Landscaping should be used in combination with such walls whenever possible.

Chain link fence is not allowed where visible from public streets. Where security fencing is required, it should be a combination of solid walls with pillars and decorative view ports, or short solid wall segments and wrought iron grill work.

Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided.

Where walls are provided they will be of a consistent decorative masonry or match building architecture.

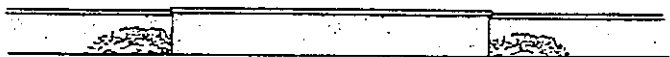
Security type fences such as barbed wire or razor tape are permitted only when concealed from public view. Overly high opaque walls are preferred.



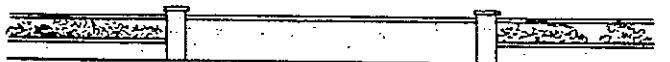
Elevation of Wall/Wrought Iron Combination



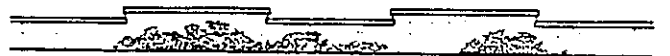
Elevation of Wrought Iron with Pilasters



Elevation of Staggered Wall



Elevation of Planters/Wall



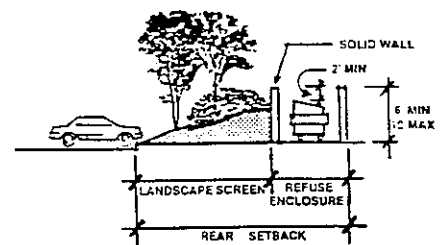
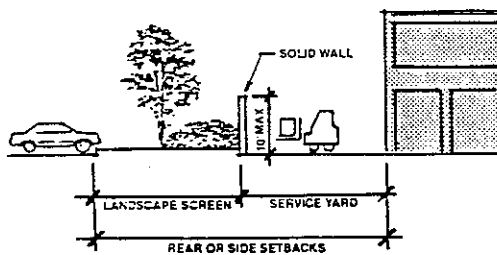
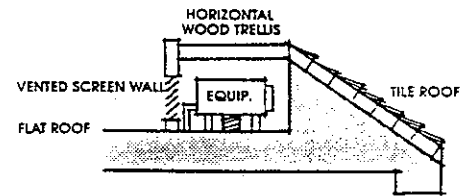
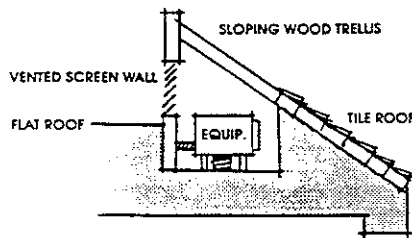
Elevation of Wall with Breaks

11. SCREENING

Screening for outdoor storage at the rear of commercial sites should be a minimum of 6' and a maximum of 10' high. The height should be determined by the height of the material or equipment being screened. Exterior storage should be confined to portions of the site least visible to public view.

Where screening is required, a combination of elements should be used including solid masonry walls, berms, and landscaping.

Any mechanical or utility equipment, whether on the roof, side of building, or ground, shall be screened. The method of screening shall be architecturally integrated in terms of materials, color, shape, and size. The screening design shall blend with the building design. Where individual equipment is provided, a continuous screen is desirable. Wherever possible, building screening shall be accomplished by primary building elements (i.e. parapet wall or mansard roof) instead of after-the-fact add-on screening.



**12. AUTOMOTIVE
DEALERSHIP
RENOVATION**

All site structures shall be redeveloped in accordance with a singular and consistent design character; conveying the image of an integrated complex of structures and open spaces.

Structures should emphasize the prevailing architectural character of the region; contemporary, low-rise buildings with natural materials. High technology and unique designs should be considered when they are of special merit and upon review and recommendation of the City.

Remodeled structures should strive to utilize traditional building materials in a contemporary way. These traditional materials include wood, stucco, plaster, architectural concrete, and ribbed metal or tile roofs. Glass areas will be limited to appropriate show room applications.

All existing unsightly operations (e.g. automobile preparation and repair facilities, paint shops, carwashes, etc.) shall be screened from adjacent areas by enclosure walls, and/or heavy landscaping. Enclosure walls should be differentiated and attractively designed, using concrete block, limestone, wood, iron grilles, and other appropriate materials. All walls shall be a minimum 6' in height.

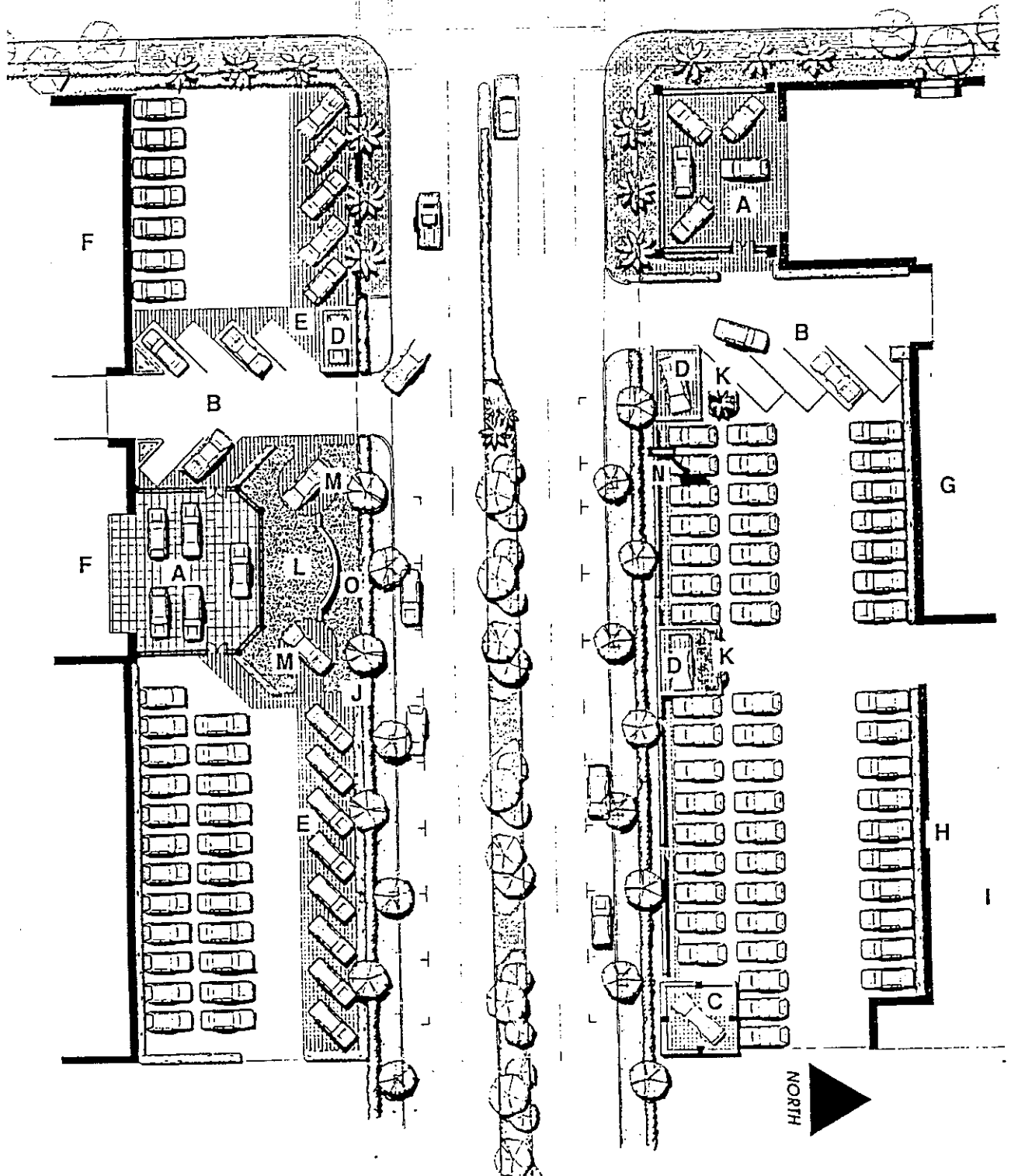
Signage requirements for renovated auto dealerships shall conform to all requirements of this Specific Plan.

**13. AUTOMOBILE
DEALERSHIP USES
AND STRUCTURES**

a. *Plan Features (Prototypes)*

The plan drawing on the next page illustrates many of the design features that are desirable for the automobile dealership sales area located along Holt Boulevard. The dealership prototype on the north side of Holt shows a showroom, located at the corner of the property and a covered outdoor display pavilion at the opposite corner of the site for balance. Between these features are the new and used car display areas which feature two podiums and a landscaped security barrier.

The south side dealership prototype shows an automobile use with the showroom located in the middle of the block or property. This showroom is set back slightly from the street and is highlighted by outdoor display pads located in a landscape "yard" with a built-in monument sign. Used and new car display areas are located to the left and right of the showroom and feature special paving along the front to highlight the cars and make a memorable impression.



HOLT BOULEVARD SPECIFIC PLAN

b. *Other Guidelines*

- (1) Showrooms: Both showrooms (A) are visibly "open" with large unobstructed windows on three sides. Their size and placement make them the prominent architectural feature of the dealership site. On-site customer parking areas (B) are clearly defined and adjacent to the showrooms.
- (2) Outdoor sales areas: Both prototypical sites feature an orderly arrangement of displayed automobiles with generous spacing between the cars. Special features in these areas include the use of a display pavilion (C), several display podiums (D), and special paving to highlight the automobiles (E).
- (3) Backdrop: Both sites feature solid backdrops (with landscaping along the edge) to enhance the appearance of the displayed automobiles. Service and storage areas are well out of site of the sales areas in both plans. The lower (southside) prototype utilizes the front wall of the service and parts department buildings (F) as a backdrop for the sales areas. The upper prototype uses the service department wall (G) for a portion of the backdrop but also uses a freestanding wall (H) to visually separate the outdoor sales area from the dealer's storage and on-site parking areas (I). Service entrance doors/gates should be limited to one opening per street frontage, all other individual service facility bays may not be visible from Holt Boulevard.
- (4) Landscaping: Both these site plans show how the generous use of landscaping can be used to effectively highlight the showroom, outdoor display areas, and the dealership as a whole. Both plans feature low landscaped security barriers (J) which effectively prevent autos from being driven out the front of the display areas while also enhancing the appearance of the sales area. The upper plan indicates how several carefully placed small trees (K) can help divide the large display area into smaller more comfortable areas. The lower plan features a spacious "lawn" area (L) with concrete pads (M) for the display of special featured cars.
- (5) Signs: The upper plan features a freestanding sign (N) located along the outdoor display area which can be seen from both directions. The lower plan shows a "built-in" monument sign (O) located in the center of the lawn with the dealer name along its curved front face.

**14. MIXED USE
PROJECTS**

a. *Design Issues*

For the purpose of these guidelines, mixed use projects are defined as developments which combine both commercial and residential uses or structures on a single lot, or as components of a single development. The uses may be combined either vertically on the site within the same structure, or spread horizontally on the site in different areas and structures.

The primary design issue related to mixed use projects is the need to successfully balance the requirements of residential uses, such as the need for privacy and security, with the needs of commercial uses for access, visibility, parking, loading, and possibly extended hours of operation.

The guidelines for mixed use projects also apply to commercial projects adjacent to residential uses and to residential projects in commercial zones.

b. *Architectural Treatment*

The architectural style and use of materials should be consistent throughout the entire mixed use project. Differences in materials and/or architectural details should only occur on a structure where the intent is to differentiate between the residential scale and character of the structure and the commercial scale and character.

The design of storefronts should be consistent with the design guidelines for commercial development. The residential portion of a mixed use structure should use design elements such as windows and balconies that reflect a residential character.

Projects three stories or less in height should incorporate full roofs on at least 50 percent of the roof area.

Structures with heights greater than three stories should set back upper portions of the structure a minimum of 10 feet for each additional two stories.

All roof mounted equipment shall be screened in accordance with the requirements of the Zoning Code and these guidelines. Special consideration should be given to the location and screening of noise generating equipment such as refrigeration units, air conditioning, and exhaust fans. Noise reducing screens and insulation may be required where such equipment has the potential to impact residential uses.

c. *Site Planning*

Mixed use projects that provide commercial space on the ground floor with residential units above are encouraged over projects that provide commercial structures on the front portion of the lot with residential uses placed at the rear of the lot. This latter configuration does not meet the intent of a true mixed use project which incorporates vertical integration of uses. However, a horizontal separation (commercial to the front, residential to the rear) of uses may be appropriate depending on the size of the site and available access.

Separate site access drives and parking facilities should be provided for residential uses and commercial uses.

If enclosed parking is provided for the entire complex, separate levels should be provided for residential and commercial uses with separate building entrances.

Site access drives should incorporate distinctive architectural elements and landscape features which help to differentiate access to commercial parking areas from residential areas. Security gates should be considered for access to residential uses and residential parking areas, as well as to securing commercial parking areas when businesses are closed.

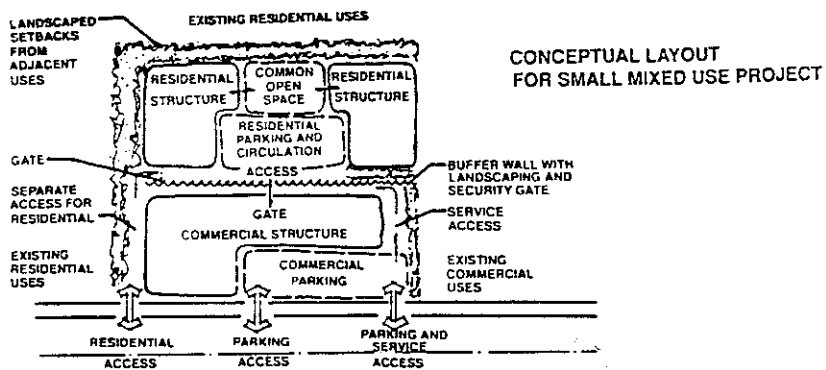
When a mixed use project is designed as separate structures on a lot with the commercial uses along the street and residential uses at the rear, a decorative masonry wall with security gates should separate the uses.

Loading areas and refuse storage facilities should be located as far as possible from residential units and should be completely screened from view from the residential portion of the project. The location and design of trash enclosures should account for potential nuisances from odors.

Parking lot lighting and security lighting for the commercial use should be appropriately shielded so as not to spill over into the residential area or impact the residential units in any way. Residential units should also be shielded from illuminated commercial signage whenever possible.

Open space intended for use by residents only, should not be accessible from commercial areas. Open space and courtyards in commercial areas should be accessible to residential occupants and visitors.

When residential and commercial uses are provided in the same structure, separate entrances should be provided for each use.



**C. DESIGN
GUIDELINES FOR
BUSINESS
PARKS/INDUSTRIAL
DEVELOPMENTS**

**1. ARCHITECTURAL
DESIGN
GUIDELINES**

As a category of building types, business park/industrial buildings often present unattractive and monotonous facades. There are, however, a variety of design techniques available to help overcome this situation and to direct development into a cohesive design statement.

Employ variety in building forms, to create visual character and interest.

Avoid long, "unarticulated" building facades. Building facades with varied front setbacks are strongly encouraged. Wall planes should not run in one continuous direction for more than 50 feet without a significant (5'-15') offset.

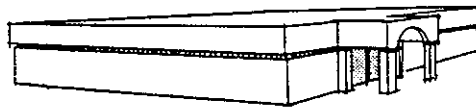
Avoid blank front and side wall elevations on street frontages.

Entries to business park buildings should portray a quality office appearance while being architecturally tied into the overall building mass and building composition.

Windows and doors are key elements of any building's form, and should relate to the scale of the elevation on which they appear. Windows and doors can establish character by their rhythm and variety. Recessed openings help to provide depth and shadow contrast on elevation planes.

Sensitive alteration of colors and materials can produce diversity and enhance architectural forms.

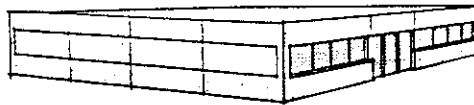
The staggering of planes along an exterior wall elevation creates pockets of light and shadow, providing relief from monotonous, uninterrupted expanses of wall.



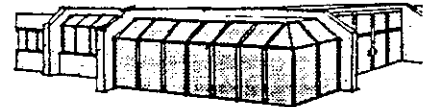
Entry projection or indentation color bands



Window glazing, color bands, textured walls and entry indentation



Vertical seams, window glazing bands and textured walls



Mix of complimentary materials, articulated surface increase window areas, and some textured walls

Design elements which are undesirable and should be avoided include:

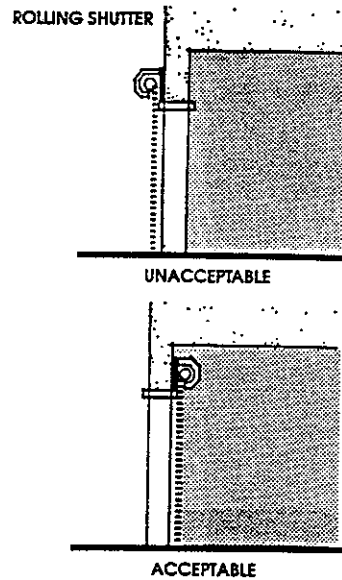
- Highly reflective surfaces at the ground story
- Large blank, unarticulated wall surfaces
- Exposed, untreated precision block walls
- Chain link fence, barbed wire
- False fronts
- "Stuck on" mansard roofs on small portion of the roofline
- Unarticulated building facades
- Materials with high maintenance such as stained wood.

Choose wall materials that will withstand abuse by vandals or accidental damage from delivery vehicles and machinery.

All metal buildings must be architecturally designed providing variety and visual interest to the streetscape. Front facades should not utilize 100 percent metal exterior. Use of stone, split face block, and other durable materials are required on the Holt Boulevard facade.

Berming, in conjunction with landscaping, can be used at the building edge to reduce building mass and height along facades.

Rolling shutter doors located on the inner side yards of the building : the preferred method for providing large loading doors while keeping a clean, uncluttered appearance from the exterior Holt Boulevard view.



2. ROOFS

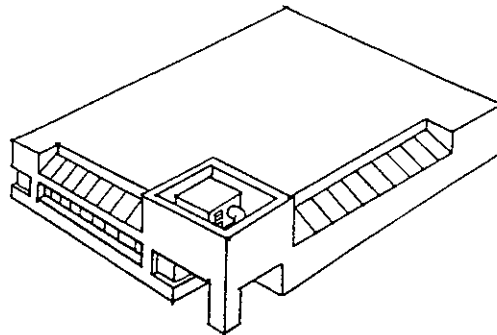
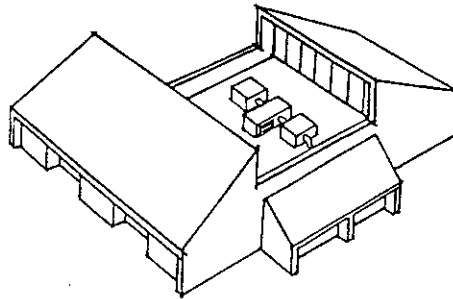
The roofline at the top of the structure should not run in a continuous plane for more than 50 feet without offsetting or jogging the roof plane.

Nearly vertical roofs (A-frames) and piecemeal mansard roofs (used on a portion of the building perimeter only) are prohibited. Mansard roofs, if utilized on structures, shall wrap around the entire building perimeter.

All rooftop equipment must be screened from public view by screening materials of the same nature as the building's basic materials. Mechanical equipment should be located below the highest vertical element of the building.

The following roof materials are not acceptable:

- Corrugated metal
- High contrast of brightly colored glazed tile
- Highly reflective surfaces
- Illuminated roofing



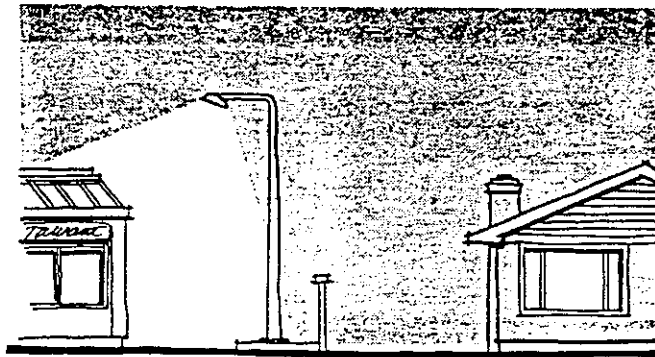
3. LIGHTING

Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways, and working areas.

The design of light fixtures and their structural support shall be architecturally compatible with main buildings on-site. Illuminators should be integrated within the architectural design for the buildings.

As a security device, lighting should be adequate but not overly bright. Tall lights should be avoided. All building entrances should be well lighted. "On structure" lighting is preferred to numerous pole lights around the building's perimeter except for areas which are for commercial type uses with active evening hours. A combination of low pole lighting and or structure lighting is a good combination.

All lighting must be shielded to confine light spread within the site boundaries.



4. SITE PLANNING

The main elements of sound business park/industrial site design include the following:

- Controlled site access
- Service areas located at the sides and rear of buildings
- Convenient access, visitor parking, and on-site circulation
- Screening of outdoor storage, work areas, and equipment
- Emphasis on the main building entry and landscaping
- Landscaped open space

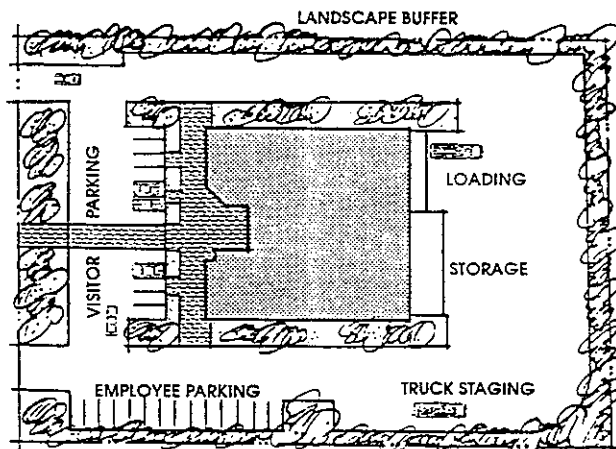
A variety of building and parking setbacks should be provided in order to avoid long monotonous building facades and to create diversity.

Buildings should be located on "turf islands", where the office portion of the building does not directly abut paved parking areas. A minimum five (5) to seven (7) foot landscape strip should be provided between parking areas and the office portion of a structure.

Building setbacks should be provided proportionate to the scale of the building and in consideration of existing development adjacent to it. Larger structures require more setback area for a balance of scale and so as not to impose on neighboring uses.

Building placement which creates opportunities for plazas, courts, or gardens is encouraged. Setback areas can often be used to provide space for patio areas.

Where heavier industrial uses are adjacent to business park uses, appropriate buffering techniques such as setbacks, screening, and landscaping need to be provided to mitigate any negative effects of the industrial operation.



5. PARKING AND CIRCULATION

The parking lot and cars should not be the dominant visual elements of the site. Large expansive paved areas located between Holt Boulevard and the building are to be avoided in favor of smaller multiple lot separated by landscaping and buildings.

Site access and internal circulation should be designed in a straight forward manner which emphasizes safety and efficiency. The circulation system should be designed to reduce conflicts between vehicular and pedestrian traffic, combine circulation and access areas where possible and provide adequate maneuvering and stacking areas and consideration for emergency vehicle access. Circulation routes and parking areas should be separated.

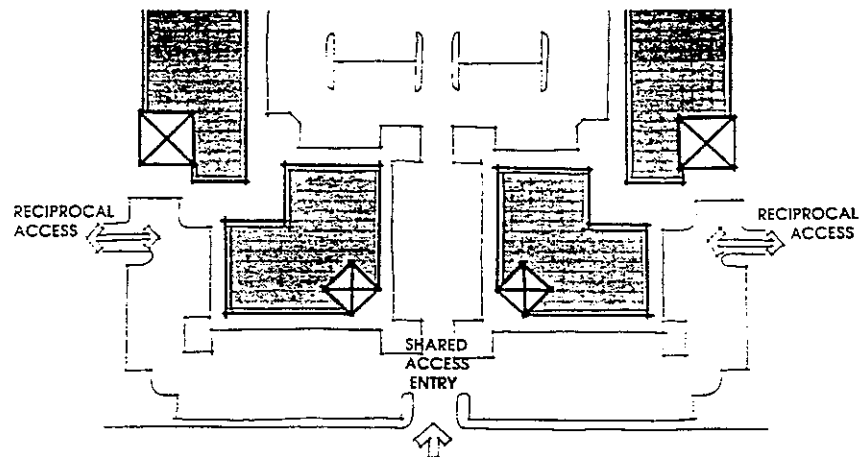
Entrances and exits to and from parking and loading facilities should be clearly marked with appropriate directional signage where multiple access points are provided.

Vehicles should not be required to enter the street in order to move from one parking area to another parking area on the same site. Reciprocal ingress/egress agreements shall be required in order to ensure access between separate properties.

Parking lots adjacent to and visible from public streets must be adequately screened from view through the use of rolling earth berms, low screen walls, changes in elevation, landscaping, or combinations thereof whenever possible.

The business park/industrial site should be a self-contained development capable of accommodating its own parking needs. The use of the public street for parking and staging of delivery trucks is not allowed.

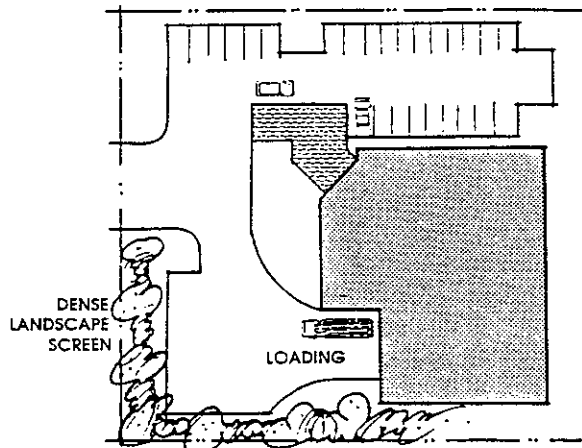
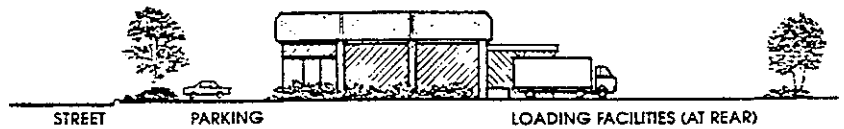
Parking lots should be visible from structures, especially entrances.



6. LOADING FACILITIES

To alleviate the unsightly appearance of loading facilities for industrial uses, these areas should not be located at the front of business park sites or at the front of buildings where it is difficult to adequately screen them from Holt Boulevard. Such facilities are more appropriate at the rear of the site where special screening may not be required.

Backing from a public street onto the site for loading into loading docks causes unsafe truck maneuvering and is not allowed except at the ends of industrial cul-de-sacs where each circumstance will be studied individually at the time of design review.



Offset the loading areas from the driveway openings.

7. LANDSCAPING

For business park uses, landscaping should be used to define areas by helping to focus on entrances to buildings, parking lots, loading areas, providing transition between neighboring properties (buffering), and providing screening for outdoor storage, loading, and equipment areas.

