

City of Montclair

North Montclair Specific Plan



North Montclair Specific Plan

(Specific Plan No. 97-1)

Prepared For:

**City of Montclair and
City of Montclair Redevelopment Agency**

Prepared By:

**Urban Design Studio
and
LSA Associates, Inc.**

**Resolution No. 97-2163
Adopted January 5, 1998**

ACKNOWLEDGMENTS

City Council:

Paul M. Eaton, Mayor
Elouise "Dolly" Lewman, Mayor Pro Tem
John Dutrey
Leonard Paulitz
Carolyn Raft

Planning Commission:

Maynard Lenhert, Chairman
Don Vodvarka, Vice Chairman
Luis Flores
Bill Ruh
Sergio Sahagun

City Staff:

Lee C. McDougal, City Manager
Marilyn Staats, Director of Redevelopment/Public Works
Robert W. Clark, Director of Community Development
Hal G. Fredericksen, City Planner
Jim S. Lai, Associate Planner
Carol Frazier-Burton, Community Development Coordinator

Consultants:

Urban Design Studio, Lead Consultant
LSA Associates, Environmental Consultant
Albert Grover and Associates, Traffic Consultant

NORTH MONTCLAIR SPECIFIC PLAN

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CHAPTER I

INTRODUCTION

Plan Overview

A. Purpose

North Montclair is an area in transitional growth - an area still adapting to the growth inducing influences of the very successful Montclair Plaza Regional Mall and, more recently, the Montclair Transcenter. As the North Montclair area continues to grow, new opportunities for commercial development and redevelopment will occur. The City must be prepared to guide appropriate new projects in ways necessary to realize the community's vision for high quality development in North Montclair.

The purpose of the North Montclair Specific Plan is to provide a comprehensive policy document to guide the continuing development/redevelopment of the plan area to the year 2010. It provides a single source

document for land use policy guidance and improvement of the physical environment. It is not a regulatory document. Therefore, recommendations of the Plan related to land use and development criteria require implementation through amendments to the Zoning Ordinance and General Plan.

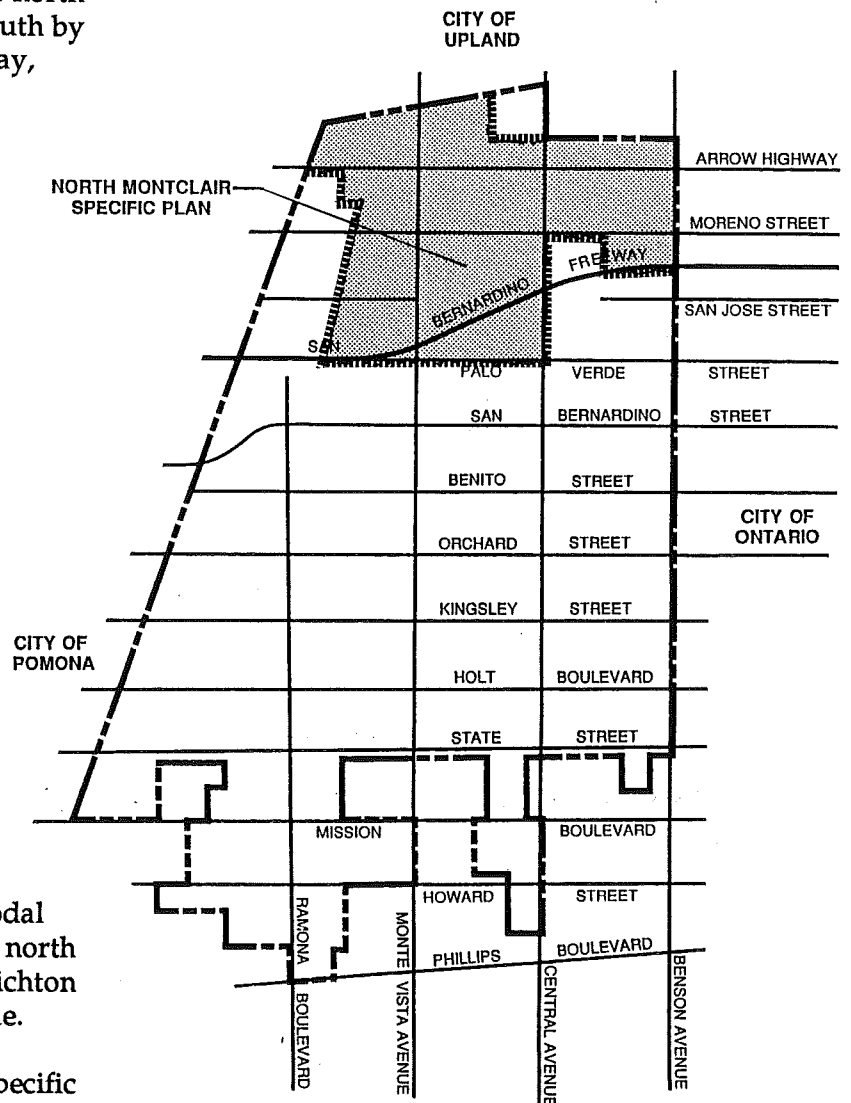
The Specific Plan establishes a level of quality for design and development that expresses the City's expectations and insures business and property owners that new development will be of the appropriate type and quality. It allows each parcel and project to be evaluated as a "puzzle piece," playing an important partial role in the overall physical development pattern envisioned for North Montclair.

B. Specific Plan Area

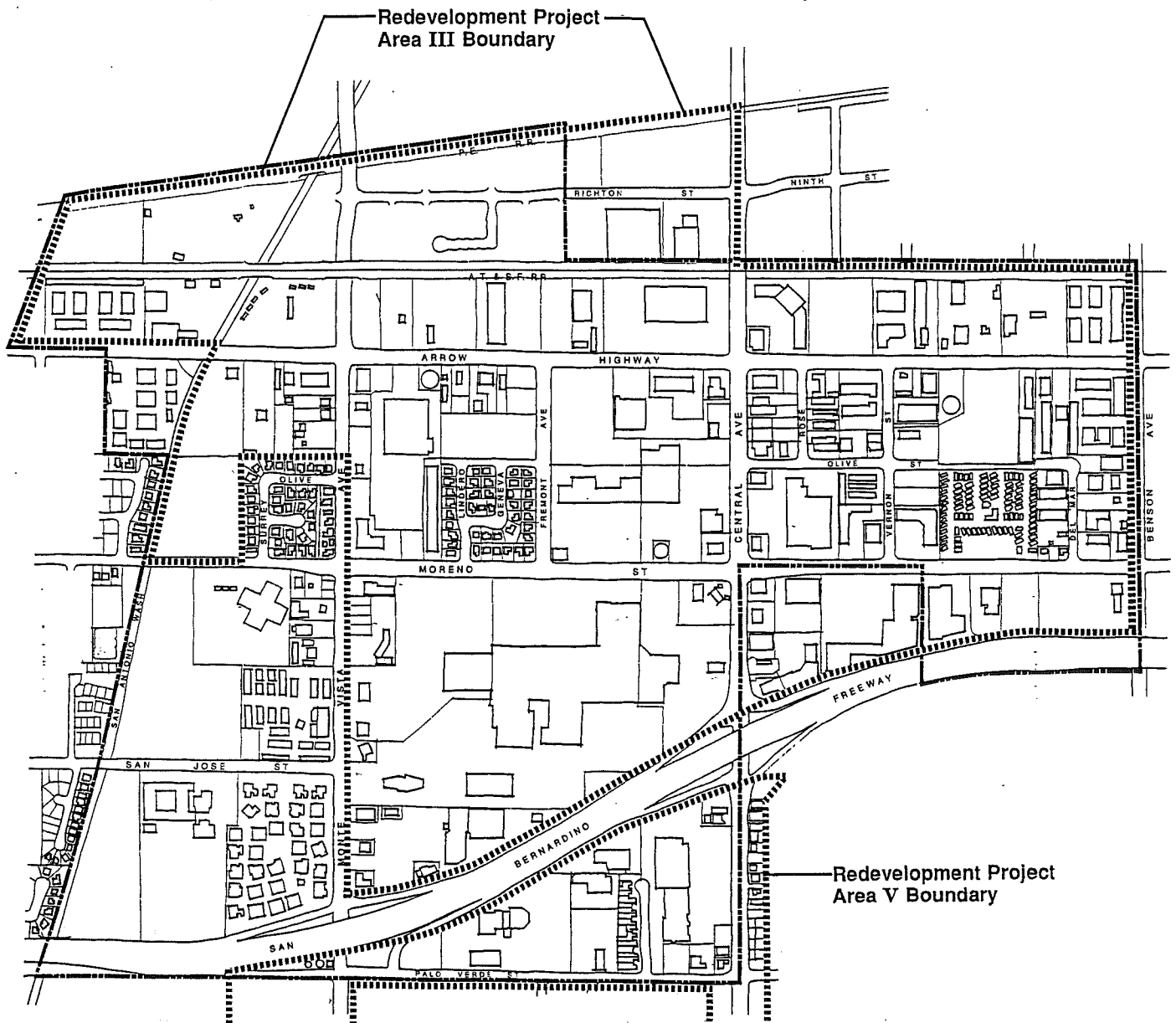
The Specific Plan area, shown at the right and on the following page, consists of approximately 640 acres - one square mile. The area is generally bounded on the north by the north City boundary, on the south by Palo Verde Street and the I-10 Freeway, on the east by Benson Avenue (east City boundary), and on the west by the San Antonio Flood Channel and the west City boundary.

The Specific Plan Area consists primarily of retail commercial and light industrial uses, with the Montclair Plaza Regional Mall and related commercial retail developments serving as the area's primary focus. These retail commercial uses are concentrated along the two major north/south arterial streets, Central Avenue and Monte Vista Avenue and the two major east/west arterials, Arrow Highway and Moreno Street. There are pockets of residential development in the area, as well as two public schools and a City mini park. The Montclair Transcenter, a multimodal transportation facility, is located at the north end of the Specific Plan area, along Richton Street, just east of Monte Vista Avenue.

Approximately 80 percent of the Specific Plan area falls within Montclair Redevelopment Agency project areas; almost all of Project Area III and a small portion of Project Area V.



SPECIFIC PLAN BOUNDARY



C. Statutory Authority

The North Montclair Specific Plan is established through the authority granted to the City of Montclair by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (specific plans).

Specific plans may be adopted either by resolution or ordinance. This allows cities to choose whether their specific plans will be policy-oriented (adopted by resolution) or regulatory (adopted by ordinance). This Specific Plan is a policy document. All zoning related portions (i.e. land use designations, permitted uses and development criteria) are provided in the way of policy guidelines. Other portions (i.e. goals and policies, design guidelines, focus area plans and streetscape design concepts) are also provided as City policies aimed at providing direction for future planning and public improvement efforts.

D. California Environmental Quality Act Compliance

In compliance with the requirements of the California Environmental Quality Act, a Mitigated Negative Declaration for the North Montclair Specific Plan has been completed.

(To be completed)

E. The Planning Process

The North Montclair Specific Plan evolved through a process organized around a series of three public workshops held in September and November 1993, and February 1994. Prior to the workshops, the City mailed special informational brochures to all property owners in the planning area (see Appendix A). A special (unscheduled) workshop was held between the second and third public workshops for the residents of the Lindero/Geneva subdivision in order to provide additional time discussing alternative development scenarios for this area. All residents of the area were notified of the special workshop by direct mail notices.

The first public workshop focused on orientating the participants to what a specific plan is; the major objectives of the North Montclair Specific Plan; a discussion of key planning opportunities and constraints; and a visual preference slide presentation. The slide presentation was aimed at soliciting participant's responses to various qualities of urban design (architecture, building height/mass, site design, etc.). Each workshop attendee recorded his or her preferences for a variety of building forms and types on a rating sheet which was later tabulated by the consultant.

Based on input from the first workshop, a draft set of goals and objectives was prepared by the consultant/City staff team and presented to attendees at the second public workshop. A series of land use alternatives were also prepared and presented along with conceptual site plans for 16 key opportunity sites. Workshop participants reviewed the plans and provided input.

Between the second and third public workshops, progress meetings were held with the Planning Commission and City Council.

Material that had been presented at the previous workshops was reviewed and each body provided further input to the planning process.

The third public workshop focused on reviewing and confirming the preferred land use alternative. The consultant presented the preliminary urban design/streetscape plan for all major street segments and for the proposed pedestrian connection between the Montclair Transcenter and Montclair Plaza Regional Mall. Participants reviewed the presentation materials and provided input. With the draft Specific Plan completed, the City Council held a joint public workshop with the Planning Commission to allow further public comment on the draft plan. Following the workshop, the draft Specific Plan was revised to incorporate workshop input, and the public hearing draft Specific Plan was prepared.

The Planning Commission held two public hearings to receive input on the Plan. The Commission recommended adoption of the North Montclair Specific Plan to the City Council.

F. Specific Plan Contents

The Specific Plan contains six chapters as follows:

- I. *Introduction* provides broad overview of the plan;
- II. *Planning Framework* describes planning area issues, opportunities and constraints and then identifies overall goals and implementation policies/programs which establish the framework for the land use plan, development standards, design guidelines and implementation mechanisms.
- III. *Development Criteria* translates framework goals and policies into a specific land use plan and associated development criteria, including preferred development scenarios for 16 key opportunity sites/areas.
- IV. *Design Guidelines* sets forth discretionary guidelines to guide the design of appropriate development including architectural characteristics, site planning, parking, landscaping and signs for commercial, business park and light industrial projects.
- V. *Urban Design Streetscape* sets forth design standards for streetscape improvements within the public rights-of-way and to provide a specific design context for adjacent private outdoor spaces.
- VI. *Implementation And Administration* provides a summary of public improvements and costs, recommendations for a capital funding program and a review of the Plan's relationship to the General Plan, Zoning Ordinance and Redevelopment Plans.

CHAPTER II

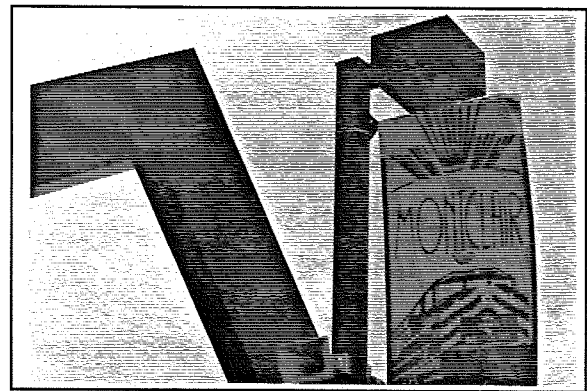
PLANNING

FRAMEWORK

Planning Context

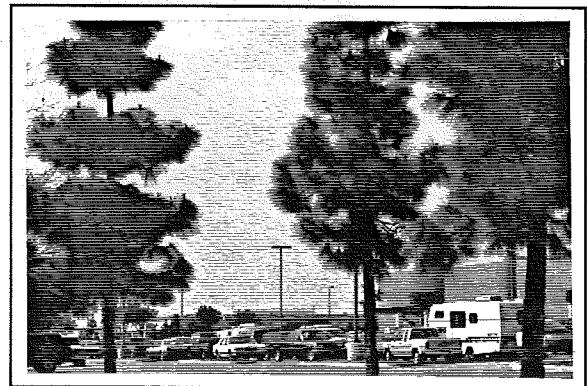
A. Introduction

The Planning Framework chapter contains the essential components of the Specific Plan that provide the overall policy direction upon which more detailed components (i.e. land use, development regulations, design guidelines, streetscape plan, etc.) of the Plan are based. The Planning Framework chapter provides the bridge between the policies of the General Plan and the recommended implementing actions of this Plan.



B. Existing Setting

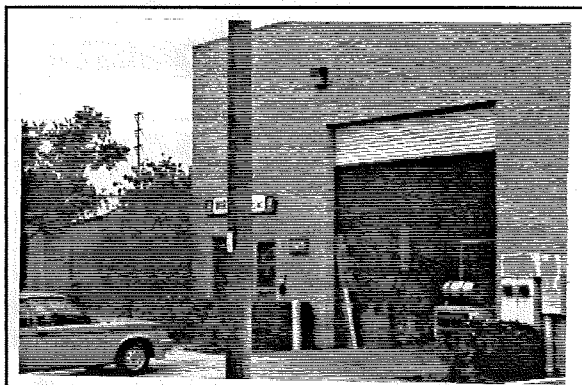
The North Montclair Specific Plan area consists of approximately 640 acres; one square mile. The area is generally bounded on the north by the north City boundary, on the south by Palo Verde Street and the I-10 Freeway, on the east by Benson Avenue (east City boundary), and on the west by the San Antonio Flood Channel and the west City boundary (see Project Area Map on page I-2).



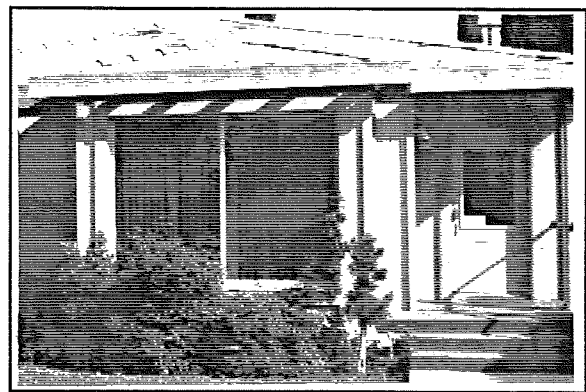
1. Land Use Summary

The primary land use in the Specific Plan area is retail commercial with the Montclair Plaza Regional Mall serving as the primary commercial focus. Land uses adjacent to the mall, primarily along Central Avenue and Moreno Street, are also regional serving commercial. This is the fastest growing land use in the area and demand appears strong for additional retail space, especially in close proximity to the Montclair Plaza. Also, the recent improvements to Monte Vista Avenue at the AT & SF Railroad underpass and future connection to the Foothill Freeway will improve access to the area from the north. Improvements to the I-10 Freeway underpasses at Monte Vista Avenue and Central Avenue in conjunction with the I-10 Freeway widening in 1997-99 will improve access to the North Montclair area from the south.

Light industrial uses are the second largest category of land uses in the project area and are primarily concentrated along Arrow Highway. Many of the industrial properties have older, marginal uses which are expected to transition to new commercial and business park development as the area continues to grow and property becomes more valuable.



Several pockets of single family and multiple family residential development are present in the project area. The most notable area is a small subdivision at the northwest corner of Moreno Street and Fremont Avenue. Because of this area's proximity to the Montclair Plaza, some pressure has been felt in the area for commercial development. A Specific Plan of Development adopted in 1981 provided for a transition from residential to commercial uses if 85% of the lots could be assembled. Today, it is recognized that the 85% requirement is too high and there is a need to provide an alternative development scenario.

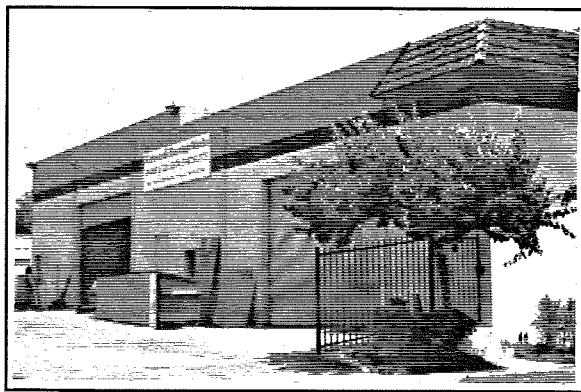


2. Development Pattern

The general development pattern throughout the Specific Plan area can be categorized as "suburban"; small, medium and large scale freestanding buildings with irregular, sometimes large, front and side setbacks. The buildings do not generally define streets as spaces. Distances between buildings are large, and for the most part adjacent developments are not interconnected, thereby discouraging pedestrian circulation and requiring increased use of automobiles.

The suburban-type development pattern also does not lend itself to the creation of identifiable districts. Everything tends to look the same from one area to the next and from one city to the next. In order to create a unique, identifiable image for the North Montclair area, the use of a strong urban design streetscape program will be necessary. Over time, as new development patterns emerge as a result of this Specific Plan, the combination of coordinated development patterns and streetscape will establish a more coherent urban design image for North Montclair.

Another important characteristic of North Montclair's development pattern is the mixture of inconsistent or incompatible development types. These inconsistencies are primarily a result of the area's growth and transition from a residential/ agricultural area to a light industrial area, and more recently, to a regional commercial area. This transition is most evident along Arrow Highway between Fremont Avenue and the San Antonio Wash where pockets of older industrial development on small lots still exist.



3. Planning Focus Areas

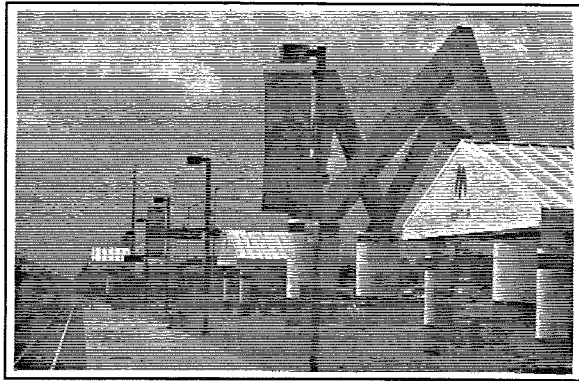
As a result of North Montclair's past development pattern and future development opportunities, 16 special focus areas (sites) have been identified for more detailed conceptual planning (see Focus Area Map on page III-38). The 16 areas consist primarily of parcels that are either presently vacant, under-utilized, contain incompatible development (e.g. asphalt batch plant) or provide unique development opportunities (e.g. Fremont Avenue connection to Transcenter).

The appropriate future development/ redevelopment of these areas is viewed by the City as being critical to the fulfillment of its overall vision for the North Montclair area. The specific design concepts for each of the 16 acres is discussed in the Development Regulations chapter on page III-40.

4. Montclair Transcenter

The Montclair Transcenter is a new, multimodal transportation facility located at the north end of the Specific Plan area along Richton Street, just east of Monte Vista Avenue.

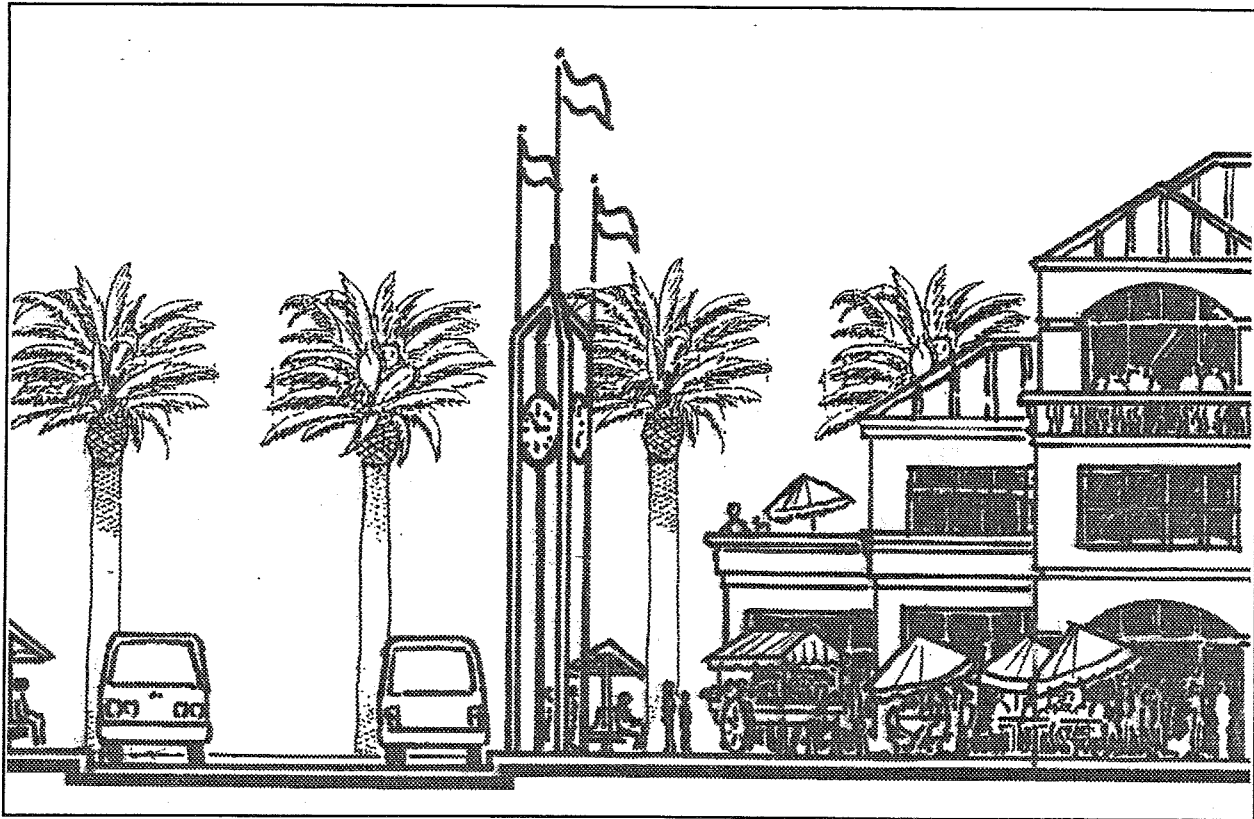
The 20-acre Transcenter is the largest such facility between Union Station in Los Angeles and the San Bernardino Station. It will accommodate approximately 1,500 commuter vehicles and includes a 1.5-acre service commercial area, which will accommodate a child day care center and other commercial/office uses. Refer to Focus Area 4(a) on pages III-50 and III-51.