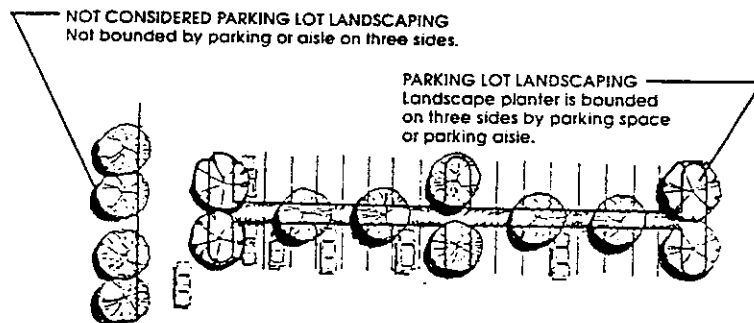
Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.

Use of vines on walls is appropriate in business park areas because such building walls often tend to be large and blank.

Landscaping around the entire base of buildings is recommended to soften the edge between the parking lot and the structure. This should be accented at entrances to provide focus.

Trees must be located throughout the parking lot and not simply at the ends of parking aisles or in setback areas. In order to be considered within the parking lots, trees must be located in planters that are bounded on at least 3 sides by parking area paving. Only trees located in landscaped "fingers" or "islands" may count towards parking lot landscaping.

Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks, or the use of curbs. Concrete mow-strips are required per development regulations between turf and shrub areas.



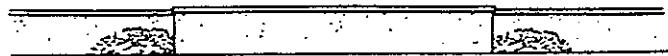
8. WALLS AND FENCES

Walls will serve a major function in the business park/industrial landscape and will be used to screen automobiles, loading and storage areas, and utility structures. However, if not required for a specific screening or security purpose they should not be utilized. The intent is to keep the walls as low as possible while performing their screening and security functions.

Where walls are used at property frontages, or screenwalls are used to conceal storage and equipment areas, they must be designed to blend with the site's architecture. Landscaping should be used in combination with such walls whenever possible.

Chain link fence is not allowed in setback areas adjacent to public streets. When security fencing is required, it should be a combination of solid pillars or short solid wall segments and wrought iron grill work. Security fencing such as barbed wire or razor tape is prohibited unless concealed from public view. Overly high opaque walls are preferred alternatives.

Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided.



Elevation of Staggered Wall



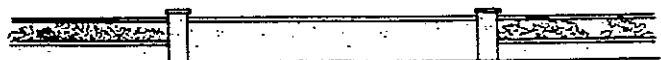
Elevation of Wall /Wrought Iron Combination



Elevation of Wrought Iron with Pilasters



Elevation of Staggered Wall



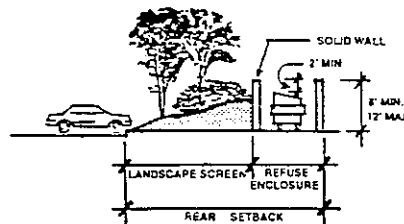
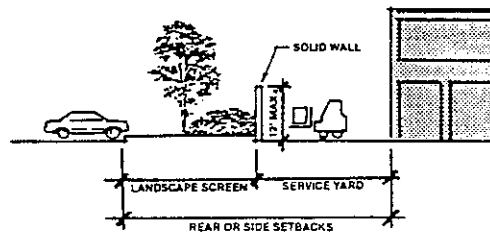
Elevation of Planters/Wall

9. SCREENING

Screening for outdoor storage should be a minimum of 8' and a maximum of 12' high. The height should be determined by the height of the material being screened. Exterior storage should be confined to portions of the site least visible to public view.

Where screening is required, a combination of elements should be used including solid masonry walls, berms, and landscaping.

Any equipment, whether on the roof, side of building, or ground, shall be screened. The method of screening shall be architecturally integrated in terms of materials, color, shape, and size. The screening design shall blend with the building design. Where individual equipment is provided, a continuous screen is desirable.



**D. DESIGN
GUIDELINES FOR
SIGNS**

**1. GENERAL DESIGN
GUIDELINES**

The following design guidelines should be consulted prior to developing signs for any project within the Specific Plan area.

Use a brief message - The fewer the words, the more effective the sign. A sign with a brief, succinct message is simpler and faster to read, looks cleaner, and is more attractive. Sign is generally to be limited to the name of the building.

Avoid hard-to-read, overly intricate typefaces - These typefaces are difficult to read and reduce the sign's ability to communicate.

Avoid faddish and bizarre typefaces - Such typefaces may look good today, but soon go out of style. The image conveyed may quickly become that of a dated and unfashionable business.

STOP

Calligraphia

Futura Black

Sign colors and materials - These should be selected to contribute to legibility and design integrity. Even the most carefully thought out sign may be unattractive and a poor communicator because of poor color selection. Day-glo colors must be avoided.

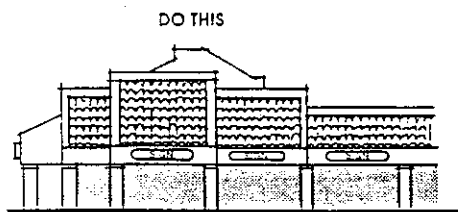
Use significant contrast between the background and letter or symbol colors - If there is little contrast between the brightness or hue of the message of a sign and its background, it will be difficult to read.

Avoid too many different colors on a sign - Too many colors overwhelm the basic function of communication. The colors compete with content for the viewer's attention. Limited use of the accent colors can increase legibility, while large areas of competing colors tend to confuse and disturb.

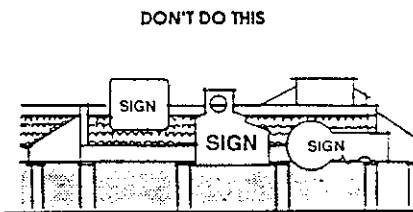
Place signs to indicate the location of access to a business - Signs should be placed at or near the entrance to a building or site to indicate the most direct access to the business.

Place signs consistent with the proportions of the scale of building elements within the facade - Within a building facade, the sign may be placed in different areas. A particular sign may fit well on a plain wall area, but would overpower the finer scale and proportion of the low storefront. A sign which is appropriate near the building entry may look tiny and out of place above the ground level.

Place wall signs to establish facade rhythm, scale, and proportion where such elements are weak- In many buildings that have a monolithic or plain facade, signs can establish or continue appropriate design rhythm, scale, and proportion.



Employ a consistent sign pattern



Inconsistent sign patterns create confusion. Signs within or above roof area are prohibited.

Avoid signs with strange shapes - Signs that are unnecessarily narrow or oddly shaped can restrict the legibility of the message. If an unusual shape is not symbolic, it is probably confusing.

Carefully consider the proportion of letter area to overall sign background area - If letters take up too much sign, they may be harder to read. Large letters are not necessarily more legible than smaller ones. A general rule is that letters should not appear to occupy more than 40 percent of the sign panel area.



Consider interior neon signs - Neon signs lend themselves to creative and exciting artistic expression. The use of neon signs inside a storefront can be used to attract attention and create a special ambience.

Make signs smaller if they are oriented to pedestrians - The pedestrian-oriented sign is usually read from a distance of 15 to 20 feet; the vehicle-oriented sign is viewed from a much greater distance. The closer a sign's viewing distance, the smaller that sign need be.

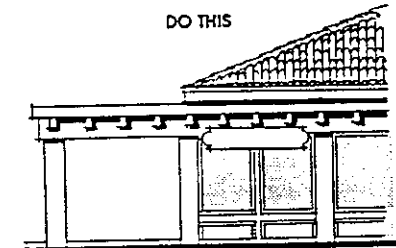
2. WALL OR FASCIA SIGNS

Building wall and fascia signs should be compatible with the predominant visual elements of the building. Commercial centers, offices, and other similar facilities are required to be part of a sign program in accordance with the provisions of this Specific Plan.

Where there is more than one sign, all signs must be complementary to each other in the following ways:

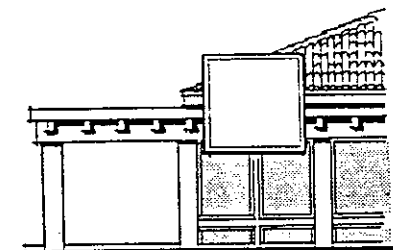
- Type of construction materials (cabinet, sign copy, supports, etc.)
- Letter size and style of copy
- Method used for supporting sign (wall or ground base)
- Configuration of sign area
- Shape of total sign and related components.

DO THIS



Sign is in scale and character with building articulation.

DON'T DO THIS



Sign is out of scale and character with building

The use of graphics consistent with the nature of the product to be advertised is encouraged, i.e., hammer symbol for a hardware store, mortar & pestal for a drug store.

Direct and indirect lighting methods are allowed provided that they are not harsh or unnecessarily bright. The use of can-type box signs with translucent backlit panels are not allowed. Panels must be opaque if can-type sign is used and only the lettering should appear to be lighted.

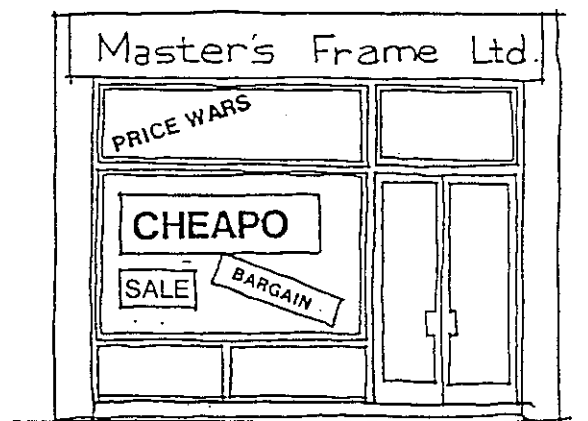
The use of backlit individually cut letter signs is **strongly** encouraged.

The use of permanent sale or come-on signs is prohibited. The temporary use of these signs is limited by the provisions of the Zoning Code.

The identification of each building or store's address in 6 inch high numbers over the main entry doorway or within 10 feet of the main entrance is encouraged.

On window signs, day glo/tempura paint shall be limited to 25 percent of glass area.

Maximum length of any sign should not span more than 60 percent the total display facade (front).



The use of permanent come-on signs is prohibited.

3. FREESTANDING SIGNS

Freestanding signs are intended to provide street addresses, and identification for the freestanding building or commercial center development as a whole and for up to three major tenants.

All tenant signs should be limited in size to the width of the architectural features of the sign and shall be uniform in size and color.

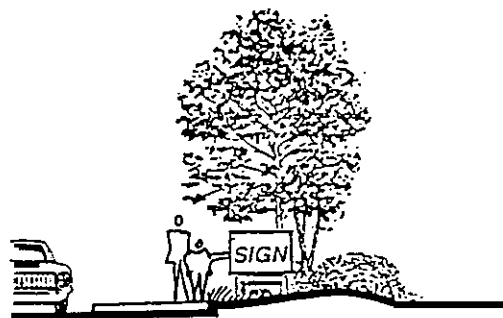
A minimum of 10 percent of the sign area of freestanding signs for large multi-story buildings or center developments should be devoted to identification of the center or building by address or name. Commercial or business park developments should display the range of addresses for that development on their freestanding sign.

Freestanding signs should be placed perpendicular to approaching vehicular traffic.

Freestanding signs should be placed in raised planters whenever possible.

Each monument sign should be located within a planted landscaped area which is of a shape and design that will provide a compatible setting and ground definition to the sign, incorporating the following ratio of landscape area to total sign area:

- Monument: Four (4) square feet of landscaped area for each square foot of sign area (one side only).
- Directory: Two (2) square feet of landscaped area for each square foot of sign area.



Place sign perpendicular to approaching traffic in landscaped area.

E. LANDSCAPE GUIDELINES

1. LANDSCAPE CONCEPT

The primary objective of the Landscape Guidelines is to promote pleasing and unified environment within the Specific Plan area, unique from the remainder of the City of Montclair.

An orderly relationship between street and on-site landscaping has been designed with specifically designated perimeter setbacks. These setbacks will contain trees, shrubs, groundcover, and annual color.

On-site landscaping should be consistent or compatible with the perimeter setbacks. In addition, the on-site landscaping should enhance the building environment, screen parking and service areas, and provide protection from exposure to the sun.

2. AESTHETIC CONSIDERATIONS

The following should be considered by the development's landscape architect when preparing concepts and designs for a site in the Specific Plan Area.

Creating a strong front setback thematic planting.

Enriched entry areas and visitor parking.

Consideration of a theme that is recognizable and usable in various media forms.

Special lighting in public/private open spaces.

3. GENERAL LANDSCAPING GUIDELINES

The purpose of these Landscape Guidelines is to establish a landscape framework that provides continuity and enhances the area's aesthetic quality while allowing a variety of distinct theme statements. A high degree of landscape quality should to be maintained throughout the Specific Plan area.

A simple design character that is easy to maintain and unique relative to other commercial areas, is sought. The overriding goal is to provide a positive visual experience for all persons visiting the Specific Plan area.

Trees, both in static lines and informal masses should be utilized to enclose exterior spaces and provide physical protection from the sun and wind in paved areas.

Contiguous parcels or areas with parcels reserved for future expansion should provide installation of the required landscape areas adjacent to the street at the time the first phase of development occurs.

Landscaping and open spaces should be designed as an integral part of the overall site plan design. Landscaping and open spaces should enhance the building design, enhance public views and spaces, provide buffers and transitions, and provide screening.

Landscape design should accent the overall architectural design theme through the use of structures such as arbors and trellises which are appropriate to the particular architectural style of adjacent buildings.

Drought tolerant plant species are highly desirable and may be used in place of any materials on the Plant Palette.

Xeriscape (low water) landscape methods are strongly encouraged.

The following are common planting design concepts that should be used whenever possible:

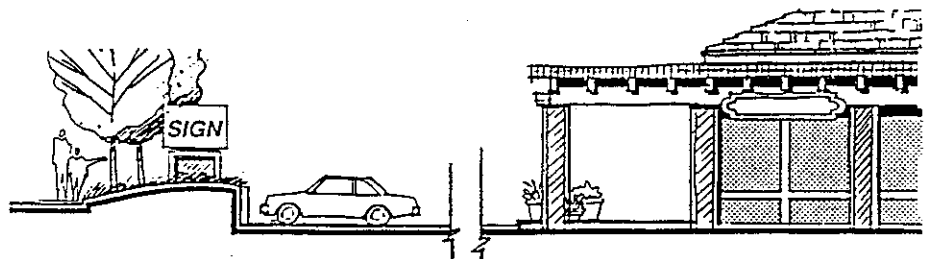
- Specimen trees used in informal grouping and rows at major focal points
- Extensive use of trees, vines, and groundcover both on garden walls and along building walls
- Pots, vases, walls, or raised planters
- The use of planting to create shadows and patterns against walls
- Trees to create canopy and shade, especially in parking areas
- The use of flowering trees in informal groups to provide color
- Informal massing of colorful plantings
- Use of distinctive plants as focal points
- Berms, plantings, and low walls to screen parking areas from view of public rights-of-way while allowing filtered views of larger buildings beyond

Landscape plant materials should be "long-lived" varieties. "Short-lived" materials such as flowering annuals and perennial may be utilized to accent or augment the "longer-lived" elements which form the landscape framework.

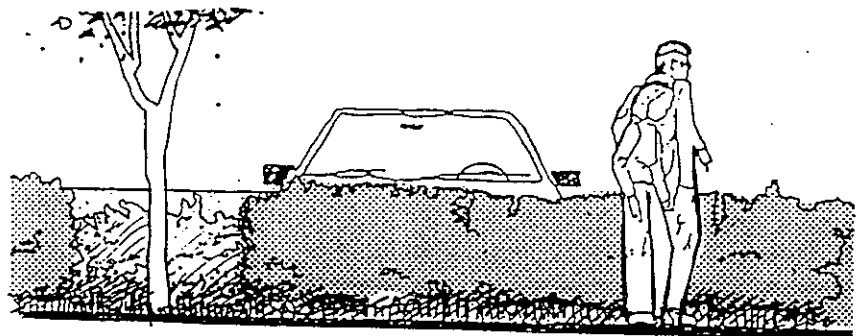
Landscape elements within the front building setback visible from the public right-of-way should blend and connect with street edge landscaping.

Landscape setback areas should incorporate plantings utilizing three tier system; 1) grasses and ground covers, 2) shrubs, and 3) tree. Trees are required at one per three hundred square feet of setback area minimum. They may be clustered for emphasis.

Where parking areas occur adjacent to the Holt Boulevard public right-of-way, such area should be screened from view by a combination of low walls (thirty-six inch maximum height), berms and landscape materials. The objective is to screen automobiles up to the height of the windshield (see graphic). Where practical, lowering the grade of the parking lot from the adjacent street elevation can also help to obscure views of automobiles while allowing views to the buildings beyond.



Parking grade differential screening.



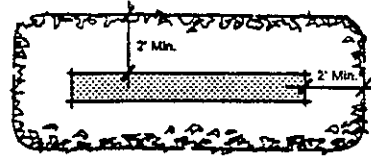
Three tier landscape screen.

Parking lots should be landscaped with the following objectives in mind:

- utilize uniform tree species and container size in the interior of the lot
- visually break up large paved areas with landscaping
- maximize distribution of landscaping
- create tree groves in orderly patterns
- shade 50 percent of the asphalt area within five years from time of installation

Mature tree branches should not be lower than ten feet from grade elevation.

A minimum two foot landscape border surrounding a monument sign should be provided (see graphic).



Landscape areas should have plant material selected and plant methods used which are suitable for the soil and climatic conditions of the site. In areas other than parking lots, the sizes of the plant materials should generally conform to the following mix to create a sense of establishment and variety:

Trees

- 50 percent, 24 inch box
- 40 percent, 15 gallon
- 10 percent, 5 gallon

Shrubs

- 90 percent, 5 gallon
- 10 percent, 1 gallon

Groundcover

100 percent coverage, within 1 year

The spacing of trees and shrubs should be appropriate to the species used. The plant materials should be spaced so that they do not interfere with the adequate lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Proper spacing should also insure unobstructed access for vehicles and pedestrians in addition to providing clear vision of the intersections from approaching vehicles.

All groundcover should be healthy, densely foliated, and well rooted cuttings, or one gallon container plants. Herbaceous and flat plant groundcovers should be planted no more than twelve inches on center and woody, shrub groundcover should be planted no more than four feet on center.

Plant material should generally conform to the following spacing standards:

- A minimum of twenty feet from the property corner at a street intersection to the center of the first tree or large shrub.

- A minimum of fifteen feet between center of trees and large shrubs to light standards.
- A minimum of fifteen feet between center of trees or large shrub and fire hydrants.
- A minimum of ten feet between center of trees or large shrub and edge of driveway.

4. PLANT PALETTE

These trees were selected for their abilities to thrive in urban conditions such as heat, smog, compaction, and drought tolerance. The trees on this list are evergreens. Deciduous trees will be allowed as accent trees at building entry.

Selections for private properties in the Specific Plan area should be made from the following:

Trees

Botanic Name

Common Name

Brachychiton populneus	Bottle Tree (in turf or groundcover areas only)
Calodendrum capense	Cape Chestnut
Cedrus deodara	Deodar Cedar
Cinnamomum camphora	Camphor Tree*
Cupaniopsis anacardioides	Carrot Wood
Ficus benjamina	Benjamin Fig*
Ficus rubiginosa	Rusty Leaf Fig*
Ficus r. 'nitida'	Indian Laurel*
Geijera parviflora	Australian Willow
Harpephyllum caffrum	Kaffir Plum
Hymenosporum flavum	Sweet Shade
Lagerstroemia indica	Crape Myrtle
Magnolia g. 'Majestic Beauty'	Majestic Beauty
Magnolia g. 'Samuel Sommer'	Samuel Sommer
Magnolia g. 'Russet'	Russet Magnolia
Magnolia g. 'St. Mary'	St. Mary Magnolia
Metrosideros tomentosus	New Zealand Christmas Tree
Pinus canariensis	Canary Island Pine
Pinus halepensis	Aleppo Pine
Schinus terebinthifolius	Brazilian Pepper
Tristania conferta	Brisbane Box

Shrubs

Hibiscus 'White Wings'
Raphiolepis l. 'Enchantress'
Pittosporum t. 'Variegata'
Buxus j. 'Green Beauty'
Camellia sasanqua

* These trees should be planted in root barriers.

NOTE: All of the trees are to be planted in root barriers when located within frontage landscape planter.

**5. HARDSCAPE
GUIDELINES**

For purposes of these Design Guidelines, hardscape elements include all paving material on private property including sidewalks, paths, walkways, courtyards, and plazas.

Paving materials for pedestrian walkways may include broom finished concrete, bomanite, colored concrete, pavers, and brick.

Pedestrian paving materials should be:

- textured enough to avoid slipping on when wet
- flat enough to avoid ankle sprains
- dark colored enough to avoid stains
- durable enough to go unmaintained for a number of years
- pleasing to look at
- fade resistant
- non reflective

Discouraged paving materials include:

- "turf block"
- loose gravel
- paving with large, gapping joints
- bomanite or paver with a cobblestone or rounded appearance
- white or very light colored paving
- decomposed granite

Asphalt is not considered a suitable paving material for pedestrian walkways.

**6. IRRIGATION
GUIDELINES**

The intent is to provide adequate irrigation coverage with the most efficient use of a limited water resource. The following suggestions are consistent with these goals.

a. *Turf Areas*

Utilize a low gallon spray head typical of Rainbird 10 series with a six inch pop-up body. This equipment with triangular spacing is an efficient means of irrigation in turf areas.

b. *Shrub Areas*

A head typical of the Rainbird 12 series is very applicable for low gallonage characteristics. Drip irrigation should be considered for some shrub areas.

c. *Individual Shrubs*

A pressure compensation bubbler has low flow characteristics and is compatible with other irrigation systems suggested.

d. *Irrigation Application*

The best practice in irrigation is for many short applications. This will reduce sheet runoff and allow the water to absorb into the soil. Watering at night and early morning is best because of reduced evaporation. When low gallon spray heads are used, longer applications may be allowed.

Care should be taken in design of lawn and planting areas to provide methods of holding excess water on site. Nuisance water must be retained on site and off public rights-of-way. Irrigation spacing should avoid spraying sidewalks, roadways, and building walls.

Approved back flow prevention devices should be installed to service any and all sprinkler irrigation systems.

Pressure reducers should be installed with backflow devices in cases of extreme water pressure.

Backflow devices should be placed out of site and/or should be screened with shrub material.

To minimize negative visual impact, all automatic valves should be installed in valve boxes, and the pop-up variety of head used whenever application allows.

7. MAINTENANCE

Landscape installation should be permanently maintained to meet the highest quality standards at all times. Maintenance will include:

- Irrigation
- Mowing
- Trimming
- Weed Control
- Fertilization
- Pruning
- Edging
- Pest Control

As plants decline, they should be immediately replaced with plants of similar quality and size. Replacement plants should be as large as possible, depending on rate of growth. Plants may not be removed unless they hinder the health of the general plant community. Trimming will be encouraged over removal.

F. STREETScape PLAN

1. INTRODUCTION

The Holt Boulevard Streetscape Plan focuses on design at specific intersections and on creation of consistent urban design elements that would emphasize a sense of place. One of the characteristics of Holt Boulevard are the Gateways located at Mills Avenue at Holt Boulevard and Benson Avenue at Holt Boulevard. These two Gateways are designated as major intersections and entrances to the City. The third major intersection is Central Avenue at Holt Boulevard, which is the area's primary focus intersection. This intersection includes all the design elements that contribute to creation of the sense of Urban Activity District. Other intersections are designated as minor intersections. The design guidelines for minor intersections repeat some of the elements of major intersections, but they emphasize a more intimate, comfortable atmosphere. Another design element for Holt Boulevard is landscaped medians, which create the impression of an accessible and distinct Parkway District. In addition, the landscaped median along the Boulevard creates one consistent design element, which ties major and minor intersections together. Other design elements include the sidewalk, which is provided along the entire Boulevard.

2. TYPICAL MAJOR INTERSECTION

The Major Commercial Intersection needs to relate to the pedestrian as well as vehicular traffic. Currently, Holt Boulevard is a highly travelled through street to the Cities of Ontario and Pomona with high rates of speed and unlimited access. There are three intersections targeted for the Major Intersection Treatment. Two of them act as Gateways to the City. They are:

- Mills Avenue at Holt Boulevard
- Benson Avenue at Holt Boulevard

Mills Avenue at Holt Boulevard will create major urban identity at the northwest and southwest corners of the intersection. Benson Avenue will receive Gateway treatment at the northeast and southeast corners. These two Gateway intersections will control through traffic and provide excellent access to this southern commercial district of the City. The typical hardscape treatment to Gateway intersections is a ten foot wide crosswalk with standard brick pavers, which is extended to the sidewalk corners. The crosswalks will have a 12" concrete accent band on both sides. To emphasize the district edge, planters will be installed on both eligible corners of the intersection. The planter area will slope upward toward the back of a 3' high monument sign wall oriented diagonally to

3. TYPICAL MINOR INTERSECTION

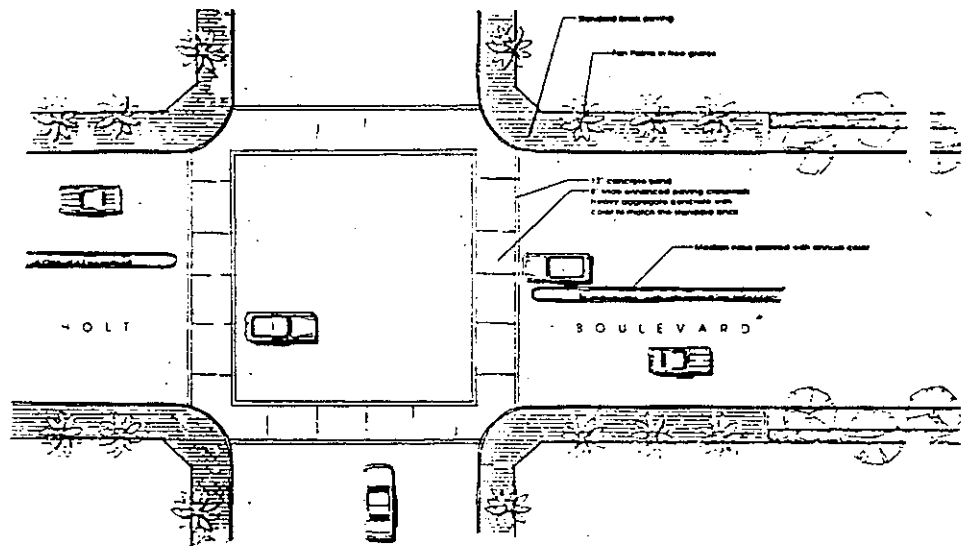
The minor intersections appear smaller and more intimate. These intersections repeat many of the design elements of the major intersections, yet many of the "special accent" elements are not repeated. There are two minor commercial intersections located in the Parkway District. They are:

- Ramona Avenue at Holt Boulevard
- Monte Vista Avenue at Holt Boulevard

The design for minor intersections is simple and it creates a comfortable "walk through" atmosphere. The improvements include ten foot wide enhanced paving crosswalks on four sides of the intersection. They are finished with heavy aggregate concrete which color matches the standard brick; they have a 12" accent concrete band on each side. Each corner sidewalk of the intersection is finished with standard brick paving, which extends 100' from the corner in both directions.

The brick paved section of the sidewalk is enhanced by Fan Palms in tree grates in groups of three on each side of the street. Other improvements include the median nose planted with annual color, which is provided only on Holt Boulevard.

TYPICAL MINOR INTERSECTION
CONCEPTUAL DESIGN

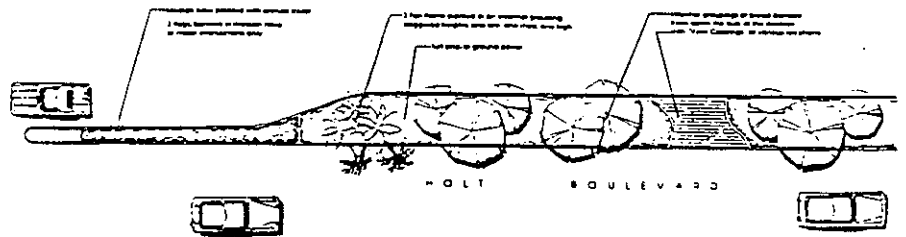


4. TYPICAL MEDIAN

The installation of a central median will eliminate the center two-way left turn lane reducing the number of conflict points. The median element of the streetscape is an attractive enhancement which contributes to consistent urban design and the image of accessibility. Median breaks are proposed to be set at a minimum interval of 660 feet and a maximum interval of 1,320 feet. Final design and turning/access points will be determined in a later design phase involving the City engineer and other City staff.

The median nose will be planted with annual color and there will be three flags/banners provided at major intersections. The transition zone between the median nose and the full median will be planted with three Fan Palms in an informal grouping with staggered heights - one low, one medium, and one high. The full median is covered with turf and/or ground cover and is planted with an informal grouping of broad canopy trees down the back of the median with a "view clearing" at strategic locations. The ground plane element that can be used for a "view clearing" would be a standard brick paving to match the crosswalks and special sidewalk treatments.

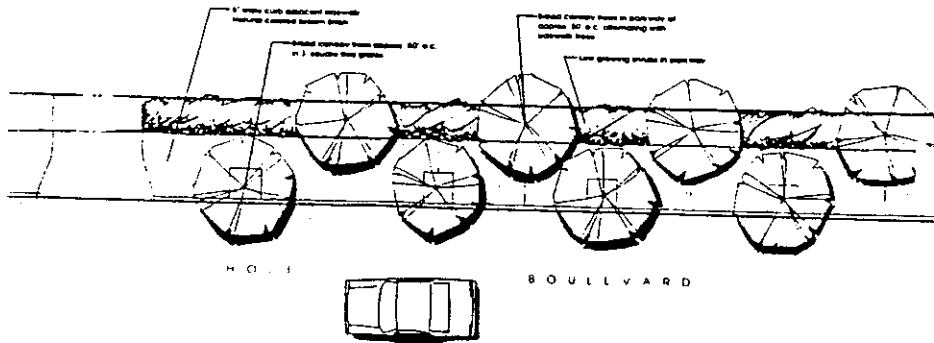
TYPICAL MEDIAN
CONCEPTUAL DESIGN



5. TYPICAL SIDEWALK

The sidewalk is a major streetscape element. Its texture color and landscaping is an essential visual enhancement for pedestrians and motorists using Holt Boulevard. The existing sidewalk on Holt Boulevard is 6' wide concrete with natural colored broom finish and could be enhanced by broad canopy trees approximately 50' o.c. in 3' square tree grates. Immediately adjacent to the sidewalk would be a 4' wide parkway planter area with low growing shrubs. The parkway area is accented by broad canopy trees at approximately 50' o.c. which alternate with sidewalk trees.

TYPICAL SIDEWALK
CONCEPTUAL DESIGN



II. DEVELOPMENT FRAMEWORK

A. PLAN ORGANIZATION

1. COMMUNITY BACKGROUND

Describes the setting as well as major opportunities and constraints of the planning area.

2. GOALS, OBJECTIVES AND POLICIES

Specifies the foundation for what the Specific Plan is designed to accomplish and establishes a framework of policies for how specific goals and objectives will be achieved.

3. CONCEPT PLANS

Describes the several component plans that form the framework of the Holt Boulevard Specific Plan.

a. Land Use

Includes the overall corridor land use plan, and subarea concept plans.

b. Community Design Plan

Provides an overall community design plan which details various image enhancement elements, including gateway and entry treatments.

c. Community Design Guidelines

Provides specific design guidelines related to architectural imagery, landscaping, and site planning.

4. DEVELOPMENT REGULATIONS

Provides the necessary development standards to assure consistency with the Specific Plan objectives and policies.

5. IMPLEMENTATION AND ADMINISTRATION

Describes strategies related to implementation of the Specific Plan, financing programs, and redevelopment agency participation.

6. INITIAL STUDY

Identifies environmental impacts associated with the proposed Specific Plan and outlines mitigation measures.

**B. COMMUNITY
BACKGROUND**

1. EXISTING SETTING

Established as a major thoroughfare, Holt Boulevard originates in Los Angeles as Valley Boulevard from the west and continues through Ontario to the east. Today, Holt Boulevard is experiencing a decline in land uses. Proven to be a primary area for auto related uses and auto dealerships in the past, the auto malls, have been soliciting the dealerships to move off the Boulevard. Although, given the failure of other auto malls it has been proven that Holt Boulevard is a viable location for an auto dealership. In addition, the commercial retail uses are relocating closer to Montclair Plaza in the northern part of the City. Land uses along Holt Boulevard today consist of a mixture of residential, commercial, auto related commercial and services, light industrial, and vacant land.

In general, Holt Boulevard reflects the lack of a comprehensive planning tool for development, which has resulted in the lack of a cohesive development theme or structure. The pattern of lots, previously established, has resulted in difficulties for the implementation of planned development, and has resulted in the establishment of a large number of curb cuts accessing the Boulevard. This access impedes traffic flow, and consequently, many areas have developed a "hodgepodge" appearance.

With the goal of eliminating existing undesirable characteristics and establishing a unified development policy for the area, the Holt Boulevard Specific Plan was initiated in 1989 by the City of Montclair, with the intent to enforce the policies and principles identified in the Community Design Element of the General Plan.

**2. OPPORTUNITIES
AND CONSTRAINTS**

As with any physical development program, especially in an already developed area, certain aspects of the built environment act as constraints to the achievement of desired development goals, while others offer opportunities which can be taken advantage of to promote the types of development envisioned for the area.

After an analysis of the existing physical conditions, a listing of opportunities and constraints within the Holt Boulevard area, which will affect the development of a Specific Plan, was generated.

This was created based on the following objectives to be achieved in the development of the Holt Boulevard Specific Plan:

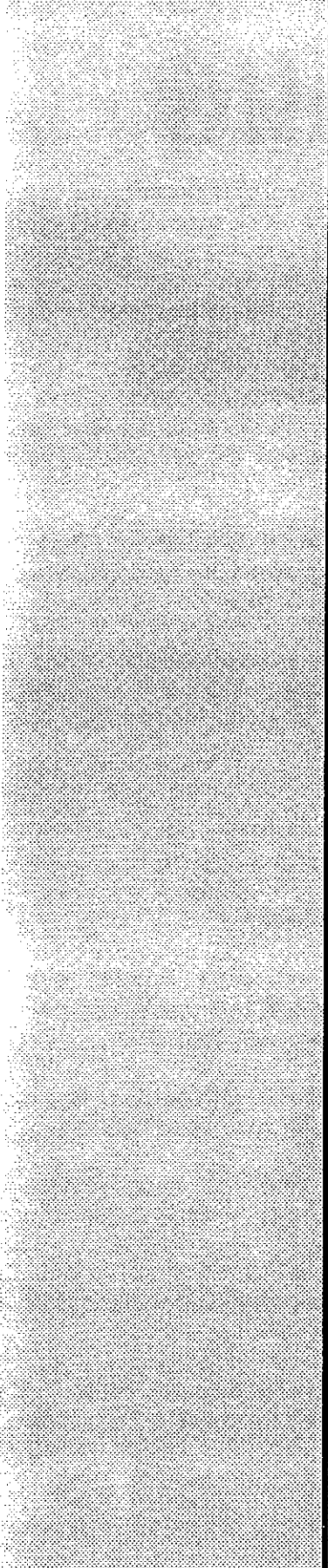
a. *Objectives*

- (1) Develop alternative land use schemes for the Project Area.
- (2) Develop architectural and landscaping theme(s) including design standards and guidelines to ensure harmony between individual developments within the Project Area.
- (3) Development of design alternatives for traffic circulation along Holt Boulevard.
- (4) Identify the non-conforming development aspects of the Specific Plan with the Montclair General Plan and propose amendments, as well as, procedures for implementing each alternative.
- (5) Create design standards reflective of the intensity of use to require existing allowed use to conform to higher design standards.

When combined with public policy input (goals and objectives) the opportunities and constraints become the ingredients used in formulating the overall direction of the Specific Plan. The identified opportunities and constraints are as follows:

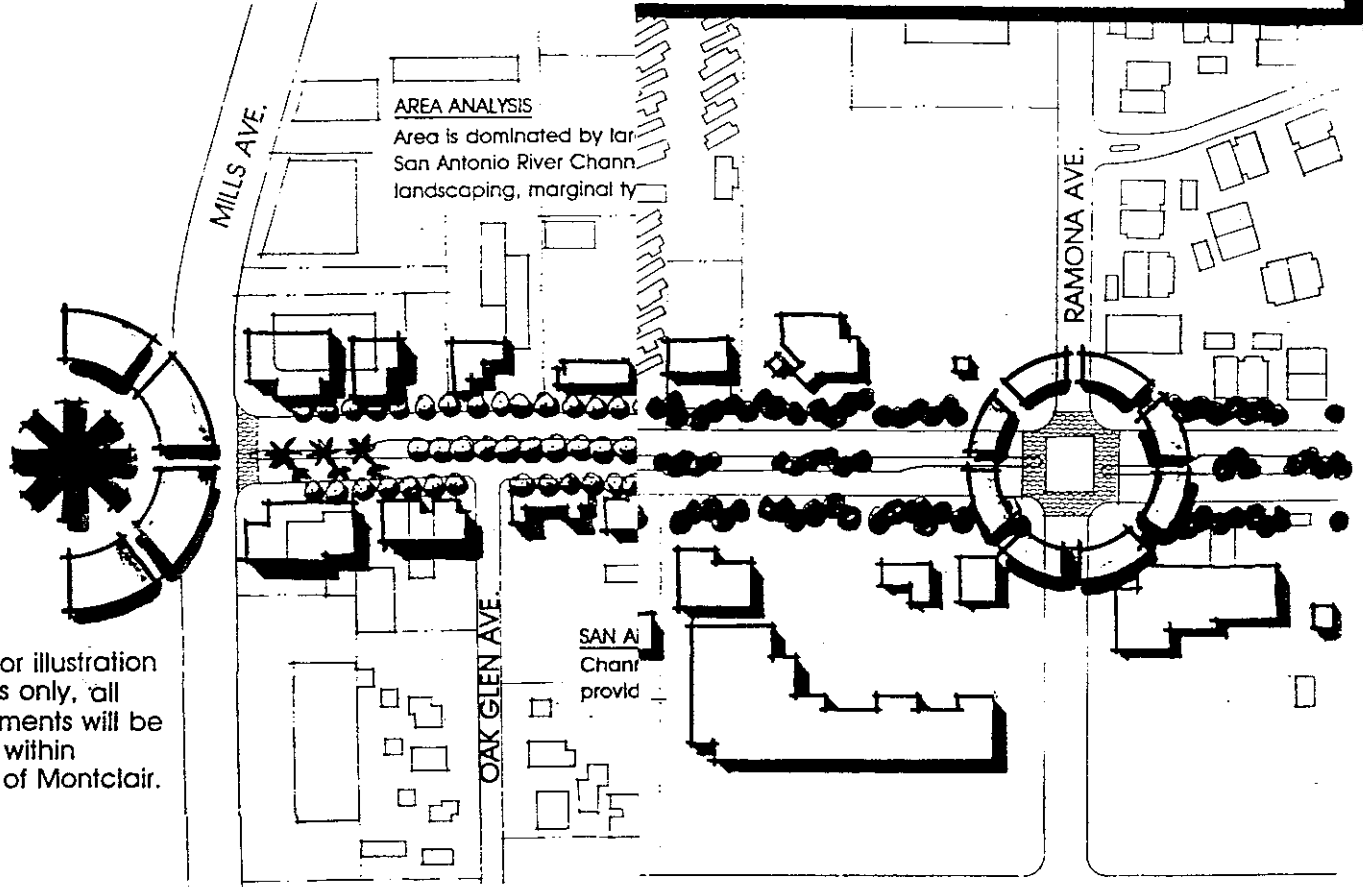
b. *Opportunities*

- (1) With the adoption of a specific plan, the area will have a clear development concept providing for maximum development potential.
- (2) The image enhancement activity proposed can be designed to improve business opportunities, increase tax revenues, and promote civic pride.
- (3) A more desirable land use can be established and encouraged to be developed by the property owners.
- (4) Discourage the continued illegal social problems in the area, by providing community design concepts.
- (5) Holt Boulevard is a major east/west transportation route for the City and can present the community with a positive image.
- (6) A wide, landscape raised/center median on Holt Boulevard can provide a singular consistent urban design element for the entire Boulevard.

- 
- (7) Designation of Mills Avenue at Holt Boulevard, Central at Holt Boulevard, and Benson at Holt Boulevard as Gateways will encourage the construction and improvement of those intersections and reduce the rate of accidents.
 - (8) The development of a sign program for removal of non-conforming signs and the establishment of sign standards to be in harmony with the surrounding environment will provide higher design standards.
 - (9) The Existing and Proposed Land-Uses in the Specific Plan Area can be subject to new design concepts that will be implemented through the Zoning Ordinance in order to encourage quality development.
 - (10) The potential exists for smaller individual parcels to be consolidated into larger parcels capable of conforming to higher design standards.

These opportunities are illustrated in graphic form in Figures II-1, II-2, II-3, and II-4.

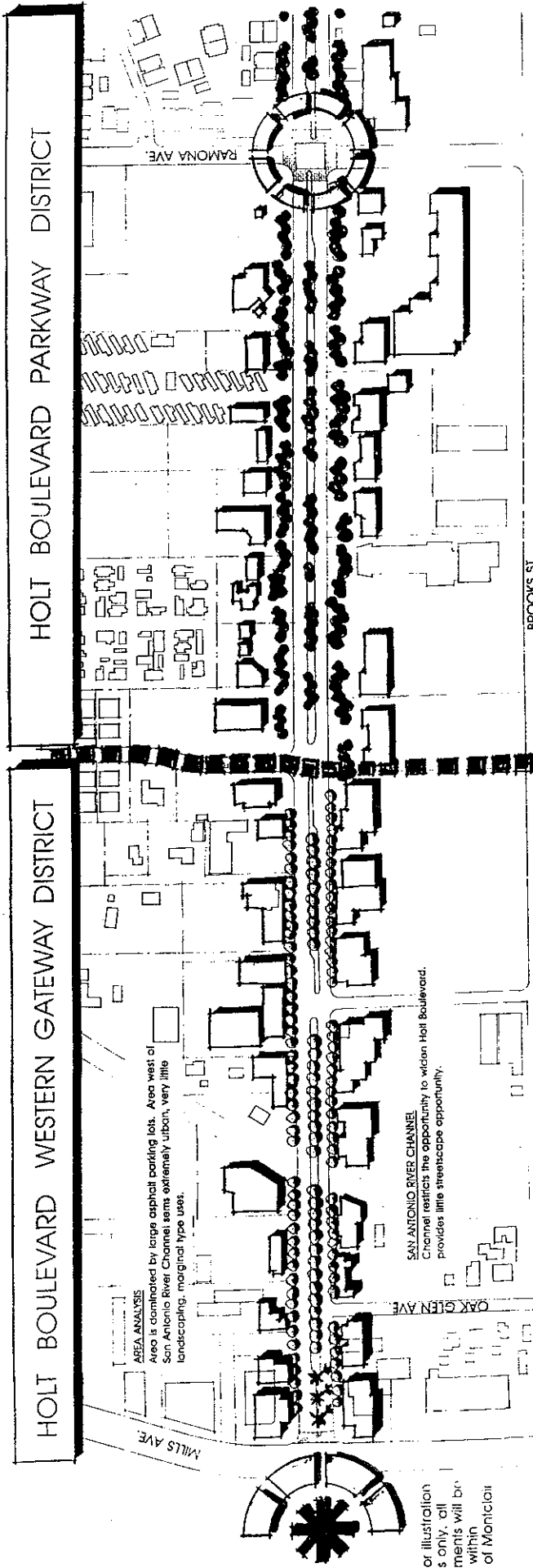
HOLT BOULEVARD PARKWAY DISTRICT



ies
 (density buildings, tall trees)
 on for streetscape design flexibility
 trees at Silicon Avenue for entry

NOTE: This graphic presentation does not represent the details for the location of median breaks. Median breaks are proposed at a minimum interval of 660' and a maximum interval of 1320', although final determination is subject to engineering evaluation and design.

FIGURE II-1



HOLT BOULEVARD WESTERN GATEWAY DISTRICT

HOLT BOULEVARD PARKWAY DISTRICT

AREA ANALYSIS
 Area is dominated by large asphalt parking lots. Area west of San Antonio River Channel seems extremely urban, very little landscaping, marginal type uses.

SAN ANTONIO RIVER CHANNEL
 Channel restricts the opportunity to widen Holt Boulevard. provides little streetscape opportunity.

BROOKS ST.

GATEWAY NODE IMPROVEMENTS

- Special crosswalk and intersection paving
- Vertical Architectural Thematic Element
- Vertical, formal tree planting
- Entry signs

HOLT BOULEVARD WESTERN GATEWAY DISTRICT

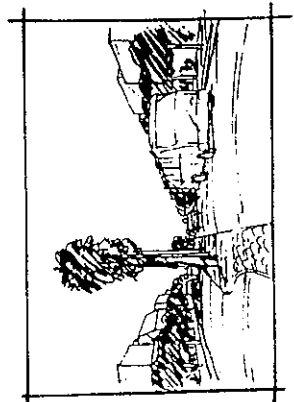
Mills Avenue at Holt Boulevard will be characterized by a gateway sign and either an architectural theme tower, flag, or a vertical tree planting of palms in the median nose. Landscape improvements will reflect a decidedly urban "formal" appearance utilizing xeriscape concepts. The two Gateway Districts at both ends of the Boulevard and the Central Avenue District will have special setback and site planning requirements to retain building facades closer to the sidewalks. New development will seek a larger (2-3 story) vertical proportion versus the dominant horizontal proportion (single story) of typical strip commercial development.

OPPORTUNITIES

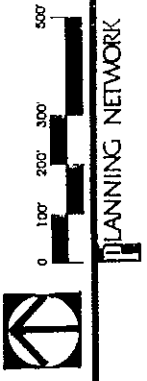
- major median opportunities
- increase development intensity
- vertical elements (higher buildings, tall trees)
- streetscape planting
- on street parking reduction for streetscape design flexibility
- relocate mature palm trees at Silicon Avenue for entry gateway plantings
- vacant lots
- open space remaining

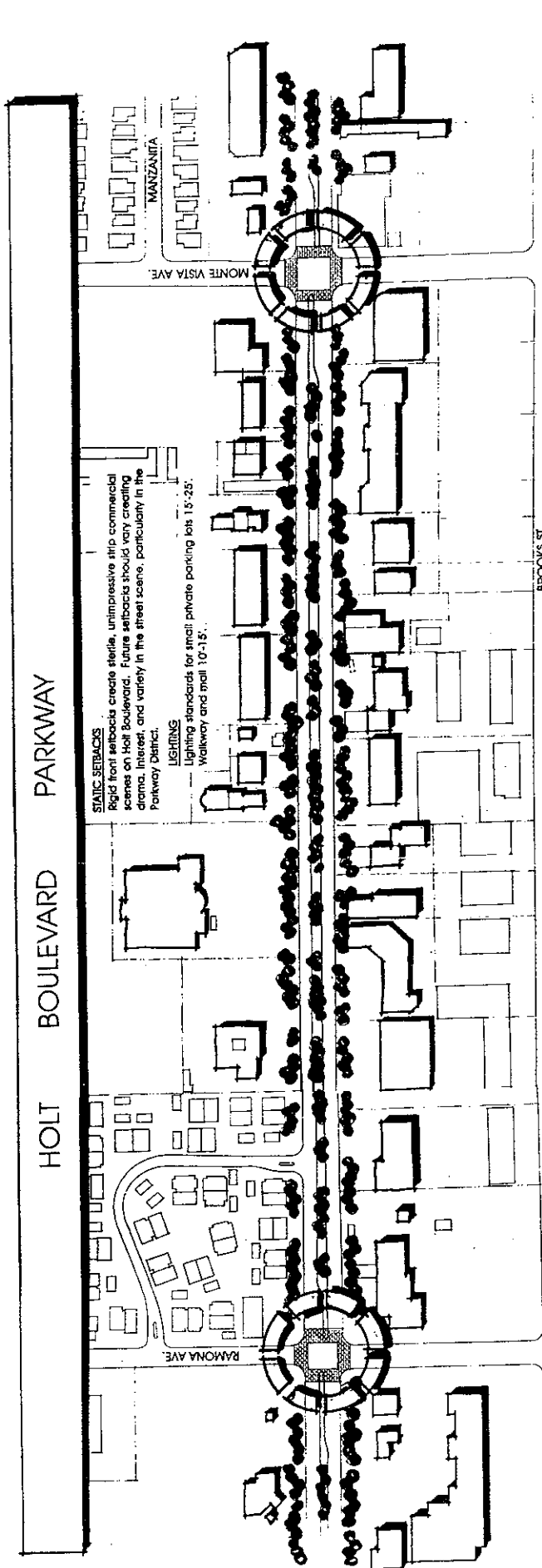
CONSTRAINTS

- various architectural styles
- "curb adjacent" sidewalks
- non conforming signs
- horizontal proportion building monotony
- mix of land uses
- topographic monotony
- uncohesive street tree program



NOTE: This graphic presentation does not represent the determination for the location of median breaks. Median breaks are proposed to be set at a minimum interval of 660' and a maximum interval of 1320'. Although final determination is subject to engineering evaluation and design.





MEDIANS

A wide, landscape raised/center median on Holt Boulevard can provide a singular consistent urban design element for the entire Boulevard. Landscape design of the median will vary from formal to informal arrangements while utilizing San Antonio River Rock. The following benefits can be realized with the implementation of a raised center median program:

- unified urban design statement
- improves aesthetic perception of roadway
- increased vehicular safety and level of service
- reduced vehicular traffic conflict points

NOTE: This graphic presentation does not represent the determination for the location of median breaks. Median breaks are proposed to be set at a minimum interval of 600' and a maximum interval of 1320', although final determination is subject to engineering evaluation and design.

HOLT BOULEVARD PARKWAY DISTRICT

This Parkway District will be designed to link the Gateway District with the Urban Activity District of Central Avenue. The Parkway District will be recognized by informal tree clusters in the medians and private landscape setbacks. Undulating tiered berms planted with spreading trees will reflect the broad valley and suburban character of this area. Buildings with decidedly horizontal proportions (1-2 story) will be appropriate.

TOPOGRAPHY

The firmness of topography and continuity of low density suburban commercial land uses emphasizes the need for vertical trees/buildings/features. Consider vertical thematic features at intersections, particularly on the north-south right-of-way.

SIDEWALKS

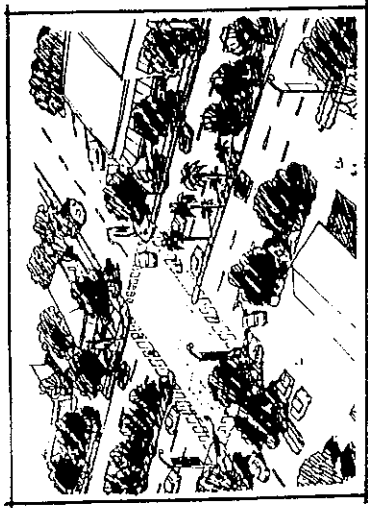
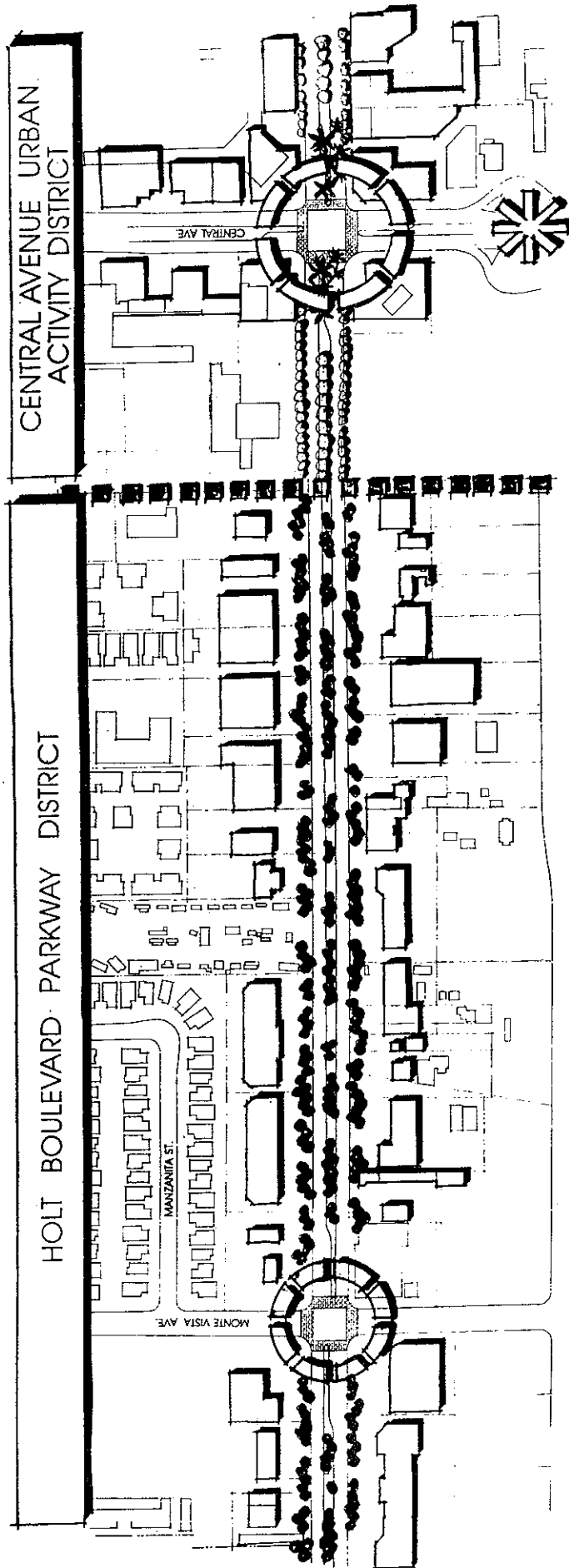
This Urban Design Framework Plan places little emphasis on enhanced sidewalk paving due to the dominant vehicular orientation of the Boulevard. Also, much of the Boulevard currently has simple unadorned curbed adjacent concrete sidewalks in place. Saw cutting trees into the existing concrete sidewalks may present an opportunity in some areas due to the relative lack of pedestrian density on any of Holt Boulevard's sidewalks.