The Montclair Transcenter is viewed as a very important component in the future development of North Montclair. The Transcenter will not only provide opportunities for the development of commuter related facilities (e.g., child care, retail, offices) within its own site, but will also eventually provide the key element in

the transportation link between the Montclair Plaza Regional Mall and outlying cities. To facilitate this linkage, a viable/convenient connection between the Transcenter and Plaza must be established as part of the overall "vision" for North Montclair.

It is envisioned that some type of "people mover" would ultimately be desirable to link the Montclair Transcenter and Montclair Plaza. However, the achievement of this goal will take time and several phases of transportation services will be necessary so that the system maintains the flexibility to grow as demand increases. It is envisioned that the phases would progress from a simple pedestrian link to possibly a peak-hour shuttle, then to a more frequent trolley/bus service, and ultimately to a fixed location people mover system.



The speed at which the succession from one transportation phase to the next occurs will depend primarily on two important factors. First, the Metrolink will need to expand service to Montclair to the extent that the Montclair Transcenter becomes a destination in its own right - not just a commuter station for westbound passengers.

Second, a greater concentration and density of commercial and office uses will be required at, and around, the Montclair Plaza.

This Plan proposes a strong pedestrian precinct along Fremont Street, as the most logical and direct connection between the Transcenter and Montclair Plaza. The circulation plan would link the Transcenter across the rail right of way, across the commercial property immediately to the south, then down Fremont Avenue to the internal circulation system of the Plaza. Various transit stops would be made along the route to encourage shopping and to accommodate rider accessibility. The goal is to make the pedestrian area of the Plaza part of the general structure of the community, with shopping arcades, sidewalks and transit extending beyond the private mall toward other sub centers such as the Transcenter.

Future planning at the Plaza should endeavor to "connect" to the community by extending structures and internal pedestrian circulation patterns closer to the intersection of Moreno and Fremont. A circulation "node" should be planned where a variety of transportation modes would meet.

5. Existing Specific Plans

Within the North Montclair Specific Plan area there are two existing specific plans. The essence of their requirements are described below.

Specific Plan 73-4 covers approximately 3.8 acres at the southeast corner of Monte Vista and Moreno Avenues, adjacent to the Montclair Plaza (Focus Area 11). The uses proposed in Specific Plan 73-4 have failed to be implemented. The Plan indicates that the properties shall be "regional/commercial and compatible with the existing regional use." The site is comprised of several individually owned lots. Development of one lot at a time (rather than the entire site all at once) is allowable if a development plan is prepared for the entire area, it is oriented to the Montclair Plaza, and if all of the property owners in the area agree with the provisions of the plan. These provisions should continue. Refer to Focus Area II on page III-68.

Specific Plan 81-2 encompasses approximately 10 acres between Olive and Moreno Streets and from 600 feet east of Monte Vista to Fremont (Focus Area 7). The uses proposed in the Specific Plan have failed to be implemented, and the existing uses described in the plan (40 single family residences) currently remain on the site.

Specific Plan 81-2 indicates that the proposed uses for the site are commercial and medium density residential. The Plan requires that at least 85 percent of the lots within the Specific Plan area be assembled and reverted to acreage prior to any development, that excluded parcels must have direct access onto the three existing streets, and that the block area between Arrow Highway and Moreno, and Monte Vista and Fremont be considered as a unit.

The requirement of assembling 85 percent of the lots prior to development constitutes a substantial constraint to the development of this area for commercial uses. The development criteria for this Specific Plan should be reevaluated to provide a variety of smaller lot combinations that allow commercial

development while protecting the remaining single family residences. Refer to Focus Area 7 on page III-60.

C. Planning Issues

In order to achieve the goals of this Specific Plan (refer to Chapter I on page I-1), it will be necessary to address a number of specific "issues" that have been identified. In some instances the issues are presented as obstacles to development that must be overcome. In other instances they are clearly opportunities that can be taken advantage of to further the goals of the Plan.

After an analysis of the existing physical conditions within the Specific Plan area and a review of all relevant documents, the following issues have been identified:

1. The area lacks a cohesive design image that identifies it as a special place. A variety of methods should be developed to help improve the physical appearance/image of the area, including new streetscape designs.
2. The Montclair Plaza Regional Mall continues to be the major commercial attraction in the area. Over time, its presence has stimulated a transition in the area from light industrial uses to increased retail and business park uses. The recognition of this trend requires a reexamination of land use policies in the area.
3. As the North Montclair area continues to grow and redevelop, mechanisms will need to be put in place to help ensure that new development is of high quality and is complimentary to the Montclair Plaza. Design guidelines for architecture, site planning and signs

should address quality and compatibility issues.

4. Areas of incompatible land use and underutilized parcels have been identified which will require more detailed planning to help them transition to compatible development sites. Sixteen special "focus areas" have been identified and are discussed in detail beginning on page III-37.
5. The Montclair Transcenter presents opportunities to develop the commercial area within its boundaries and to create a connection to the Montclair Plaza. Fremont Avenue should provide a strong pedestrian link to the Transcenter and offer pedestrian oriented shopping.
6. During the preparation of this Specific Plan the idea of providing pedestrian bridges between the Montclair Plaza and adjacent commercial centers (e.g., Montclair East) was presented. The idea was explored with the management of the Montclair Plaza and it was decided that bridge connections do not appear to be practical at this time, or for the foreseeable future, because of the incompatibility of surrounding commercial development (primarily off-price retail) and the low density of retail floor space at the adjacent centers. In the future, if uses around the Plaza intensify and change to be more compatible with the Plaza (including office uses) bridge connections may become feasible, as they are now in more dense urban downtown areas.

Goals, Policies And Programs

A. Introduction

The purpose of this section of the North Montclair Specific Plan is to provide the policy direction that will help guide the development of subsequent, more detailed sections of the Plan (i.e. land use and development recommendations, design guidelines, etc.)

The importance of the Goals section is two-fold. First, it provides a list of the City's essential ideas about how the North Montclair Specific Plan Area should develop in the future. Secondly, the goals (and their implementing policies/actions) provide a means of testing new ideas, proposals, and suggestions to ensure that they support the goals of the Plan. Used in conjunction with the Land Use Plan, the goals will help to provide a basis for judging whether a project will be a contribution or a detriment to North Montclair's future.

B. Overall Goals

1. Economic Development - Regional Center

The North Montclair commercial district should continue to grow and develop as the shopping and employment center for the region. The Transcenter should play a key role in the long term development of this important regional commercial district.

2. Image, Identity and Function

The image (visual character) of North Montclair should be strengthened and enhanced through an appropriate combination of urban design techniques in order to create a strong visual identity for the area, improve its function as a regional

commercial center and to create an aesthetically pleasing environment that projects high standards for development.

3. Access, Circulation and Transit

The convenient and safe movement of people and goods within and through North Montclair is a high priority. Circulation improvements should keep pace with new development. The use of public transportation shall be encouraged with special consideration for improving access from the Transcenter to the Montclair Plaza. The use of pedestrian moving elements that connect significant commercial/transit nodes should be considered.

C. Land Use And Development Goals

1. Provide a pattern and appropriate mix of land uses that will complement and strengthen the Montclair Plaza and stimulate further quality commercial and business park development throughout North Montclair.
2. Encourage the upgrading/development of underutilized properties and incompatible uses so that they can achieve their full market potential.
3. Create identifiable land use districts that provide a variety of environments appropriate to the functions/uses of each district and which recognize existing development patterns/trends.

4. Establish a strong pedestrian link between the Montclair Transcenter and the Montclair Plaza along Fremont Avenue (and its extension) and provide for intensified "urban" commercial development within this corridor.

5. Recognize the changing importance of Monte Vista Avenue as a significant transportation corridor serving North Montclair and provide for appropriate adjacent land use development.

6. Provide for transit related uses within and adjacent to the Montclair Transcenter in order to further encourage its use and development.

Implementing Policies/Actions

- Identify key development opportunity sites that are presently vacant or underutilized and prepare conceptual site development criteria for each area which address the unique opportunities and constraints of each.
- Prepare a conceptual development plan for the Montclair Transcenter.
- Provide appropriate development policies and design guidelines that clearly support the City's expectation for quality development.
- Provide development criteria to help ensure the compatibility between existing and new development through well designed buffers, landscaping and building setbacks.

- Formulate appropriate incentive programs (i.e. density bonus, parking reduction, etc.) which encourage quality development consistent with the conceptual site development criteria for key opportunity sites (focus areas).
- Provide for the expansion of commercial uses in the vicinity of Monte Vista Avenue and Arrow Highway.
- Ensure that retail uses, including restaurants and theaters, occupy at least 80% of the ground level frontage of the pedestrian precinct (Fremont Avenue and its extension) between the Montclair Plaza and the Transcenter.

D. Urban Design Goals

1. Create a high quality, distinct urban design image for North Montclair that is attractive and that helps tie the various functional components of the area together to create a strong sense of place.
2. Utilize a variety of streetscape amenities to define a hierarchy of circulation corridors and land use districts.
3. Ensure that new development and rehabilitation of existing structures is of high quality and makes a positive contribution to the overall appearance of North Montclair.
4. Enhance pedestrian circulation opportunities and make walking safer and more comfortable using a variety of techniques.

5. Create a strong visual, pedestrian link between the Montclair Transcenter and Montclair Plaza.

Implementing Policies/Actions

- Develop an Urban Design Framework Plan to provide the organizing element for the more detailed streetscape designs and design guidelines for architecture and site planning.
- Develop a comprehensive set of design guidelines based on development-type (e.g. commercial, business park, etc.) in order to implement the Urban Design Framework Plan and encourage high quality development.
- Create a street tree planting concept that establishes a hierarchy of streets appropriate to their function.
- Utilize the hierarchy of streets to establish primary and secondary entrances into the North Montclair commercial area. Enhance the entries with appropriate streetscape materials to create a special sense of arrival to the area.
- Create pedestrian activity nodes at major intersections to establish an identity for the area and enhance pedestrian safety. Use special paving materials, planting designs, lighting, signing, works of art and architectural features.
- Use architectural elements, buildings and plantings to frame specific views within the area and distant views to the mountains.

vehicular entrance statements for the area.

- Provide development incentives for the provision of urban design amenities within private projects (e.g. landscaped open space, plazas, pedestrian links, enhanced project entries, etc.).

E. Circulation And Transportation Goals

1. Ensure that North Montclair is easily accessible from all parts of the surrounding region.
2. Provide North Montclair with a well defined, safe and efficient circulation system for vehicles and pedestrians.
3. Encourage the use and further development of public transportation systems.
4. Ensure that future traffic circulation improvements are timed to keep pace with proposed development and growth in the region.
5. Provide pedestrian safety and comfort through appropriate public improvements.

Implementing Policies/Actions

- Encourage the joint use of access drives and parking facilities between abutting uses whenever feasible.
- Extend Richton Street westerly across Monte Vista, then align with the abandoned railroad tracks and First Street to Claremont Boulevard.
- Extend Fremont Avenue or equivalent public access right-of-way (easement)

from Arrow Highway north to the Montclair Transcenter.

- Design Fremont Avenue as a pedestrian friendly urban street to create a walking precinct between the Transcenter and Montclair Plaza.
- Require new development projects to provide bus stops and bus turn out lanes when appropriate.
- Develop a comprehensive public sign program to provide clear and distinct directional information for motorists.

F. Environmental Goal

Maintain a high quality of environment throughout North Montclair by balancing the impacts of development with protection for the environment.

Implementing Policies/Actions

- Provide development criteria and design guidelines that address environmental concerns including the need to maintain an aesthetically pleasing environment.
- Through the project review process, establish conditions of approval and project mitigation measures to ensure that new development is an asset to North Montclair and does not decrease the environmental quality of the area.

- Recognize that existing mature trees are an important element in the North Montclair environment and preserve them, to the greatest extent feasible, whenever new public or private development occurs.

CHAPTER III

DEVELOPMENT

CRITERIA

Introduction

A. Purpose Of The Development Criteria

This chapter establishes the proposed land use districts within the North Montclair Specific Plan area and the development criteria that apply within each district. Together, the land use districts and the development criteria provide the general development provisions for the Specific Plan area.

The development criteria, together with the design guidelines (following chapter) implement the goals of the Specific Plan. They are consistent with the goals and policies of the General Plan and, in most cases, with the requirements of the Zoning Ordinance. Where the development criteria of the Specific Plan are different from the standards in the Zoning Ordinance, the Zoning Ordinance will be amended to implement the Specific Plan.

B. Organization Of The Development Criteria

This chapter contains the following sections:

1. **General Provisions**
2. **Land Use**
3. **Site Development Criteria**
 - a. General Criteria
 - b. Landscaping
 - c. Parking
 - d. Loading
 - e. Outdoor Storage
 - f. Performance Standards
4. **Development Criteria For Special Uses**
 - a. Automobile Sales
 - b. Automobile Repair Facilities
 - c. Convenience Stores
 - d. Drive-Thru Establishments
 - e. Service Stations
5. **Development Criteria For Focus Areas**

General Provisions

A. Relationship To Zoning Ordinance

Development criteria contained in this chapter will be incorporated into the City of Montclair's Zoning Ordinance. If any of the regulations contained herein conflict with the existing provisions of the Zoning Ordinance, the criteria contained herein shall take precedence and the Zoning Ordinance will be amended. Any development criteria or land uses not specifically covered herein shall be subject to the requirements of the Zoning Ordinance.

B. Interpretation

The Director of Community Development shall have the responsibility to interpret the provisions of this Specific Plan.

C. Definitions

Words, phrases and terms not specifically defined herein shall have the same definition as provided for in the Montclair Zoning Ordinance.

When used in this Specific Plan, the term "Director" shall mean the Director of Community Development or an appointed representative.

When used in this Specific Plan, the term "Commission" shall mean the City of Montclair Planning Commission.

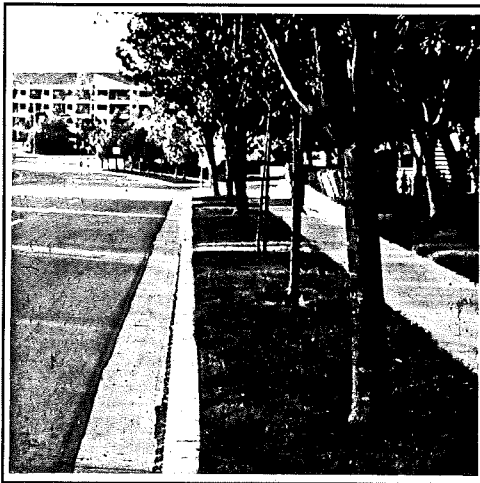
D. Development Review

To ensure compliance with all applicable policies, land use provisions, development criteria, and design guidelines contained in this Specific Plan area will be reviewed by the appropriate review authority (Director or Commission) using the recommended

development criteria and design guidelines contained herein. The Specific Plan shall provide policy direction for the approval of discretionary permits in addition to the requirements set forth in the Zoning Ordinance.

E. Installation Of Public Improvements

Project developers are responsible for providing adjacent public improvements (e.g., curbs, gutters, sidewalks, street lighting, parkway landscaping, street trees, and other similar items) in compliance with standards of the City at the time of development.



Land Use

A. Establishment Of Land Use Districts

The North Montclair Specific Plan area is comprised of 6 land use districts as indicated on the Land Use Map on the following page.

Each of the land use districts coincides with the same zone contained in the City of Montclair Zoning Ordinance. The purpose/intent of each district is as follows:

1. Small Lot Single Family Residential Zone (R1/SL)

The Small Lot Single Family Residential Zone is intended to increase development options within areas which may otherwise be difficult to develop/redevelop due to existing lot configurations, access and development patterns. Only one dwelling per lot is permitted.

2. Single Family Residential Zone (R1)

The R1 (Single Family Residential) Zone is intended as a district of single family homes with not more than one dwelling and customary accessory building permitted on a lot.

3. Multiple Family Residential Zone (R3)

The R3 (Multiple Family Residential) Zone is intended to provide appropriate areas for the provision of multiple family dwelling types in order to promote comfort, desirable living areas and permanency for residents.

4. Restricted Commercial Zone (C2)

The C2 (Restricted Commercial) Zone is intended to provide for neighborhood retail and service oriented business activities serving a localized need under development standards which ensure compatibility and harmony with adjoining residential neighborhoods.

5. General Commercial Zone (C3)

The C3 (General Commercial) Zone is intended to provide for a broad range of commercial activities, including retail sales, office, personal service and related commercial uses that serve a community wide and regional population.

6. Industrial Park Manufacturing Zone (M-I-P)

The M-I-P (Industrial Park Manufacturing) Zone is intended to provide an appropriate physical environment for the establishment of light manufacturing and business services, including: assembling, fabricating, processing and compounding; research and development; servicing of light vehicles; administrative offices; sale of materials/ products wholly or partially manufactured on the site; and sale of goods and services required for the convenience of persons employed or living in the area. Retail and wholesale uses may be permitted or conditionally permitted if they do not occupy more than 50% of the building/property.

All uses shall be conducted within a completely enclosed building. Uses permitted in this zone do not produce odors, noise, vibration or particulate which adversely affect other uses on or off the site. Uses which store or handle hazardous, highly flammable or explosive materials are not permitted in this zone.

7. Institutional Zone (I)

The I (Institutional) Zone is intended to recognize areas of existing institutional-type uses and provide for their continuance in appropriate areas. Examples of allowed uses include: adult day care, residential care, religious assembly, medical/outpatient clinic, funeral parlor and similar uses.

B. Land Use Matrix

The Land Use Matrix (Table 3-1) provides the recommended uses in each of the six land use categories within the Specific Plan area. The provisions contained in this section apply not only to the primary use of property, but also to each accessory and support use.

P = Permitted Uses

C = Conditionally Permitted Uses

In addition to the above symbols, references to special notes have been included on the following pages. These notes appear at the end of the matrix on Page III-11.

Key To Land Use Matrix:

R1/SL = Single Family Small Lot

R1 = Single Family

R3 = Multiple Family

C2 = Restricted Commercial

C3 = General Commercial

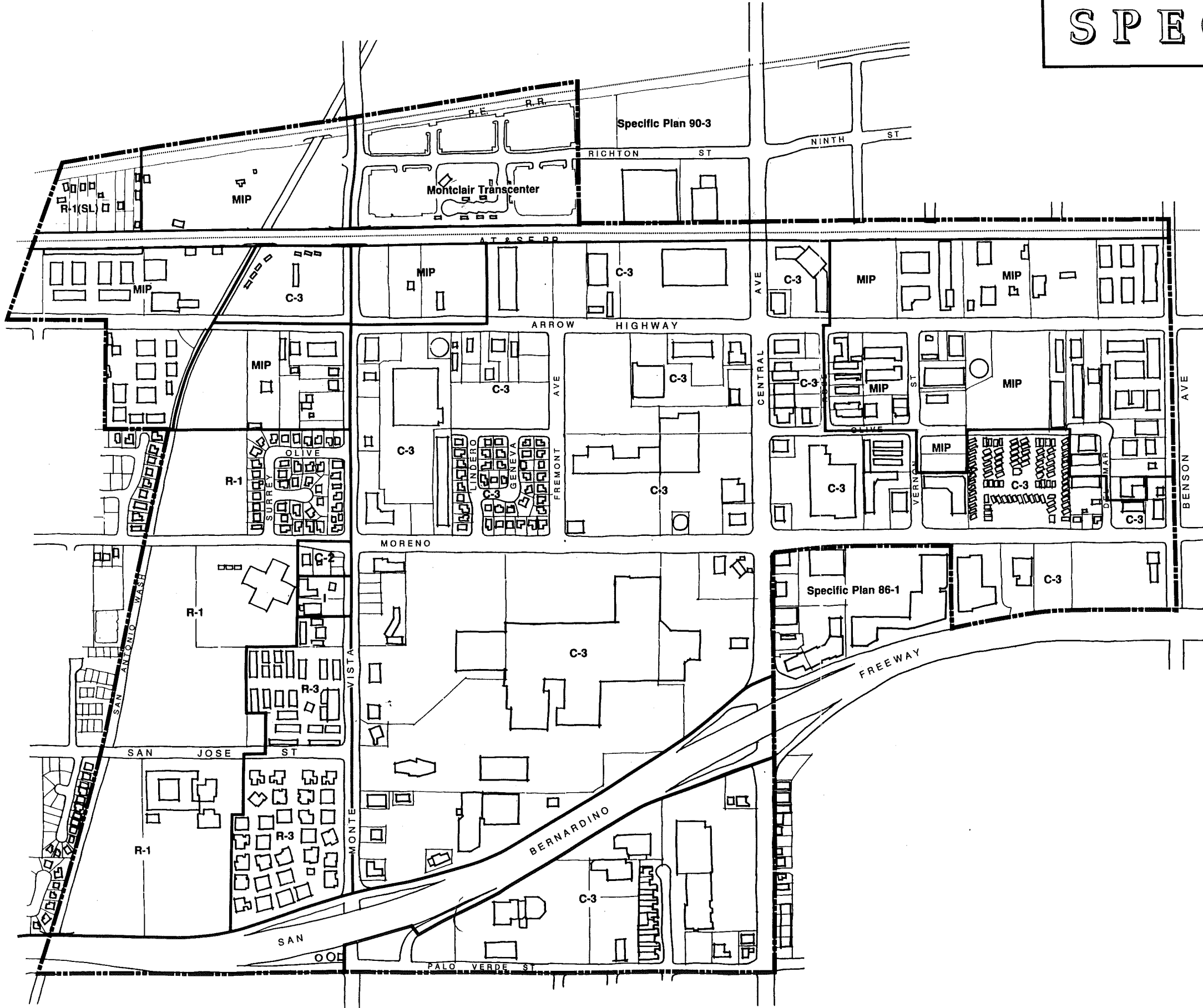
MIP = Industrial Park Manufacturing

I = Institutional

NORTH MONTCLAIR SPECIFIC PLAN

Land Use Plan

- Legend
- R-1** Single Family
 - R-3** Residential Medium Density
 - C-2** Restricted Commercial
 - C-3** General Commercial
 - MIP** Industrial Park Manufacturing
 - I** Institutional



**TABLE 3-1
LAND USE MATRIX**

LAND USE ACTIVITY	R1/ SL	R1	R3	C2	C3	MIP	I
1. Residential Uses							
a) Multi Family Dwellings			P				
b) Single Family Dwellings	P	P	P				
c) Two Family Dwellings (detached/attached)			P				
d) Uses and Structures Incidental to Primary Use	P	P	P				
2. Administrative And Professional Offices							
Establishments where the administrative, clerical and managerial functions of a business or industry are conducted or where members of a profession conduct their practice.				P	P	P	
3. Automotive Related Uses							
a) Auto Parts Sales - No Installations					P	P ¹	
b) Auto Parts Sales - With Installations					P	P ¹	
c) Auto and Light Truck Repair (i.e., bodywork, engine and drive train, painting and miscellaneous work, excluding heavy truck repair)					P	P ¹	
d) Car Washes					C	C	
e) Fuel/Service Stations					C	C ²	
f) Vehicle Impound Yards						C	
g) Vehicle Leasing/Rental					C	C	
h) Vehicle Sales (car, RV, truck and boat)					C	C ¹	
4. Boarding And Lodging Facilities							
Commercial establishments which provide boarding and lodging facilities, including hotels/motels and associated conference facilities.					C		