COMMERCIAL SIGNS

Develop a program for removal of non conforming signs by offering either zoning or financial relief. All roof and pole signs should receive the highest priority.



PLANNING NETWORK

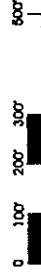
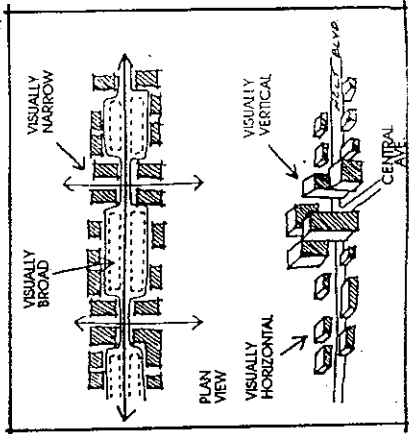


NOTE: This graphic presentation does not represent the determination for the location of median breaks. Median breaks are proposed to be set at a minimum interval of 650' and a maximum interval of 1,320', although final determination is subject to engineering evaluation and design.

INTERSECTIONS
 Intersections on Holt Boulevard will provide the "pizzazz" for urban design aesthetics. With appropriate special design emphasis, an intersection can be used to identify gateways to the City, transition from district to district and give the community a readily identifiable image.

COLOR
 Initially, all street furniture should be painted a single consistent color. This treatment will further reinforce Holt Boulevard as a unique commercial corridor from other streets.

TOPOGRAPHY
 The flatness of topography and continuity of low density suburban commercial land uses emphasizes the need for vertical trees/buildings/features. Consider vertical thematic features of intersections, particularly on the north-south right-of-way.



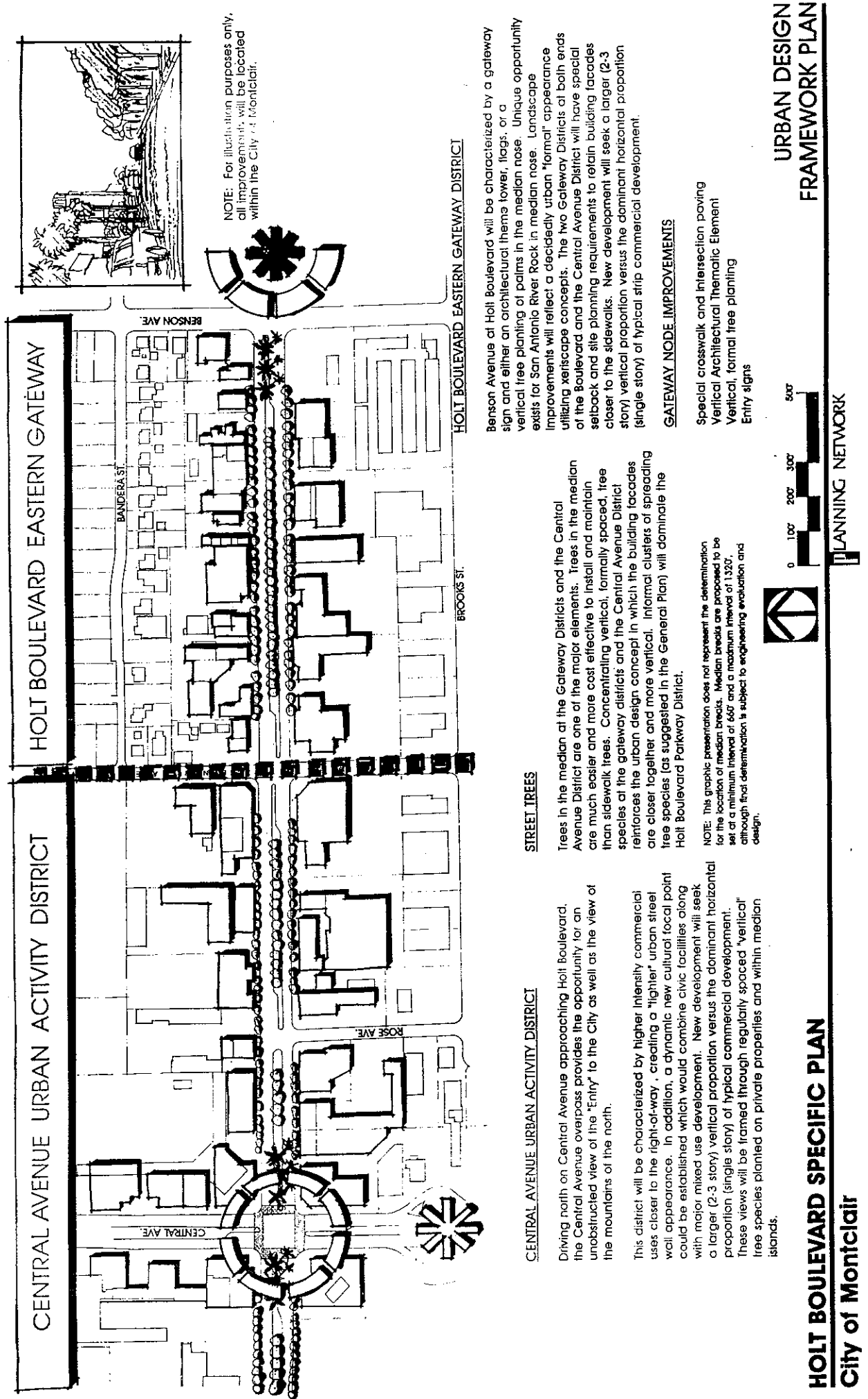
HOLT BOULEVARD SPECIFIC PLAN

City of Montclair

PLANNING NETWORK

URBAN DESIGN FRAMEWORK PLAN

FIGURE II-4



CENTRAL AVENUE URBAN ACTIVITY DISTRICT

HOLT BOULEVARD EASTERN GATEWAY

HOLT BOULEVARD EASTERN GATEWAY DISTRICT

CENTRAL AVENUE URBAN ACTIVITY DISTRICT

Driving north on Central Avenue approaching Holt Boulevard, the Central Avenue overpass provides the opportunity for an unobstructed view of the "Entry" to the City as well as the view of the mountains of the north.

This district will be characterized by higher intensity commercial uses closer to the right-of-way, creating a "lighter" urban street wall appearance. In addition, a dynamic new cultural focal point could be established which would combine civic facilities along with major mixed use development. New development will seek a larger (2-3 story) vertical proportion versus the dominant horizontal proportion (single story) of typical commercial development. These views will be framed through regularly spaced "vertical" tree species planted on private properties and within median islands.

STREET TREES

Trees in the median at the Gateway Districts and the Central Avenue District are one of the major elements. Trees in the median are much easier and more cost effective to install and maintain than sidewalk trees. Concentrating vertical, formally spaced, tree species at the gateway districts and the Central Avenue District reinforces the urban design concept in which the building facades are closer together and more vertical. Informal clusters of spreading tree species (as suggested in the General Plan) will dominate the Holt Boulevard Parkway District.

NOTE: This graphic presentation does not represent the determination for the location of median breaks. Median breaks are proposed to be set at a minimum interval of 600' and a maximum interval of 1,320', although that determination is subject to engineering evaluation and design.

GATEWAY NODE IMPROVEMENTS

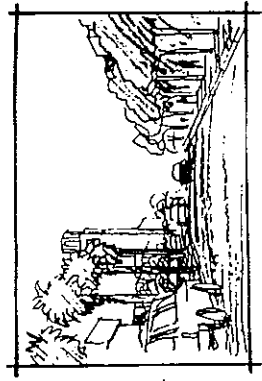
Special crosswalk and intersection paving
 Vertical Architectural Thematic Element
 Vertical, formal tree planting
 Entry signs



PLANNING NETWORK

**URBAN DESIGN
 FRAMEWORK PLAN**

NOTE: For illustration purposes only, all improvements will be located within the City of Montclair.

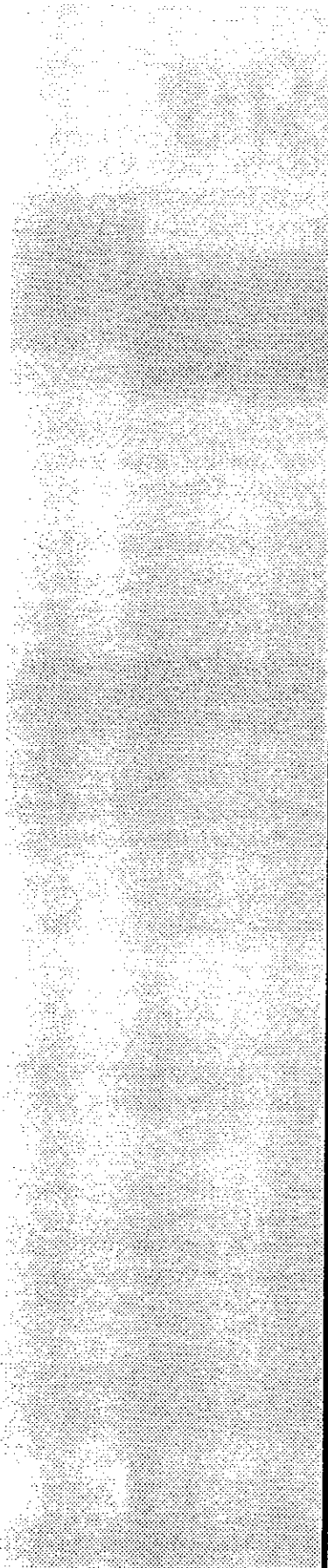


c. *Constraints*

As stated in the purpose of this report, constraints serve as the parameters within which the Specific Plan must function and the challenges which must be met in order to achieve project objectives. Sometimes, these constraints are the result of physical or other features which serve to place direct limits on the types and character of land use which can be established, such as the excessive number of small individual parcels. In other instances, constraints may result from more aesthetic and or nuisance-related factors, such as the presence of the San Antonio Flood Control Channel which restricts the opportunity to widen Holt Boulevard, allowing for greater access.

Constraints upon the development of the Holt Boulevard Specific Plan Area include the following:

- Holt Boulevard is a highly traveled through street to the cities of Ontario and Pomona, with high rates of speed and abundant points of access.
- The San Antonio Flood Control Channel serves as a physical barrier, dividing the Specific Plan Area, and limiting interaction across the Channel.
- The individual parcels are constrained by their size, development standards, and parcel configuration limiting development potential.
- The location and parcel configuration inhibits the visibility and access generally desired by potential commercial tenants.
- Fragmented ownership inhibits the master-planning of areas by introducing the need to satisfy the differing needs of multiple property owners and the need to convince a large number of property owners to develop or redevelop their parcels.
- Individual property owners are expressing interest in development or redevelopment of their properties. The size and configuration, as well as the surrounding land uses, is restricting development of these parcels.

- 
- Certain portions of the Specific Plan are designated as "Special Studies Area". A requirement of the Special Studies Area is to complete Site Specific Analyses, and to establish development standards prior to development. The lack of a Specific Plan is limiting the maximum development potential for the Special Studies Area.
 - Within certain areas of the Specific Plan, the continued development of the individual parcel is reducing the opportunity to combine them into larger parcels capable of conforming to higher design standards.

C. GOALS AND OBJECTIVES STATEMENTS

The purpose of this section is to articulate the Goals, Objectives, and Policies for the Holt Boulevard Specific Plan and begin the foundation for subsequent sections of this plan.

Goals: *Are broad statements that define the community's hope for the future. They are general in nature and do not indicate when and how these goals are to be accomplished.*

Objectives: *Are statements of intent that generally guide future decisions in specific topic areas.*

Policies: *Are more specific statements of intent to deal with particular topics in certain fashion. They begin to define the approach to be taken to achieve the plan objectives, and are in themselves the first step in the development of a solution as they form the basis for standards and regulations.*

OVERALL STATEMENT

- Goals:**
- To create a dynamic concourse that is attractive and of high quality with a unifying community design image and identity.
 - To provide an economically viable setting for a balanced mixture of commercial and industrial uses.
 - To ensure for a safe and efficient, circulation and access.

In an effort to further define and prioritize this overall goal statement, the following Community Design, Land Use/Economic, Circulation, Environmental, Infrastructure, and Crime Prevention Service goals and objectives are provided.

1. COMMUNITY DESIGN

Community Design Goals, Objectives and Policies are organized by elements that make up a comprehensive urban design enhancement plan, as follows:

- Commercial Development
- Business Parks/Industrial Development
- Landscape Guidelines
- Signs
- Streetscape Plan

a. *Commercial Development and Business Parks*

Goals: *Develop urban design techniques that enhance proper land use associations, buffering, and protection.*

Objectives: Require master planning at key sites within the Specific Plan study area to assure integrated development utilizing coordinated access parking, building orientation/locations, pedestrian, and transit facilities.

Employ a Mixed-use Planned Development concept to reduce the number of larger vacant commercial properties, and encourage infill development, while providing opportunities for manufacturing/industrial development.

Ensure the gradual upgrading of under utilized land uses functioning at less than their market potential.

Encourage activities and building uses that enhance the role of Holt Boulevard as a commercial center (in the southern part of the City).

Encourage and promote increased commercial densities in activity nodes, such as those at Mills, Central, and Benson Avenues to enhance pedestrian shopping opportunities within pedestrian walking distances.

Encourage shared parking and access to facilitate efficient parcel usage to minimize traffic support facilities such as drives, parking spaces, etc.

Discourage new commercial developments from taking any access from local residential streets, by developing internal circulation system which direct traffic away from surrounding residential neighborhoods.

Policies: Require compliance with community design guidelines for new development, expansion, or redevelopment and make community design a major consideration in site plan review and approval.

Develop an incentive program which rewards private sector development for providing certain design amenities with their developments. Of particular interest are building overhangs, awnings, and extra tree plantings in parking lot areas. Consideration should be given to special paving materials used in place of asphalt in vehicular areas.

Provide for the elimination of visually objectionable views from public streets for uses, such as outdoor storage and loading areas through the use of design guidelines.

Provide economic incentives for owners who wish to architecturally rehabilitate, refurbish, or upgrade landscaping on existing properties.

Promote distinctive, intense commercial clusters at key activity center nodes such as those at Mills Avenue, Central Avenue, and Benson Avenue.

Prepare Development Regulations and Guidelines which simplify the development review process.

Establish regulations and general guidelines for mixed use developments which assure proper land use mixture, minimum lot size, access restrictions, buffering, and master planning.

Establish an incentive/bonus program of lot consolidation for parcels along the corridor to encourage development into planned concentrations, as opposed to a linear strip commercial configuration.

Encourage nonconforming uses to discontinue as their use amortizes over time.

Encourage the formation of new redevelopment programs to assist retail activity center nodes.

Reduce inter-site vehicular trips onto Holt Boulevard by requiring private developments to secure Reciprocal Access Agreements prior to development.

b. Landscape Guidelines

Goals: *Develop urban design techniques that minimize the feeling of crime. Enhance proper buffering and protection.*

Objectives: Encourage urban treatment that creates the feeling of security and safety.

Encourage design that does not make the area appear as a "crime ridden environment".

Policies: Discourage visible security fencing such as barbed wire or razor tape.

Minimize the height of screening such as shrubs and wall.

Reduce to the extent possible exterior security treatments such as bars on the windows, exposed razor tape, etc.

Develop the regulations that require landscape around the entire base of the buildings.

Discourage the accumulation of storage and equipment that may create a feel of depressed environment or left over spaces.

c. *Streetscape and Signs*

Goals: *Develop a comprehensive streetscape program for Holt Boulevard which promotes and enhances an identifiable image for the area. The streetscape system will distinguish the various sub-districts within the Boulevard. Utilize the streetscape plan specifically at certain intersections as the major organizing element for implementing the Urban Design Program and provide a special identity for Holt Boulevard.*

Objectives: Maintain the highest possible quality of environment within the Specific Plan area, by balancing the impacts of development with environmental concerns.

Develop a streetscape system which designates major street intersections as pedestrian activity nodes, throughout the corridor.

Provide a system of vehicular gateway nodes which announce and identify entries into Montclair and achieve an overall positive identity for the area.

Provide contemporary landscape treatments throughout the corridor particularly those that are low maintenance, drought resistant, and able to withstand winds and intense urban conditions.

Develop consistent streetscape and architectural palette which are sensitive to creating a quality environment for Holt Boulevard. It is not the intent of this policy to discourage innovative or contemporary architectural expression, or to imitate the architecture of the past, but to promote the harmonious coexistence of architectural styles varying from restoration to contemporary architectural themes.

Promote the installation of a raised median within Holt Boulevard in order to insure efficient and safe future traffic operations.

Develop a circulation system which facilitates efficient and safe vehicular and pedestrian movement and enhances the community design character chosen for Holt Boulevard.

Develop a sign program for removal of nonconforming signs and the establishment of sign standards to be in harmony with the surrounding environment.

Policies: Limit vehicular access points onto Holt Boulevard.

Develop a mixture of land uses which reduce home/work trips within the corridor, and the City.

Encourage the use of local transit programs whenever possible.

Provide pedestrian-oriented amenities and circulation features within key activity nodes by providing incentives to private development for pedestrian improvements.

Establish intersection geometric standards which provide for acceptable levels of traffic operations.

Establish regulations which preserve significant environmental features which are worthy of preservation.

Require that all public services and facilities are available, or that financial arrangements have been instituted, prior to issuing building permits.

Monitor utilization of public infrastructure systems and program improvements, to ensure capacity for future planned development.

Coordinate all development activity with the construction of public infrastructure.

Designate special landscape and architectural features at major intersection locations, combining thematic planting with complementary architectural statements designed to promote a distinctive thematic character for these activity center nodes. Changes in paving materials, plant materials, lighting, signing, and siting of adjacent structures should occur at major intersections to enhance their distinctiveness.

Utilize landscape materials within the corridor which are clean, safe, wind resistant, and relatively low maintenance. Formal forms and configurations should be utilized at activity nodes, while less formal, suburban type configurations should be utilized throughout the parkway links in-between.

**2. LAND
USE/ECONOMIC**

Goals: *Insure the Specific Plan will be sensitive to community land use and fiscal needs.*

Objectives: Maximize the economic position of the Holt Boulevard commercial activities, capturing neighborhood and subregional demand.

Promote distinctive, intense commercial clusters at key activity center nodes, such as those at Mills Avenue, Central Avenue, and Benson Avenue.

Require master planning at key sites within the Specific Plan study area to assure integrated development utilizing coordinated access, parking, building orientation/locations, pedestrian, and transit facilities.

Employ a Mixed-use Development concept to reduce the number of larger vacant commercial properties, and encourage infill development, while providing opportunities for manufacturing/industrial development.

Ensure the gradual upgrading of under-utilized land uses functioning at less than their market potential.

Policies: Establish regulations and provide incentive bonuses which promote pedestrian oriented plazas; and encourage intensive commercial uses at key activity nodes along Holt Boulevard.

Prepare Development Regulations and Guidelines which simplify the development review process.

Establish regulations for mixed use developments which assure proper land use mixture, minimum lot size, access restrictions, buffering, and master planning.

Establish an incentive/bonus program of lot consolidation for parcels along the corridor to encourage development into planned concentrations, as opposed to a linear strip commercial configuration.

Encourage the removal of nonconforming uses as ownerships change and allow them to continue as per current City policies and regulations

Encourage the formation of new redevelopment programs to assist retail and service businesses with property improvements.

Develop incentives to encourage specialty commercial clusters at key activity center nodes.

3. CIRCULATION

Goals: *Develop a circulation system which facilitates efficient and safe vehicular and pedestrian movement and enhances the community design character chosen for Holt Boulevard.*

Objectives: Limit vehicular access points on Holt Boulevard.

Develop a mixture of land uses which reduce home/work trips within the corridor, and the City.

Provide for street improvements to maximize the efficiency of traffic along Holt Boulevard.

Minimize vehicular through traffic on adjacent residential streets.

Encourage the use of local transit programs whenever possible

Policies: Provide pedestrian-oriented amenities and circulation features within key activity nodes by providing incentives to private development for pedestrian improvements.

Reduce inter-site vehicular trips onto Holt Boulevard by requiring private developments to secure Reciprocal Access Agreements prior to development.

Provide for bus stops along Holt Boulevard at convenient pedestrian locations.

Encourage shared parking and access to facilitate efficient parcel usage and to minimize traffic support facilities such as drives, parking spaces, etc.

Discourage new commercial developments from taking any access from local residential streets, by developing internal circulation systems which direct traffic away from surrounding residential neighborhoods.

Designate selected residential streets within the planning area as local streets, where conditions promote pedestrian movements over those of the automobile.

Promote the installation of a raised median within Holt Boulevard in order to insure efficient and safe future traffic operations

Establish intersection geometric standards which provide for acceptable levels of traffic operations, as per the City's General Plan.

4. ENVIRONMENTAL

Goals: *Maintain the highest possible quality of environment within the Specific Plan area, by balancing the impacts of development with environmental concerns.*

Objectives: Establish regulations which preserve significant environmental features, such as mature trees within the Holt Boulevard corridor.

Policies: Require mapping of all significant on-site environmental features which are worthy of preservation.

Through the project approval process and the imposition of conditions or mitigation measures, ensure that all development within the Specific Plan area will not result in a decrease in environmental quality, and will, wherever possible, create a higher quality environment.

5. INFRASTRUCTURE

Goals: Provide a high level of public services and facilities to all residents and businesses along Holt Boulevard.

Objectives: Coordinate all development activity with the construction of public infrastructure.

Policies: Require that all public services and facilities are available, or that financial arrangements have been instituted prior to issuing building permits.

Monitor utilization of public infrastructure systems, and program improvements to ensure capacity for future planned development.

6. CRIME PREVENTION SERVICE

Goals: Reduce or eliminate the prostitution, crime, and drug activities from the Specific Plan Area.

Objectives: Enhance the deployment of police officers to the Specific Plan Area.

Reduce the risk and fear of crime through physical planning strategies that will maximize surveillance opportunities and minimize opportunities to crime.

Provide lighting for the purpose of providing illumination for security and safety of on-site areas such as parking lots, loading, shipping and receiving, and working areas.

Discourage crime through the incorporation of "defensible space" concepts into the design of structures and public facilities.

Policies: Study regional, state, and federal programs to determine where opportunities for law enforcement assistance can be utilized.

Explore the availability of state and federal grants to offset any additions in law enforcement staffing and/or equipment for the study area.

Actively pursue more stringent sentencing and probationary standards for individuals engaging in such crimes within the study area.

Law enforcement personnel should be involved in the developer review process for all new development proposals.

The City shall promote the establishment of a businessman watch program to encourage community participation in the patrol of the commercial areas, and increase awareness of any suspicious activity.

Lighting shall be in accordance with the recommended lighting levels established within the Specific Plan.

Landscaping, location of buildings and walls, etc., should facilitate surveillance from the street and from neighboring structures, and should not provide places for concealment.

The street system should allow emergency vehicle access fully around buildings to the full extent possible.

Parking and walkways should be located where surveillance from streets or by an attendant is possible to reduce worker or customer isolation when walking to and from cars.

Access to roofs by pallets, flag poles, etc., should be eliminated or avoided.

Where possible, areas should be designed so that they can be sealed off when not in use.

Street names and building numbers should be well lit for easy identification.

The provision of adequate off-street parking in commercial areas and near public facilities for the purpose of minimizing parking violations.

The installation of a median to discourage midblock turns for quick access by participants in solicitation of criminal activity.

D. CONCEPT PLANS

1. LAND USE PLAN

a. Overall Land Use Concept

The overall land use concept has been designed as a composite of various individual land use zones which provide rich texture and diversity to the community.

The plan was based on the following more specific planning concepts and issues, concerning development of the Holt Boulevard Specific Plan:

The intersection of Central and Holt Boulevard could be reserved solely for automobile sales and related services. This would expand and redevelop the surrounding area as a "True" auto mall.

Small "Vest Pocket" parks could be developed as integral parts of the site plans of individual developments within the specific plan. These vest pocket parks would be primarily oriented to passive lunch time activities.

Encouraging land uses that would stay open after dark, bringing a greater volume of people, making this area more visible, in turn, discouraging crime.

Strip Commercial development on small desegregated lots does not lend itself to the image, scale, or function for the Holt Boulevard corridor.

The consolidation of small parcels needs to be required and "incentives" provided for exceptional consolidation schemes.

b. Sub-Areas

For ease of presenting and discussing the land use concept, the corridor has been divided into four sub-areas as follows:

- Mills Avenue to Ramona Avenue
- Ramona Avenue to Monte Vista Avenue
- Monte Vista Avenue to Central Avenue
- Central Avenue to Benson Avenue

Immediately surrounding the Holt Boulevard Specific Plan study area is a mixture of suburban land uses which include: residential, commercial, industrial, and vacant/open space.

- (1) **Sub-Area 1:** Land uses within Sub-Area 1 are presently dominated by commercial uses, which comprise 40 percent, the highest concentration for the Study Area. This sub-area also has the highest concentration of residential uses within the Specific Plan Area.

The commercial uses within this sub-area are characterized by older rundown buildings. Bars, used car dealerships, personal service goods, and an adult entertainment use are some of the types of uses existing within this sub-area. The area that is designated Industrial Park is a new building housing a glass company. This sub-area has the highest concentration of multiple-family type housing, much of which is experiencing a low level of maintenance. The single-family residential on Oak Glen Avenue is in disrepair with scattered well kept housing backing up to the commercial area along Mills Avenue and the Flood Control Channel. Some of the parcels located in Sub Area 1 are triangular and odd shaped, not fronting any streets which inhibits the visibility and access generally desired by potential commercial tenants.

- (2) **Sub Area 2:** Sub-Area 2 is marked by an almost even split of vacant land and commercial land, and a relative lack of other types of uses. This significant amount of vacant land in this area has the potential for establishing new development within this portion of the Study Area.

Another relative aspect of this quadrant is the relationship of the residential neighborhoods to the commercial district. On the north side of Holt Boulevard, the Specific Plan boundaries are adjacent to residential housing, of both single and multiple-family type. On the south side of Holt Boulevard, the commercial uses are adjacent to the light industrial uses. Lots extend between Holt Boulevard to Brooks which have split Zoning and General Plan designations.

- (3) **Sub-Area 3:** Land uses within Sub-Area 3 are marked by a high portion of commercial land uses with a relative lack of other types of uses and is very similar to Sub-Area 2. This sub-area is characterized by commercial uses fronting Holt Boulevard with either vacant, residential, or industrial uses in the rear. The intended use for the vacant land behind the commercial use will be vital to the overall design concept for this area. Vacant land occurs as a transition area between the commercial and residential areas to the north. These residential areas are in well kept condition abutting the Specific Plan. In addition, this sub-area has one of the older existing mobile home parks.