**TABLE 3-1
LAND USE MATRIX (CONTINUED)**

LAND USE ACTIVITY	R1/ SL	R1	R3	C2	C3	MIP	I
5. Eating/Drinking Establishments Commercial establishments which serve prepared food or beverages for consumption on or off the premises. Examples of allowable uses include, but are not limited to, the following:				P	P	P ¹	
a) Delicatessen/Sandwich Shop							
b) Night Clubs/Bars/Lounges					C		
c) Restaurants - No Drive-Thrus ³				P	P	P ¹	
d) Restaurants - With Drive-Thrus					C	C ¹	
e) Restaurants - Serving Alcoholic Beverages				C	C	C ¹	
6. Entertainment/Recreation Establishments							
a) Auditoriums, Convention Halls and Theaters					C		
b) Commercial Recreation Facilities					C		
7. Financial							
Commercial establishments which engage in monetary transactions not directly related to the sale of a product/service (i.e., banks, savings and loans, etc.)				P	P	P	
8. Medical/Care Facilities Commercial establishments which provide needed services of a medical/care nature. Examples of allowable uses include, but are not limited to, the following:							
a) Child Day Care (up to 6 children) ⁴	P	P	P	P	P	P	P
b) Child Day Care (7 or more children) ⁴	C	C	C	C	C		C
c) Doctors/Dentists Offices					P		P
d) Outpatient - Treatment Programs					C		P
e) Residential Care Facilities (convalescent care)			C				P

**TABLE 3-1
LAND USE MATRIX (CONTINUED)**

LAND USE ACTIVITY	R1/ SL	R1	R3	C2	C3	MIP	I
9. Personal Service Commercial establishments which provide services of a personal nature. Examples of allowable uses include, but are not limited to, the following:							
a) Barber/Beauty/Nail Shops				P	P	P ¹	
b) Dance School/Karate Studio					C		
c) Dry Cleaners				P	P	P	
d) Health/Athletic Clubs					C		
e) Laundromat (retail only)				P	P		
f) Spiritual Consultant (fortune teller)					C		
10. Retail Commercial Commercial establishments which sell merchandise generally needed/desired by the residents, employees and visitors of/to the community. Examples of allowable land use activities include, but are not limited to, the following:							
a) Art Galleries and Studios				P	P		
b) Convenience Stores				C	C	C ¹	
c) Furniture (new only)				P	P	P ¹	
d) Flower/Gift Shops				P	P	P ¹	
e) General Merchandise (i.e., apparel, supermarkets, drug stores, etc.)				P	P	P ¹	
f) Home Improvement Center					P	P ¹	
g) Home Improvements with Outdoor Display of Lumber, Garden and Nursery Items					C	C ¹	
h) Liquor Stores					C		
i) Nurseries/Garden Supplies with Outdoor Display					C	C ¹	

**TABLE 3-1
LAND USE MATRIX (CONTINUED)**

LAND USE ACTIVITY	R1/ SL	R1	R3	C2	C3	MIP	I
j) Office Supplies/Equipment				P	P	P ¹	
k) Pet Stores				P	P	P ¹	
l) Specialty Food Stores (no alcohol sales)				P	P		
m) Thrift Stores, Antique Shops, Used Merchandise					C		
11. Service Commercial Commercial establishments which provide business, repair and miscellaneous services. Examples of allowable uses include, but are not limited to, the following:							
a) Bakery Establishments				P	P	P	
b) Cleaning/Janitorial						P	
c) Copy Center/Postal Service and Blueprinting				P	P	P	
d) Equipment Rental/Sales/Service					C	C	
e) Interior Design Shops				P	P	P	
f) Laboratories (i.e., film, medical and dental, "R & D," etc.)					C	P	
g) Miscellaneous Light Repairs (indoors only)					P	P	
h) Printing Facilities					P	P ¹	
i) Recycling Facilities				C	C	C	
j) Veterinary Services/Hospitals					C	C	
12. Industrial Uses							
a) Assembly, Light (small appliances, computers)						P	
b) Distribution (includes only items manufactured/assembled on the site; • excludes truck terminals)						P	
c) Manufacturing, Light (cabinets, clothing, instruments, computers, pharmaceuticals, bakery, signs)						P	

**TABLE 3-1
LAND USE MATRIX (CONTINUED)**

LAND USE ACTIVITY	R1/ SL	R1	R3	C2	C3	MIP	I
d) Laboratories (medical, mechanical, optical)						P	
e) Processing (bottling, cleaning/dyeing, packaging; except hazardous materials)						P	
f) Research/Development (design of new products)						P	
g) Repairing (appliances, electronic equipment, plumbing/electrical shops)						P	
h) Storage (household items, office files, self-serve storage [indoors only])						P	
i) Treatment, Light (non-toxic coating/plating)						P	
j) Wholesaling (household goods, office supplies, electronic components)						P	
k) Welding, Machine and Sheet Metal Shops						P	
13. Other							
a) Assembly Facilities (clubs, lodges, meeting halls)					C		
b) Bus and Rail Facilities						P	
c) Funeral Parlors/Mortuaries					C		C
d) Ice and Cold Storage Plants						C	
e) Parking Structures					P	P	
f) Parks and Playgrounds	P	P	P	P			
g) Private Schools (trade schools)					C	C	
h) Public Schools		P					
i) Private Schools (parochial, elementary, business)					C		
i) • Public Utility Facilities					C	C	
j) Religious Facilities	C	C	C	C	C		C

¹ Retail or wholesale uses are permitted or conditionally permitted provided they do not occupy more than 50% of the building/property area and provide the appropriate parking ratio for commercial use.

² Subject to the provisions of Section 9-6.101 of the Municipal Code.

³ Subject to the provisions of Section 9-6.200 of the Municipal Code.

⁴ Subject to the provisions of Section 9-4.1816 of the Municipal Code.

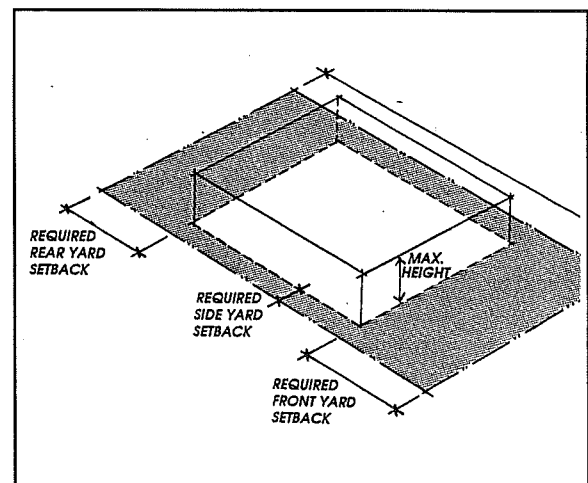
Site Development Criteria

A. General Criteria

The Development Criteria Matrix (Table 3-2) provides recommended development criteria for land uses in the North Montclair Specific Plan area. These criteria cover all property within the Specific Plan area.

Certain uses and activities, due to their nature, have the opportunity to create more significant impacts upon the community than others. As a result, specific regulation of these uses is warranted. Therefore, in addition to the general development criteria in this section, the following uses should be subject to the additional criteria outlined in the Development Criteria for Special Uses section beginning on Page III-33.

- *Automobile Sales;*
- *Automobile Service and Repair Facilities;*
- *Convenience Stores;*
- *Service Stations; and*
- *Drive-Thru Establishments.*



**TABLE 3-2
DEVELOPMENT CRITERIA MATRIX**

SUMMARY OF DEVELOPMENT CRITERIA						
	R1/ SL	R1	R3	C2	C3	MIP
1. Minimum Lot Area (Square Feet)	4,500	7,500	3 Net Acres	10,000	10,000	30,000
2. Dwelling Units/Lot	1	1	7-11 ¹ DU/AC	N/A	N/A	N/A
3. Minimum Lot Width: • Interior Lot • Corner Lot	50 ² 55	60 ² 65	100 ² 100	60 65	60 65	150 150
4. Minimum Lot Depths	— ³	100	130	100	100	190
5. Maximum Lot Coverage	40%	35%	— ³	50%	50%	45%
6. Minimum Depth of Front Landscape Setback	20	25	25	— ⁴	— ⁴	— ⁴
7. Minimum Front Building Setback	20	25	25 ⁵	35 ⁶ or 75	35 ⁶ or 75	35 ⁶ or 75
8. Minimum Side Yard Setback: • Interior Lot • Corner Lot	5&8 5&15	5&12 5&15	10 ⁵ 20 ⁵	— ⁷	— ⁷	— ⁷
9. Minimum Rear Yard Setback	15 ⁵	15 ⁵	10 ⁵	— ⁷	— ⁷	— ⁷
10. Maximum Building Height (Story)	— ³	35 (2)	35		75	50 (4)

¹ Refer to Article 9 (Section 9-4.906) of the Municipal Code.

² Less width allowed on cul-de-sacs.

³ To be determined by site plan review.

⁴ The minimum depth of landscaping along the street frontage varies by street according to the following:

- | | | |
|------------------------------------|-------------------------------|-----------------------------------|
| • Benson Avenue: 15 feet | • Monte Vista Avenue: 15 feet | • Moreno Street: 20 feet |
| • Central Avenue: 20 feet | (I-10 to Moreno) | (Central to Monte Vista) |
| • Fremont Avenue: None required | • Monte Vista Avenue: 15 feet | • Moreno Street: 20 feet |
| except street trees, plaza areas | (Moreno to Richton) | (Monte Vista to San Antonio Wash) |
| and freestanding planters | • Moreno Street: 15 feet | • Palo Verde Street: 15 feet |
| (Refer to Urban Design Streetscape | (Benson to Central) | • Arrow Highway: 20 feet |
| Chapter for details) | | • All Others: 15 feet |

⁵ Greater setbacks required for 2 stories.

⁶ Minimum front building setback is 35 feet if no parking is provided between front property line and building, otherwise the minimum setback is 75 feet unless otherwise provided in the development standards for specific key sites.

⁷ Side and rear yard setbacks vary according to the type of land use district that is adjacent to the proposed project. Refer to the appropriate zoning district in the Municipal Code for specific requirements.

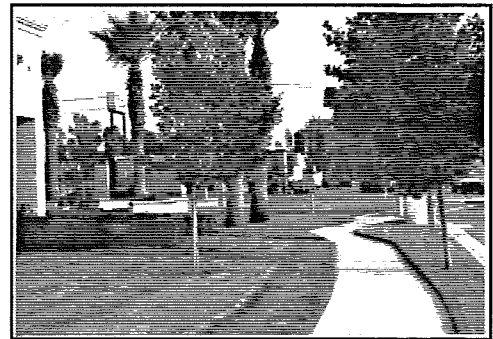
B. Landscape Standards

1. Minimum on-site landscape area for developments within the specific zoning districts shall be as follows:

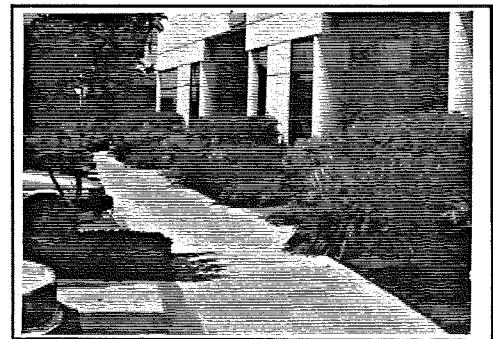
- a. **R1 - Single Family:** The front and street corner side yards of each lot shall be permanently landscaped with trees, shrubs and groundcover.
- b. **R3 - Multiple Family:** All front, side and rear yards shall be permanently landscaped with trees, shrubs and ground cover.
- c. **C2 - Restricted Commercial:** A minimum of 12% of the development site (net area) shall be provided as landscaped area exclusive of parking lot landscape areas and required front yard landscape setback areas.
- d. **C3 - General Commercial:** A minimum of 12% of the development site (net area) shall be provided as landscaped area exclusive of parking lot landscape areas and required front yard landscape setback areas.
- e. **MIP - Industrial Park Manufacturing:** A minimum area of 10% of the development site (net area) shall be provided as landscaped area exclusive of parking lot landscape areas and required front yard landscape setback areas.

2. Landscape areas may include hardscape areas (i.e., pedestrian paths, courtyards, plazas, etc); however, at least 75% of the required landscape area shall be in plant material.

3. The minimum area adjacent to public streets to be maintained in permanent landscaping shall be as provided in Table 3-2, Development Criteria Matrix (page III-14).



4. Building setback areas that are not used for drive entries, parking or approved outdoor uses shall be fully landscaped. All unpaved non-work areas shall be landscaped. All future development phase areas shall be hydroseeded or otherwise appropriately landscaped as approved by the Director.



5. Prior to issuance of building permits or prior to final approval of street improvement plans, as applicable, 3 sets of detailed landscape and irrigation construction drawings shall be submitted for review and approval by the Planning Division. All landscaping irrigation systems shall be installed prior to the granting of final inspection by the City.
 6. Permanent automatic irrigation systems shall be provided in all landscaped areas. To the extent possible, water conserving irrigation techniques, such as drip irrigation, shall be provided.
 7. All landscaped areas shall be maintained in a healthy and thriving condition, free from weeds, trash and debris.
 8. Any parking areas visible from a public or private street shall be screened by a combination of berms (maximum 3:1 slope) planted with trees, shrubs and turf; garden walls; and shrubs.
-
9. Trees shall be planted throughout the project in areas of public view, predominantly adjacent to and along structures and street frontages.
 10. Landscape areas shall be a minimum of 4 feet wide (excluding curbs).
 11. Trees and shrubs shall be planted so that at maturity they do not interfere with utility service lines, street lighting, traffic safety site areas, on-site signs and basic property rights of adjacent property owners, particularly the right of solar access.
 12. Sizes of the plant materials shall conform to the following minimum mix:

Trees

50% 24 inch box (min. 3" caliper)
 40% 15 gallon (min. 1.5" caliper)
 10% 5 gallon (min. 0.5 caliper)

Shrubs

60% 5 gallon
 40% 1 gallon

Groundcover

100% Coverage within 1 year
 13. Mature specimen trees in 36 inch and 48 inch boxes may be required to be provided in sufficient quantity, subject to the approval of the Director, to provide variety and emphasis at entrances and focal areas within the project.
 14. All 15 gallon trees shall be doubled staked per City standards. All box trees shall have guy wires.
 15. Any tree within 25 feet of a public sidewalk requires a root retainer per City standards.
 16. Concrete mow strips are required to separate all turf areas from other landscaped areas.

17. Appropriate shrubbery and creeping vines are required along all walls and fences adjoining public rights-of-way.
18. The use of water-efficient, drought tolerant and native plant material is strongly encouraged.
19. Plants with similar water requirements shall be grouped together into hydrozones. Plant locations shall consider slope and sun exposure.
20. The maximum allowable turf area shall be 20% of the total area landscaped except for detached single family projects.

C. Parking Requirements

1. Number of Parking Spaces Required

The minimum off-street parking requirements for uses within the North Montclair Specific Plan area shall be as provided in either Table 3-3, below or Table 3-4 on page III-19).

Use Table 3-3 in the absence of precise floor plans, ultimate user or mix of uses.

Use Table 3-4 when the ultimate land use or mix of uses is known, especially where a greater percentage of more parking intensive uses are proposed.

**TABLE 3-3
GENERAL PARKING REQUIREMENTS**

LAND USE DESIGNATION	PARKING REQUIREMENT
Business Park	1 space per 350 square feet of gross floor area (GFA); 10% maximum compact spaces; 50% maximum long-term spaces; 10% of the spaces provided shall be designated for carpool and vanpool use. (Based on 60 % of the GFA as manufacturing, 40% as office or general retail.)
Commercial	1 space per 250 square feet of gross floor area (GFA); 10% maximum compact spaces; 10% maximum long-term spaces permitted. (Based on 100% of the GFA as general retail.)
Commercial/Office	1 space per 250 square feet of gross floor area (GFA); 10% maximum compact spaces; 35% maximum long-term spaces; 7% of the spaces provided shall be designated for carpool and vanpool use. (Based on 70% of the GFA as office and 30% as general retail.)
Industrial	1 space per 500 square feet of gross floor area (GFA); 10% maximum compact spaces; 25% maximum long-term spaces; 10% of the spaces provided shall be designated for car and van pool use. (Based on 75% of the GFA as manufacturing and 30% as office.)
Mixed Use Development	In limited situations where a combination of uses are developed on a site, including multiple uses under single ownership, the number of spaces required shall be equal to the sum of the requirements for each of the uses, unless a reduction is granted pursuant to a shared parking agreement.

Note: The maximum number of compact and long-term spaces is based on the total number of spaces provided.

**TABLE 3-4
PARKING REQUIREMENTS**

USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES*	MAXIMUM PERCENT LONG-TERM SPACES*	NOTES
Industrial Uses				
Manufacturing	1 space/500 square feet of gross floor area (GFA) up to 30,000. 1 additional space for every 650 square feet from 30,000 to 50,000 square feet. 1 additional space for every 750 square feet over 50,000 square feet, or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	25%	10% of the spaces provided shall be designated for carpool/vanpool use.
Research and Development	1 space/500 square feet of gross floor area (GFA) up to 30,000. 1 additional space for every 650 square feet from 30,000 to 50,000 square feet. 1 additional space for every 750 square feet over 50,000 square feet, or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	25%	10% of the spaces provided shall be designated for carpool/vanpool use.
Wholesale, Warehouse and Distribution	1 space/1,000 square feet of gross floor area (GFA) or 1 space for every 2 employees, whichever is greater. Each site shall have a minimum of 6 parking spaces.	10%	25%	10% of the spaces provided shall be designated for carpool/vanpool use.
Commercial and Office Uses				
Auditoriums, Places of Public Assembly	1 space/6 seats or 1 seat/100 square feet where there are no fixed seats.	10%	None	
Auto Repair and Service	1 space/250 square feet GFA.	10%	None	Spaces may not be used for vehicle sale, service, storage, rental or repair.
Auto Sales	1 spaces/500 square feet of interior and satellite office and show room, plus 2 spaces/service bay.	10%	None	Spaces may not be used for vehicle sale, service, storage, rental or repair.
Auto Service Station	1 space/250 square feet of GFA.	10%	None	

**Note: The maximum number of compact and long-term spaces is based on the total number of spaces provided.*

**TABLE 3-4
PARKING REQUIREMENTS (CONTINUED)**

USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES*	MAXIMUM PERCENT LONG-TERM SPACES*	NOTES
Automobile Washing	3 spaces/washing stall.	10%	None	
Bank, Savings and Loan	1 space/250 square feet of GFA.	10%	25%	10% of the spaces provided shall be designated for carpool/vanpool use.
Barber Shop, Beauty Parlor	3 spaces/station.	10%	None	
Bowling Alley	2 spaces/alley, plus 2 spaces/billiard table, plus 1 space for each 75 square feet of game arcade area, plus required for other uses.	10%	None	10% of the spaces provided shall be designated for carpool/vanpool use.
Business and Personal Services	1 space/250 square feet GFA.	10%	None	
Day Care Center	1 space/employee, plus 1 space/10 children capacity.	10%	None	Parking requirements are to be based upon facility's maximum capacity.
Eating and Drinking Establishment	1 space/4 seats or occupants based on building occupant capacity.	10%	None	
Health Club	1 space/150 square feet of GFA.	10%	None	
Hospital	1 space/2 beds.	10%	25%	
Hotel, Motel	1 space/guest room.	10%	None	
Lumber Yard, Nursery, Hardware Store	1 space/250 square feet GFA retail sales area, plus 1 space/1,000 gross square feet of outdoor display and storage area.	10%	None	
Medical and Dental Clinics	1 space/160 square feet of GFA.	10%	10%	
Offices	1 space/250 square feet of GFA for the first 30,000 square feet. 1 space for every additional 250 square feet up to 50,000 square feet. 1 space for every additional 300 square feet over 50,000 square feet.	10%	35%	10% of the spaces provided shall be designated for carpool/vanpool use.

**Note: The maximum number of compact and long-term spaces is based on the total number of spaces provided.*

**TABLE 3-4
PARKING REQUIREMENTS (CONTINUED)**

USE	MINIMUM REQUIRED OFF-STREET PARKING	MAXIMUM PERCENT COMPACT SPACES*	MAXIMUM PERCENT LONG-TERM SPACES*	NOTES
Retail (General)	1 space/250 square feet of GFA.	10%	10%	
Retail (Furniture, Major Appliances)	1 space/250 square feet of GFA.	10%	10%	
Tennis, Handball, Racquetball Facilities	1 space/court, plus required parking for other on-site uses.	10%	None	10% of the spaces provided shall be designated for carpool/vanpool use.
Theaters (Movie)	1 space/6 fixed seats and 1 space for each 100 square feet used for amusement.	10%	None	
Civic Use Types				
Community Education, Adult	1 space/each 3 person capacity, plus 1 space/employee or faculty.	10%	25%	10% of the spaces provided shall be designated for carpool/vanpool use.
Government Offices	1 space/250 square of GFA.	10%	50%	10% of the spaces provided shall be designated for carpool/vanpool use.
Libraries, Museums, Galleries	1 space/4 seats or 6 occupants based on building occupant capacity.	10%	None	
Religious Assembly	1 space/100 square feet of main assembly/auditorium area, or 1 space/6 seats when fixed seats are provided, whichever is greater.	10%	10%	
Utilities	To be determined case-by-case	10%	None	A parking study shall be submitted for approval by Planning and Engineering.

**Note: The maximum number of compact and long-term spaces is based on the total number of spaces provided.*

2. Maximum Percentage of Compact Spaces and Long-Term Spaces

The maximum percentage of compact spaces and long-term spaces referred to in the parking requirements tables shall be calculated based on the total number of parking spaces required. Compact and long-term spaces shall generally be located towards the peripheral, least used sections of the parking lot.

3. Required Number of Handicapped Parking Spaces

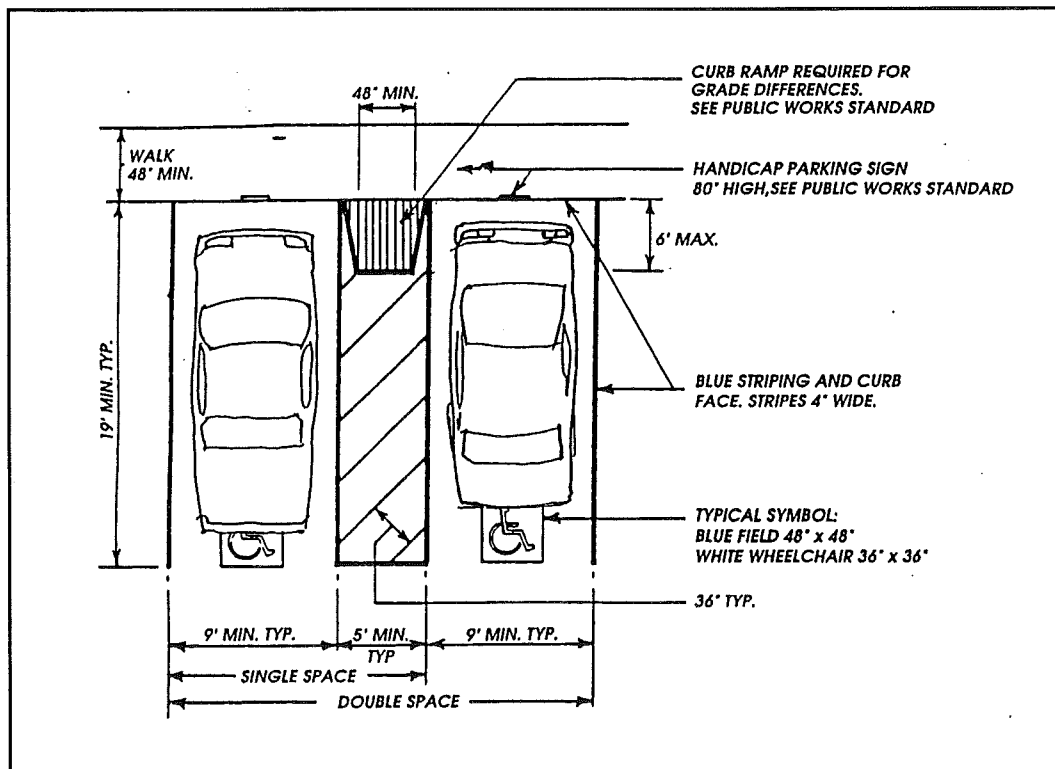
Handicapped parking spaces shall be provided at the rate and configuration required by the State of California (see Figure below).

4. Required Number of Motorcycle Parking Areas

MOTORCYCLE PARKING REQUIREMENTS

Number Of Automobile Spaces	Number Of Motorcycle Parking Areas
1 - 25 spaces	None required
26 - 100 spaces	1 area (56 sq. ft.)
Over 100 spaces	1 area for each additional 100 automobile spaces or majority portion thereof.

Required motorcycle parking areas shall consist of a minimum usable area of 56 square feet and shall count toward fulfilling automobile parking spaces at the rate of one parking space per one motorcycle parking area.



5. **Required Amount and Type of Bicycle Parking**

Bicycle parking for not less than 5% of the number of required automobile parking spaces shall be provided in close proximity to the primary employee entrance(s) they serve.

The location of bicycle parking should be safe, well-lit and of adequate space to accommodate bicycle users.

6. **Use of Required Parking Areas**

Areas required to meet parking requirements may not be used for any purpose other than parking.

7. **Parking Space Dimensions**

Automobile parking spaces shall meet the following design standards:

Full-Sized Space: Minimum width 9 feet; minimum depth 20 feet.

Long-Term Space: Minimum width 8-1/2 feet; minimum depth 18 feet.

Compact Space: Minimum width 8 feet; minimum depth 16 feet.

Parallel Space: Minimum dimensions 10 feet by 22 feet.

8. **Dimensions of Parking Bays and Drive Aisles**

The minimum dimensions of parking bays and maneuvering aisles shall be as shown in Table 3-5, Page III-24.

9. **Parking Area Improvement Standards**

Parking areas shall be improved as follows:

a. *Directional Arrows and Signs*

Aisles, approach lanes and maneuvering areas which are one-way shall be clearly marked with directional arrows as required by the City Engineer.

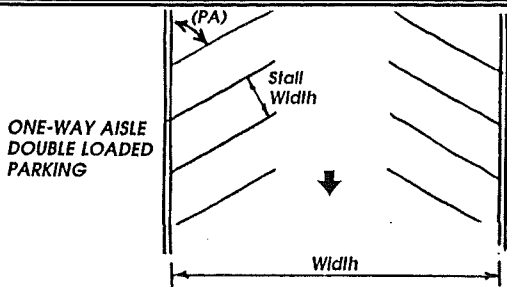
In addition to directional arrows, the Planning and Engineering Divisions may require installation of signs and other devices to ensure safe and efficient vehicular movement.

b. *Drainage*

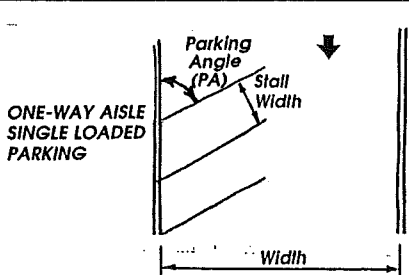
All parking facilities shall be graded and provided with drainage facilities to eliminate surface flow over sidewalks and over driveways within public rights-of-way and comply with federal standards regulating surface flows from private property.

**TABLE 3-5
PARKING BAY DIMENSIONS**

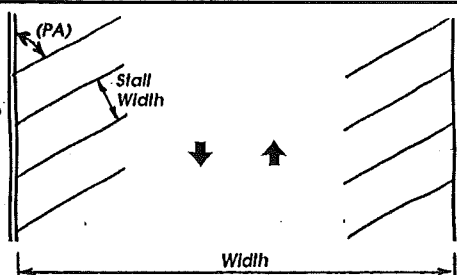
WIDTHS FOR DOUBLE LOADED PARKING AISLES WITH ONE-WAY TRAFFIC

PARKING ANGLE (DEGREES)	8'-0" STALLS	8'-6" STALLS	9'-0" STALLS	
45	55'-0"	56'-0"	57'-0"	
60	59'-6"	60'-6"	61'-6"	
90	65'-0"	66'-0"	67'-0"	

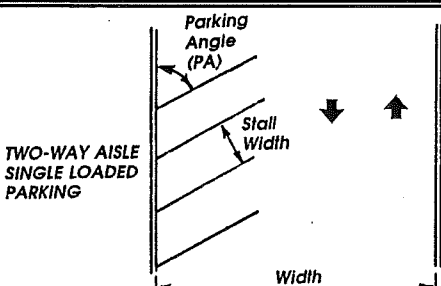
WIDTHS FOR SINGLE LOADED PARKING AISLES WITH ONE-WAY TRAFFIC

PARKING ANGLE (DEGREES)	8'-0" STALLS	8'-6" STALLS	9'-0" STALLS	
45	34'-6"	35'-6"	36'-6"	
60	38'-0"	39'-0"	40'-0"	
90	45'-0"	46'-0"	47'-0"	

WIDTHS FOR DOUBLE LOADED PARKING AISLES WITH TWO-WAY TRAFFIC

PARKING ANGLE (DEGREES)	8'-0" STALLS	8'-6" STALLS	9'-0" STALLS	
45	63'-0"	64'-0"	65'-0"	
60	64'-0"	65'-0"	66'-0"	
90	65'-0"	66'-0"	67'-0"	

WIDTHS FOR SINGLE LOADED PARKING AISLES WITH TWO WAY TRAFFIC

PARKING ANGLE (DEGREES)	8'-0" STALLS	8'-6" STALLS	9'-0" STALLS	
45	36'-3"	38'-6"	43'-9"	
60	37'-9"	41'-0"	45'-0"	
90	45'-0"	46'-0"	47'-0"	

- NOTES:**
1. Parking stall angles different from those shown above may be considered by city on a case-by-case basis.
 2. Where vehicle bumpers will overhang an adjacent landscape planter, parking stall depth may be reduced (2'-0" for 90 degree spaces and 1'6" for all others).

c. *Lighting*

Parking areas shall have lighting to provide adequate illumination for security and safety. The minimum requirement is one footcandle, maintained across the surface of the parking area. The maximum average illumination at the surface in commercial areas is 3 footcandles; manufacturing/business parks 2 footcandles. A parking area lighting study with manufacture's performance specifications may be required by the Director.

Lighting fixtures shall be energy-efficient and standards shall be in scale with the height and use of the on-site structure(s).

Any illumination, including security lighting, shall be directed away from adjoining properties and public rights-of-way.

d. *Safety*

Project entry drive aisles with no parking shall be provided where necessary and as required by the City Engineer in developments with more than 100 vehicle parking spaces to increase overall safety.

Parking area design shall ensure adequate visibility of pedestrians, bicyclists and motorists within a parking facility. To the extent possible, the parking facility shall be designed so that primary pedestrian access to and from building entrances is along, rather than across, parking aisles.

Bicycle and automobile parking areas shall be separated by a physical barrier or sufficient identification and distance to protect parked bicycles from damage by cars.

e. *Striping and Identification*

All automobile parking spaces shall be clearly outlined with double lines on the surface of the parking area (see Figure 3-6, page 26).

All handicapped parking spaces shall be striped and marked according to State of California standards.

All compact parking spaces shall be clearly identified as "Compact Car Only."

All spaces reserved for car and van pools shall be clearly marked with the words "Car Pool Only" on either the tire restraints, on the pavement at the opening of the space, or both.

All motorcycle spaces shall have bollards installed and appropriately spaced so as to prevent automobile usage. Motorcycle spaces shall be marked so as to be clearly identified for motorcycle use.

f. *Surfacing*

All permanent automobile and handicapped parking spaces and loading and maneuvering areas shall be paved and permanently maintained with asphalt, concrete or other all weather surface approved by the Planning and Engineering Divisions.

In order to prevent damage from motorcycle kick stands, required motorcycle parking areas shall be paved with concrete or equivalent surfacing approved by the Planning and Engineering Divisions. All bicycle parking and storage areas shall be paved.

g. *Location of Parking*

All handicapped parking spaces shall be located as close as possible to the entrance(s) which they are intended to serve and oriented so that the user is not required to maneuver past the rear of parking spaces in order to reach the building's main entrance.

Car and van pool spaces shall be located as close as possible to the primary employee entrance(s) which they are intended to serve.

Compact parking spaces shall be located towards the periphery of parking areas.

h. *Landscaping and Screening*

A minimum of 5% of the total off-street parking area shall be landscaped in compliance with Section 9-4.1408 of the Municipal Code.

Parking areas adjacent to public rights-of-way shall provide a 2.5 to 3 foot high landscaped screen across the entire parking frontage except for driveways. Landscape screening may include a combination of low hedge plantings, landscaped berms or low decorative masonry walls subject to the approval of the Director.

Parking areas shall include landscape islands having a minimum width of 5 feet (exclusive of curbing and car overhang), at a ratio of one island for each 12 parking stalls. Planting islands shall be provided at the terminus of each parking row.

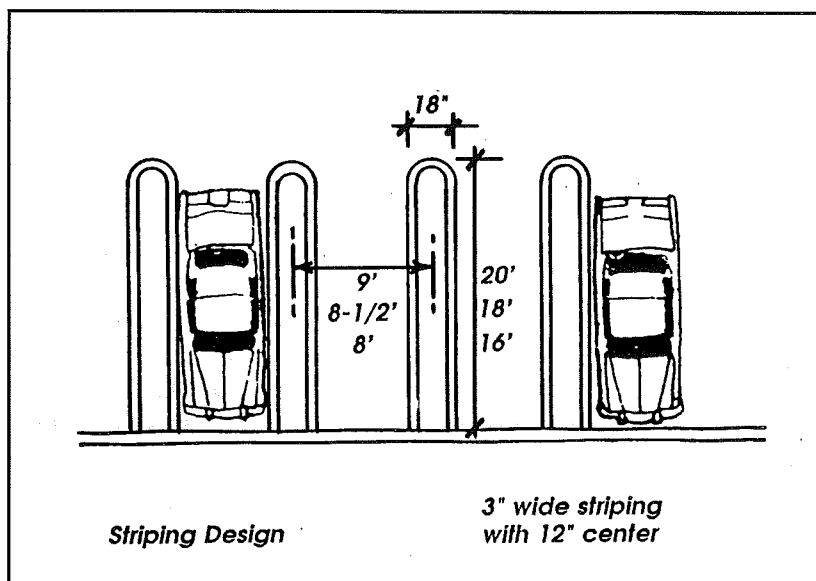


Figure 3-6
Parking Space Striping

Commercial, industrial and public parking facilities abutting residentially zoned/used parcels shall provide a 6 foot high, solid, architecturally treated, masonry wall to screen the parking area(s). All wall treatments shall occur on both sides unless waived by the Director.

Curbs, concrete wheel stops and similar permanent devices shall be provided within parking areas so that vehicles do not bump buildings, perimeter walls, or trees and do not overhang sidewalks.

i. *Parking Access From Streets*

Parking spaces shall be located and designed so that motorists entering and leaving a parking space are not required to back onto a public or private street or onto a private drive other than a specifically designated parking aisle.

j. *Passenger Loading Areas*

Passenger loading areas shall be provided for any building or building complex which will generate 100 or more employees.

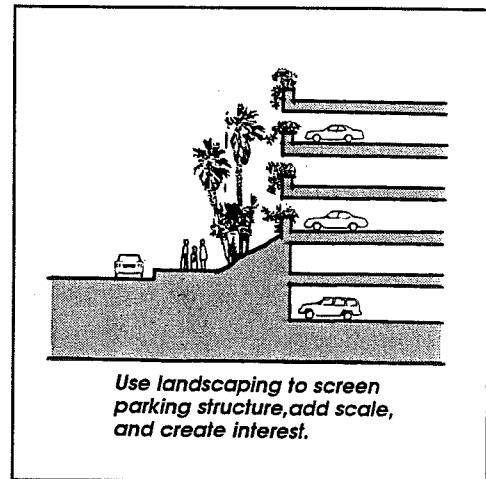
Such loading areas shall be located convenient to the primary employee entrance(s) which they are intended to serve, generally adjacent to the frontage street, and shall be so designated either by signs or painted pavement. Passenger loading areas shall be designed such that vehicles can safely stop and discharge passengers.

10. **Parking Structures**

a. *Requirements For All Parking Structures*

Development of parking structures shall be subject to issuance of a Precise Plan of Design.

No interior parking lot landscaping shall be required for structured parking areas; however, all required parking area landscape setbacks shall be provided. Landscaping will be required on exterior walls.



Stair wells within parking structures shall be open so as to facilitate casual observation.

Ramps within parking structures shall not have a slope greater than 20 percent; provided, however, that the first and last 10 feet shall have a transitional slope no greater than 10 percent.

The minimum ramp width shall be 20 feet.

Vehicle clearance for parking structures shall be a minimum of 7 feet.

b. Subterranean Parking Facilities

All openings for ingress and egress facing a public street shall be situated a minimum of 30 feet from the property line.

A subterranean parking structure may be constructed and maintained within any setback area provided that all required landscape setback areas are provided.

Exits from subterranean parking structures shall provide adequate sight distance in compliance with applicable standards established by the Engineering Division.

c. Semi-Subterranean Parking Facilities

A parking structure shall be considered semi-subterranean if the structure is partially underground and if the finished floor of the upper level of the structure does not exceed 3 feet above the average natural or existing grade of the parcel.

All openings for ingress and egress facing a public street shall be situated a minimum of 30 feet from the property line.

Semi-subterranean parking structures shall meet applicable building setback requirements.

d. Above-Ground Parking Structures

Above-ground parking structures shall be no greater in height than 50 feet and shall meet applicable building setback requirements.

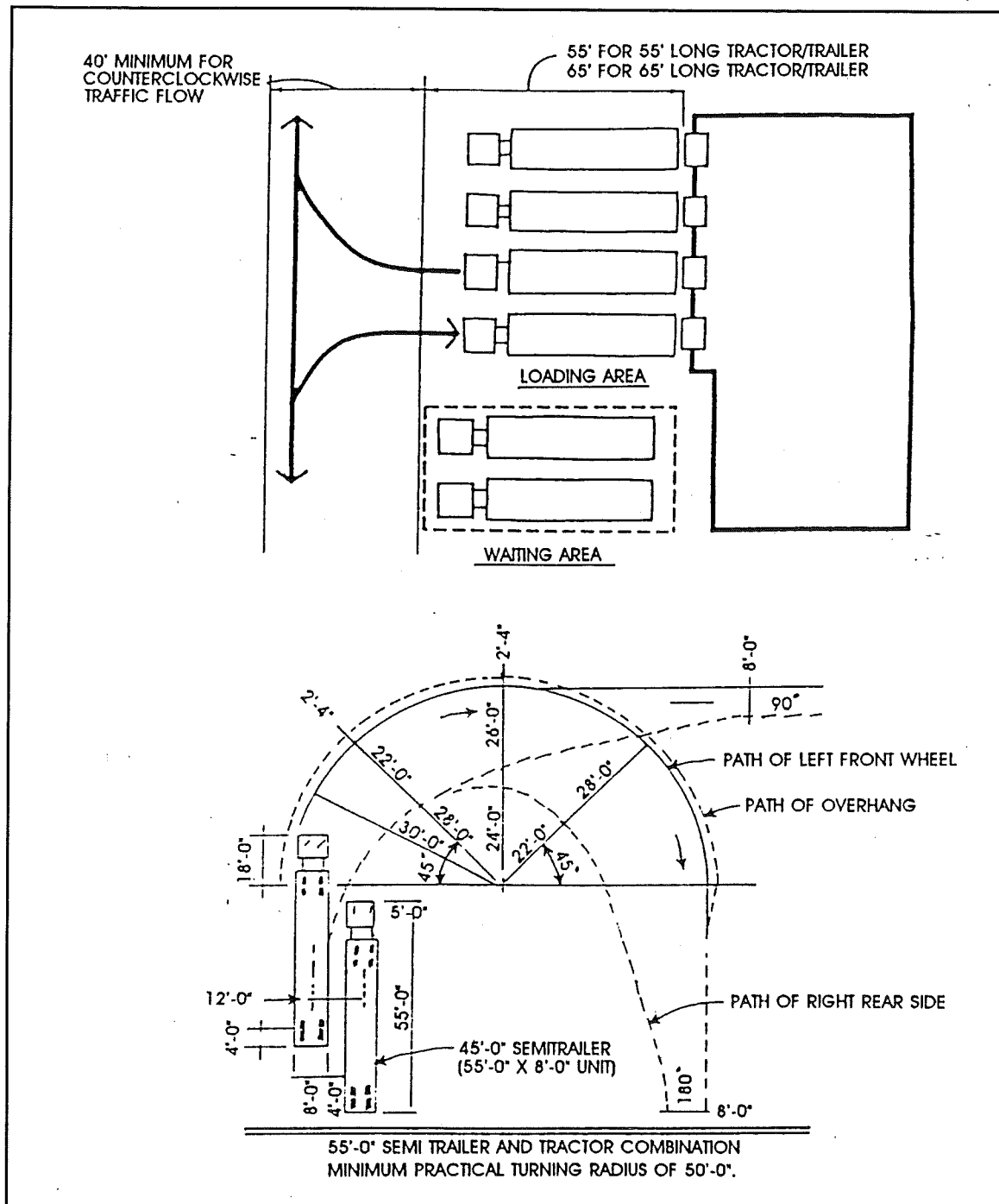
The exterior elevations of above-ground parking structures shall be designed so as to avoid the use of blank concrete facades.

The perimeter of the parking structure shall be landscaped with a minimum of one tree for every 20 feet of linear distance of facade in addition to required streetscape landscaping. Groundcover and shrubs shall also be provided. Suspended planters and/or planters integrated into the structure's perimeter walls shall be provided to help break up the mass of the structure, subject to the approval of the Director.

D. Loading Requirements

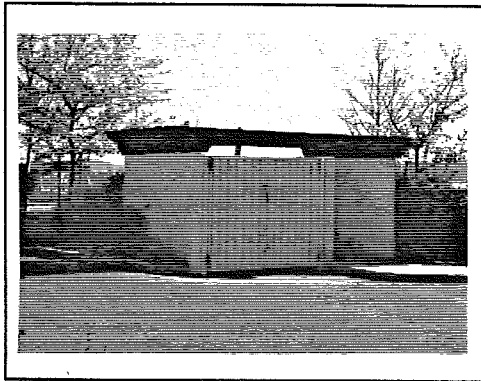
1. Loading areas shall be provided as required by the Planning Division.
2. Standards for maneuvering areas are illustrated in Figure 3-7, page 30.
3. Loading areas shall be designed to provide for backing and maneuvering on-site and not from or within a public street.
4. All loading areas shall be screened from view from public and private streets by a decorative screen wall, ornamental landscaping and/or portions of on-site buildings.
5. Loading docks and doors shall only be permitted within an interior side or rear yard area. Direct loading from a public street shall not be permitted.
6. Loading and maneuvering areas shall not encroach into side or rear building setback areas when adjacent to a residential zone.
7. All screen walls and wing walls shall be of a compatible material with adjacent buildings and shall be sufficient height to provide adequate visual screening.
8. A sight line analysis shall be prepared by the project sponsor and filed with the development application to demonstrate compliance with loading door screening requirements.
9. A maximum of 50 percent of any interior side or rear building elevation may be devoted to loading doors.
10. Truck maneuvering areas shall not encroach into required parking areas.
11. On-site truck maneuvering to and from loading areas shall be provided to accommodate a minimum of a 55 foot semi tractor and trailer. This requirement may be modified by the Director if the applicant sufficiently demonstrates that a lesser requirement is adequate for the proposed use and potential future tenants.
12. For each two (2) loading spaces provided, one trailer parking/ waiting space shall also be provided.

FIGURE 3-7
TRUCK LOADING AND MANEUVERING



E. Trash Enclosures

1. Refuse storage and disposal areas shall be provided within trash enclosures which are screened on at least three sides from public view by a solid decorative masonry wall not less than 6 feet in height. The fourth side shall consist of a solid metal gate painted to match or coordinate with the building as approved by the Planning Division (slatted chain link is not permitted).



2. Refuse bins shall be provided in sufficient number and shall be placed in convenient locations.
3. All trash shall be deposited in the trash enclosure. The gate leading thereto shall remain closed except when in use and shall be maintained in good working order at all times.
4. Trash enclosures shall be designed to accommodate recyclable materials in compliance with the City's current recycling program.

5. Trash areas shall not be used for storage or other purposes. The premises shall be kept in a neat and orderly condition at all times, and all improvements shall be maintained in a condition of good repair and appearance.

F. Outdoor Storage

1. Outdoor storage and display of operable vehicles offered for sale shall be permitted in the C3 zoning districts only, subject to a Conditional Use Permit.
2. Uses for which outdoor storage is a primary use (e.g., vehicle salvage, contractor yards, etc.) are not permitted within the Specific Plan area.
3. Outdoor storage may be permitted as an accessory use in the MIP zoning district, subject to the following standards:
 - a. Outdoor storage shall not be located in an area between a structure and a public or private street;
 - b. Outdoor storage shall be completely screened from view from public streets by permanent opaque material which is compatible with the materials of adjacent buildings as approved by the Director. (Slatted chainlink fence and non-permanent materials are not permitted.); and
 - c. The location of outdoor storage areas shall be limited to those areas so indicated on approved site development plans.

G. Screening

Wherever a parcel zoned for commercial or industrial purposes abuts a residential zoning district or use, there shall be erected along the property line abutting the residential zoning district or use a 6 foot high (8 foot high when required/approved by the Director) solid decorative masonry wall. The wall shall be architecturally treated on both sides, subject to the approval of the Director.

Any equipment, whether on the roof, side of structure or ground shall be screened from public view. The method of screening shall be architecturally compatible in terms of colors, materials, shape and size. The screening design/construction shall be subject to the approval of the Director and shall blend with the design of the structure and include landscaping when on the ground.

H. Security Doors And Grilles

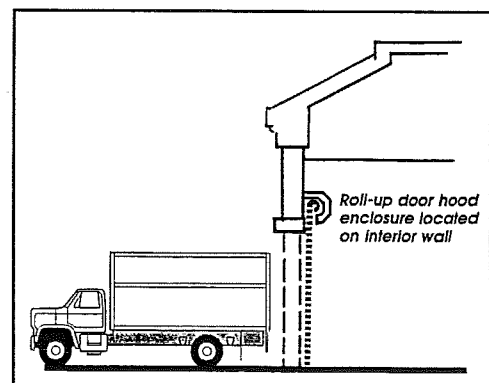
For non-residential uses, security doors or grilles shall be permitted only on the interior of the structure or tenant space, except as follows:

1. Garage roll-up doors/gates shall be permitted on the exterior of the structure only when in conjunction with a vehicle repair facility. These doors/gates shall be permitted on the repair bays only;
2. Loading doors/gates shall be permitted on the side or rear of a structure only. The Director may permit the doors/gates on the front or side if vehicular access is available only on the front or side respectively;

3. All roll-up doors/gates shall be installed so that the hood enclosure is either integrated into the structure or hidden from view from all adjoining public rights-of-way; and
4. Service doors, repair bays and similar items/facilities shall be oriented away from direct public view.

I. Fences And Walls

1. Fences and walls shall be architecturally compatible with the subject development and adjoining area.
2. All fences and walls require the approval of a building permit.
3. The use of barbed wire, razor wire or similar security wire is only permitted in areas located out of direct public view.



Development Criteria For Special Uses

Certain land uses, because of their operational characteristics, have the opportunity to create more significant impacts on adjacent properties than others. As a result, specific development criteria for these uses is recommended.