- (4) **Sub-Area 4:** The major characteristic of this sub-area is the high concentration of developed commercial properties. This area has the least amount of vacant land available for development. The significant land uses in the area are the remaining new car dealerships and motels. In addition, the intersection of Holt Boulevard and Central Avenue has a vacant parcel about nine acres in size on the southwest corner. This parcel has a used car dealership on the corner which will have to be designed around, and providing access will be a problem. The prostitution and solicitation is more prevalent in this sub-area than any other within the Specific Plan.

c. *Definitions of Land Use Categories*

Specifically the Land Use Categories (districts) consist of the following components:

- (1) **Commercial Land Uses:** Commercial land uses within the Holt Boulevard Specific Plan are intended to promote the following objectives:

- Establish commercial areas which are conveniently located, efficient, and attractive in order to serve the durable goods retail needs of area residents and to meet the retail and service needs of area businesses.
- Provide a positive municipal cash flow through the generation of retail sales taxes.

Auto Mall

The Auto Mall designation is intended to be a prestigious center for automobile sales, services, and related uses. It is not the intent of this designation to create a shopping strip of auto agencies, but to provide a center in which buyers can view and purchase automobiles in a quality setting. In addition to new car sales agencies, the Auto Mall designation provides for the development of commercial businesses which are primarily associated with the automobile industry and which will assist in attracting additional customer traffic to the area. Potential businesses include:

- Banks, financial institutions
- Diagnostic centers
- Restaurants
- Upholstery/auto glass
- Auto parts, specialty accessory shops

- Insurance centers
- Car wash and auto detailing
- Automobile and light services truck repair
- Auto Dealerships
- Used Car Dealerships

Commercial

The commercial category is intended to meet a variety of retail, commercial service, and commercial recreation needs of area residents and businesses. Restaurants, business services, and business-oriented retailing is encouraged. Commercial service and retail uses oriented toward residents of Montclair will include the retailing of durable and other goods which are not generally purchased on a daily basis, as well as the "day to day" retailers like grocery stores. Examples of non-daily retailers include furniture and major appliances. Limited commercial recreation uses are also permitted.

(2) **Employment-Oriented Land Uses:** Employment-oriented land uses are intended to promote the following objectives:

- Establish a mix of employment opportunities which will, along with other similar areas, provide the City of Montclair with a sound and diversified economic base and ample opportunities for City residents.
- Provide a well-defined pattern of industrial and office-based activities which are compatible with residential, commercial, industrial, institutional, and open space uses located elsewhere in the City and surrounding unincorporated land.
- Establish a working environment which meets the personal needs of workers and business visitors.

Business Park

This category is primarily intended for mix of light manufacturing, research and development, sales agencies, and office-based firms seeking an attractive and pleasant working environment. Limited commercial support services and the retailing of durable goods may also be permitted.

Industrial

The primary purpose of the industrial category is to provide for a mix of light and medium manufacturing, research and development, and warehousing and distribution uses. In addition, administration and professional offices will be permitted where they support the primary industrial uses of the property. Within some portions of the industrial land uses category, limitations of the extent of warehouse and distribution uses may be established. Buildings within this designation are to be designed for a single tenant user.

Commercial/Office

The primary purpose of areas designated Office is to provide for the establishment of park-like office-based working environments for corporate, general, and professional and administrative businesses. In addition, commercial services that are required to support major business development and retail facilities which support office development can benefit from high visibility locations and be permitted within an overall "campus-like" atmosphere.

(3) Residential Land Uses

Planned Development

The purpose of this designation is to promote a more efficient use of the land, and to create a more desirable and affordable living environment by providing greater design flexibility than would be possible through the strict application of standard development regulations required by a land use district.

Residential

The purpose of this designation is to maintain the existing residential uses. This designation identifies the Mobile Home parks, single family neighborhoods, and the multiple family developments.

IV. DEVELOPMENT REGULATIONS

IV. DEVELOPMENT REGULATIONS

A. GENERAL PROVISIONS

This chapter includes the development regulations and guidelines for all new development within the Holt Boulevard Specific Plan¹. The Holt Boulevard Specific Plan is intended to carry out the goals and policies contained in the City of Montclair General Plan in an orderly and attractive fashion in order to establish a well designed, high quality working and shopping environment. All development within the Holt Boulevard Specific Plan shall therefore be consistent with the provisions of the Montclair General Plan. The provisions contained herein are also intended to accomplish the goals and objectives identified in Chapter II, and to facilitate implementation of the design guidelines contained in Chapter III.

1. RELATIONSHIP TO THE CITY OF MONTCLAIR MUNICIPAL CODE

Development standards and criteria contained in this document will supplement or replace those of the City of Montclair's Zoning Ordinance. Whenever the regulations contained herein conflict with the provisions of the Montclair Zoning Ordinance, the regulations contained herein shall take precedence. Any standards or land use proposals not specifically covered herein shall be subject to the regulations, standards, and specifications of the City of Montclair Municipal Code.

Unless otherwise specifically approved as part of this specific plan, all off-site improvements shall be subject to the City of Montclair's policies and standards in effect at the time improvement plans are submitted. If any provision of this document conflicts with the regulations of the Montclair Zoning Ordinance, the provisions of this document shall take precedence.

¹ Each of the regulations contained in this section are identified as to the land use categories to which they apply. Abbreviations used to denote land use categories are as follows:

AM	=	Auto Mall
BP	=	Business Park
C	=	Commercial
CO	=	Commercial/Office
I	=	Industrial
PD	=	Planned Development
RES	=	Residential

2. INTERPRETATION

The Director of Community Development or the Director's designee shall have the responsibility to interpret the provisions of this Specific Plan. All such interpretations shall be in written form, and shall be permanently maintained. Any person aggrieved by an interpretation may request that such interpretation be reviewed by the Planning Commission.

3. ENFORCEMENT

The Director of Community Development shall enforce the provisions of this Specific Plan and all applicable codes of the City of Montclair, including, but not limited to, building, mechanical, fire, and electrical codes, and codes related to drainage, wastewater, public utilities, subdivisions, and grading. In addition, the Director of Community Development shall have the responsibility to ensure that the applicable codes of all governmental agencies and jurisdictions are being met prior to the issuance of any permit or approval requested pursuant to this plan.

4. AMENDMENTS

The procedures to amend this Specific Plan shall be the same as a Zoning Ordinance Amendment per the City of Montclair Municipal Code. In addition, an amendment to any element of the City of Montclair General Plan may be required if a conflict is found to exist with any proposed specific plan amendment.

5. DEFINITIONS

Words, phrases, and terms not specifically defined herein shall have the same definition as provided for in the Montclair Zoning Ordinance.

**6. COMPLIANCE WITH
UNIFORM BUILDING
AND FIRE CODES**

All construction within the Holt Boulevard Specific Plan shall be in compliance with the Uniform Building Code, Uniform Fire Code, and all other ordinances adopted by the City pertaining to construction and safety features. All other City standards and policies shall apply at the time of submittal.

**7. PROPERTY
OWNERS'
ASSOCIATION,
CC&R'S**

Where required or established, CC&R's and/or property owners' association by-laws along with a typical deed, shall be submitted for review and approval by the Planning and Engineering Divisions and the City Attorney. The approved CC&R's shall be recorded concurrently with the final map or prior to building permits if a tract map is not filed. A copy of the recorded CC&R's shall be submitted to the Planning Division. A copy of the signed deed for the first parcel shall also be submitted to the Planning Division.

8. SEVERABILITY

If any regulation, condition, program, or portion thereof of this Specific Plan is for any reason held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and the invalidity of the such provision shall not affect the validity of the remaining portions.

B. PERMITTED USES

The following matrix establishes the uses which are permitted, conditionally permitted, or not permitted within each of the five land use categories within the Holt Boulevard Specific Plan. The regulation contained in this section apply not only to the primary use of property, within the specific plan area, but also to each accessory and support use.

- P = Permitted uses
- C = Conditionally permitted uses
- = **Not** permitted.

In addition to the above symbols, references to special notes have been included on the following pages. These notes appear at the end of the listing of land use types on Page IV-23.

Chapter V, Implementations, describes requirements for filing and processing of developments within the Holt Boulevard Specific Plan including requirements for site plans, and conditional use permits.

All other uses not listed as permitted, or conditionally permitted are prohibited unless a finding is made by the Planning Commission that the use is consistent with the purpose and intent of the Holt Boulevard Specific Plan, and is similar to, and no more objectionable than, uses which are permitted, conditionally permitted within the district in which the use proposed.

1. MANUFACTURING AND ASSEMBLY USE TYPES

a. *Automotive and Light Truck Repair*

Subject to the provisions of Section IV-D2, Page IV-55, activities include, but are not limited to, automotive and light truck (less than 6,000 lbs.) repair and the incidental retail sale of goods and services for automobiles and light trucks. Uses typically include, but are not limited to, brake, muffler, and tire shops; transmission and engine repair; automotive painting and body work; and the installation of major accessories.

AM	BP	C	CO	I	PD	RES
C	C	C	■	C	■	■

b. *Light Manufacturing and Assembly*

Activities typically include, but are not limited to, the mechanical or chemical transformation of raw or semi-finished materials or substances into new products, including manufacture of products, assembly of component parts (including required packaging of individual items for retail sale), and treatment and fabrication operations. Light manufacturing is conducted wholly within an enclosed building. Operations are typically undertaken for the wholesale market, but may include direct sales. A key feature is the creation of added value to the final product (i.e., product is created which has a greater retail value than the sum of the individual components used in the manufacturing process). Also included are labor intensive manufacturing, assembly or repair processes which do not involve frequent truck trips (more than eight 8 truck trips daily) or the transport of large scale products. Light manufacturing activities do not produce odors, noise, vibration, or particulates which would adversely affect uses within the same structure or on the same site and also exclude those uses described as "Special Industrial Operations".

AM	BP	C	CO	I	PD	RES
■	P[5]	■	■	P	■	■

c. *General Manufacturing and Assembly*

Activities typically include, but are not limited to, the mechanical or chemical transformation of raw or semi-finished materials or substances into new products, including manufacture of products; assembly of component parts (including required packaging for retail sale); blending of materials such as lubricating oils, plastics, and resins; and treatment and fabrication operations. Activities are conducted wholly within an enclosed building, and uses which require massive structures outside of buildings such as cranes or conveyer systems, or open air storage of large quantities of raw or semi-refined materials are not included within this land use type. Also not included are those uses described as "Special Industrial Operations". General manufacturing is typically undertaken for the wholesale market, but may include direct sales. A key feature is the creation of added value to the final product.

AM	BP	C	CO	I	PD	RES
■	P	■	■	P	■	■

d. *Outdoor Manufacturing and Assembly*

Activities typically include, but are not limited to, the mechanical or chemical transformation of raw or semi-finished materials or substances into new products, including manufacture of products, assembly of component parts (including required packaging for retail sale); blending of materials such as lubricating oils, plastics, and resins and treatment and fabrication operations conducted outside of an enclosed building. Not included are uses which require massive structures outside of buildings such as cranes or conveyer systems, or open air storage of large quantities of raw or semi-refined materials. Also not included are those uses described as "Special Industrial Operations". Outdoor manufacturing is typically undertaken for the wholesale market, but may include direct sales. A key feature is the creation of added value to the final product. The use and handling of hazardous materials must be in compliance with Section IV-C6, Page IV-52.

AM	BP	C	CO	I	PD	RES
■	■	■	■	C	■	■

e. *Research, Development, and Testing*

Activities typically include, but are not limited to, scientific research and theoretical studies and investigations in the natural, physical, or social sciences; engineering, fabrication, and testing of prototypes developed with the objective of creating marketable end products; and the performance of physical and environmental testing and related activities by or under the supervision of professional scientists and highly trained specialists. Research, development, and testing shall be conducted wholly within an enclosed building. The use and handling of hazardous materials must be in compliance with Section IV-C6, Page IV-52.

AM	BP	C	CO	I	PD	RES
■	P	■	C	P	C	■

**2. WHOLESALE,
STORAGE, AND
DISTRIBUTION USE
TYPES**

f. *Special Industrial Operations*

Activities include those activities which might otherwise be defined as Light, General, or Outdoor Manufacturing or Research, Development, and Testing, but which use, store, or handle hazardous, highly flammable, or explosive materials. These uses include all "H" category occupancies as described in the currently adopted Uniform Building Code, including facilities which conduct semiconductor fabrication, and comparable research and development.

AM	BP	C	CO	I	PD	RES
■	■	■	■	C	■	■

a. *Inoperable Vehicle Storage*

Activities include the parking and/or storage of inoperable vehicles. Typical uses include truck storage yards, tow storage yards, and junk yards.

AM	BP	C	CO	I	PD	RES
■	■	■	■	■	■	■

b. *Operable Vehicle Storage*

Activities include the parking and/or storage of operable vehicles. Typical uses include fleet storage lots and recreational vehicle storage.

AM	BP	C	CO	I	PD	RES
[1]	■	■	■	C	■	■

c. *Personal Storage*

Activities typically include storage services and facilities primarily for personal effects and household goods within enclosed storage area having individual access. Excluded are uses such as workshops, hobby shops, manufacturing, and commercial activities. Typical uses include mini-warehouses.

AM	BP	C	CO	I	PD	RES
■	C[5]	■	■	C	■	■

d. *Business Storage*

Activities include the on-site, indoor storage of business material such as, but not limited to, "dead files". Included are storage areas provided by individual businesses, as well as by building/complex owners as an amenity for on-site tenants.

AM	BP	C	CO	I	PD	RES
[1]	[1]	[1]	[1]	[1]	[1]	[1]

e. *Wholesale, Storage, and Distribution*

Activities typically include, but are not limited to, wholesaling storage, and warehousing services. Excluded are trucking services and freight terminals; wholesaling from the premises of unfinished, raw semi-refined products requiring further processing, fabrication, or manufacturing; and storage on the premises of raw and semi-refined products requiring further processing, fabrication, or manufacturing. A key feature of these uses are that no value is added to product handled within the site. In addition, on-site wholesaling operations do not typically generate sales taxes.

AM	BP	C	CO	I	PD	RES
■	C	■	■	P	■	■

f. *Retail and Wholesale in Conjunction with Manufacturing*

Activities typically include, but are not limited to, retailing and wholesaling of goods and products manufactured on-site. Typical uses include furniture manufacturing and sales, computer and electronics assembly and sales, and paper manufacturing and sales, storage, and warehousing services. A key feature of these uses is that they are designated as "point of sale" and therefore generate sales taxes.

AM	BP	C	CO	I	PD	RES
■	P	■	■	P	C	■

3. **COMMERCIAL
TYPES**

a. *Administrative and Professional Offices*

Activities typically include, but are not limited to, executive management, administrative, or clerical uses of private and public utility firms. Additional activities include the provision of advice design, information, or consultation of a professional nature. Uses typically include, but are not limited to, corporate headquarters, branch offices, data storage centers, architect's, lawyer's, insurance sales, financial planner's, travel agencies, and accountant's offices.

AM	BP	C	CO	I	PD	RES
[1]	P	[1]	P	[1]	P	■

b. *Adult Businesses*

Adult businesses are defined in Section 9-6.400 of the Montclair Municipal Code.

AM	BP	C	CO	I	PD	RES
■	■	■	■	P[6]	■	■

c. *Agricultural Supplies and Services*

Activities typically include, but are not limited to, the retail sale from the premises of feed and grain, fertilizers, pesticides, herbicides and similar goods. Uses typically include, but are not limited to, feed and grain stores, well drilling, and tree service firms. Also included are showroom and sales of farm equipment.

AM	BP	C	CO	I	PD	RES
■	C	■	■	C	C	■

d. *Alcohol Sales, On-site (On-sale)*

Activities typically include the sale, subject to required license of alcoholic beverages for consumption on the premises. Uses typically include restaurants and bars.

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	■

e. *Alcohol Sales, Off-site (Off-sale)*

Activities typically include the sale, subject to required license, of alcoholic beverages in sealed containers for consumption off the premises. Uses typically include grocery and liquor stores.

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	■

f. *Animal Care*

Activities typically include, but are not limited to, the provision of animal care, treatment, and boarding services of large and small animals. Uses typically include, but are not limited to, animal clinics, large and small animal hospitals, and kennels which support and are incidental to animal clinics and hospitals. Excluded are grooming and pet stores.

AM	BP	C	CO	I	PD	RES
■	C	C	■	C	C	■

g. *Car Washes*

Activities typically include the washing and polishing of automobiles. Uses typically include automobile laundries; car washes, excluding self service washes; and automotive detailing.

AM	BP	C	CO	I	PD	RES
C	C	C	[1]	[1]	[1]	■

h. *Automotive Rental Agencies*

Activities typically include, but are not limited to, the rental from the premises of motor vehicles, with provision for incidental maintenance services.

AM	BP	C	CO	I	PD	RES
C	C	C	C	C	C	■

i. *Automotive Sales*

Activities typically include the retail sale of predominantly new or vintage vehicles together with their incidental maintenance. Typical uses include automobile and recreational vehicle sales agencies.

AM	BP	C	CO	I	PD	RES
C	■	C	■	■	■	■

j. *Automotive Service Station*

Activities typically include, but are not limited to, the sale from the premises of goods and the provision of services normally required in the daily operation of motor vehicles, including the principal sale of petroleum products, the incidental sale of replacement items, and the performance of minor repairs.

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	■

k. *Building Maintenance Services*

Activities typically include, but are not limited to, maintenance and custodial services, window cleaning services, disinfecting and exterminating services, and janitorial services.

AM	BP	C	CO	I	PD	RES
■	P	■	■	P	■	■

l. *Building Supplies and Sales*

Activities typically include, but are not limited to, the retail sale or rental from the premises of goods and equipment, including paint, glass, hardware, fixtures, electrical supplies, and lumber and hardware stores. Outdoor storage may be permitted subject to screening requirements.

AM	BP	C	CO	I	PD	RES
■	P	C	■	■	■	■

m. *Business Supply Retail and Services*

Activities typically include, but are not limited to, retail sale, rental or repair from the premises of office equipment, office supplies and similar office goods primarily to firms and other organizations utilizing the goods rather than to individuals. The sale or rental of motor vehicles and the sale of materials used in construction of buildings or other structures is excluded from this use type.

AM	BP	C	CO	I	PD	RES
■	P	P	P	■	P	■

n. *Business Support Services*

Activities typically include, but are not limited to, firms rather than individuals of a clerical, employment, or minor processing nature, including multi-copy and blue-print services. The printing of books, other than pamphlets and reports, for another firm is excluded from this use type.

AM	BP	C	CO	I	PD	RES
■	P	P	P	■	P	■

o. *Communication Services*

Activities typically include, but are not limited to, broadcasting and other information relay services accomplished primarily through use of electronic and telephonic mechanisms. Uses typically include, but are not limited to, television and radio studios and telegraph offices.

AM	BP	C	CO	I	PD	RES
■	C	C	C	C	C	■

p. *Conference and Convention Facilities*

Activities typically include, but are not limited to, meeting rooms and halls for conferences and conventions along with ancillary catering services. While these uses are typically associated with a hotel, conference/convention facilities may occur as free-standing structures.

AM	BP	C	CO	I	PD	RES
■	C	■	C	■	C	■

q. *Construction Sales and Services*

Uses typically include establishments or places of business primarily engaged in construction activities and incidental storage or lots other than construction sites of construction equipment. Also included is the retail sale of materials used in the construction of buildings or other structures other than incidental sales of paint, fixtures and hardware. Typical uses include tool and equipment rental, sales yards, and building materials stores.

AM	BP	C	CO	I	PD	RES
■	■	■	■	P	■	■

r. *Convenience Sales and Services*

Uses typically include establishments or places of business primarily engaged in the provision of frequently or recurrently needed items or services typically associated with residential neighborhood. Included are neighborhood grocery stores and supermarkets, drug stores, mini-markets (other than those established as part of a otherwise permitted automotive service station), and video stores. Establishments with less than 8,000 gross floor area are required to have a conditional use permit.

AM	BP	C	CO	I	PD	RES
■	C[7]	C[7]	C[7]	■	C[7]	■

s. *Day Care Services*

Activities typically include the day time care of individuals under the age of eighteen (18) years of age. Uses typically include nurseries, schools, preschools, and day care centers. Excluded are uses classified under "Community Education".

AM	BP	C	CO	I	PD	RES
[2]	C	C	C	[2]	C	■

t. *Drive-Through Sales and Services*

Activities typically include establishments which, by design, physical facilities, service, or packaging procedures, permits or encourage customers to place orders, receive services, and/or obtain goods while remaining in their motor vehicle.

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	■

u. *Durable Goods Sales*

Activities typically include, but are not limited to, the retail sales from premises of durable goods which are purchased infrequently. Uses typically include, but are not limited to, furniture, piano and organ, major appliance (e.g. refrigerators), and carpet and flooring stores.

AM	BP	C	CO	I	PD	RES
P	C	P	P	[1]	P	■

v. *Eating and Drinking Establishments*

Activities typically include, but are not limited to, the retail sale from the premises of food or beverages prepared for on-premises and off-site consumption. Uses typically include, but are not limited to, restaurants, fast food type services, sandwich shops, and mini-markets where the primary use is food preparation. Excluded are drive-through sales and services. Employee cafeterias are permitted in all zones as an accessory use.

AM	BP	C	CO	I	PD	RES
P	P	P	P	■	P	■

w. *Entertainment/Recreation*

Activities typically include, but are not limited to, sports performed either indoors or outdoors, entertainment services, or recreation for participants or spectators. Specific entertainment/recreation use types include:

- (1) **Amusement Center:** Public places of amusement or business in which four or more coin-operated amusement devices are installed, such as photography machines, video games, muscle testers, fortune telling machines, and other games of skill or science, but not including games of chance or other similar devices. Included are any place open to the public, whether or not the primary use of the premises is devoted to operation of such devices.

AM	BP	C	CO	I	PD	RES
■	■	C	■	■	■	■

- (2) **Indoor Entertainment:** Predominantly spectator uses conducted within an enclosed building. Typical uses include motion picture theaters, meeting halls, dance halls, and private clubs and lodges.

AM	BP	C	CO	I	PD	RES
■	■	C	C	■	C	■

- (3) **Indoor Sports and Recreation:** Predominantly participant sports conducted within an enclosed building. Typical uses include bowling alleys, billiard parlors, ice and roller skating rinks, and indoor racquetball courts.

AM	BP	C	CO	I	PD	RES
[2]	C	C	C	[2]	C	■

- (4) **Outdoor Entertainment:** Predominantly spectator uses conducted in open, partially enclosed, or screened facilities. Typical uses include sports arenas, racing facilities, and amusement parks.

AM	BP	C	CO	I	PD	RES
■	■	C	■	■	■	■

- (5) **Outdoor Sports and Recreation:** Predominantly participant sports conducted in open, partially enclosed, or screened facilities. Typical uses include driving ranges, miniature golf courses, swimming pools, and tennis courts.

AM	BP	C	CO	I	PD	RES
[2]	[2]	C	[2]	[2]	[2]	■

x. *Financial Institutions*

Uses typically include, but are not limited to, banks, savings and loans, and credit unions.

AM	BP	C	CO	I	PD	RES
[3]	[3]	P	P	[3]	P	■

y. *Fortune Telling Services*

Activities typically include those in which a person professes to foretell events in other people's lives in return for money or other remuneration.

AM	BP	C	CO	I	PD	RES
■	■	■	■	■	■	■

z. *Health Clubs and Spas*

Activities typically include, but are not limited to, sport and health-related activities performed either indoors or outdoors. Uses typically include, but are not limited to, health clubs, spas, gyms, and tennis clubs.

AM	BP	C	CO	I	PD	RES
[2]	C	C	[2]	[2]	[2]	■

aa. *Hotels and Motels*

Activities typically include, but are not limited to, lodging services to, transient guests on a less-than-monthly basis, other than in the case of uses such as private boarding houses.

AM	BP	C	CO	I	PD	RES
■	■	C	C	■	C	■

ab. *Industrial Medical and Health Care Services*

Activities typically include, but are not limited to, the provision of work-related therapeutic, preventive, or correctional personal treatment (including out-patient surgical services) by physicians, dentists, and other medical practitioners, as well as the provision of work-related emergency medical treatment.

AM	BP	C	CO	I	PD	RES
■	■	C	C	■	C	■

ac. *Laundry Services*

Activities typically include, but are not limited to, institutional and commercial linen supply and laundry services, dry cleaning plants, rug cleaning, and diaper service laundries.

AM	BP	C	CO	I	PD	RES
■	C	■	■	C	■	■

ad. *Medical and Health Care Services*

Activities typically include, but are not limited to, therapeutic, preventive, or correctional personal treatment by physicians, dentists, and other medical practitioners, as well as the provision of medical testing and analysis services. Health care uses typically include those performed by:

- Medical Clinics
- Family Planning Clinics
- In-Patient Health Care Facilities
- Hospitals

Also included are pharmacies related to the above.

AM	BP	C	CO	I	PD	RES
■	P	P	P	■	C	■

ae. *Personal Services and Sales*

Activities typically include, but are not limited to, retail sales of small personal convenience items and professional services which are used frequently by the business community. Uses typically include, beauty and barber shops, florist shops, photography studios, and apparel laundering and dry cleaning agencies.

AM	BP	C	CO	I	PD	RES
■	P	P	P	■	P	■

af. *Repair Services*

Typically an outdoor activity which includes, but are not limited to, repair of such articles as upholstery, furniture, electronic equipment, and large electrical appliances.

AM	BP	C	CO	I	PD	RES
■	P	P	■	P	C	■

4. CIVIC USE TYPES

a. Civic Administration

Activities typically include, but are not limited to, management administration, or clerical services performed by public, quasi-public and utility agencies.

AM	BP	C	CO	I	PD	RES
■	C	C	P	■	P	■

b. Community Education

Typical activities include educational services provided by public and private or parochial institutions. Typical uses include elementary, junior high, and high schools; junior colleges, and vocational and trade schools.

Public Schools

AM	BP	C	CO	I	PD	RES
P	P	P	P	P	P	P

Private Schools:

- Community education for minors

AM	BP	C	CO	I	PD	RES
■	■	■	■	■	■	■

- Community education for adults

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	C

c. Cultural Facilities

Activities typically include, but are not limited to, those performed by public and private museums and art galleries, public and private libraries and observatories. Also included are facilities for the performing arts.

AM	BP	C	CO	I	PD	RES
■	C	P	C	■	P	■

d. *Parks*

Uses typically include lands reserved for active and passive recreational use and leisure time sports, picnicking, and other similar activities.

AM	BP	C	CO	I	PD	RES
P	P	P	P	P	P	P

e. *Religious Assembly*

Activities typically include religious services and assembly such as customarily occurs in churches, synagogues, and temples.

AM	BP	C	CO	I	PD	RES
■	C	C	C	■	C	C

f. *Utility Facilities*

Activities typically include, but are not limited to, those performed by public agencies or publicly regulated utility firms. Uses typically include, but are not limited to, electric, gas, and oil transmission facilities; mail processing facilities; bus and multi-modal transit stations; and water reservoirs, water tanks, wells, and pumping stations.

AM	BP	C	CO	I	PD	RES
■	C	C	C	C	C	P

5. **RESIDENTIAL USE TYPES**

a. *Family Residential*

Activities typically include the occupancy of living accommodations on a wholly or primarily non-transient basis, but exclude institutional living arrangements and caretaker facilities.

AM	BP	C	CO	I	PD	RES
[4]	[4]	[4]	[4]	[4]	P	[8]

- (1) **Detached Residential:** A building designed and/or used to house not more than one family, including all domestic employees of such family, and which dwelling is not attached to any other dwelling.
- (2) **Attached Residential:** A building designed and/or used to house more than one family within individual dwelling unit including duplexes, triplexes, etc., as well as townhouse, condominiums, and apartments.

AM	BP	C	CO	I	PD	RES
[4]	[4]	[4]	[4]	[4]	C	[8]

b. *Group Residential*

Activities include the occupancy of living quarters by a group or groups of persons on a weekly or longer basis. Included are voluntary living arrangements such as boarding houses and fraternities, as well as involuntary living arrangements such as prisons and asylums.

AM	BP	C	CO	I	PD	RES
■	■	■	C	■	C	[9]

c. *Caretaker's Residence*

Activities include the occupancy of living accommodations (not to exceed 1,000 square feet) associated with an industrial use type for the purpose of providing 24-hour surveillance of the industrial property.

AM	BP	C	CO	I	PD	RES
■	C	■	■	C	C	[9]

Permitted Uses Notes:

1. May be established as an accessory use to uses which are otherwise permitted in this designation subject to the approval requirements of the primary use.
2. May be established as a noncommercial employee amenity incidental to an otherwise permitted use.
3. Financial institutions which support and are incidental to ongoing operations which are otherwise allowable may be permitted (e.g. company credit union).
4. Existing family residences located within the Specific Plan area as of the adoption of the Specific Plan by the Montclair City Council are permitted to continue.
5. Except in portions of design intended for commercial purposes.
6. As per the location requirements of section 9-6.400 of the Montclair Municipal Code.
7. Establishments with greater than 8,000 gross floor area are permitted.
8. As allowed per the underlying zoning designation.
9. As per the uses allowed per the underlying zoning designation.
10. Large users require Condition Use Permit (generating substantial number of employees and traffic).

**Table IV-1
Summary of Permitted Uses**

USE TYPES	AM	BP	C	CO	I	PD	RES
MANUFACTURING AND ASSEMBLY [10]							
Automotive and Light Truck Repair	C	C	C	▪	C	▪	▪
Light Manufacturing and Assembly	▪	P[5]	▪	▪	P	▪	▪
General Manufacturing and Assembly	▪	P	▪	▪	P	▪	▪
Outdoor Manufacturing and Assembly	▪	▪	▪	▪	C	▪	▪
Research, Development, and Testing	▪	P	▪	C	P	C	▪
Special Industrial Operations	▪	▪	▪	▪	C	▪	▪
WHOLESALE, STORAGE, AND DISTRIBUTION USE TYPES [10]							
Inoperable Vehicle Storage	▪	▪	▪	▪	▪	▪	▪
Operable Vehicle Storage	[1]	▪	▪	▪	C	▪	▪
Personal Storage	▪	C[5]	▪	▪	C	▪	▪
Business Storage	[1]	[1]	[1]	[1]	[1]	[1]	[1]
Wholesale, Storage, and Distribution	▪	C	▪	▪	P	▪	▪
Retail and Wholesale in Conjunction with Manufacturing	▪	P	▪	▪	P	C	▪
COMMERCIAL TYPES [10]							
Administrative and Professional Offices	[1]	P	[1]	P	[1]	P	▪
Adult Businesses	▪	▪	▪	▪	P[6]	▪	▪
Agricultural Supplies and Services	▪	C	▪	▪	C	C	▪
Alcohol Sales, On-site (On-sale)	▪	C	C	C	▪	C	▪
Alcohol Sales, Off-Site (Off-sale)	▪	C	C	C	▪	C	▪
Animal Care	▪	C	C	▪	C	C	▪
Car Washes	C	C	C	[1]	[1]	[1]	▪
Automotive Rental Agencies	C	C	C	C	C	C	▪
Automotive Sales	C	▪	C	▪	▪	▪	▪
Automotive Service Station	▪	C	C	C	▪	C	▪
Building Maintenance Services	▪	P	▪	▪	P	▪	▪

P = Permitted Uses
 C = Conditionally Permitted Uses
 ▪ = Not Permitted

USE TYPES	AM	BP	C	CO	I	PD	RES
Building Supplies and Sales	•	P	C	•	•	•	•
Business Supply Retail and Services	•	P	P	P	•	P	•
Business Support Services	•	P	P	P	•	P	•
Communication Services	•	C	C	C	C	C	•
Conference and Convention Facilities	•	C	•	C	•	C	•
Construction Sales and Services	•	•	•	•	P	•	•
Convenience Sales and Services	•	C[7]	C[7]	C[7]	•	C[7]	•
Day Care Services	[2]	C	C	C	[2]	C	•
Drive-Through Sales and Service	•	C	C	C	•	C	•
Durable Goods Sales	P	C	P	P	[1]	P	•
Eating Establishments	P	P	P	P	•	P	•
Entertainment/Recreation							
Amusement Center	•	•	C	•	•	•	•
Indoor Entertainment	•	•	C	C	•	C	•
Indoor Sports and Recreation	[2]	C	C	C	[2]	C	•
Outdoor Entertainment	•	•	C	•	•	•	•
Outdoor Sports and Recreation	[2]	[2]	C	[2]	[2]	[2]	•
Financial Institutions	[3]	[3]	P	P	[3]	P	•
Fortune Telling Services	•	•	•	•	•	•	•
Health Clubs and Spas	[2]	C	C	[2]	[2]	[2]	•
Hotels and Motels	•	•	C	C	•	C	•
Industrial Medical and Health Care	•	•	C	C	•	C	•
Laundry Services	•	C	•	•	C	•	•
Medical and Health Care Services	•	P	P	P	•	C	•
Personal Services and Sales	•	P	P	P	•	P	•
Repair Services	•	P	P	•	P	C	•
CIVIC USE TYPES							
Civic Administration	•	C	C	P	•	P	•

- P = Permitted Uses
 C = Conditionally Permitted Uses
 • = Not Permitted

USE TYPES	AM	BP	C	CO	I	PD	RES
Community Education							
Public Schools	P	P	P	P	P	P	P
Private Schools							
Community education for minors	▪	▪	▪	▪	▪	▪	▪
Community education for adults	▪	C	C	C	▪	C	C
Cultural Facilities	▪	C	P	C	▪	P	▪
Parks	P	P	P	P	P	P	P
Religious Assembly	▪	C	C	C	▪	C	C
Utility Facilities	▪	C	C	C	C	C	P
RESIDENTIAL USE TYPES							
Family Residential	[4]	[4]	[4]	[4]	[4]	P	[8]
Attached Residential	[4]	[4]	[4]	[4]	[4]	C	[8]
Group Residential	▪	▪	▪	C	▪	C	[9]
Caretaker's Residence	▪	C	▪	▪	C	C	[9]

- P = Permitted Uses
C = Conditionally Permitted Uses
▪ = Not Permitted

Permitted Uses Notes:

1. May be established as an accessory use to uses which are otherwise permitted in this designation subject to the approval requirements of the primary use.
2. May be established as a noncommercial employee amenity incidental to an otherwise permitted use.
3. Financial institutions which support and are incidental to ongoing operations which are otherwise allowable may be permitted (e.g. company credit union).
4. Existing family residences located within the Specific Plan area as the adoption of the Specific Plan by the Montclair City Council are permitted to continue.
5. Except in portions of design intended for commercial purposes.
6. As per the location requirements of section 9-6.400 of the Montclair Municipal Code.
7. Establishments with greater than 8,000 gross floor area are permitted.
8. As allowed per the underlying zoning designation.
9. As per the uses allowed per the underlying zoning designation.
10. Large users require Conditional Use Permit (generating substantial number of employees and traffic).

C. DEVELOPMENT STANDARDS

The following regulations and criteria establish minimum development standards for land uses proposed within the Holt Boulevard Specific Plan. These regulations shall govern the development of all property within the Holt Boulevard Specific Plan, and shall supersede the provisions of the City of Montclair Municipal Code. Special notes indicated in the development standards may be found on page IV-25.

Certain uses and activities, due to their nature, have the opportunity to create more significant impacts upon the community than others. As a result, specific regulation of these activities and uses is warranted. Thus, in addition to the provisions of this section, the following uses will be subject to the additional regulations outlined in Section IV.D, Page IV-54:

- Automobile and Light Truck Repair Facilities (see Section IV.D.1, Page IV-54)
- Automobile Dealerships (see Section IV.D.2, Page IV-55)
- Automobile Rental Agencies (see Section IV.D.3, Page IV-55)
- Automobile Service Stations (see Section IV.D.4, Page IV-55)

	AM	BP	C	CO	I	PD
1. Minimum Parcel Size (in acres)	2.5	1	1	.5	1	3
2. Minimum Parcel Width (in feet)	200	200	100	100	170	[1]
3. Minimum Parcel Depth (in feet)	250	250	250	350	170	[1]
4. Building Height Limit (in feet)	50[2]	28[2]	75[2]	75[2]	40[2]	28[2]
5. Maximum Lot Coverage (%)	45	45	45	45	45	45[1]
6. Minimum Parking Setbacks and Depth of Landscaping (in feet)	15	30	20	20	30	20
7. Minimum Front Building Setback [3] (in feet)	35[4]	35[4]	35[4]	35[4]	35[4]	25[4]
A. Interior Side Property Lines [1]	10	10	10	10	10	
(1) One Side	0	0	0	0	0	
(2) Other Side						
B. Rear Property Line [1]	0	0	0	0	0	

Development Standards Notes:

- [1] Subject to the provisions of Section IV-C, Note[7], Page IV-52, Planned Developments, minimum lot sizes, widths, and depths; parking and landscape setbacks for interior, rear and side parcel lines; building setbacks for interior side and rear property lines; and building coverage requirements may be modified.
- [2] Building height shall be measured from finished grade of the curb along the street or drive which the building is located.
- [3] Setbacks shall be measured from property line or from the face of curb when adjacent to a private street.
- [4] In order to provide variety to streetscapes within the Holt Boulevard Specific Plan, encourage innovation in site design, and to enhance the articulation of building facades, encroachment of certain building projections into otherwise required street setbacks may be desirable. Portions of buildings may be permitted to encroach into the minimum building setback along public streets if the City, in approving the proposed site plan, makes the following findings:

- The proposed encroachment shall maintain a minimum building setback no less than forty percent (40%) of the otherwise required building setback.
- The proposed encroachment into the minimum required setback occupies no more than twenty-five percent (25%) of the normally required setback area.
- Portions of the building other than the proposed encroachment are set back a greater distance than the required minimum setback so that the landscaped area located between the normally required setback line and the building is equal to or greater than two times the surface area of the proposed encroachment into the minimum setback (see Figure IV-1).

On a dimensioned site plan of the proposed structure, add lines drawn at right angles to the curb as far apart as necessary so as to encompass the entire building width (lines 1 and 2), as well as the normally required setback line for the district within which the site is located (Line 3).

Calculate the surface area of the proposed encroachment into the minimum setback (the proposed building area located between the required building setback line and the curb identified as Area A).

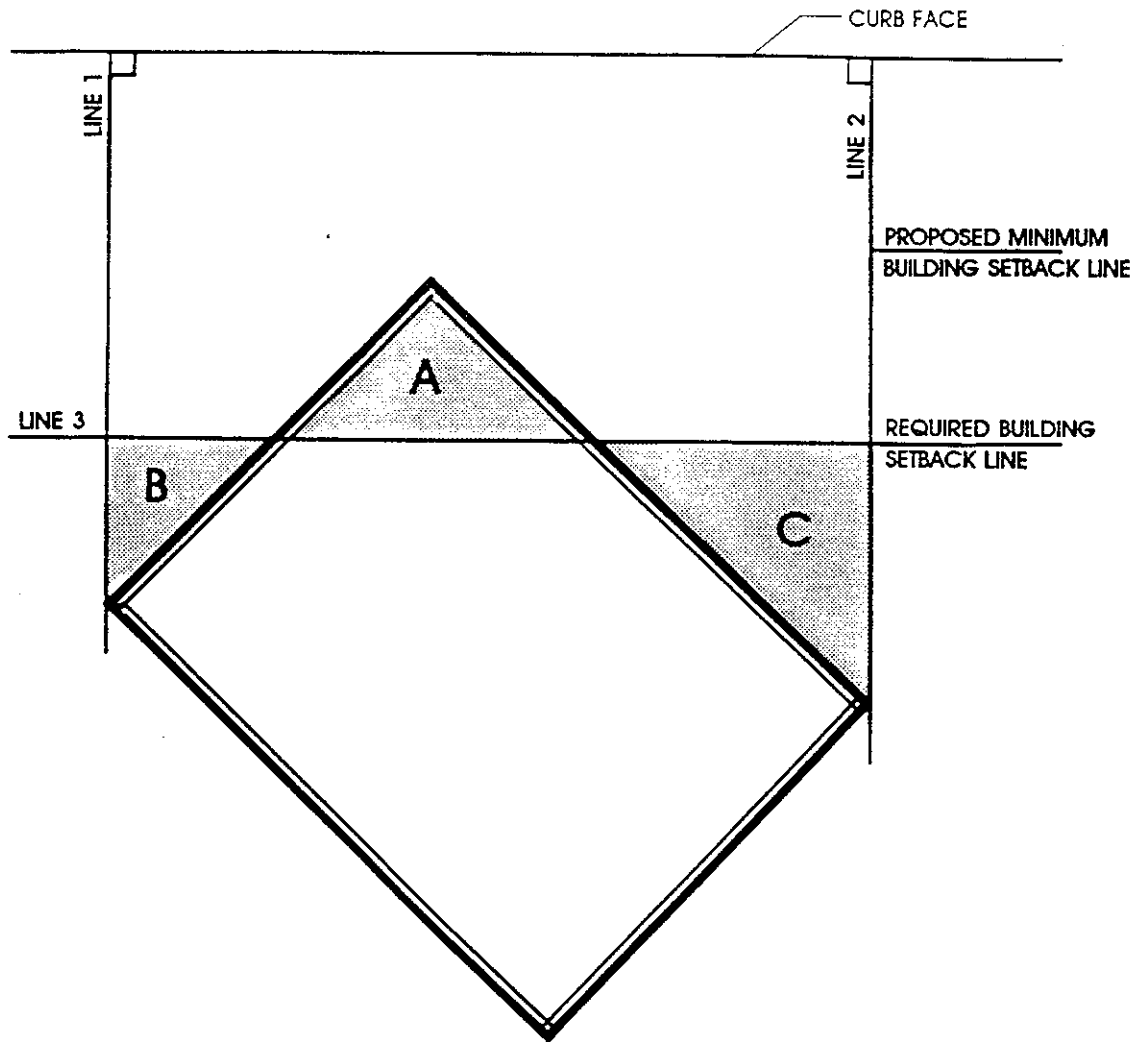
Calculate the proposed building setback area provided in excess of the required minimum (the area formed by lines 1 and 2 and the building identified as Areas B, C, D, etc.).

The total square footage of the proposed building setback area provided in excess of the required minimum (Areas B, C, etc.) shall be greater than or equal to twice the square footage of Area A (proposed encroachment in the minimum setback) as identified in the following formula.

$$(\text{Area B} + \text{Area C} + \text{Area D} \dots) \geq (2 \times \text{Area A})$$

- The amount of landscaping provided on the site exceeds the requirement for the district within which the proposed encroachment is located by an area at least equal to the surface area of the proposed encroachment.
- The proposed encroachment into the minimum required setback is an item of architectural interest (e.g. office element of an industrial building, arcade, etc.); alternatively, the proposed encroachment into the minimum required setback is a corner of a building which has been sited at an angle to the street.
- The mass and scale of the proposed encroachment into the minimum required setback is consistent with the mass and scale of the building, of the overall site design, and of the overall design intent of the Holt Boulevard Specific Plan.

Figure IV-1 Alternative Setback



1. LANDSCAPING

- a. Minimum on-site landscape area (includes hardscape, pedestrian facilities other than within parking areas and along public streets, plazas, courtyards, etc.) measured on a net basis. (A substantial majority (75 percent +) of this landscape area shall be in vegetation rather than in hardscape.)

	AM	BP	C	CO	I	PD
Corner Lot	12%	15%	15%	20%	15%	
Interior Lot	12%	15%	15%	20%	10%	

- b. The minimum area adjacent to streets to be maintained in landscaping shall be as provided for in Table IV.C.1, Page IV-28.
- c. Prior to issuance of building permits or prior to final approval of street improvement plans, as applicable, three (3) sets of detailed landscape and irrigation construction drawings shall be submitted for review and approval by the Planning Division. All landscaping irrigation systems shall be installed prior to the granting of final inspection by the City.
- d. Permanent automatic irrigation systems shall be provided in all landscaped areas. To the extent possible, water conserving irrigation techniques such as drip irrigation shall be provided.
- e. All landscaped areas shall be maintained in a healthy and thriving condition, free from weeds, trash, and debris.
- f. Building setbacks which are not used for drive entries, parking, or approved outdoor uses shall be fully landscaped. All unpaved non-work areas shall be landscaped. All future development phase areas shall be hydroseeded.
- g. To soften building elevations, a variable landscape setback shall be used. This variable setback shall include planter boxes and atrium treatments adjacent to buildings, and landscaping along drive isles and walkways. In order to minimize access restrictions, solid landscape buffers along building frontages shall not be permitted.
- h. All parking visible from any public or private street shall be shielded by a combination of berms (maximum 3:1 slope) planted with trees, shrubs and turf; garden walls; and shrubs per the Holt Boulevard Landscape Guidelines.
- i. Curbs, bumpers, and similar permanent devices shall be provided within parking areas so that parked vehicles do not bump building, screen, landscape, or perimeter walls and also do not overhang sidewalks, planters, or other landscaped areas.

- j. Planters shall function as landscape and have a minimum width of five feet (5') and minimum length not more than six inches (6") less than the longest abutting parking stall (i.e. 15'-6" for compact stalls and 18'-6" for standard and long-term stalls), exclusive of curbing, and shall have sufficient room to accommodate tree growth.
- k. Parking facilities containing more than 3,600 square feet of parking area shall include landscape islands having a minimum width of five feet (5'), exclusive of curbing and car overhang, at the ends of all parking aisles.
- l. The total number of trees used within parking areas shall not be less than one (1) canopy tree per five (5) parking spaces, and meet the requirements of the Landscape Guidelines.
- m. Trees used within required landscape areas, including parking areas shall have a minimum size of 15 gallons. Trees provided within project sites shall be of the following minimum sizes:
 - 24" box (min. 1.75" caliper) 50 percent
 - 15 gal. (min. 0.75" caliper) 40 percent
 - 5 gal. (min. 0.50" caliper) 10 percent
- n. All 15-gallon trees shall be double staked per City standards. All box trees shall have guy wires.
- o. Any tree within 25 feet of a public sidewalk requires root retainers per City standards.

2. PARKING REQUIREMENTS

All parking area plans shall be reviewed and approved by the Planning Division. The following standards shall apply to all parking facilities within the Holt Boulevard Specific Plan.

a. On-Street Parking

On-street parking is generally prohibited along Holt Boulevard. However, on-street parking shall be allowed in the Auto Mall District, subject to approval of the planning division.

b. *Required Number of Parking Spaces*

Minimum off-street parking requirements for uses within the Holt Boulevard Specific Plan shall be as provided in Table IV-2, Page IV-41 of this chapter. Requirements for uses not listed in Table IV-2, Page IV-41 shall be as required by the by the Montclair Zoning Ordinance or by the Montclair Planning Commission if no standard is provided in the Zoning Ordinance.

In the absence of floor plans, ultimate user, and/or a proposed mix of uses, the required number of parking spaces shall be calculated as follows:

LAND USE DESIGNATION	PARKING REQUIREMENT
Business Park	1 space per 470 square feet of gross floor area (GFA); 10 percent maximum compact spaces; 50 percent maximum long-term spaces; 10 percent of the spaces provided shall be designated for car and van pool use. (Based on 60 percent of the GFA as Manufacturing, 40 percent as Office or Retail, General)
Commercial	1 space per 250 square feet of gross floor area (GFA); 10 percent maximum compact spaces; long-term spaces not permitted (Based on 100 percent of the GFA as Retail, General)
Commercial/Office	1 space per 250 square feet of gross floor area (GFA); 10 percent maximum compact spaces; 35 percent maximum long-term spaces; 7 percent of the spaces provided shall be designated for car and van pool use. (Based on 70 percent of the GFA as office and 30 percent as Retail, General)
Industrial	1 space per 500 square feet of gross floor area (GFA); 10 percent maximum compact spaces; 50 percent maximum long-term spaces; 10 percent of the spaces provided shall be designated for car and van pool use. (Based on 75 percent of the GFA as Manufacturing and 30 percent as office)
Planned Development	In situations where a combination of uses are developed on a site, including multiple uses under single ownership, the number of spaces required shall be equal to the sum of the requirements for each of the uses, unless a reduction is granted pursuant to a shared parking agreement.

Where information exists regarding proposed uses, especially where a greater percentage of more parking intensive uses are proposed, the requirements of the specific uses presented in Table 2, Page IV-41, shall be used to determine parking requirements.

c. *Maximum Percentage of Compact Spaces and Long-Term Spaces*

The maximum percentage of compact spaces and long-term spaces referred to in Table IV-2, Page IV-41, shall be calculated based on the total number of parking spaces required.

d. *Location of Compact Spaces and Long-Term Spaces*

Compact and Long-Term Spaces shall be located in areas away from primary entry of the facility they serve.

e. *Required Number of Handicapped Parking Spaces*

Handicapped parking spaces shall be provided at the rate and configuration required by the State of California.

f. *Required Number of Motorcycle Parking Areas*

Motorcycle parking areas shall be provided as follows:

NUMBER OF AUTOMOBILE SPACES	NUMBER OF MOTORCYCLE PARKING AREAS
1 - 25 spaces	None required
26 - 100 spaces	1 area
Over 100 spaces	1 area for each additional 100 automobile spaces or majority portion thereof.

Required motorcycle parking areas shall consist of a minimum usable area of 56 square feet, and shall count toward fulfilling automobile parking spaces at the rate of one parking space per one motorcycle parking area.

g. *Required Amount and Type of Bicycle Parking*

- (1) Bicycle parking for not less than five percent (5 percent) of the number of required automobile parking spaces shall be provided in close proximity to the primary employee entrance(s) of the use(s) they serve.
- (2) The location of bicycle parking should be safe, well-lit, and of adequate space to accommodate bicycle users.

h. *Use of Required Parking Areas*

Areas required to meet applicable parking requirements may not be used for any purpose other than parking.

i. *Parking Space Dimensions*

Automobile parking spaces shall meet the following design standards:

- (1) **Full-sized Space:** Minimum width and minimum depth (9' x 20') as defined in Section 9-4.1807 of the Montclair Municipal Code (see Figure IV-2, Page IV-44).
- (2) **Long-term Space:** Minimum width of 8-1/2 feet and minimum depth of 18 feet (see Figure IV-2, Page IV-44).
- (3) **Compact Space:** Minimum width of 8 feet and minimum depth of 16 feet (see Figure IV-2, Page IV-44).
- (4) **Parallel Space:** Minimum dimension of 10 feet by 22 feet.
- (5) **Handicapped Parking Space:** All handicapped parking spaces shall be designed, constructed, and signed pursuant to State requirements (see Figure IV-3, Page IV-46).

j. *Dimensions of Parking Bays and Drive Aisles*

The minimum dimensions of parking bays and maneuvering aisle shall be as shown in Tables IV-2, 3, 4, and 5, Page IV-45.

k. *Improvement Standards*

Unenclosed parking areas within the Holt Boulevard Specific Plan shall be improved as follows:

- (1) **Directional Arrows and Signs:** Aisles, approach lanes, and maneuvering areas which are one-way shall be clearly marked with directional arrows to facilitate vehicular movement as required by the City Engineer.

In addition to directional arrows, the Planning and Engineering Divisions may require installation of signs to ensure safe and efficient vehicular movement.

- (2) **Drainage:** All parking facilities shall be graded and provided with drainage facilities so as to provide for the disposal of water without surface flow over sidewalks and over driveways within public rights-of-way and comply with federal standards regulating surface flows from private property.
- (3) **Lighting:** All parking facilities shall be provided with night time security lighting designed so as to confine emitted light to the parking area.

The maximum average illumination at ground level within commercial areas shall be 3.0 foot-candles. The maximum average illumination at ground level within manufacturing and business park areas shall be 2.0 foot-candles.

- (4) **Safety:** Unloaded throated drive aisles shall be provided where necessary and as required by the City Engineer in developments which exceed 100 vehicles to prevent safety hazards and vehicular congestion on public streets which might be created by vehicles queuing or utilizing public streets to drive between parking aisles.
- (a) Parking area design shall ensure adequate visibility of and between pedestrians, bicyclists, and motorists when entering or exiting parking facilities and individual spaces, and when circulating within a parking facility. To the extent possible, the parking facility shall be designed so that primary pedestrian access to and from building entrances is along, rather than across, parking aisles and to discourage pedestrians from crossing landscaped areas to reach building entrances.
- (b) Bicycle and automobile parking areas shall be separated by a physical barrier or sufficient identification and distance to protect parked bicycles from damage by cars.
- (5) **Striping and Identification:** All automobile parking spaces shall be clearly outlined with double lines on the surface of the parking facility (see Figure IV-3, Page IV-46).

All handicapped parking spaces shall be striped and marked according to applicable state standards.

All parking spaces for compact cars shall be clearly identified as "Compact Car Only".

All spaces reserved for car and van pools shall be clearly marked with the words "Car Pool Only" on either the tire restraints at the back of each space, on the pavement at the opening of the space, or both.

All motorcycle spaces shall have bollards installed and appropriately spaced so as to prevent automobile usage. Motorcycle spaces shall be marked so as to be clearly identified for motorcycle use.

- (6) **Surfacing:** All permanent automobile and handicapped parking spaces and loading and maneuvering areas shall be paved and permanently maintained with asphalt, concrete, or other all weather surface approved by the Planning and Engineering Divisions.

In order to prevent damage from motorcycle kick stands required motorcycle parking areas shall be paved with concrete or equivalent surfacing approved by the Planning and Engineering Divisions. All bicycle parking and storage areas shall be paved.

- (7) **Location of Parking:** All handicapped parking spaces shall be located pursuant to state requirements as close as possible to the entrance(s) to the use which they are intended to serve, and oriented so that the user of the handicapped parking space need not go past the rear of automobile parking spaces in order to reach the building's main entrance.

All car and van pool spaces which are provided shall be located in the most advantageous and reasonable location as close as possible to the primary employee entrance(s) to the use(s) which they are intended to serve.

Permitted compact parking spaces shall be reasonably dispersed throughout the parking facility. The location of compact parking facilities shall be approved by the Planning Division.

- (8) **Parking Access from Streets:** Parking spaces shall be located and designed such that motorists entering and leaving a parking space need not back onto a public or private street or onto a private drive other than a specifically designated parking aisle.