A. Vehicle Sales

New and used vehicle sales (including cars, trucks, motorcycles, recreational vehicles and boats) are allowed in the C3 and MIP zoning districts, subject to approval of a Conditional Use Permit. Such facilities shall be developed and operated in the following manner:

1. The minimum site area for a new dealership shall be 40,000 square feet;
2. All parts, accessories, etc., shall be stored within a fully enclosed structure;
3. Service areas shall be completely screened from public view;
4. All on-site lighting shall be energy efficient, stationary and directed away from adjoining properties and public rights-of-way;
5. All loading and unloading of vehicles shall occur on-site where feasible and not in adjoining public rights-of-way;
6. All vehicles associated with the business shall be displayed, parked or stored on-site on paved surfaces only and not in adjoining streets or alleys;
7. An adequate on-site queuing area for servicing customers shall be provided. The queuing area or lanes shall be large enough to hold at least 1.5 vehicles for

each service bay in the facility based on a queuing distance of 25 feet per vehicle. On-site driveways may be used for queuing, but may not interfere with access to required parking spaces. Required parking spaces may not be used for queuing.

8. No vehicle repair or service work shall occur outside of a fully enclosed structure. Service bays with individual access from the exterior of the structure shall not directly face or front on a public right-of-way; and
9. Noise levels within automobile dealerships including intercoms and paging systems shall be maintained at a level that would not be considered a nuisance to surrounding property owners. Where possible, the use of "beepers" or "pagers" should be used in place of an intercom.

B. Automobile Service And Repair Facilities

Automobile (motor vehicle) repair and service facilities are only permitted in the C3 and MIP zoning districts, subject to the approval of a Precise Plan of Design and shall be developed/operated in the following manner:

1. The site shall be entirely paved, except for structures and landscaping, so that vehicles are not parked in a dirt or otherwise not fully improved area;
2. All stored (for more than 5 consecutive days), damaged or wrecked vehicles shall be completely screened so as not to be visible from adjoining properties or public rights-of-way;
3. Service access shall be located at the rear or side of structure(s) and as far as

possible from adjoining residential uses;

4. Repair/service activities and vehicle loading and unloading shall only occur on-site and not in adjoining streets or alleys;
5. Service bays with individual access from the exterior of the structure shall not directly face or front on a public street;
6. All repair/service activities and operations shall be conducted entirely within an enclosed structure. Outdoor hoists are prohibited;
7. All repair facilities shall maintain closed windows when performing body and fender work, hammering, sanding or other noise-generated activity;
8. All on-site lighting shall be energy efficient, stationary and directed away from adjoining properties and public rights-of-way; and
9. The premises shall be kept in a neat and orderly condition at all times and all improvements shall be maintained in a condition of good repair and appearance. No used or discarded automotive parts or equipment or permanently disabled, junked or wrecked vehicles may be stored outside the main building.

C. Convenience Stores

The retail sales of groceries, staples and sundry items within structures of less than 8,000 square feet of gross floor area is permitted in the C2 and C3 zoning districts, subject to the approval of a Conditional Use Permit. Such uses shall be developed/operated in the following manner:

1. The minimum site area for a new convenience store in the C2 or C3 zoning district shall be 15,000 square feet;
2. The site shall have direct frontage along a major or secondary street. In the C2 zoning district the site shall not have direct access on a local residential street;
3. One access drive may be permitted for each street frontage. The design and location of the access drive(s) shall be subject to the approval of the Director;
4. All on-site lighting shall be energy efficient, stationary and directed away from adjoining properties and public rights-of-way;
5. The premises shall be kept in a neat and orderly condition at all times;
6. If on-site dispensing of automotive fuels is provided, the design, location and operation of these facilities shall be consistent with the provisions of Section 9-6.101 of the Municipal Code. Additionally, the cashier location shall provide direct visual access to the pump islands and the vehicles parked adjacent to the islands;

7. A bicycle rack designed to accommodate a minimum of 4 bicycles shall be installed in a convenient location visible from the inside of the store;
8. Each convenience store shall provide at least one public, disabled-accessible restroom located within the store;
9. Public telephone facilities shall be located within the main building; and
10. A convenience store located adjacent to any residential zoning district/use shall have an 8 foot high decorative masonry wall along all property lines adjacent to the district(s). The design of the wall and its construction materials shall be subject to the approval of the Director.

D. Drive-Thru Establishments

New drive-thru establishments are permitted only in the C3 zoning district, subject to the approval of a Conditional Use Permit. Such uses shall be developed/operated in compliance with Section 9-6.200 of the Municipal Code and the following requirements:

1. Drive-thru aisles shall have a minimum of 10 foot interior radius at curves and a minimum 12 foot width. Each drive-thru entrance/exit shall be at least 100 feet from an intersection of public rights-of-way. Also, each entrance to a driveway and the direction of traffic flow shall be clearly designated by signs/pavement marking(s) or raised curbs;

2. Each drive-thru aisle shall provide sufficient stacking area behind the ATM's, menu board(s) etc. to accommodate a minimum of 6 vehicles or 180 feet, whichever is greater;
3. Pedestrian walkways should not intersect drive-thru aisles, but where they do, they shall have clear visibility and shall be emphasized by enhanced paving and marking/striping;
4. Drive-thru aisles shall be constructed with (PCC) concrete;
5. Landscaping shall screen drive-thru or drive-in aisles from public rights-of-way and shall be used to minimize the visual impact of menu boards and/or directional signs;
6. Menu boards shall not exceed 36 square feet in area, with a maximum height of 6 feet and shall face away from public rights-of-way. Outdoor speakers shall be located at least 50 feet from any residential zoning district/use; and
7. Drive-thru facilities within an integrated shopping center shall have an architectural style consistent with the theme established in the center. The architecture of any drive-thru facility shall provide compatibility with surrounding uses in terms of form, materials, color, scale, etc.

E. Service Stations

Service stations are permitted only in the C3 zoning district, subject to the approval of a Conditional Use Permit. Such uses shall be located/ developed/operated in compliance with Section 9-6.101 of the Municipal Code.

Development Criteria For Focus Areas

A. Introduction

Within the North Montclair Specific Plan area, 16 special "focus" areas have been identified for detailed conceptual planning. The City considers the proper planning of these areas to be of great importance in the fulfillment of the overall vision for the North Montclair area. The 16 focus areas (some areas are comprised of multiple individually owned sites) are shown on the following page.

The development criteria for each of the 16 focus areas includes a discussion of preferred land use(s), physical development criteria and front parkway design. Where appropriate, special development incentives may be provided as encouragement for areas to develop according to the conceptual site plan and development criteria.

B. Applicability

Whenever a development project is contemplated on any of the 16 special focus areas, the project proponent should first consult the development criteria in this section for the appropriate sites. The development criteria and conceptual site plan should be followed as close as possible; however, the City recognizes that some flexibility may be necessary to accommodate specific development proposals.

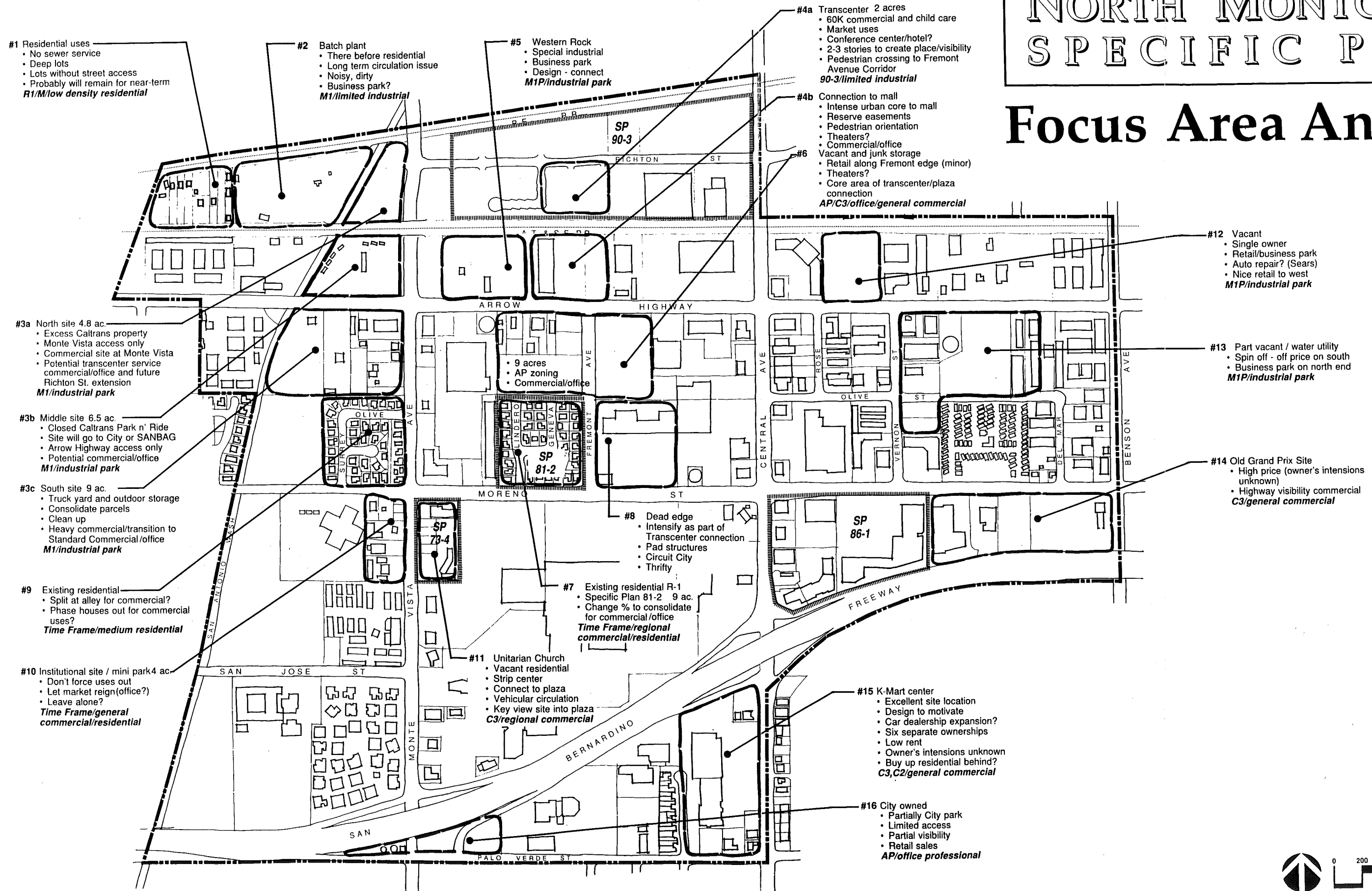
The development criteria in this section may be interpreted with some discretion as opposed to the development regulations in the previous sections of this chapter which are established requirements that do not allow discretionary interpretation.

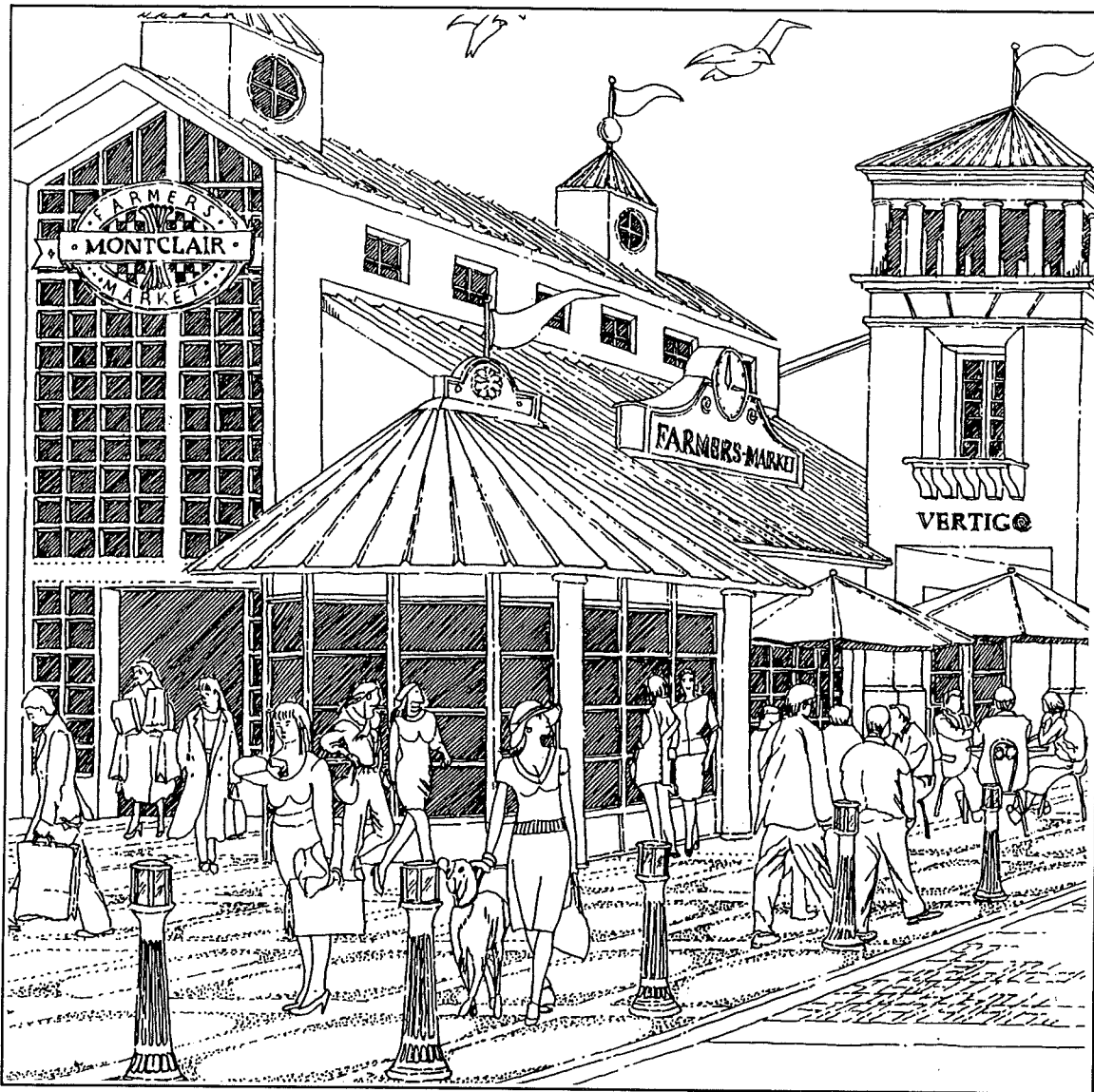
C. Development Criteria

The following text and accompanying site plans represent the preferred development scenario for each of the 16 focus areas.

NORTH MONTCLAIR SPECIFIC PLAN

Focus Area Analysis





1. Area 1**a. Site Profile**

Located in the northwestern corner of the Specific Plan area. Currently a number of small single family homes appear on approximately a dozen narrow and deep lots. There is currently no sewer service and an agreement to connect to the City of Claremont's sewer system may be required. Several lots do not have direct street access. Uses will likely remain for the near term (4.82 acres approximately).

b. Land Use Concept

The primary use for this site is for additional residential units and an improved access and circulation system.

c. Development Criteria

- ▲ Allow small lot subdivision lots (4,500 sq. ft.) if direct public street access for all lots can be attained.
- ▲ A landscape/sound buffer should be incorporated adjacent to incompatible land uses (e.g., industrial uses, railroad tracks).

d. Front Parkway Design

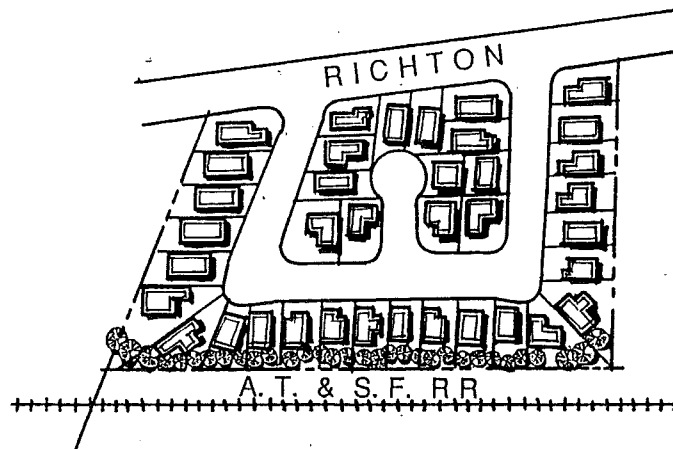
- ▲ To be determined by site plan review.

e. Undesirable Uses

- ▲ Any non-residential use.

Focus Area Design Concepts

Area 1



LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

2. Area 2**a. Site Profile**

In the northwestern section of the Specific Plan area. Currently an asphalt producing batch plant. Site is relatively flat and has been graded (9.09 acres approximately).

b. Land Use Concept

The primary use for this site is light industrial/business park.

c. Development Criteria

- ▲ The site's primary access shall be on Richton Street (when extended).
- ▲ Least obtrusive uses to be adjacent to Focus Area 1.
- ▲ Pedestrian access to focus Site 1 highly desirable.
- ▲ The western property line adjacent to Focus Area 1 shall be heavily landscaped and shall contain decorative screen walls to buffer residential uses.
- ▲ A landscape buffer shall also be provided adjacent to the railroad tracks.
- ▲ Two monument signs are to be allowed; one at either end of the Richton Street frontage.

d. Front Parkway Design

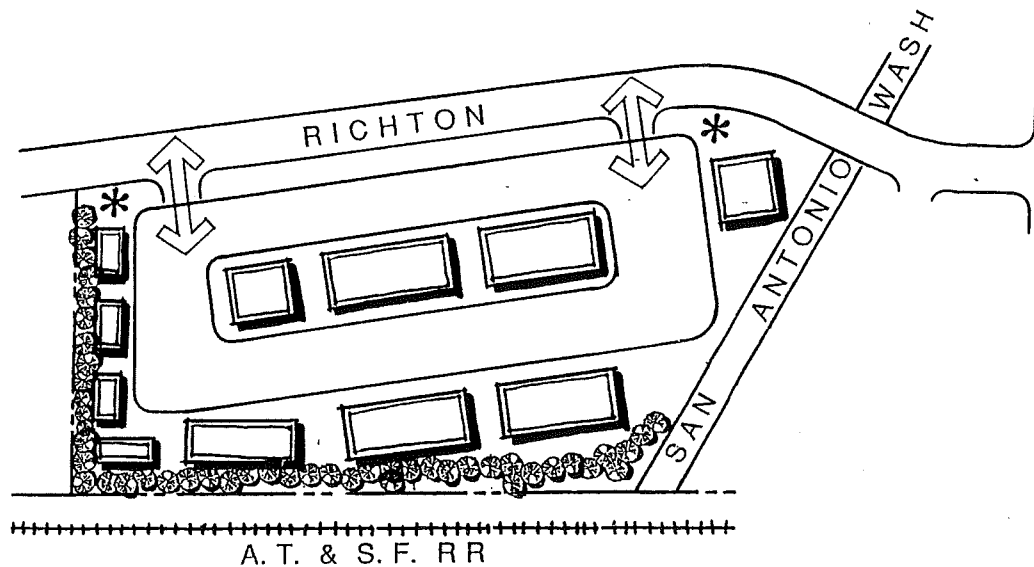
- ▲ To be determined by site plan review.

e. Undesirable Uses

- ▲ Heavy industrial uses.

Focus Area
Design Concepts

Area 2



* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

3. Area 3a**a. Site Profile**

Distinctive triangular shaped site is located between Monte Vista Avenue and the San Antonio Wash. Site is excess Caltrans property. Portion of site will likely be required for extension of Richton Street (4.24 acres approximately).

b. Land Use Concept

The primary use for this site is a business park. A secondary use consideration is service commercial, serving the patrons of the Montclair Transcenter.

c. Development Criteria

- ▲ The site's primary access shall be on Richton Street.
- ▲ Vehicular/pedestrian connections to Focus Area 2 highly desirable.
- ▲ Building mass should be placed near Monte Vista Avenue.
- ▲ Two monument signs are allowed; one along Richton and one along Monte Vista Avenue.
- ▲ A landscape buffer should be provided adjacent to the railroad tracks.

d. Front Parkway Design

- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Canary Island Pine or Brisbane Box set back 12' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Special driveway entry paving.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

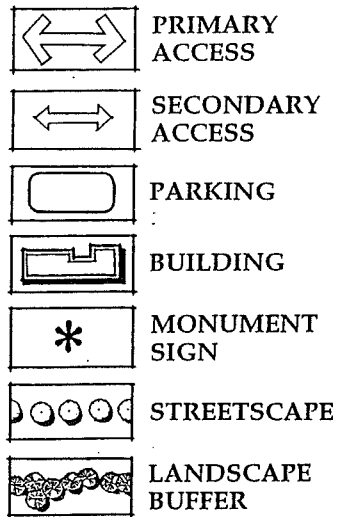
e. Undesirable Uses

- ▲ Heavy industrial uses.

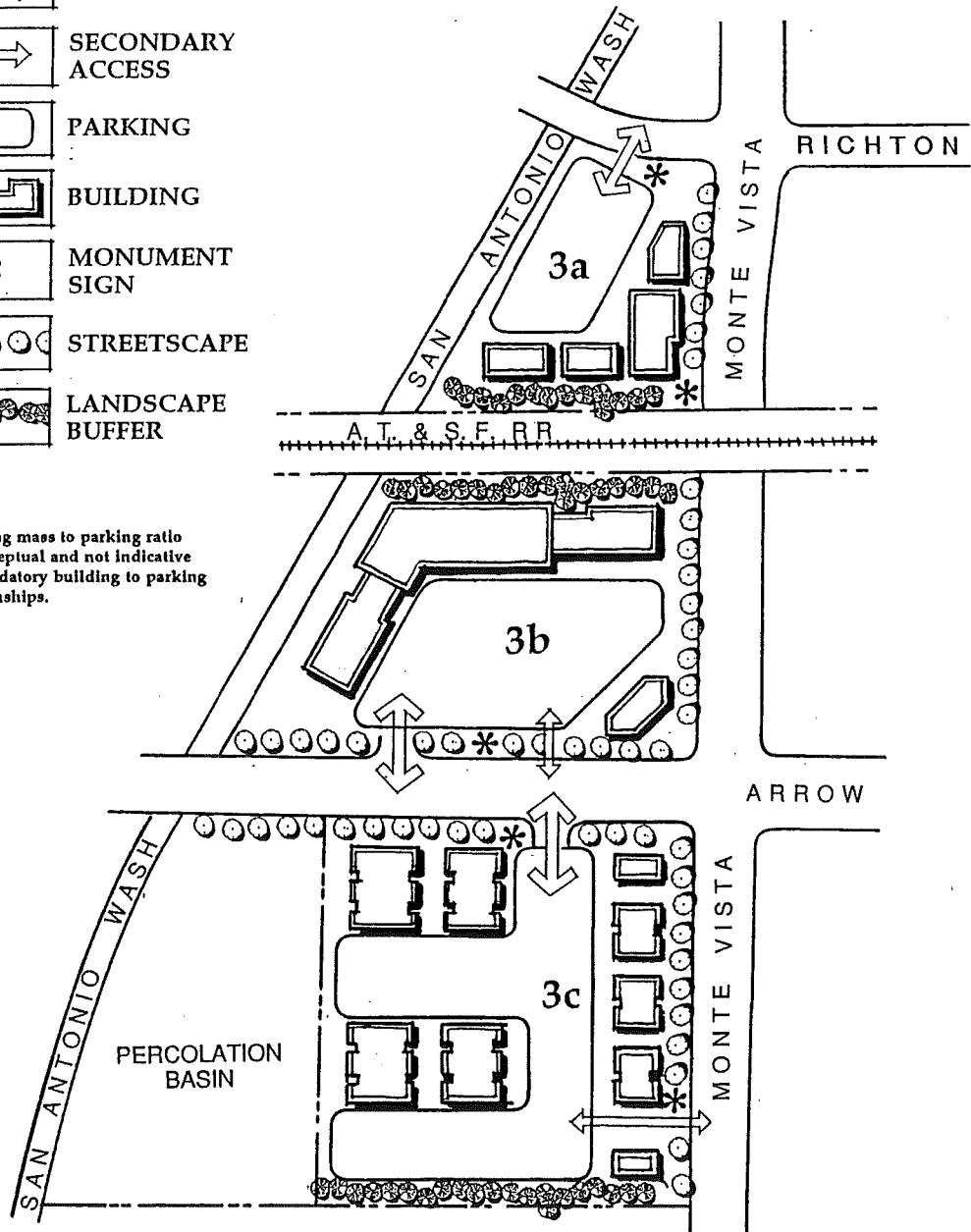
Focus Area Design Concepts

Area 3a

LEGEND



* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.