- (9) **Passenger Loading Areas:** Passenger loading areas shall be provided for any building or building complex which will generate 100 or more employees. Such loading areas shall be located convenient to the primary employee entrance(s) to the use(s) which they are intended to serve, generally adjacent to the frontage street, and shall be so designated either by signs or painted pavement. Passenger loading areas shall be designed such that vehicles, and car and van pools can safely stop and discharge passengers.

i. *Requirements for Subterranean, Semi-subterranean, and Above-Ground Parking Structures*

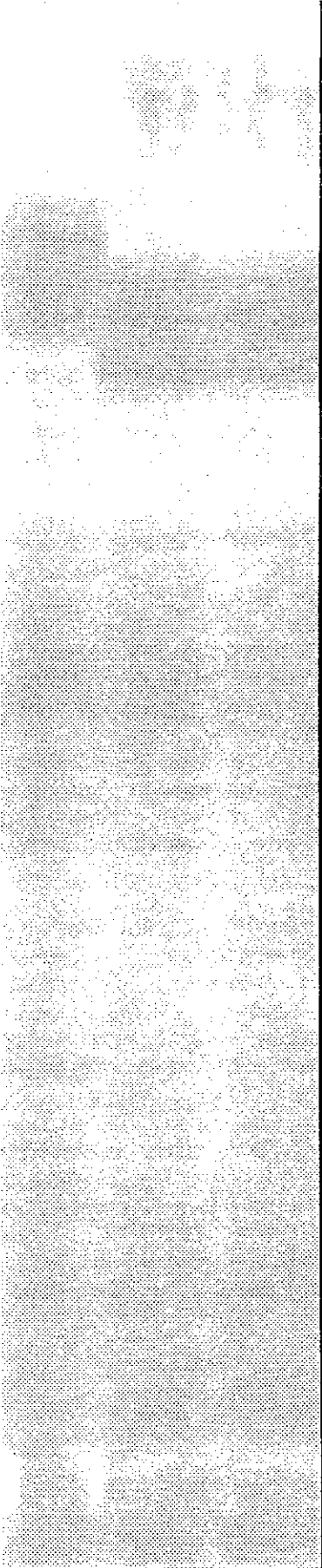
- (1) **Requirements for All Parking Structures:** Development of parking structures shall be subject to issuance of Conditional Use Permit.

- (a) No interior parking lot landscaping shall be required for structured parking areas; however, all required parking area landscape setbacks shall be provided. Landscaping will be required on exterior walls and exposed top deck.

- (b) Stair wells within parking structures shall be open so as to facilitate casual observation.
- (c) Ramps within parking structures shall not have a slope greater than twenty percent (20%); provided, however, that the first and last ten feet (10') shall have a transitional slope no greater than ten percent (10%).
- (d) The minimum ramp width shall be eighteen feet (18').

Entry level clearance for multi-story parking structures shall be a minimum of seven feet (7') to accommodate van pool vehicles. If preferential parking for van pools is to be provided on other levels, access to those levels must also be provided for van pool vehicles.

- (2) **Subterranean Parking Facilities:** All openings for ingress and egress facing a public street shall be situated a minimum of thirty feet (30') from the right-of-way line.
 - (a) A subterranean parking structure may be constructed and maintained within any setback area provided that all required landscape setback areas are maintained.
 - (b) Exits from subterranean parking structures shall provide adequate sight distance in compliance with applicable standards established by the Engineering Division.
- (3) **Semi-Subterranean Parking Facilities:** A parking structure shall be considered to be semi-subterranean if the structure is partially underground and if the finished floor of the upper level of the structure does not exceed three feet (3') above the average natural or existing grade of the parcel, except for openings for ingress and egress.
 - (a) All openings for ingress and egress facing a public street shall be situated a minimum of thirty feet (30') from the right-of-way line or from the curb face if it is located along a private street or drive.
 - (b) Semi-subterranean parking structures shall meet applicable parking setback requirements.

- 
- (4) **Above-Ground Parking Structures:** Above-ground parking structures shall be no greater in height than fifty feet (50') and shall meet applicable building setback requirements.
- (a) The exterior elevations of above-ground parking structures shall be designed so as to avoid the use of blank concrete facades.
 - (b) The perimeter of the parking structure shall be landscaped at ground level with a minimum of one (1) tree for every twenty feet (20') of linear distance of facade in addition to required streetscape landscaping. Groundcover and shrubs shall also be provided to break up the mass of the structure.

**Table IV-2
Off-Street Parking Space Requirements**

USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES	MAXIMUM PERCENT LONG-TERM SPACES	NOTES
Manufacturing and Assembly Uses; Wholesale, Warehouse, and Distribution Uses				
Manufacturing	1 space/500 square feet of gross floor area (GFA) up to 30,000. 1 additional space for every 650 square feet from 30,000 to 50,000 square feet. 1 additional space for every 750 square feet over 50,000 square feet, or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	50%	10% of the spaces provided shall be designated for car and van pool use.
Research and Development	1 space/500 square feet of gross floor area (GFA) up to 30,000. 1 additional space for every 650 square feet from 30,000 to 50,000 square feet. 1 additional space for every 750 square feet over 50,000 square feet, or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	50%	10% of the spaces provided shall be designated for car and van pool use.
Wholesale, Warehouse, and Distribution	1 space/1,000 square feet of gross floor area (GFA) or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	50%	10% of the spaces provided shall be designated for car and van pool use.
Commercial Uses				
Auditoriums, places of public assembly	1 space/6 seats or 1 seat/100 square feet where there are no fixed seats.	10%	None	
Auto Repair and Service	1 space/250 square feet GFA.	10%	None	Spaces may not be used for vehicle sale, service, storage, rental, or repair.
Auto Sales	2 spaces/1,000 square feet of interior and satellite office and show room, plus 2 spaces/services bay	10%	None	Spaces may not be used for vehicle sale, service, storage, rental, or repair.
Auto Service Station	1 space/250 square feet GFA	10%	None	
Automobile Washing	3 spaces/washing stall	10%	None	
Bank, Savings & Loan	1 space/250 square feet of GFA, plus 1/employee in the largest shift.	10%	25%	10% of the spaces provided shall be designated for car and van pool use.
Barber Shop, Beauty Parlor	3 spaces/station	10%	None	

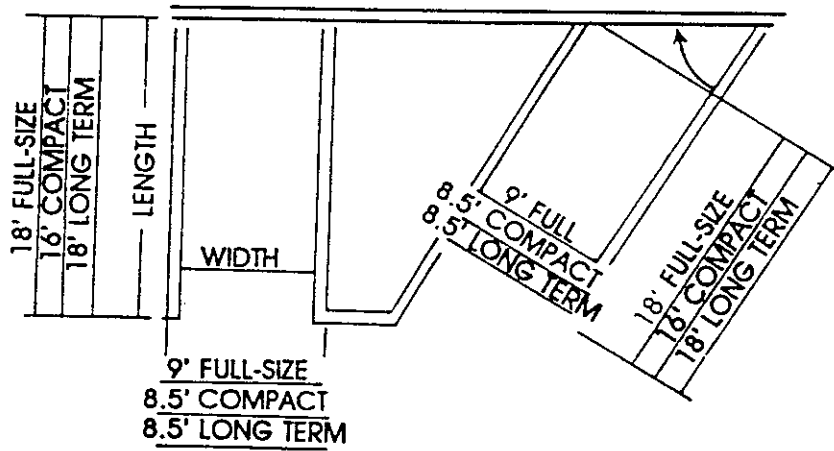
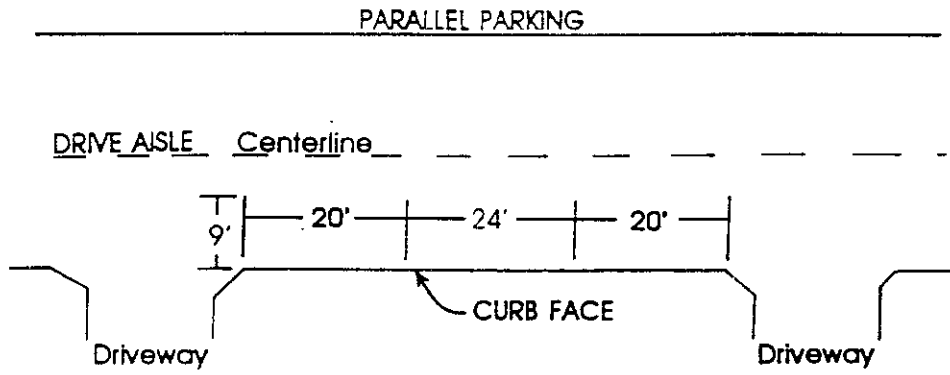
**Table IV-2
Off-Street Parking Space Requirements**

USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES	MAXIMUM PERCENT LONG-TERM SPACES	NOTES
Bowling Alleys	5 spaces/alley, plus 2 spaces/billiard table, plus 1 space for each 75 square feet of game arcade area, plus required for other uses	10%	None	10% of the spaces provided sh o be designated for car and van oc use.
Business And Personal Services	1 space/250 square feet GFA	10%	None	
Day Care Centers	1 space/employee, plus 1 space/10 children capacity	10%	None	Parking requirements are to be based upon facility's max um capacity.
Eating and Drinking Establishments	1 space/4 seats or 6 occupants based on building occupant capacity.	10%	None	
Health Club	1 space/150 square feet GFA	10%	None	
Hospitals	1 space/2 beds	10%	25%	
Hotel, Motel	1 space/guest room.	10%	None	
Lumber Yard, Nursery, Hardware Store	1 space/250 square feet GFA retail sales area, plus 1 space/1,000 gross square feet of outdoor display and storage area	10%	None	
Medical and Dental Clinics	1 space/160 square feet of GFA	10%	10%	
Offices	1 space/250 square feet of GFA for the first 30,000 square feet, 1 space for every additional 250 square feet up to 50,000 square feet, 1 space for every additional 300 square feet over 50,000 square feet.	10%	50%	10% of the spaces provided sh o be designated for car and van oc use.
Retail, General	1 space/250 square feet of GFA	10%	None	
Retail, Furniture, Major Appliances	1 space/250 square feet of GFA	10%	None	
Tennis, Handball, Racquetball Facilities	3 spaces/court, plus required parking for other on-site uses	10%	None	10% of the spaces provided sh o be designated for car and van oc use.
Theaters, Movie	1 space/6 fixed seats and 1 space for each 100 square feet used for amusement.	10%	None	

**Table IV-2
Off-Street Parking Space Requirements**

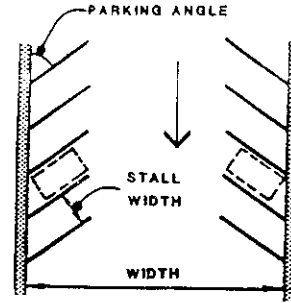
USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES	MAXIMUM PERCENT LONG-TERM SPACES	NOTES
Civic Use Types				
Community Education, Adult	1 space/each 3 persons capacity, plus 1 space/employee or faculty	10%	25%	10% of the spaces provided shall be designated for car and van pool use.
Government Offices	1 space/250 square feet GFA	10%	50%	10% of the spaces provided shall be designated for car and van pool use.
Libraries, Museums, Galleries	1 space/4 seats or 6 occupants based on building occupant capacity.	10%	None	
Religious Assembly	1 space/100 square feet of main assembly/auditorium area, or 1 space/6 seats when fixed seats are provided whichever is greater	10%	None	
Utilities	To be determined case-by-case	10%	None	A parking study shall be submitted for review and approval by the Planning and Engineering Divisions

Figure IV-2 Dimensions of Parking Spaces



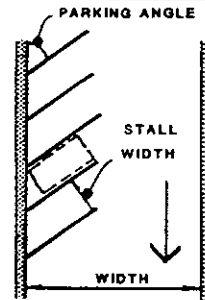
**Table IV-3
WIDTHS FOR DOUBLE LOADED PARKING AISLES WITH ONE-WAY TRAFFIC**

PARKING ANGLE (degrees)	8'-0" STALLS	8'-6" STALLS	9'-0" STALLS
30	40'-0"	43'-0"	42'-0"
45	44'-6"	49'-10"	52'-6"
60	48'-6"	55'-11"	60'-0"
70	51'-0"	59'-0"	61'-0"
80	52'-9"	61'-7"	62'-6"
90	54'-0"	63'-4"	64'-0"



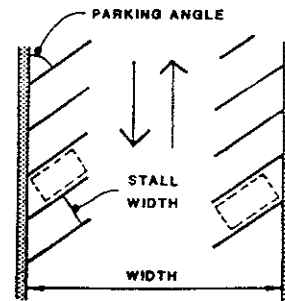
**Table IV-4
WIDTHS FOR SINGLE LOADED PARKING AISLES WITH ONE-WAY TRAFFIC**

30	26'-0"	27'-6"	28'-6"
45	28'-3"	31'-6"	32'-9"
60	31'-9"	36'-9"	39'-0"
70	34'-3"	39'-8"	40'-0"
80	36'-9"	42'-6"	43'-3"
90	39'-0"	45'-4"	45'-0"



**Table IV-5
WIDTHS FOR DOUBLE LOADED PARKING AISLES WITH TWO-WAY TRAFFIC**

30	48'-3"	51'-2"	56'-6"
45	52'-0"	56'-4"	63'-6"
60	54'-0"	59'-9"	66'-0"
70	54'-0"	61'-5"	67'-0"
80	54'-0"	62'-8"	66'-6"
90	54'-0"	63'-4"	64'-0"



**Table IV-6
WIDTHS FOR SINGLE LOADED PARKING AISLES WITH TWO WAY TRAFFIC**

30	34'-0"	35'-6"	41'-3"
45	36'-3"	38'-6"	43'-9"
60	37'-9"	40'-11"	45'-0"
70	38'-6"	42'-6"	46'-0"
80	39'-0"	44'-0"	46'-0"
90	54'-0"	63'-4"	64'-0"

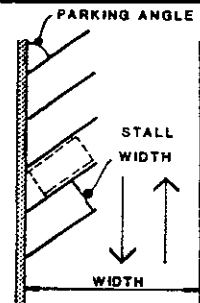
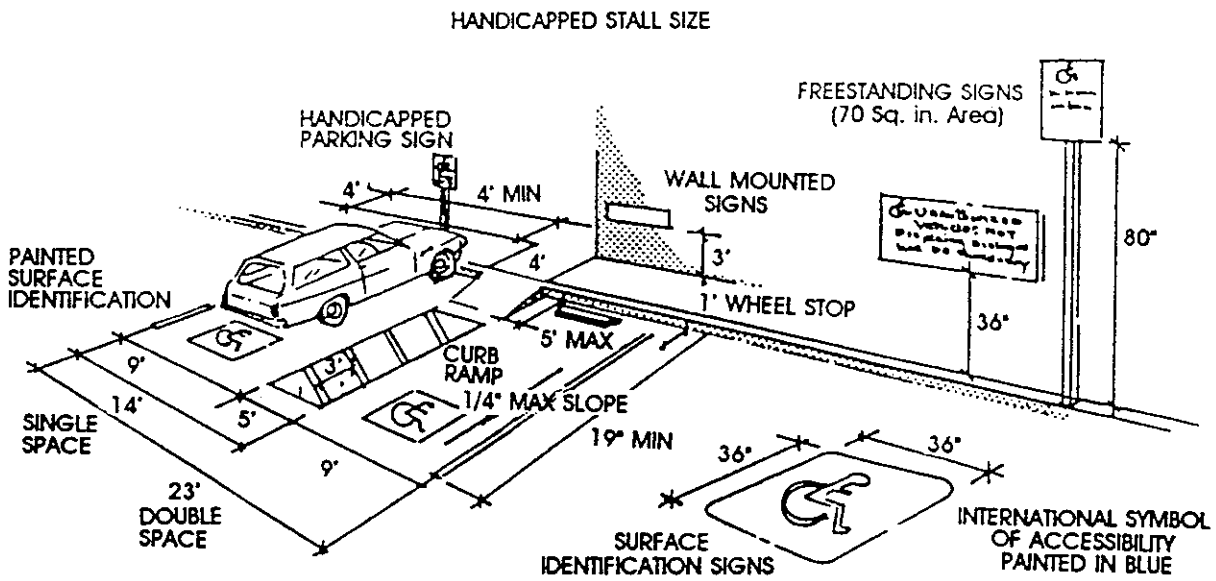
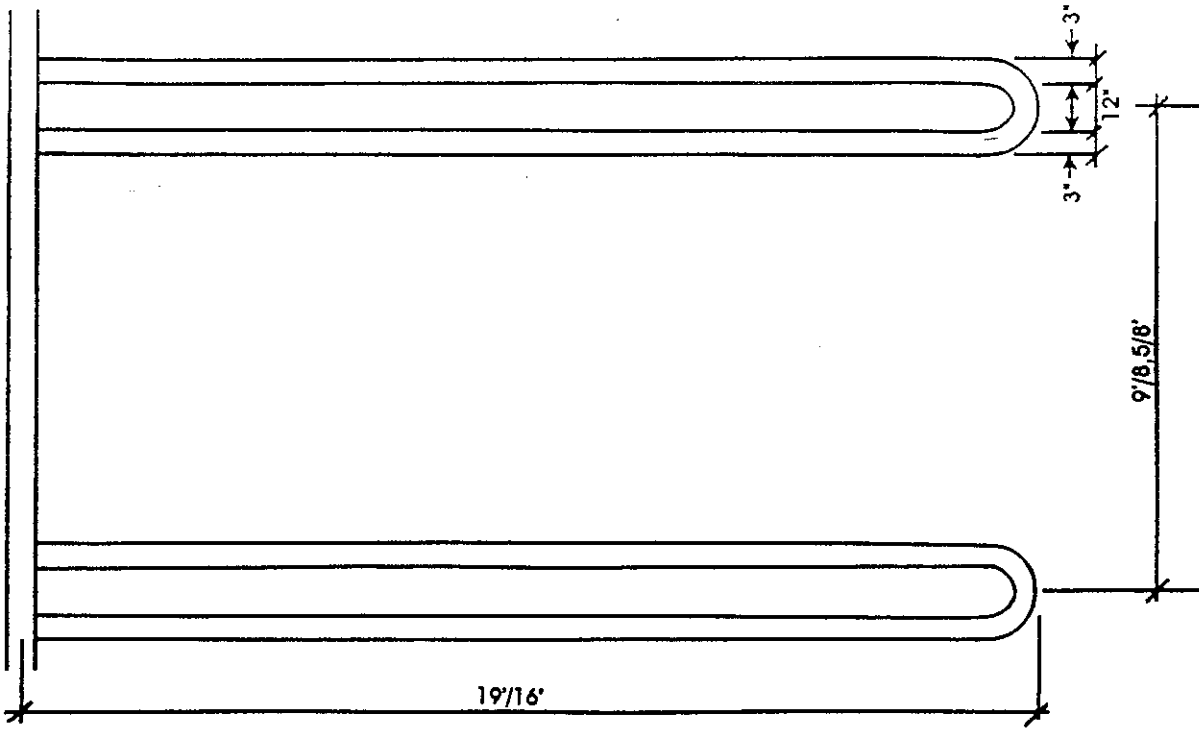


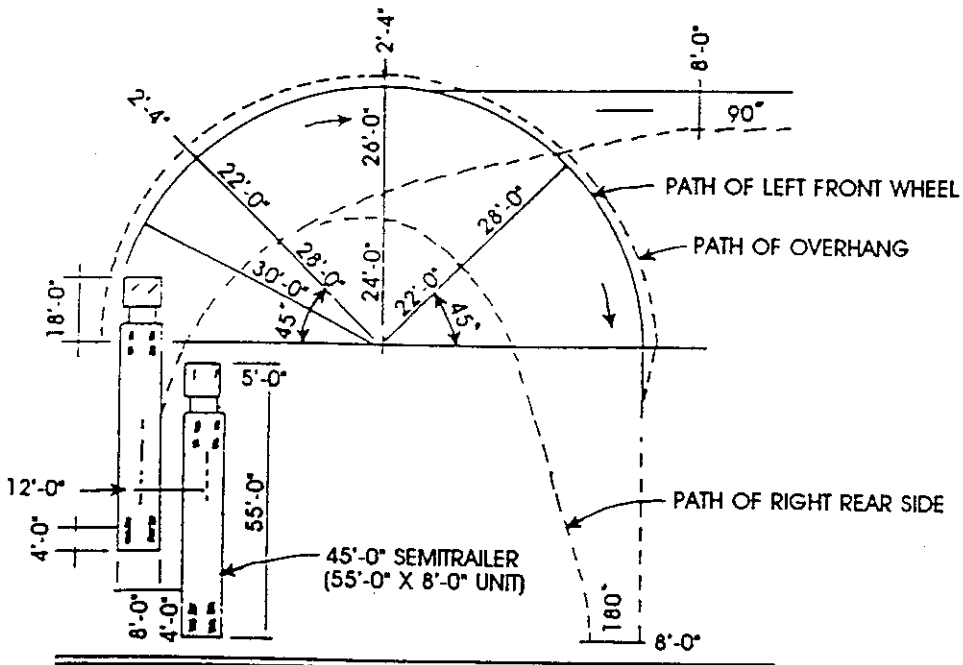
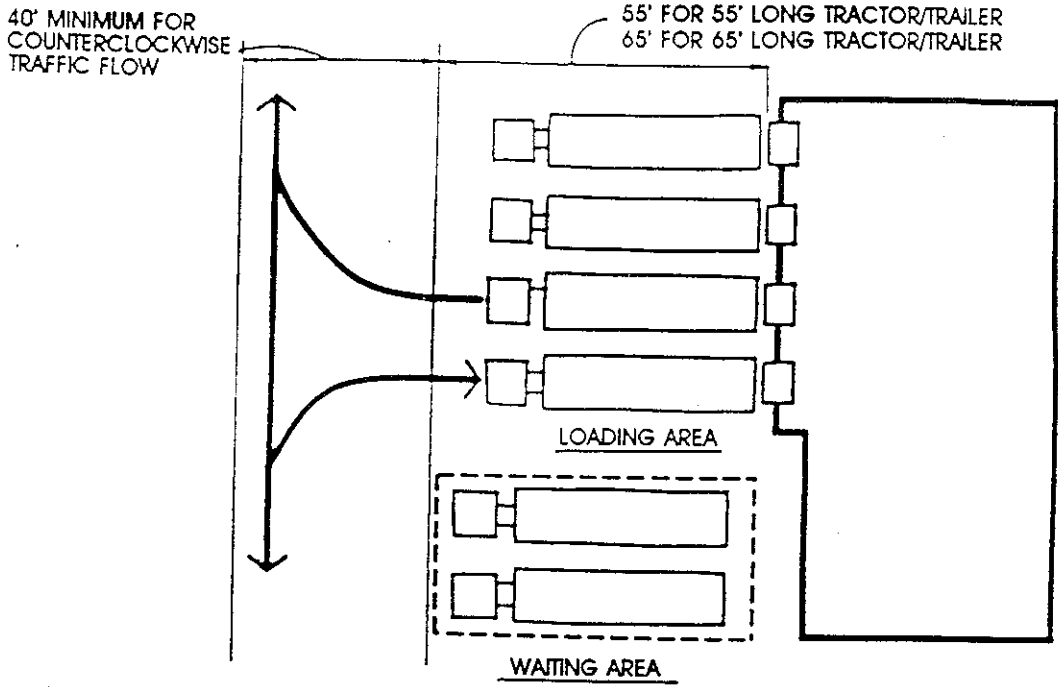
Figure IV-3 Parking Space Striping Requirements



**3. LOADING
REQUIREMENTS**

- a. Loading area(s) shall be provided as required by the Planning Division.
- b. Standards for maneuvering areas are illustrated in Figure IV-4, Page IV-48.
- c. Loading areas shall be designed to provide for backing and maneuvering on-site and not from or within a public street.
- d. Loading and maneuvering areas shall not encroach into building setback areas.
- e. All loading areas shall be screened from view from public and private streets by a decorative screen wall, ornamental landscaping, and/or portions of on-site buildings.
- f. Direct loading from a public street shall not be permitted. Loading docks and doors shall only be permitted within the interior side or rear yard area.
- g. Where loading doors or service bays face a private street or drive aisle, roll-up doors and openings in the screen wall shall be positioned such that doors are not visible from the street.
- h. All screen walls and wing walls shall be of a compatible material with adjacent buildings, and shall be of sufficient height to provide adequate visual screening.
- i. A sight line analysis shall be prepared by the project sponsor and filed with all development applications to demonstrate compliance with loading door screening requirements.
- j. A maximum of fifty percent (50%) of the lineal footage of any interior side or rear building elevation may be devoted to loading doors.
- k. Truck maneuvering areas shall not encroach into required parking stalls and drive aisles.
- l. On-site truck maneuvering to and from loading areas shall be provided to accommodate a minimum of a WB-50 truck. With the approval of the Planning and Engineering Divisions, a building owner may modify this maneuvering area requirement with the approval of the Engineering and Planning Divisions of a use and site specific traffic study.
- m. For each two (2) loading spaces provided, one trailer parking/waiting space shall also be provided.

Figure IV-4 Truck Loading and Maneuvering



55'-0" SEMI TRAILER AND TRACTOR COMBINATION
MINIMUM PRACTICAL TURNING RADIUS OF 50'-0".

4. OUTDOOR STORAGE

- a. Refuse storage and disposal areas shall be provided within trash enclosures which are screened on at least three sides from public view by a solid decorative wall not less than six feet (6') in height. The fourth side shall consist of a solid metal gate painted to match or coordinate with the building as approved by the Planning Division (slatted chain link is not acceptable).
- (1) Refuse bins shall be provided in sufficient number, and shall be placed in convenient location(s).
 - (2) All trash shall be deposited in the trash enclosure, and the gate leading thereto shall remain closed except when in use, and shall remain in good working order.
 - (3) Trash areas shall not be used for storage. The premises shall be kept in a neat and orderly condition at all times, and all improvement shall be maintained in a condition of good repair and appearance.
- b. Outdoor storage and display of operable vehicles offered for sale shall be permitted in the AM, BP, and C districts.
- c. Outdoor storage may be permitted subject to the following standards in the I (Industrial) districts:
- Outdoor storage areas shall not be located in the area between a primary structure and a public or private street.
 - Outdoor storage shall be adequately screened from view from public streets by an opaque material which is compatible with the materials of adjacent buildings as approved by the City of Montclair (slatted chain link is not acceptable).
 - The location of outdoor storage areas shall be limited to those areas so indicated on approved site development plans, outdoor storage shall be prohibited within areas reserved for off-street parking.