4. Area 3b**a. Site Profile**

Site is located between Monte Vista Avenue and the San Antonio Wash. Site was previously a Caltrans park and ride facility (6.42 acres approximately). Site has access constraints on all but the Arrow Highway frontage.

b. Land Use Concept

The primary use for this site is retail or service commercial. A secondary use consideration is a commercial/office mixed use project.

c. Development Criteria

- ▲ The site's primary access shall be on Arrow Highway.
- ▲ A landscape/sound buffer should be incorporated adjacent to the railroad tracks (north property line).
- ▲ Building mass should be located adjacent to the north and west property lines which represent the most constrained edges of the site.
- ▲ A single monument sign is be allowed only on the Arrow Highway frontage.

d. Front Parkway Design**Arrow Highway Edge:**

- ▲ 20' wide parkway landscape strip
- ▲ Formal, 40' o.c. single row of Oak Trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Monte Vista Avenue:

- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Canary Island Pine or Brisbane Box set back 12' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.

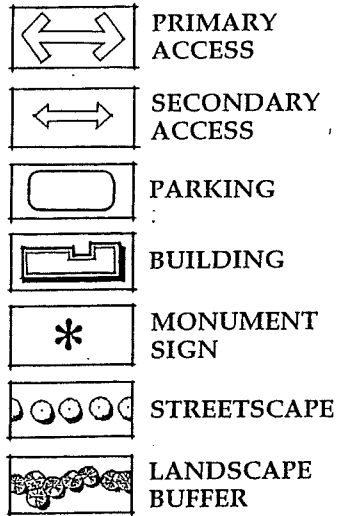
e. Undesirable Uses

- ▲ Industrial uses.

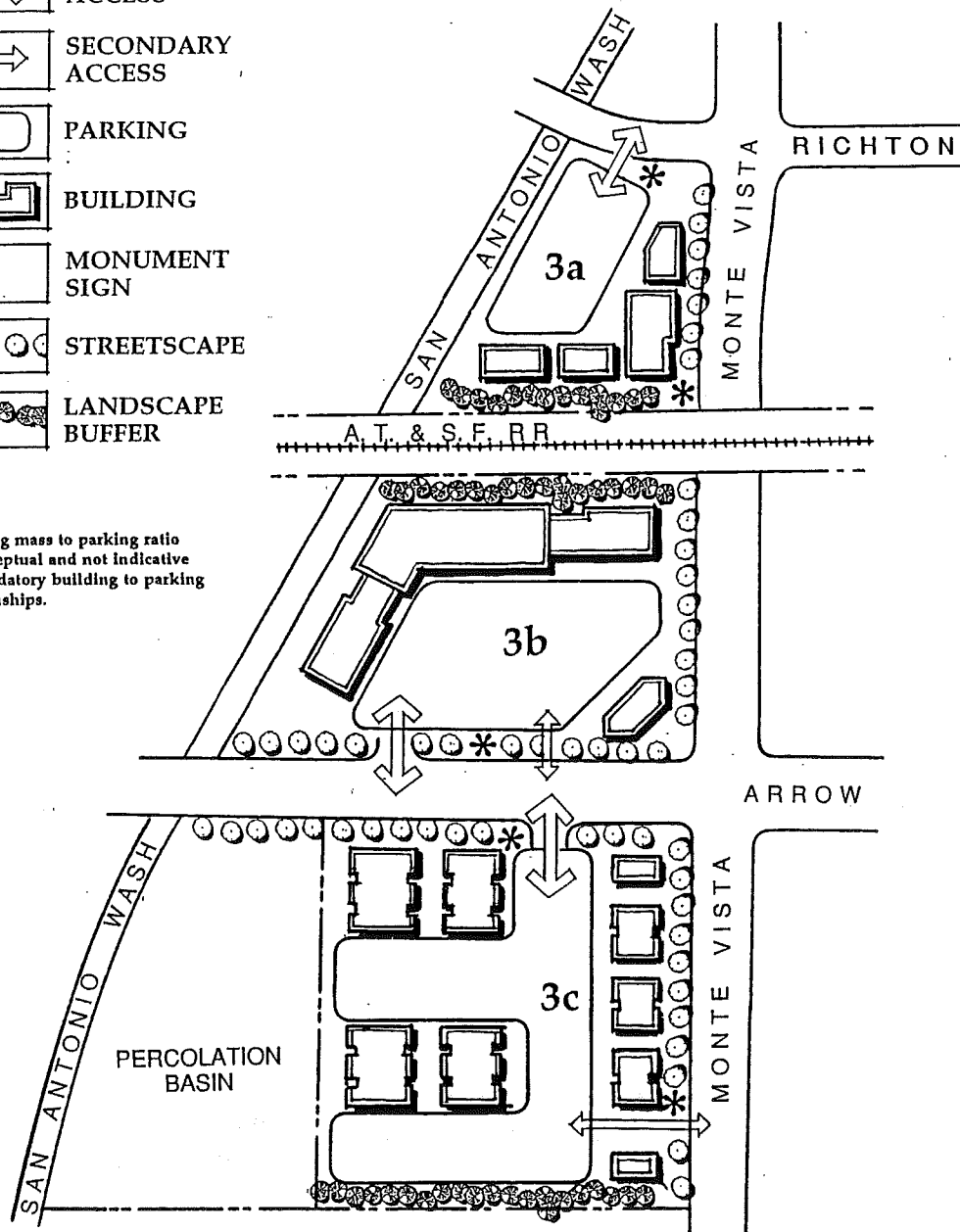
Focus Area Design Concepts

Area 3b

LEGEND



* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.



5. Area 3c**a. Site Profile**

Located on the southwest corner of the Arrow Highway/Monte Vista intersection, this focus area is comprised of approximately nine existing parcels with a mixture of auto repair, service commercial, truck yards, outdoor storage and other construction/industrial uses. A large portion of the area is comprised of a percolation basin for the San Antonio Wash (12 acres approximately).

b. Land Use Concept

The primary use envisioned for this focus area is to consolidate existing lots into a planned business park/light industrial use.

c. Development Criteria

- ▲ The site's primary access shall be on Arrow Highway with secondary access on Monte Vista Avenue.
- ▲ Least obtrusive tenants along Monte Vista Avenue.
- ▲ A landscape/sound buffer shall be incorporated adjacent to south property line which abuts single family residential.
- ▲ Building mass should be clustered in a campus format with limited parking (single bay) between the structures and Arrow Highway and Monte Vista Avenue.
- ▲ Two monument signs are allowed; one along Arrow Highway and one along Monte Vista Avenue.

d. Development Incentives

- ▲ Reduced parking requirements for consolidation of 2 or more lots.
- ▲ Increased development intensity/density if 3 or more lots are master planned as a single unified project.

e. Front Parkway Design**Arrow Highway Edge:**

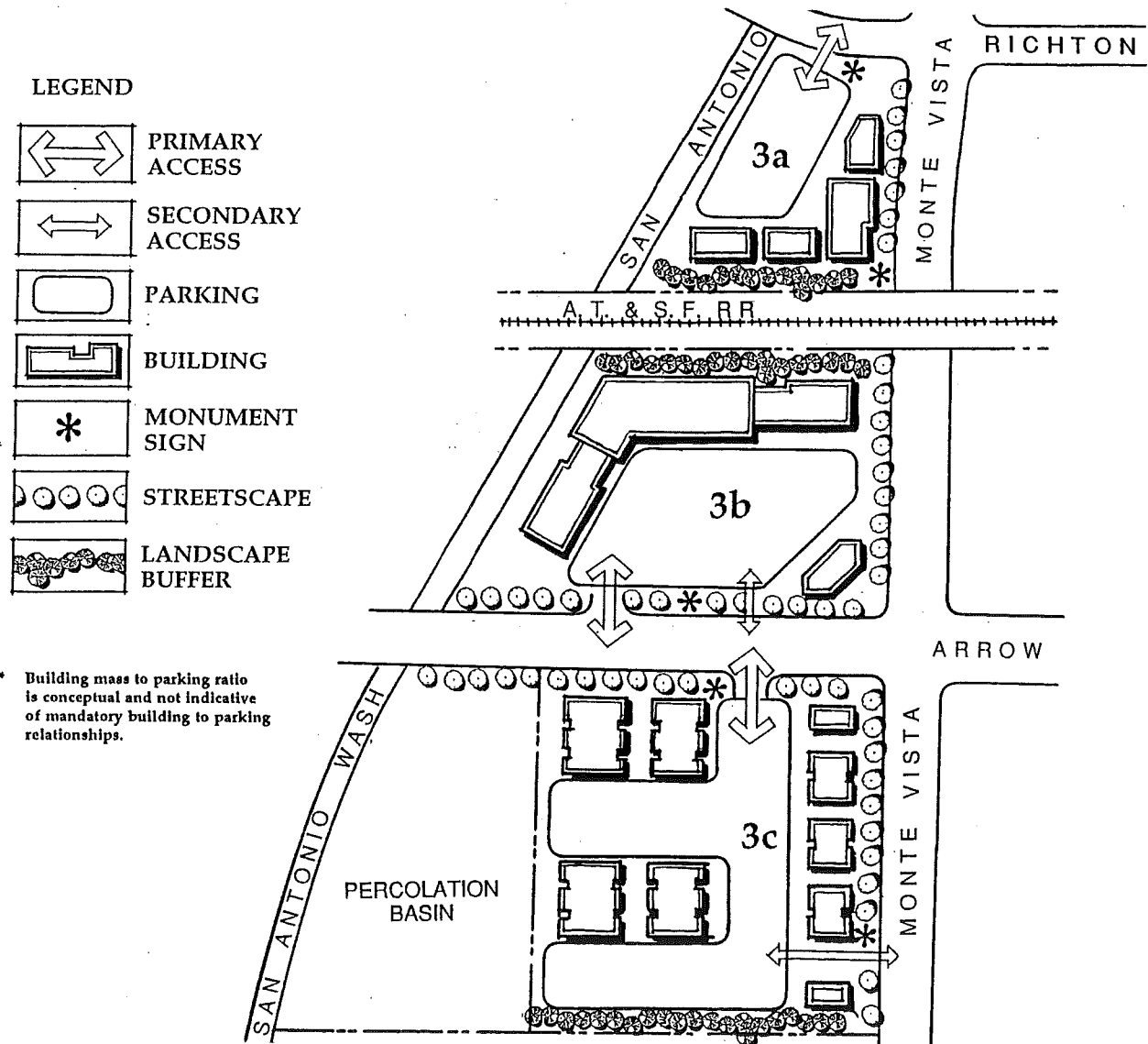
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Oak trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Monte Vista Avenue Edge

- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Canary Island Pine or Brisbane Box trees set back 12' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Special driveway entry paving.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

f. *Undesirable Uses*

- ▲ Heavy industrial uses.
- ▲ Outdoor material or vehicle storage uses.

**Focus Area
Design Concepts****Area 3c**

6. Area 4a**a. Site Profile**

This site is the Montclair Transcenter commercial site located between the railroad tracks and Richton Street (3.93 acres approximately).

b. Land Use Concept

The primary uses envisioned for this site include those considered necessary to serve the needs of transcenter users, such as a daycare center, bakery, restaurant, conference center, hotel, retail services and personal services with teleconferencing/office center on upper floors. Senior citizen housing may also be considered on upper floors. Development of the site could proceed in phases whereby initial development may include moveable vendor stalls/carts. The next development phase would include permanent commercial facilities, followed by offices, teleconferencing facilities and perhaps ultimately a hotel and/or senior housing.

A convenient pedestrian connection across the railroad tracks to the south is also an important planning concept. This connection would provide pedestrian access across the railroad tracks to Fremont Street and ultimately to the transportation link to the Montclair Plaza.

c. Development Criteria

- ▲ The site's vehicular access is from Richton Street.
- ▲ The site's primary pedestrian access will be from the surrounding Transcenter parking lots and across the railroad tracks southward toward Arrow Highway.
- ▲ Building mass of up to three stories acceptable.
- ▲ A single monument sign is allowed on Richton Street.

d. Development Incentives

- ▲ Reduced parking incentive through the use of shared parking with Transcenter.

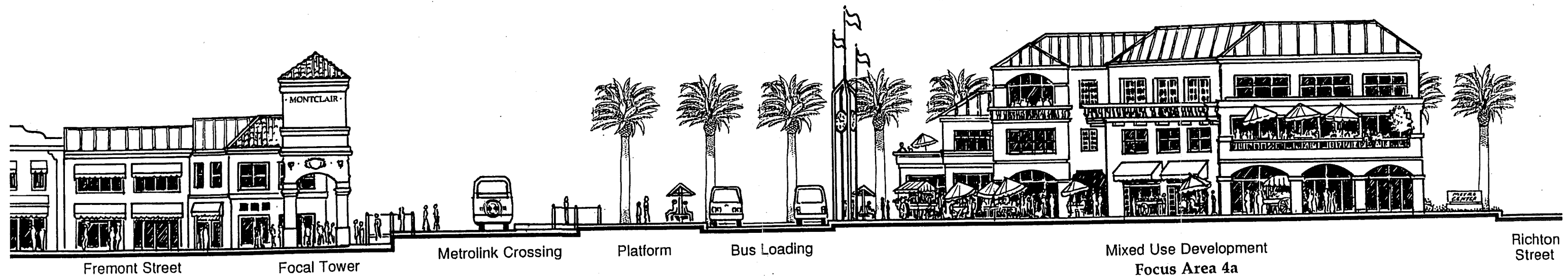
e. Front Parkway Design

- ▲ To be determined by site plan review.

f. Undesirable Uses

- ▲ Industrial uses, furniture/appliance/home decoration, automotive sales.
- ▲ Drive-thru uses.

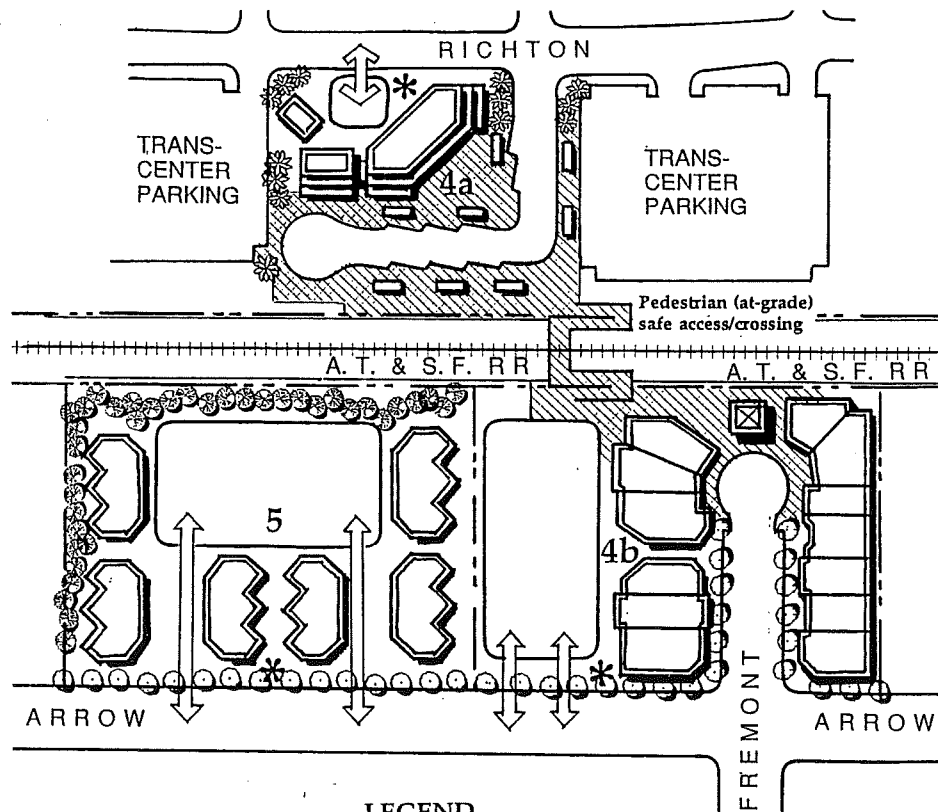
Transcenter
Cross Section
(Focus Area 4a)



Note: Trees in the foreground are not shown in this graphic.
Street Trees do exist along Fremont Street storefronts,
and along the bus entry drive/plaza area.

Focus Area Design Concepts

Area 4a



LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

7. Area 4b**a. Site Profile**

Located near the center of the Specific Plan area, Focus Area 4b is located between Arrow Highway and the railroad right-of-way. The site is currently occupied with a quonset-type structure with several small businesses. Overall site utilization is low (6.31 acres approximately).

b. Land Use Concept

The primary development concept for this site is for mixed use commercial and business park. A key design element is the provision of a pedestrian access between Arrow Highway, across the railroad right-of-way, to the Montclair Transcenter. This site represents a critical opportunity for a pedestrian/public transit linkage between the Montclair Transcenter and Montclair Plaza.

c. Development Criteria

- ▲ The site's primary vehicular access is Arrow Highway.
- ▲ A landscape buffer should be developed adjacent to the railroad tracks.
- ▲ Building mass of up to three stories acceptable.
- ▲ A single monument sign is allowed on the Arrow Highway frontage.
- ▲ Building mass should create a focal element for pedestrians walking north along Fremont Avenue.

d. Development Incentive

- ▲ Reduced parking requirement in consideration of transit and pedestrian accessibility and amenities.
- ▲ Other density and height increases may also be available as development incentives.

e. Front Parkway Design

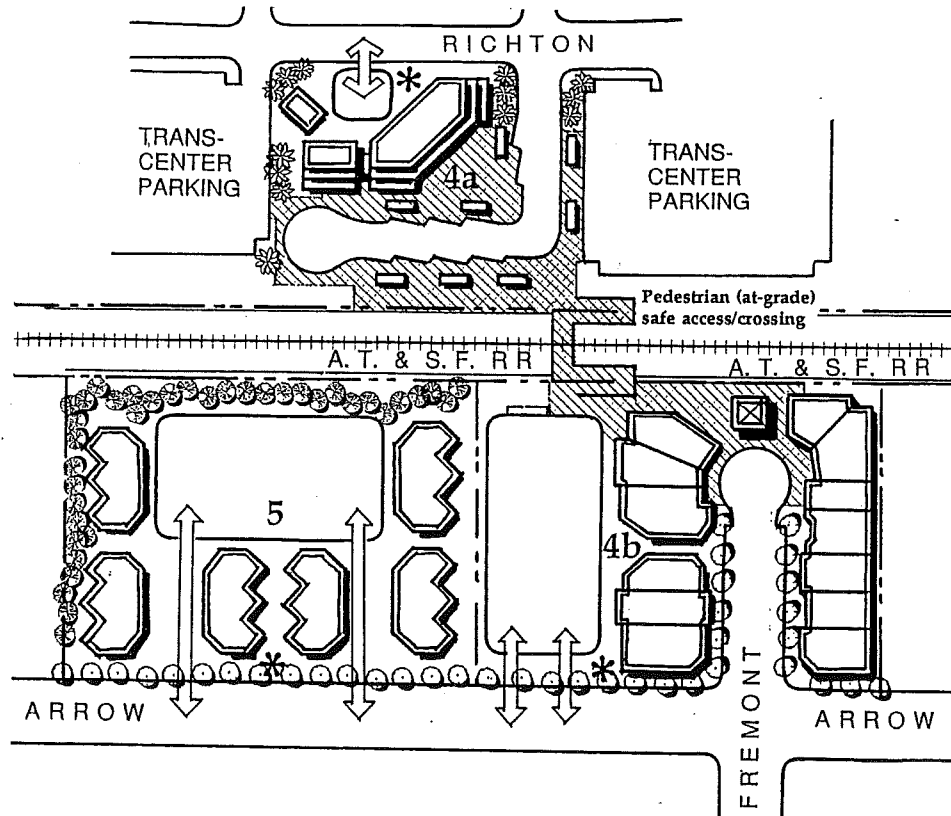
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Oak trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

f. Undesirable Uses

- ▲ Heavy industrial uses.
- ▲ Automotive sales, repair and service.
- ▲ Drive-thru uses.

Focus Area Design Concepts

Area 4b



LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

8. Area 5**a. Site Profile**

Located near the center of the Specific Plan area, the area is located between Arrow Highway and the railroad right-of-way. The site is currently a cement, sand and gravel processing and sales facility. Current use is likely to remain for an indefinite period (6.85 acres approximately).

b. Land Use Concept

The primary use for this area is for a business park.

c. Development Criteria

- ▲ Vehicular access is limited to Arrow Highway.
- ▲ A landscape buffer should be developed adjacent to the rail right-of-way.
- ▲ Building mass should be broken into multiple structures sited in a well landscaped campus-like arrangement.
- ▲ Two monument signs are allowed on Arrow Highway.

d. Frontage Parkway Design

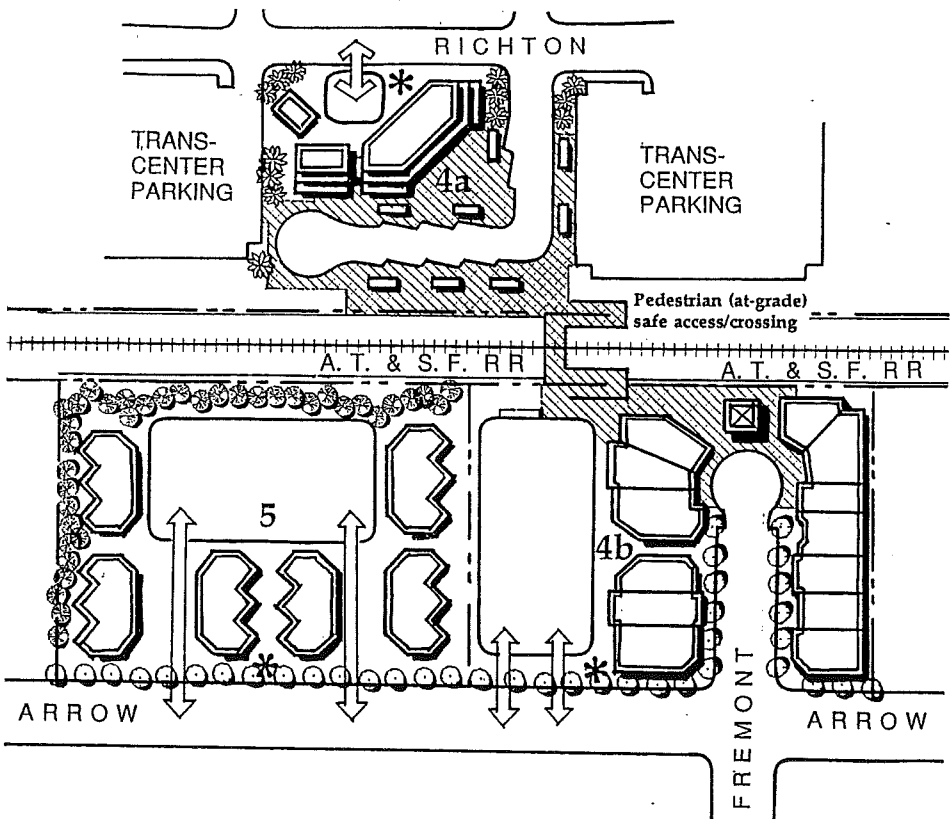
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single rows of Oak Trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

e. Undesirable Uses

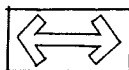
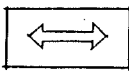

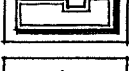
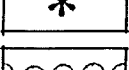
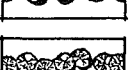
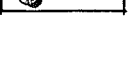
- ▲ Sand and gravel processing.
- ▲ Heavy industrial uses.

Focus Area
Design Concepts

Area 5



LEGEND

-  PRIMARY ACCESS
-  SECONDARY ACCESS
-  PARKING
-  BUILDING
-  MONUMENT SIGN
-  STREETScape
-  LANDSCAPE BUFFER

* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

9. Area 6**a. Site Profile**

Located in the center of the Specific Plan area, Focus Area 6 comprises numerous separately owned parcels at the southeast and southwest corners of Fremont Avenue and Arrow Highway. Portions of the parcels are developed with marginal uses which are inappropriate for the future of this area (15.09 acres approximately).

b. Land Use Concept

The development envisioned for this area is commercial with retail storefronts along Fremont Avenue. Offices may be permitted as long as they do not directly front on Fremont Avenue. Mixed use projects (i.e., residential units above commercial) may be considered by the City Council on a case-by-case basis and will require an amendment to the Plan.

c. Development Criteria

- ▲ Building mass shall be located adjacent to Fremont Avenue to create a solid "street wall" with a pedestrian orientation (refer to Design Guidelines on page IV-13). Additional buildings may be set back behind a "courtyard" of parking bays.
- ▲ Two and three story building masses are encouraged.
- ▲ Vehicular access shall be primarily off of Arrow Highway with limited secondary access from Fremont Avenue.
- ▲ Small pedestrian plazas are strongly encouraged along the Fremont Avenue frontage.
- ▲ No freestanding signs permitted along Fremont Avenue.
- ▲ Two monument signs are allowed on Arrow Highway.
- ▲ Sidewalks along Fremont Avenue are to be 15' wide.

d. Front Parkway Design**Arrow Highway Edge:**

- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Oak Trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Fremont Avenue Edge:

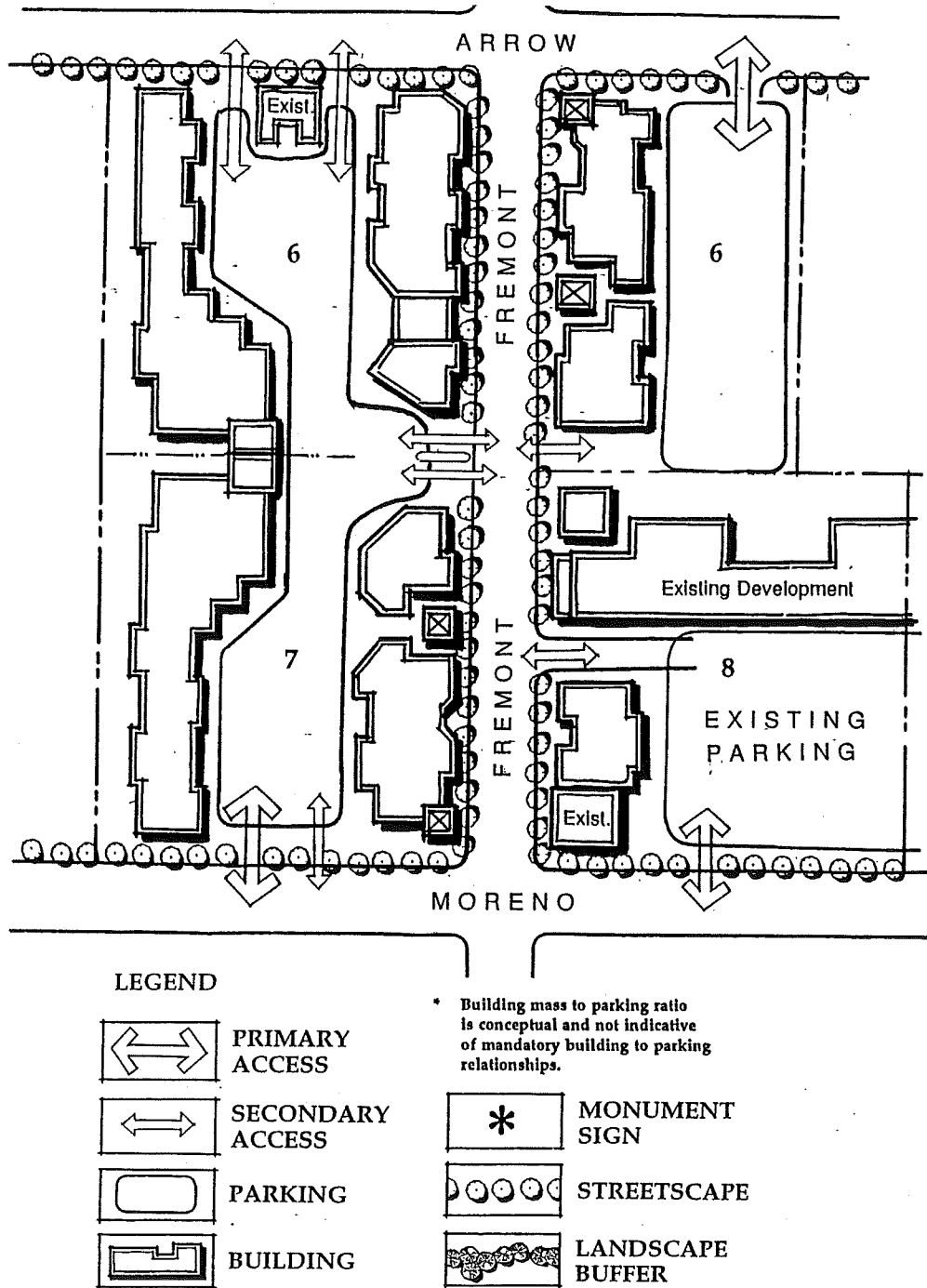
- ▲ 15' wide sidewalk (in public R.O.W.)
- ▲ Formal, 40' o.c. single row of London Plane trees in a tree grate adjacent to curb (in public R.O.W.)
- ▲ Parking lot lights: "Duluth" style (FXDL30) double fixture on W209 pole (by Western Lighting)

e. *Undesirable Uses*

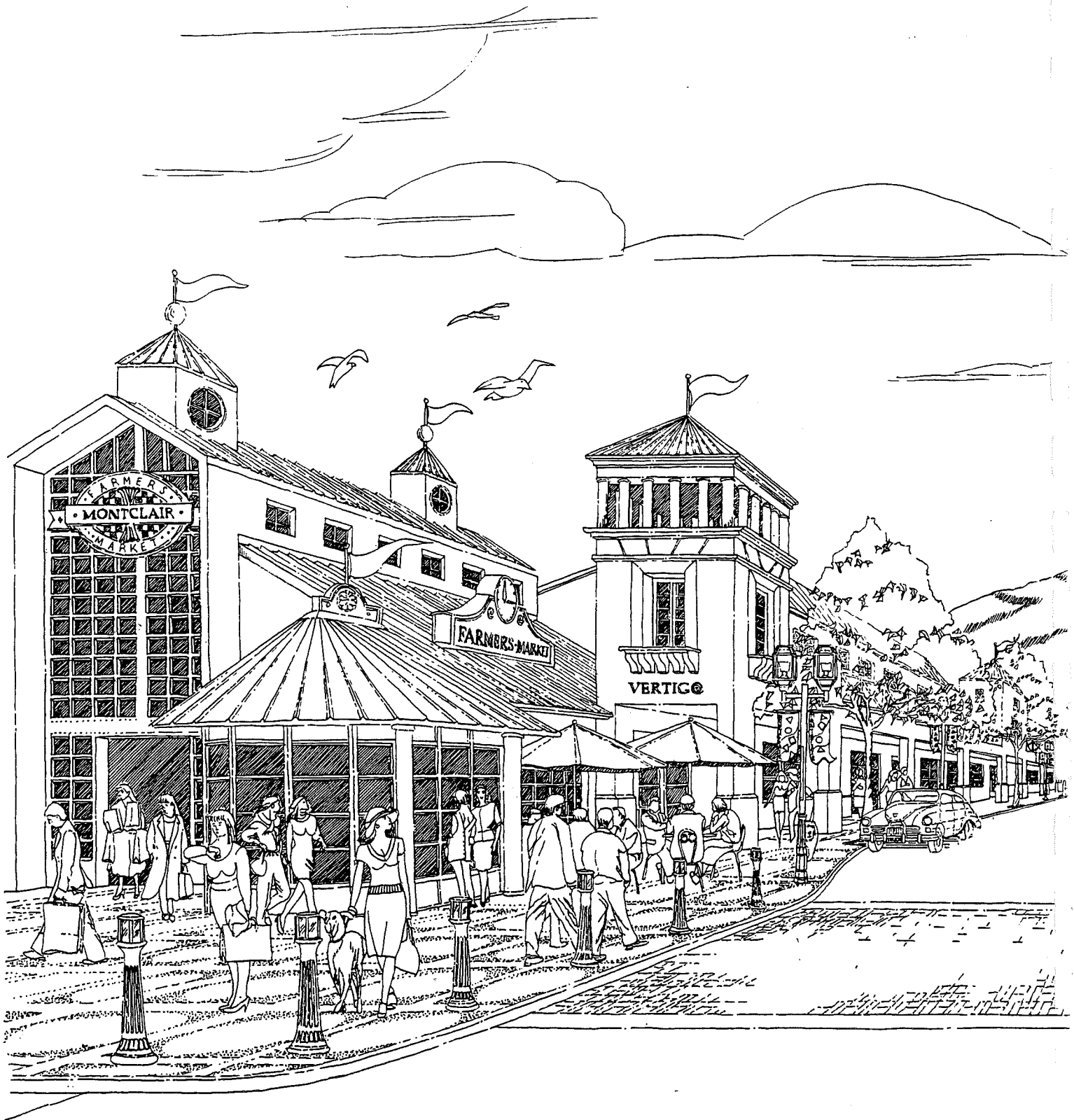
- ▲ Industrial uses.
- ▲ Automotive sales, repair and service.
- ▲ Drive thru uses.

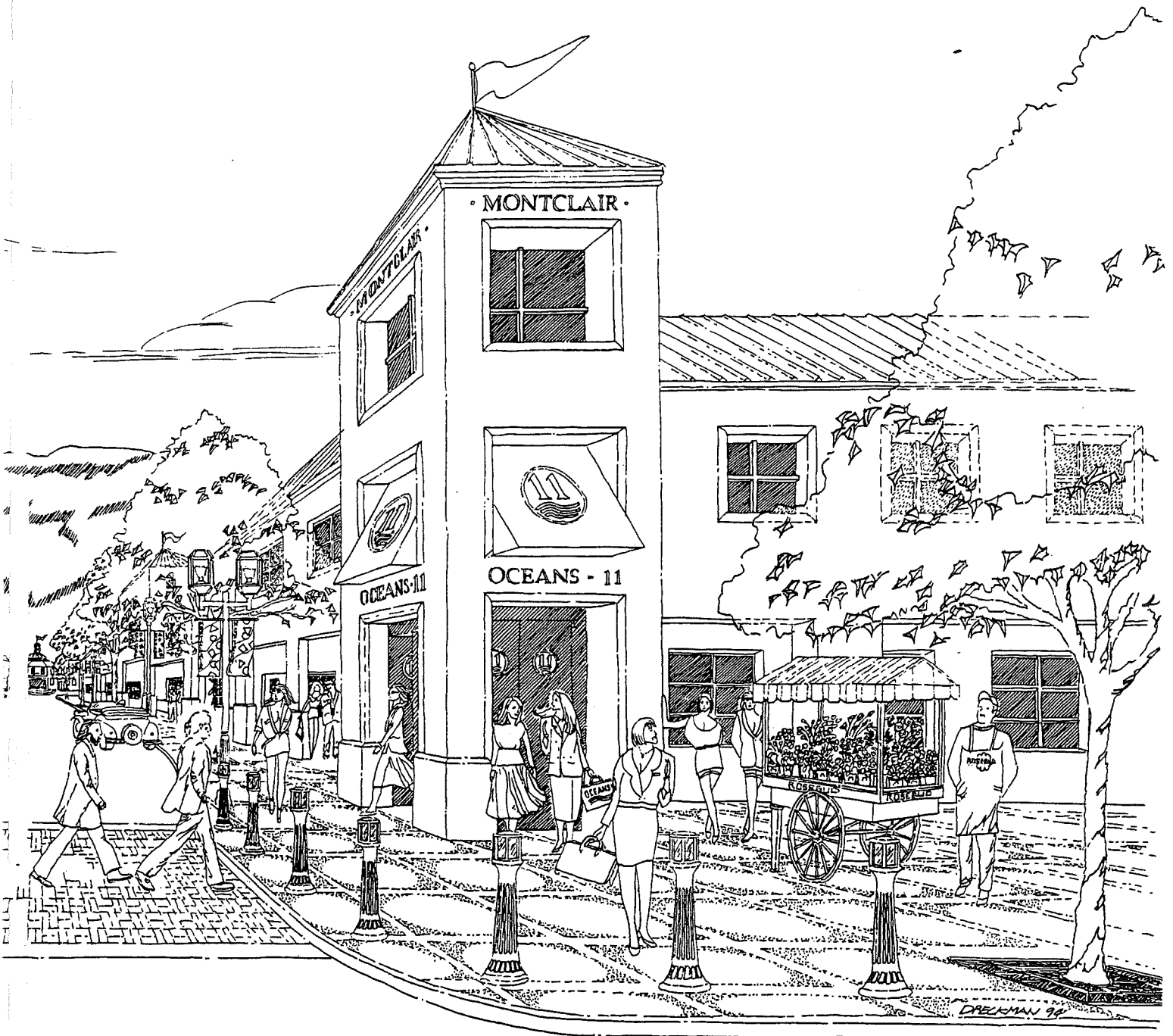
Focus Area Design Concepts

Area 6



FREMONT AVENUE PEDESTRIAN PROMENADE





10. **Area 7**a. **Site Profile**

Located on the northwest corner of Fremont Avenue and Moreno Street, this area is in a prominent location with regard to many of the Plan's features. It may also contain the highest number of individual property owners. Currently this area contains 40 single family homes (7.12 acres approximately).

b. **Land Use Concept**

The development envisioned for this area is commercial with retail storefronts along Fremont Avenue. Offices may be permitted as long as they do not directly front on Fremont Avenue or Moreno Street.

c. **Development Criteria**

In order for this site to develop to a commercial use from its present single family homes, approved combinations of lots shall be assembled and reverted to acreage prior to submittal of plans to the City for a commercial use. The minimum acceptable combinations of lot consolidations are illustrated on the following pages. However, it is preferred that the entire residential area be consolidated and reverted to acreage at the same time in order to avoid small piecemeal development. The following development criteria shall apply:

- ▲ Only one vehicle access driveway is permitted on Fremont Avenue (shared). Any development proposing a Fremont Avenue access shall illustrate how their site plan will accommodate vehicular access to future development. The City shall require the recordation of such reciprocal access to the site as a condition of approval;
- ▲ Two vehicular access driveways shall be permitted on Moreno Street. The first development proposing a Moreno Street access shall illustrate how their site plan will accommodate vehicular access to adjacent sites. The City shall require recordation of such reciprocal access to the site as a condition of approval;
- ▲ Building mass adjacent to Fremont Avenue shall be located to create a solid "street wall" with pedestrian orientation. (Refer to Design Guidelines on page IV-13);
- ▲ Two and three story building masses are encouraged;
- ▲ Small pedestrian plazas are encouraged along Fremont Avenue;
- ▲ Sidewalks along Fremont Avenue are to be 15' wide;
- ▲ No freestanding signs along Fremont Avenue; and
- ▲ One monument sign allowed on Moreno Street.

d. **Front Parkway Design****Fremont Avenue Edge:**

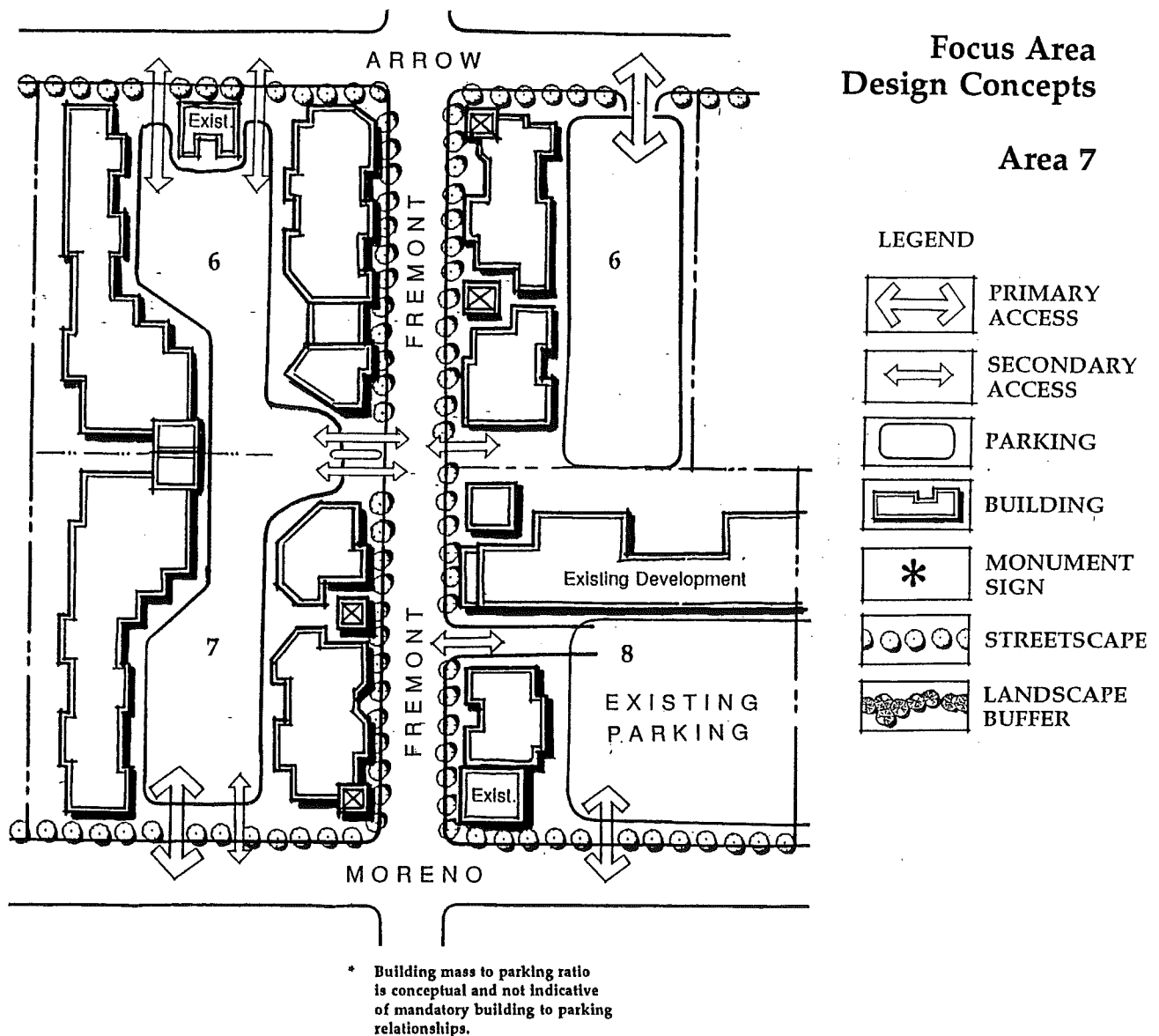
- ▲ 15' wide sidewalk (in public R.O.W.).
- ▲ Formal, 40' o.c. single row of London Plane trees in a tree grate adjacent to curb (in public R.O.W.).
- ▲ Parking lot lights: "Duluth" style (FXDL30) double fixture on W209 pole (by Western Lighting).

Moreno Avenue Edge:

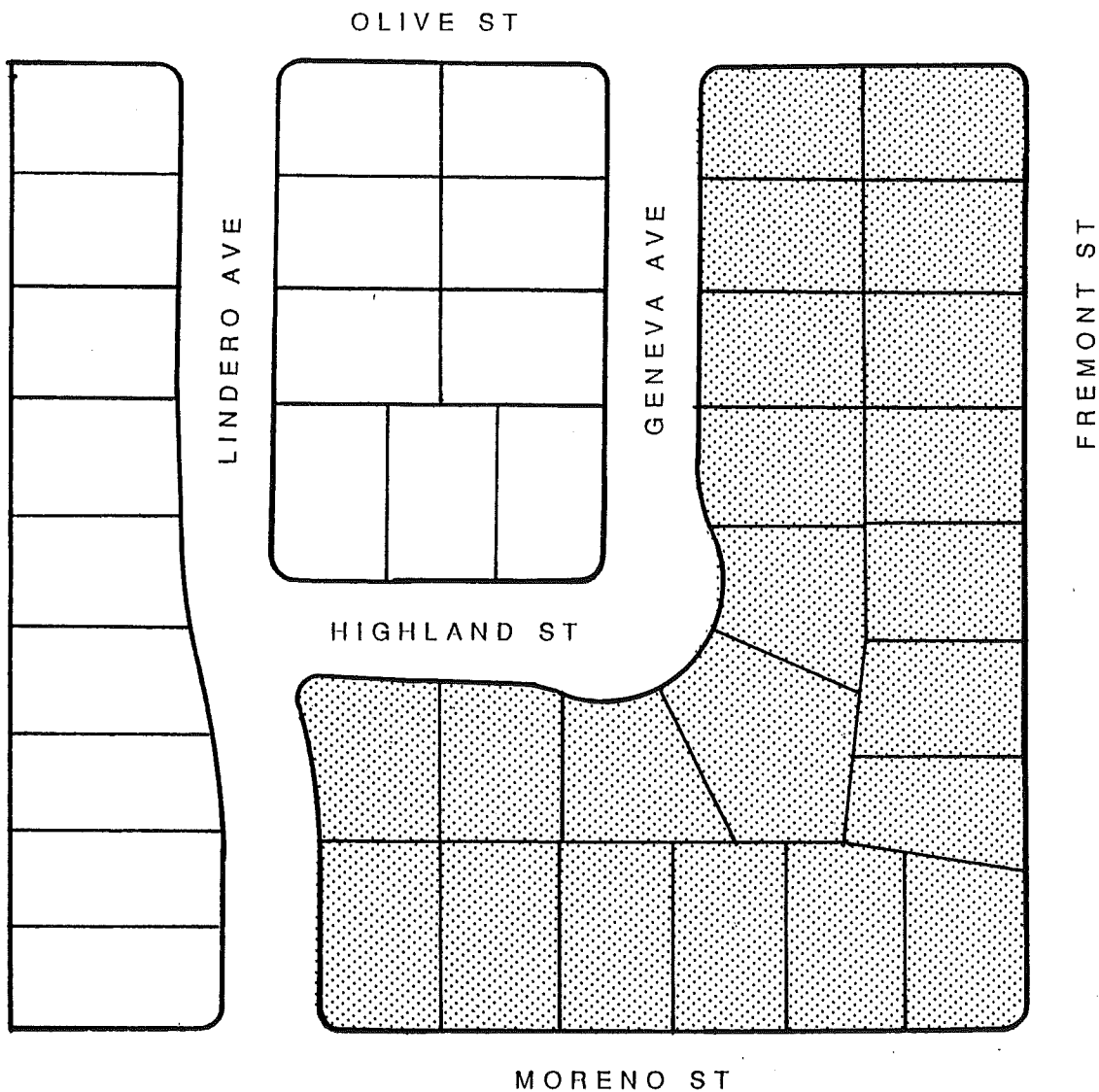
- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Evergreen Pear trees set back 4' from back side of sidewalk.
- ▲ KIM Lighting O.T.S. parking lot lighting to match street lighting.
- ▲ Monument sign to provide a polished stone base. Opaque field/background required on internally illuminated can-type monument sign.

e. *Undesirable Uses*

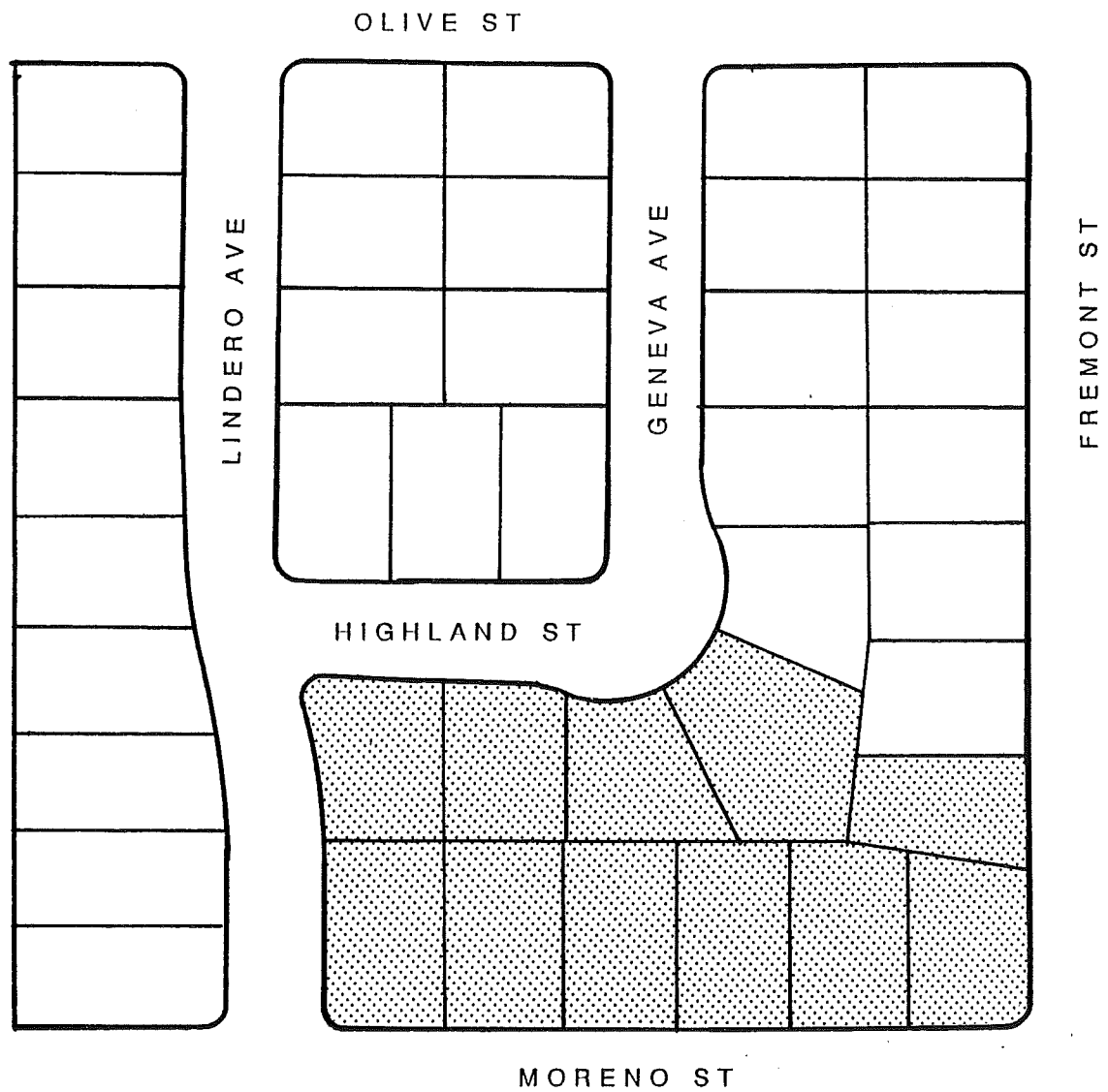
- ▲ Industrial Uses.
- ▲ Automobile sales, repair and service.
- ▲ Drive-thru uses.