5. PERFORMANCE STANDARDS

a. *Applicability*

Unless otherwise specified, the performance standards contained herein are intended to be applied to all applicable development and land uses within the Holt Boulevard Specific Plan in addition to other standards of the Holt Boulevard Specific Plan and the Montclair Zoning Ordinance.

b. *Exemptions*

The following sources of nuisances are exempt from the provisions of this section:

- Emergency equipment, vehicles, devices, and activities;
- Temporary construction, maintenance, or demolition activities conducted between the hours of 6:00 a.m. and 7:00 p.m.

c. *Air Quality*

No operation of activity shall cause the emission of any smoke, fly ash, dust, fumes, vapors, gases, or other forms of air pollution which can cause damage to human health, vegetation, or other forms of property, or which can cause excessive soiling on any other parcel. No emission shall be permitted which exceeds the requirements of the South Coast Air Quality Management District or the requirements of an Air Quality Plan adopted by the City of Montclair.

d. *Electrical or Electronic Interference*

No operation of activity shall cause any source of electrical or electronic disturbance that adversely affects persons or the operation of any equipment on any other parcel and that is not in conformance with the regulations of the Federal Communications Commission.

e. *Heat and Cold*

No operation or activity shall emit heat or cold which would cause a temperature increase or decrease on any adjacent property in excess of ten degrees Fahrenheit (10°F).

f. *Light and Glare*

Unless intended as part of a master lighting plan, no operation, activity, or lighting fixture shall create illumination which exceeds five (5) foot-candles on any adjacent property, whether the illumination is direct or indirect light from the source. Glare levels shall be measured with a photoelectric photometer following standard spectral luminous efficiency curves adopted by the International Commission on Illumination.

g. *Liquid and Solid Wastes*

No operation shall discharge at any point into any public or private street, public sewer, storm drain, private stream, body of water, or into the ground, any materials which can contaminate any water supply, interfere with bacteriological processes in sewage treatment, or otherwise cause the emission of dangerous or offensive elements, except in accordance with standards approved by the California Department of Public Health or other governmental agency with jurisdiction. The discharge of non-approved wastes into a sanitary sewer, storm drain, or other public or private-property shall invalidate the ability of the discharging use to locate within the specific plan area.

h. *Maintenance of Open Areas*

All open areas shall be landscaped or paved.

i. *Mechanical and Electrical Equipment*

All mechanical and electrical equipment, such as, but not limited to, air conditioners, antennas, pumps, transformers, heating and ventilating equipment shall be located and operated in a manner that does not disturb adjacent uses and activities.

j. *Noise and Sound*

Unless otherwise specified, loudspeakers, bells, gongs, buzzers, or other noise attention or attracting devices shall not exceed 60 decibels at any one time beyond the boundaries of the property within which the noise is created.

k. *Odors*

No operation or activity shall be permitted which emits odorous gases or other odorous matter in such quantities as to be dangerous, injurious, noxious, or otherwise objectionable to a level that is detectable with or without the aid of instruments at or beyond the property within which the odor is created.

l. *Vibration*

No operation or activity shall be permitted to cause a steady-state, earth-borne oscillation which is continuous and occurring more frequently than 100 times per minute beyond the property within which the vibration was originally created. Ground vibration caused by moving vehicles, trains, aircraft, or temporary construction or demolition is exempted from this requirement.

**6. HAZARDOUS
MATERIALS AND
WASTES**

- a. Prior to issuance of a business license, individual businesses, with the exception of office uses, locating within the Holt Boulevard Specific Plan shall provide the Montclair Fire Department with an inventory of all hazardous materials used at the site, a description of where and how each is stored, and how each react in a fire. This inventory shall be updated quarterly.
- b. Placards or other appropriate signage shall be maintained in all uses indicating the storage location of hazardous materials.
- c. Prior to issuance of a business license, individual businesses, with the exception of office uses, shall submit for the review and approval of the Planning and Engineering Divisions and the Montclair Fire Department a hazardous materials and waste management plan. The plan shall provide for the use of the best available technology within the production process. The plan shall also outline proposed method for source reduction, treatment, handling, transportation, and disposal of hazardous materials and wastes, including emergency response and employee training procedures. A copy of the approved business plan submitted to the San Bernardino County Environmental Health Services Department shall be provided to the Planning Division.
- d. Prior to occupancy, a truck routing plan shall be submitted to the City for review and approval. The truck routing plan shall include the routes and hours of transport to be used for all hazardous materials and wastes.

**7. PLANNED
DEVELOPMENTS**

The purpose of planned developments within the Holt Boulevard Specific Plan is to achieve superior development to that which can be achieved through the standards contained in the Specific Plan by permitting greater design flexibility.

- a. Subject to all of the provisions of this section, the following development standards may be modified:
 - Minimum parcel sizes, widths, and depths;
 - Parking and landscape setbacks for interior side parcel lines;
 - Building setbacks for interior and rear property lines;
 - Parking and landscaping requirements for individual parcels, except for required landscaping along streets and private drives and
 - Maximum lot coverage.

- b. A completed application for a Planned Development shall be submitted showing the information required by the City of Montclair for a Special Conditional Use Permit.
- c. Although standards for landscape coverage and number of parking spaces within a planned development need not be met on a parcel by parcel basis, each phase of the planned development shall contain the required amount of landscaping and number of parking spaces.

If parking standards are not met on a parcel by parcel basis, the required number of parking spaces shall be provided within common parcels for which a property owners' association is responsible or adequate reciprocal parking agreements shall be recorded prior to issuance of building permits.

- d. Although buildings within a planned development need not be set back from interior property lines, unless a zero separation is provided, a minimum building separation equal to fifteen feet (15') or fifty percent (50%) of the height of the taller building, whichever is greater, shall be maintained.
- e. Individual buildings and lots need not have direct access to a public street; however, sufficient easements and/or reciprocal access agreements shall be recorded to ensure that adequate ingress and egress is available to each lot and building.
- f. Flag lots shall be prohibited.
- g. If separate parking lots are provided within a planned development, each area shall separately meet the landscape requirements of Section IV-C1, Page IV-31; however, the motorcycle parking requirements of Section IV-C2f, Page IV-35 shall be met on a cumulative basis based on the number of parking spaces within each phase of the planned development.

**D. SPECIFIC USE
DEVELOPMENT
STANDARDS**

**1. AUTOMOBILE AND
LIGHT TRUCK
REPAIR FACILITIES**

Certain uses and activities, due to their nature, have the opportunity to create more significant impacts upon the community than others. As a result, specific regulation of these activities and uses is warranted. The purpose of this section is to identify and provide appropriate regulation for such uses.

a. *Orientation of Structures*

Entrances to individual service bays shall not face public or private streets.

b. *Sound Transmission*

All structures shall be constructed to achieve a minimum Standard Transmission Coefficient (STC) sound rating of 45 - 50.

c. *Repair Activities*

Except as specifically provided herein, all repair activities and operation shall be conducted entirely within an enclosed building. Outdoor hoists are prohibited.

d. *Litter*

The premises shall be kept in a neat and orderly condition at all times, and all improvements shall be maintained in a condition of good repair and appearance. No used or discarded automotive parts or equipment or permanently disabled, junked, or wrecked vehicle may be stored outside the main building.

e. *Storage of Vehicles*

Required parking areas, and public and private streets and alleys shall not be used for the repair or finishing work or storage of vehicles to be repaired or waiting for customer pickup.

**2. AUTOMOBILE
DEALERSHIPS**

a. *Washing of Vehicles, Storage of Vehicles to be Repaired, and Repair of Vehicles*

All washing, rinsing, or hosing down of vehicles and of the property; storage of vehicles to be repaired; and repair of vehicles shall comply with applicable requirements for vehicle repair facilities. All runoff shall be drained from the site in a controlled manner subject to the approval of the Engineering Division.

b. *Loading and Unloading of Vehicles*

In addition to the provisions of Section IV.C3, Page IV-44 of this plan, off-loading shall be on-site or off-site subject to the approval of the Planning Division. Loading and unloading shall not block the ingress and egress of any adjacent property.

c. *Queuing of Vehicles*

An adequate on-site queuing area for service customers shall be provided. The queuing area or lanes shall be large enough to hold at least one and one-half (1.5) vehicles for each service bay in the facility based on a queuing distance of twenty-five feet (25') per vehicle. On-site driveways may be used for queuing, but may not interfere with access to required parking spaces. Required parking spaces may not be used for queuing.

d. *Noise and Sound Standards*

Noise levels within automobile dealerships including intercoms and paging systems shall be maintained at a level that would not be considered a nuisance to surrounding property owners. Where possible the use of "beepers" or "paggers" should be used in place of an intercom.

**3. AUTOMOBILE
RENTAL AGENCIES**

a. *Washing, Storage, and Repair of Vehicles*

All washing, rinsing, or hosing down of vehicles and of the property; storage of vehicles to be repaired; and repair of vehicles shall comply with applicable requirements for vehicle repair facilities. All runoff shall be drained from the site in a controlled manner subject to the approval of the Engineering Division.

No vehicles to be displayed, rented, or sold, shall be parked or stored on any street or alley. In addition, such vehicles shall not be stored or parked within required parking areas.

**4. AUTOMOTIVE
SERVICE STATIONS**

See Section 9-6.101 of the Montclair Municipal Code.

V. IMPLEMENTATIONS

V. IMPLEMENTATIONS

A. PHASING

1. LAND DEVELOPMENT PHASING

Because the Specific Plan area is primarily developed, phasing of the development within the Specific Plan area can be accommodated in any order. Based on discussions with the City staff, it is apparent that initial public improvements within the Holt Boulevard Specific Plan occur initially in the first calendar year after adoption in the following areas:

- entry statements on both ends of the plan area;
- street scape improvements for one sub area; and
- encouragement of site redevelopment for existing businesses.

B. REMOVAL OF NON CONFORMING SIGNS

1. PURPOSE

The City finds that in some circumstances it is desirable to encourage the removal of nonconforming signs at a rapid pace. Therefore, in order to encourage the removal of nonconforming signs, the City may offer owners of nonconforming signs the following incentives to hasten their removal.

2. REQUIREMENTS

In order to qualify for the nonconforming sign removal incentive, a sign proposed to be removed must meet the following requirements:

- a. The sign must have been a legal conforming sign at the time of its placement.
- b. The sign shall not be an abandoned sign at the time of application as defined within this Specific Plan.
- c. In the granting of nonconforming sign removal incentives, the City will apply the following priority ranking schedule:
 - Roof signs

- Pole signs
- Changeable copy signs
- Signs painted on walls, fences, or roofs
- Can-type wall signs
- Single cut letter wall signs

d. The owner of the sign must have previously applied for and received a sign permit to allow the continuance of a nonconforming sign unless the removal incentive is applied for within one hundred and eighty (180) days of the effective date of this Specific Plan.

3. INCENTIVE PROGRAM

Owners of nonconforming signs may at their discretion choose to participate in the following sign removal incentive program. The City will select a number of applicants each year to participate in the program based on the priority list of signs provided above and the availability of funds.

The nonconforming sign removal incentive program is a four (4) year program commencing on the effective date of this Specific Plan. The program is designed to encourage the rapid removal of the least desirable types of signs by providing larger incentives in the early year and tapering off to no incentives in the fifth year.

The incentives which may be provided are as follows:

(1) Year one

Free removal of nonconforming sign.
 Payment of an incentive bonus of \$1,000 for roof signs and pole signs, \$500 for all others.
 Free design service to provide alternative sign sketches.

(2) Year two

Free removal of nonconforming sign.
 Free design service to provide alternative sign sketches.

(3) Year three and four

Free removal of nonconforming sign.

Those participants availing themselves of the free design service must agree to utilize one of the alternative sketches provided and to erect new sign within three (3) months of receiving the sketch designs. If said new sign is not erected the participant will be required to reimburse the City for the cost of the service.

C. FUNDING MECHANISMS

1. POTENTIAL FUNDING MECHANISMS

In order for the study area to develop consistent with the Specific Plan, infrastructure and community facilities must be funded. Summarized below are possible methods for funding improvements within the Holt Boulevard Specific Plan. In general, only a limited number of financing sources are available to public agencies which can be used to finance infrastructure and community facilities. These are described below:

a. Construction by Developers

This is a well established method of constructing infrastructure within California. Under this method of financing, individual developers could be required to construct infrastructure and other facilities as part of their projects. Recent case law limits this form of financing to requiring developers to mitigate all impacts related to their project; exactions and requirements must be reasonably related to the project impacts.

Often, facilities built by original developers must be over-sized to allow for future development, or off-site facilities must be constructed to extend services and facilities to the property in question. In some cases, developers may be required to pay in advance for engineering and design costs for proposed infrastructure. In most cases, the developer is eventually repaid from project financing or a reimbursement agreement. A reimbursement agreement is a contract between the City and a developer to reimburse the developer for advances made to a project if and when funds are received from future development. The City does not usually have any liability to repay the developer if other development does not occur. Reimbursement agreements are generally written to be valid for a ten (10) year period.

Construction of facilities by individual developers is the easiest financing method to implement, and should be employed to the extent possible. This method allows developers to move ahead with their projects subject to their own timing without the potential delays which might accrue in the creation of more elaborate financing schemes. However, issues of equity preclude developer construction for features of area wide or general benefit unless reimbursement is involved. In addition, in requiring construction of facilities by

developers, even if there is to be reimbursement, care must be taken to avoid overloading front end development costs on a private development project to the point that on-site project amenities must be sacrificed in favor of up-front costs for off-site facilities or the point that the project is no longer financially feasible.

b. *"Pay-As-You-Go"*

This method involves the accumulation of funds from fees or other sources prior to acquisition and construction of facilities. The collection of fees may be imposed as a condition of approval from individual projects within the Specific Plan area, and is not subject to vote as are assessment districts and special taxes.

Although pay-as-you-go reduces interest costs which accrue from long-term financing, interest savings may be offset by price escalation in the period during which funds collect. Another disadvantage is that the benefits of the improvement are delayed until sufficient fees are collected. State law presently requires that fees collected for a specified purpose may only be used for that purpose, and cannot be commingled with funds to be used for other purposes. State law also limits the amount of time that fees can be held without being used. Thus, if pay-as-you-go is used for financing facilities within the Holt Boulevard Specific Plan, it may be necessary to secure up-front funds from other sources, and use fees which are subsequently collected as reimbursement. The same care must be taken in the levying of fees as with requirements for developer construction: the extent of the fees should not jeopardize the financial feasibility of an otherwise desirable project.

c. *Assessment Districts*

State law authorizes various types of assessment districts which can be used to finance capital expenditures. Through the assessment district, financing is arranged through the sale of bonds. Obligation for repayment of the bonds is shared by those within the district. Formation of an assessment district generally requires agreement of a least a majority of affected property owners.

Capital improvements are often financed through the use of assessment bonds issued pursuant to the Improvement Act of 1911 or the Improvement Bond Act of 1915. These do not require voter approval, but involve notice and a public hearing. Except under certain conditions, a majority of property owners protesting the assessment negate the proceedings.

This method involves an allocation of the costs of the facilities being financed to the land being benefitted. After assessments are confirmed, property owners are given 30 days during which they may pay all or part of the assessments in cash. Assessments over \$150 which are not paid then go to bond. Special assessment bonds may be prepaid on any interest payment date.

State law places a maximum on the interest rate which may be paid; however, no constraints other than market forces apply to the discount which may be bid.

The formation of assessment districts can be procedurally complex, particularly as it relates to spreading of assessments in relation to the benefit received. This system works best for facilities which have a clearly defined scope, purpose, and area of benefit. Its primary advantage is that it provides a means of constructing one or more large facilities at a single time, thereby enhancing the efficiency of project completion. There are, however, few facilities which need to be constructed in large increments which could not be constructed by individual developers.

d. *Mello-Roos Community Facilities Act*

The Mello-Roos Community Facilities District Act of 1982 authorizes formation of community facilities districts which may provide certain additional public services or facilities to be financed through elector-approved special taxes or funded through special taxes securing long-term debt for construction of facilities.

"Services" are defined as additional services performed by employees to provide police protection; fire protection and suppression; and/or recreation programs including operation, maintenance, and repair activities for parks and parkways. Services may be financed through a Mello-Roos Community Facilities District to the extent that they are in addition to those provided within the district prior to its formation.

"Facilities" includes the purchase, construction, expansion, or rehabilitation of property including, but not limited to the following which may be relevant to the study area: local parks, recreation, or parkway facilities; governmental facilities which the legislative body is authorized to construct, own, or operate; natural gas, telephone, and electrical lines for customers who do not have access to those facilities.

If fewer than twelve (12) registered voters reside in a proposed community facilities district, a vote is taken by the landowners based on one vote per acre or portion thereof. If there are twelve or more registered voters, a vote is taken of those voters. If long-term bonded indebtedness is required, a resolution and public hearing is also required regarding the necessity of incurring the debt. A two-thirds

(2/3) vote of the property owners or registered voters is necessary for any special tax levy, as well as for the issuance of long-term debt finance facilities; a single ballot proposition may be used for both purposes.

Bonds issued pursuant to a Community Facilities District are secured by the voted special taxes levied within the District. The special taxes are levied in the same manner as general taxes for the legislative body, with the funds paid to the District for the particular debt service. The bonds may be sold at a private sale without advertising for bids if the legislative body determines that such a sale would result in a lower interest cost on the bonds. Otherwise, the bonds must first be offered at public sale; if no satisfactory bid is received, the legislative body may reject all bids and readvertise, or sell at private sale. These bonds are exempt from limits on general taxation, and cannot exceed a forty (40) year term.

The advantages and disadvantages of Mello-Roos districts are similar to those for assessment districts. The Mello-Roos Community Facilities District Act is more flexible than assessment district law, however, in the facilities which may be constructed and in the manner in which it is formed.

e. *Redevelopment (Tax Increment) Funds*

The Holt Boulevard Specific Plan is within an established redevelopment project area. Within this area property taxes were "frozen" at a base level when the area was created. Increases in property taxes occurring since that time accrue to the agency (with the exception of certain funds which are passed through to outside service agencies) for financing of eligible projects. Redevelopment law permits increments to be used to finance land acquisition for public purposes; construction of public facilities such as roads, water and sewer facilities, and parks, and payment of administrative, legal, planning, and engineering costs related to the proposed redevelopment project. In addition, bonds can be issued to be repaid by tax increments.

Tax increment financing of infrastructure and community facilities within redevelopment project areas is an inviting mechanism. However, it should be used cautiously. As long as the Redevelopment Agency has incurred debt affecting the project area, tax increments must be used to retire that debt. While increments can and are passed through to outside agencies supplying essential services to the project area, funds cannot be passed through to the City itself. The use of tax increments should therefore be conceived of as a municipal investment to be used on such projects as will repay themselves through increased revenue to the City.

If a portion of the Project is undertaken in conjunction with a Redevelopment Agency, it may be possible to fund that portion with the proceeds of tax allocation bonds. Tax allocation bonds are issued pursuant to Section 33670 of the California Health and Safety Code to provide funds to redevelopment agencies to revitalize blighted and economically depressed areas of the community and to promote economic growth. In addition to tax increment (incremental property taxes within a redevelopment area) and other revenues of the redevelopment agency, income and revenue from specified redevelopment projects and state/federal aid can be pledged as security. Tax allocation bonds do not need voter approval. The current statutory maximum interest rate of tax allocation bonds is 12 percent (a bond discount of up to 50 percent of par is permitted) and the bonds must be sold at competitive sale. Tax allocation bonds may be offered for sale before tax increment revenue is generated; however, strong assurances should exist that future revenues will be sufficient to meet debt service. There is no contingent liability to the general fund of any public agency. Current law requires that redevelopment projects be 80 percent urbanized.

f. *Landscape and Lighting Act of 1972*

The Landscape and Lighting Act of 1972 authorizes local agencies to impose assessments on benefitted property to finance the construction as well as for maintenance of various landscaping, lighting, park, and recreational improvements. The Act permits creation of a district divided into zones which can have different assessments depending upon the type of service being provided within each zone, or to have no assessment.

A report must be prepared and a public hearing held for each fiscal year for which assessments are to be levied. The report must include plans and specifications, an estimate of costs, a diagram of the district, and an assessment of the costs. At the first year public hearing, the proceedings must be abandoned if a majority protest is filed, unless the protests are overruled by a four-fifths (4/5) vote of the local legislative body. In subsequent years, an annual hearing must be held concerning the report as to the state of the improvements; however, the above-mentioned provisions regarding abandonment do not apply to subsequent years.

The use of landscape and lighting districts provides a good method for maintaining the improvements that will occur on Holt Boulevard. Its scope is limited, and would need to be used in combination with other methods to finance the construction of other types of facilities.

2. "DEVELOPER,"
"SHARED," AND
"COMMON"
FACILITIES

g. *City Contributions*

In certain circumstances, the City might wish to contribute funds from the general fund or from an enterprise fund if identified costs would not be to the exclusive benefit of the Holt Boulevard Specific Plan. Certain facilities may be appropriate for larger scale funding mechanisms than just from the Holt Boulevard Specific Plan area. The primary issue with the use of City contributions is the extent to which facilities related to the Holt Boulevard Specific Plan should be paid for by parties outside of the area.

In determining the appropriate method of financing, infrastructure and facilities need to support buildout of the Holt Boulevard Specific Plan, a determination was made as to the general scope of benefit for the facilities being financed. Three categories of infrastructure and facilities were then identified: "Developer," "Shared," and "Common." Table V-2, Page V-1, identifies responsibility for improvements.

"Developer" facilities are those which will primarily serve an individual development within the Holt Boulevard Specific Plan, or which will serve a small number of individual developments, but which need not be constructed pursuant to City-sponsored financing schemes. These facilities will, therefore be constructed by individual developers as part of individual projects within the Holt Boulevard Specific Plan.

"Shared" facilities are developer facilities which will serve more than one development. Although these facilities will be constructed by developers, reimbursement agreements will be entered into so that each development pays its "fair share" of facilities.

"Common" facilities are defined as those which serve large portions of the study area, and could not reasonably be assigned to one or two developments. Common facilities include intersections and medians.

3. RECOMMENDED
FUNDING
MECHANISMS

Recommended methods of financing facilities within the Holt Boulevard Specific Plan are identified in Table V-1, Page V-9. Many of the needed facilities will be financed citywide through the Capital Improvement Plan and property owners within the Holt Boulevard Specific Plan will be required to pay for an appropriate portion of these improvements. Those facilities needed exclusively for the Holt Boulevard Specific Plan will be financed as noted in Table V-1, Page V-9 and will be required of property owners/developers above and beyond the citywide fees required for the development.

Table V-1
Estimated Construction Costs

FEATURE	ESTIMATED CONSTRUCTION COSTS	ESTIMATED TOTAL
Major Intersection Mills and Holt Benson and Holt Central and Holt		\$288,000 for three major intersections
Demolition	\$20,000 per intersection	
Pavers	\$25,000 per intersection	
12" Band	\$ 3,000 per intersection	
Monument Sign Walls (4)	\$33,000 per intersection	
Parkway Landscape	\$15,000 per intersection	
Minor Intersections Ramona and Holt Monte Vista and Holt		\$123,000 for two minor intersections
Demolition	\$20,000 per intersection	
Pavers	\$24,000 per intersection	
Crosswalk Concrete	\$ 7,500 per intersection	
Palms (24)	\$20,000 per intersection	
Median Improvements (8,000 linear feet total)		\$950,000 for 8,000 linear feet of median
Demolition	\$ 7,500 per 300 feet	
Concrete Curb	\$ 4,000 per 300 feet	
Landscaping	\$10,000 per 300 feet	
Special Paving	\$ 5,000 per 300 feet	
Flags (3)	\$ 200 per 300 feet	
Trees (20)	\$ 9,000 per 300 feet	
Sidewalk Improvements		\$1,280,000 for total sidewalk improvements
Parkway Landscaping	\$ 4,000 per 300 feet	
Canopy Trees for sidewalk (6)	\$ 3,000 per 300 feet	
Canopy Trees for Parkway (6)	\$ 3,000 per 300 feet	
Tree Grates	\$ 8,000 per 300 feet	
Mid Block Pop-Out	\$ 5,000 per 300 feet	

Table V-1
Estimated Construction Costs

FEATURE	ESTIMATED CONSTRUCTION COSTS	ESTIMATED TOTAL
COST ESTIMATE RECAP		
Major Intersections		\$288,000
Minor Intersections		\$123,000
Median Improvements		\$950,000
Sidewalk Improvements		\$1,280,000
	GRAND TOTAL	\$2,641,000
		or \$228 per foot

**D. NONFINANCE
IMPLEMENTATION**

**1. REGULATORY
PROCEDURES**

Implementation of the Holt Boulevard Specific Plan will require the coordination of existing City of Montclair review procedures and other implementation actions discussed below.

a. Zoning Ordinance

Upon adoption of the Holt Boulevard Specific Plan, all land use and development regulations and guidelines set forth in this Specific Plan shall replace those of the City of Montclair Zoning Ordinance.

b. Special Conditional Use Permits and Planned Developments

Certain uses, because of their particular site development requirements or potential for creating negative impacts, are subject to the granting of a conditional Use Permit by the Planning Commission as set forth by the City of Montclair Zoning Ordinance. Uses subject to granting of a CUP are listed in Section IV-B, Page IV-4, Development Regulations. The purpose of the CUP is to provide an opportunity for public review and evaluation of site design and use requirements and characteristics in order to ensure adequate consistency with the goals and objectives of the Specific Plan; compliance with the plan's requirements, guidelines, and performance standards; mitigation of potential impacts; and adjustment authorized by the Specific Plan and Planning Commission. The Planning Commission shall hold a public hearing on the CUP, and may grant approval, approval with reasonable conditions, or denial of a CUP application subject to a right of appeal.

c. Administrative Approval/Site Approval/Development Review

To ensure compliance with all applicable standards and regulations of the Specific Plan, all projects shall be subject to City of Montclair development review processing procedures and requirements, including Planning Commission and/or City Council Review as applicable.

Review of site plans will include review of items such as, but not limited to, location of buildings, off-street parking, landscape areas, loading facilities, and entries/exits; dedication of streets as required by the Specific Plan; recommendations of the City Engineer as to required infrastructure provision; compatibility with surrounding development; consistency with the provisions of this Specific Plan and the City Zoning Ordinance; and other specific conditions affecting public health, safety, and welfare.

In addition to the above, where a land use is identified in Section IV, Page IV-1, Development Regulations as "Administratively Permitted", a more detailed review of the proposal will be undertaken. Essentially this review will be similar to that of a Conditional Use Permit with the exception that the City Planner or his/her designee shall be the approving authority rather than the Planning Commission, and that a public hearing need not be held. The City Planner may grant approval, approval with reasonable conditions, or denial of an administratively permitted use, subject to a right of appeal. The City Planner may decide that a public hearing before him/her is required because of the nature of the project proposed for Administrative Approval.

**2. CITY
IMPLEMENTATION
RESPONSIBILITIES**

City of Montclair departmental responsibilities for the implementation of the Holt Boulevard Specific Plan are presented in Table V-2, Page V-13

**Table V-2
IMPLEMENTATION RESPONSIBILITY MATRIX**

TOPIC	RESPONSIBLE AGENCY OR DEPARTMENT							Reference Location(s) In HBSP Text		
	CD-P	CD-B	PW-E	PW-S	AS	PD	FD	D	S	C
1. CIRCULATION										
Public Transit	X		X							IV
Streetscape Concept	X		X						X	III-F
Traffic Signals, Striping, Signage			X	X					X	III-F
Vehicular			X							III-F
2. DEVELOPMENT FEES/FINANCING	X	X	X		X		X		X	VA
3. DEVELOPMENT REGULATIONS								X		III-B,C,D
Building Design	X	X				X			X	IV-B
Development Standards	X		X				X	X		III-E
On-site Landscaping	X					X		N/A		IV-B
Permitted Uses	X		X						X	III-B,C,D
Site Design Criteria	X		X				X	N/A		V-C
4. DEVELOPMENT REVIEW PROCESS	X	X	X			X	X			

Key:

CD-P Community Development Department - Planning Division
 CD-B Community Development Department - Building Division
 PW-E Public Works Department - Engineering Division
 PW-S Public Works Department - Services Division
 AS Administrative Services Department
 PD Police Department
 FD Fire Department
 D Developer
 S Shared
 C Common