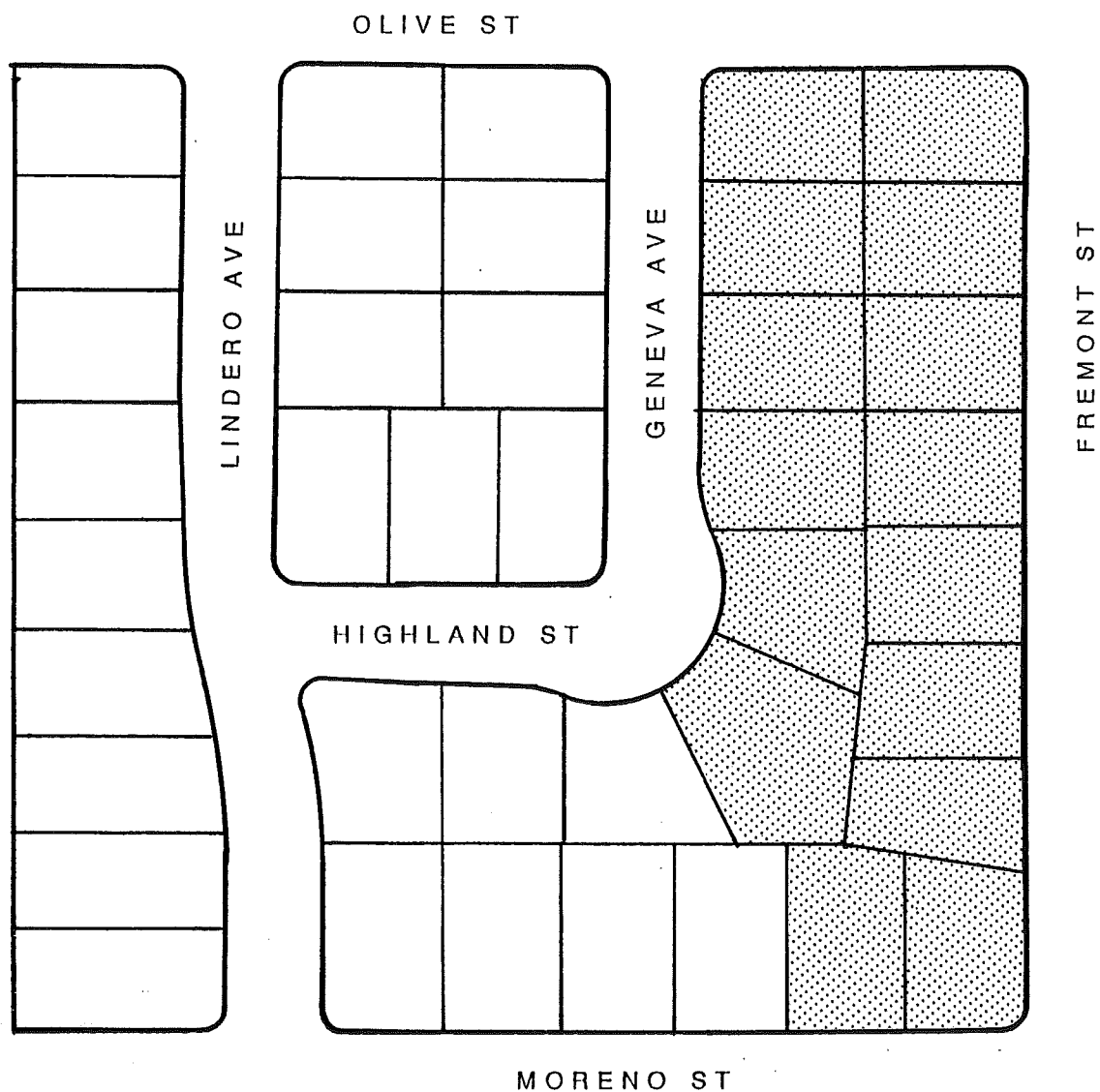
Lot Consolidation Alternatives



Lot Consolidation Alternatives



Lot Consolidation Alternatives



11. Area 8**a. Site Profile**

Located on the northeast corner of Fremont Avenue and Moreno Street, this area is in a prominent position to help establish the desirable pedestrian shopping environment along Fremont Avenue. The parcel currently contains a shopping center with a large chain electronics store, bank and drug store. The development concept is to keep the existing buildings and enhance the pedestrian scale along the Fremont Avenue frontage. This could be accomplished by adding more storefronts up to the sidewalk and extending existing buildings up to the same edge ("street wall").

b. Land Use Concept

The primary focus for this area is to intensify the Fremont Avenue edge with pedestrian oriented retail commercial stores.

c. Development Criteria

- ▲ All new building mass shall be adjacent to Fremont Avenue to assure a solid "street wall." Use street adjacent edge condition (see page IV-13)
- ▲ Plazas are encouraged along Fremont Avenue.
- ▲ No freestanding sign allowed along Fremont Avenue.
- ▲ Sidewalks along Fremont Avenue are to be 15' wide.
- ▲ One monument sign allowed on Moreno Street.

d. Front Parkway Design**Fremont Avenue Edge:**

- ▲ 15' wide sidewalk (in public R.O.W.)
- ▲ Formal, 40' o.c. single row of London Plane trees in a tree grate adjacent to curb (in public R.O.W.)
- ▲ Parking lot lights: "Duluth" style (FXDL30) double fixture on W209 pole (by Western Lighting).

Moreno Street Edge:

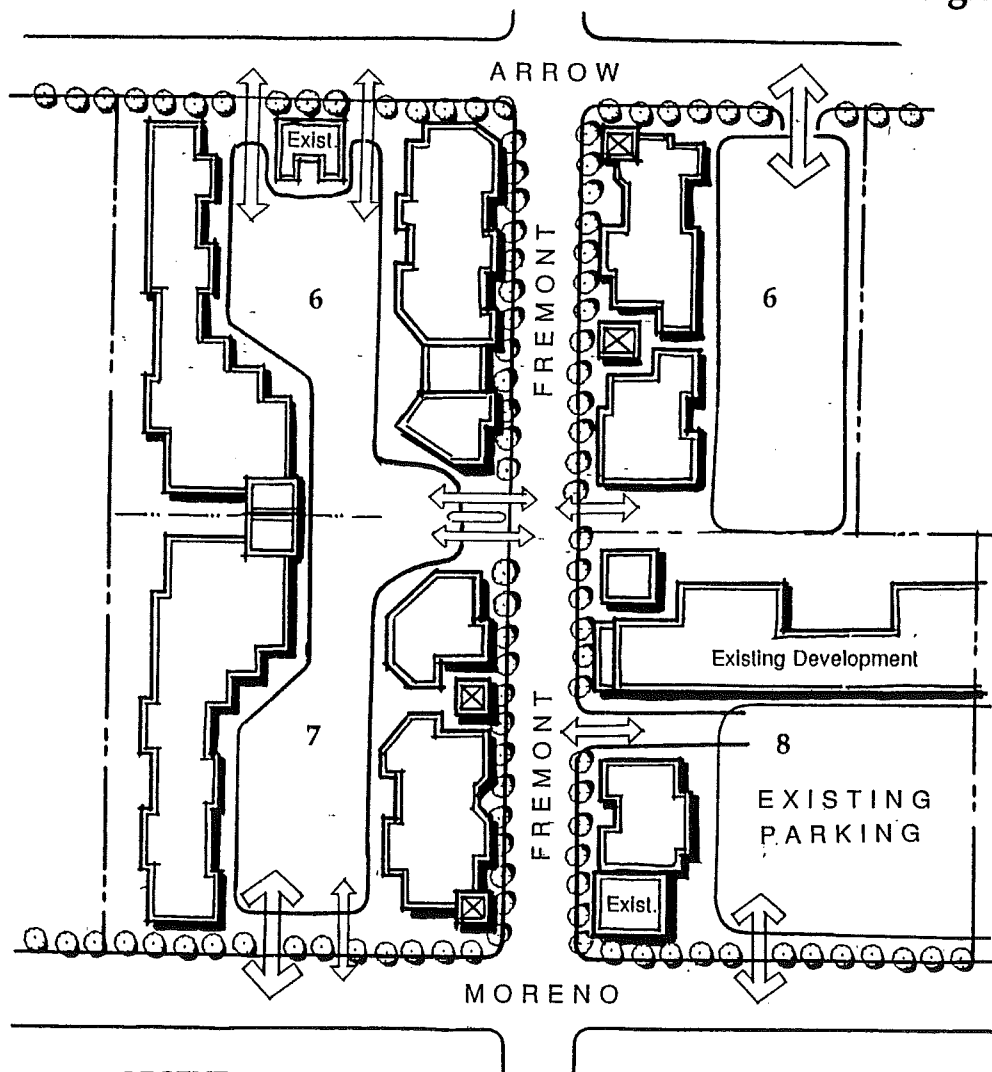
- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Evergreen Pear trees set back 4' from back side of sidewalk.
- ▲ KIM Lighting O.T.S. parking lot lighting to match street lighting.
- ▲ Monument sign to provide a polished stone base. Opaque field/background required on internally illuminated can-type monument sign.

e. Undesirable Uses

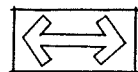
- ▲ Industrial Uses.
- ▲ Automobile sales, repair and service.
- ▲ Drive-thru uses.

Focus Area Design Concepts

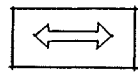
Area 8



LEGEND



PRIMARY
ACCESS



SECONDARY
ACCESS



PARKING

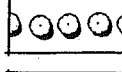


BUILDING

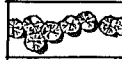
* Building mass to parking ratio
is conceptual and not indicative
of mandatory building to parking
relationships.



MONUMENT
SIGN



STREETSCAPE



LANDSCAPE
BUFFER

12. Area 9**a. Site Profile**

Area 9 is located on the northwest corner of the Monte Vista Avenue and Moreno Street intersection. The purpose of this focus area is to provide development criteria for potential rezoning of the 9 single family homes along Monte Vista from single family residential to neighborhood commercial (C2). Since Monte Vista is projected to carry significantly more traffic in the future, these homes may become severely impacted (1.5 acres approximately).

b. Land Use Concept

The primary use envisioned for this area is retail commercial once all 9 lots have been assembled and reverted to acreage. Commercial uses in this area are not considered appropriate unless all parcels fronting on Monte Vista Avenue are assembled and developed at one time. An amendment to the existing Specific Plan land use designation will be required.

c. Development Criteria

- ▲ The access from Olive Avenue to Monte Vista Avenue is to be abandoned.
- ▲ A landscape/sound buffer shall be incorporated adjacent to the remaining single family homes.
- ▲ Due to projected heavy traffic on Monte Vista Avenue, only one vehicular access shall be permitted on Monte Vista Avenue. A secondary access may be permitted on Moreno Street, subject to the findings of a traffic study.
- ▲ Two monument signs are permitted on the Monte Vista Avenue frontage.
- ▲ A pedestrian access point shall be established to connect the existing single family neighborhood to any new commercial development.

d. Front Parkway Design

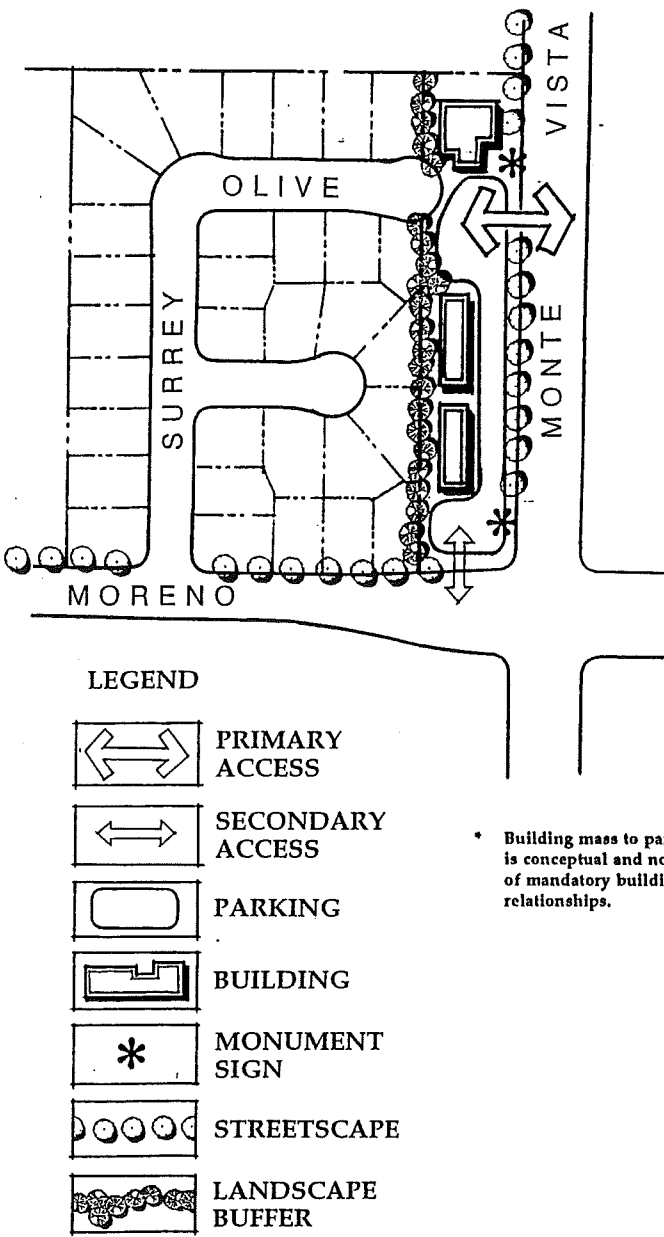
- ▲ 25' wide parkway landscape strip.
- ▲ Formal. 40' o.c. single row of Canary Island Pines or Brisbane Box trees set back 12' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Special driveway entry paving.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

e. Undesirable Uses

- ▲ Industrial use.
- ▲ Residential uses.
- ▲ Outdoor storage.

Focus Area
Design Concepts

Area 9



13. **Area 10**a. ***Site Profile***

Area 10 is located on the southwest corner of Monte Vista Avenue and Moreno Street. This focus area includes only the first two parcels along Moreno Street (.39 acres approximately).

b. ***Land Use Concept***

The land use concept is to allow commercial development on this corner.

c. ***Development Criteria***

- ▲ The site's primary vehicular access shall be from Moreno Street.
- ▲ One monument sign on Monte Vista Avenue is allowed.
- ▲ Building mass to be directly behind landscape parkway.
- ▲ Project design shall demonstrate sensitivity to adjoining school, day care and institutional uses.

d. ***Front Parkway Design***Monte Vista Edge:

- ▲ 40' wide parkway landscape strip.
- ▲ Formal, 40' o.c. double row of Canary Island Pine or Brisbane Box trees set back 12' and 24' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Special basketweave pattern of blue and red brick pavers at driveways.
- ▲ Monument signs to contain a polished stone veneer base. Opaque field/background required on internally illuminated can-type monument sign.

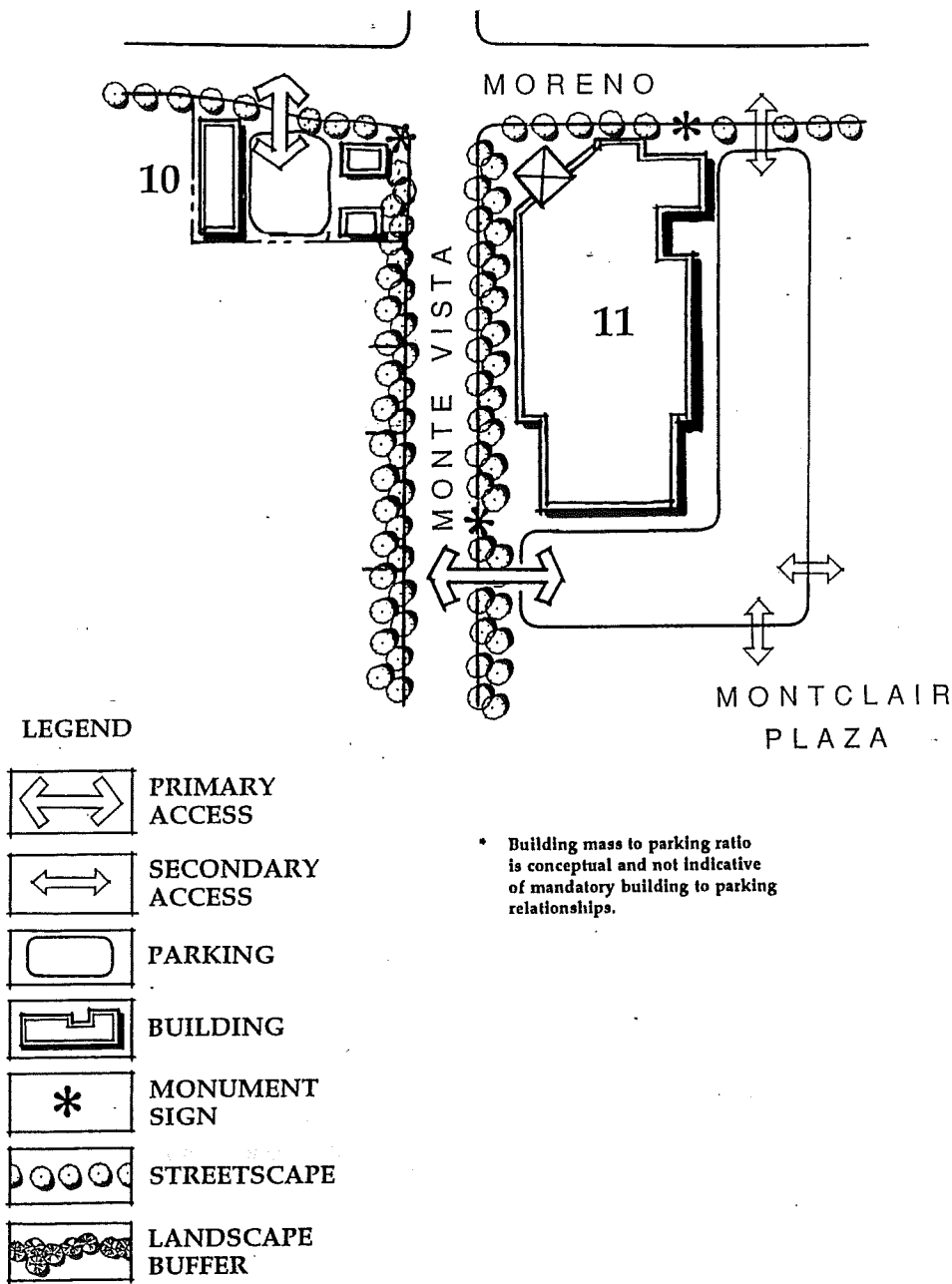
Moreno Street Edge:

- ▲ 15' wide parking landscape strip.
- ▲ Formal, 30' o.c. single row of trees (to be approved by City).

e. ***Undesirable Uses***

- ▲ Industrial uses.
- ▲ Residential uses.
- ▲ Automotive service or sales use.
- ▲ Outdoor storage.

Focus Area
Design Concepts
Area 10



14. Area 11**a. Site Profile**

Area 11 is located at the southeast corner of Monte Vista Avenue and Moreno Street and includes six separate parcels utilized for the Unitarian Church, multi-tenant commercial center and several vacant parcels. The site is located in the northwest corner of the Montclair Plaza (3.9 acres approximately).

b. Land Use Concept

The land use concept for this area is to provide for development that will be compatible with surrounding regional commercial land uses and avoid strip commercial development. Compatibility of architectural design and building materials will be important to enhance the visual unity with the adjoining uses. To achieve compatibility of land use and design, the development of individual parcels is prohibited except under special circumstances with the cooperation of all property owners through a comprehensive development plan.

c. Development Criteria

- ▲ Development shall be regional commercial in nature and compatible with adjoining regional land uses.
- ▲ Development shall be tied physically (through access and design) and economically to the adjoining regional shopping center.
- ▲ The development of individual parcels is prohibited except that phased development may occur if: 1) a development plan is prepared for the entire area, and 2) if all property owners of the area agree with the provisions of the development plan.
- ▲ Development shall be compatible with the architectural design, building materials and architectural detailing of the adjoining regional/commercial land uses.
- ▲ The conversion of single-family dwelling units to commercial uses shall not be permitted.
- ▲ Buildings shall be oriented to both Monte Vista Avenue and to the regional shopping center.
- ▲ All development shall be subject to the approval of a development plan for the entire area which should follow one of the alternative design concepts shown on pages III-74 and III-75.
- ▲ The site's primary access shall be from Monte Vista Avenue.
- ▲ Two monument signs shall be allowed; one along Monte Vista Avenue and one along Moreno Street.
- ▲ Two and three story building mass is encouraged.
- ▲ A corner tower element is desirable.

d. *Front Parkway Design*

Monte Vista Avenue Edge:

- ▲ 40' wide parkway landscape strip.
- ▲ Formal, 40' o.c. double row of Canary Island Pine or Brisbane Box trees set back 12' and 24' respectively from back of sidewalk.
- ▲ KIM Lighting O.T.S. parking lot lighting.
- ▲ Basketweave pattern of blue and red brick pavers at driveway entries.
- ▲ Monument signs to contain a polished stone veneer base. Opaque field/background required on internally illuminated can-type monument sign.

Moreno Street Edge:

- ▲ 25' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Evergreen Pear trees set back 4' from back of sidewalk.
- ▲ KIM Lighting O.T.S. parking lot lighting.
- ▲ Basketweave pattern of blue and red brick pavers at driveway entries.
- ▲ Monument signs to contain a polished stone veneer base. Opaque field/background required on internally illuminated can-type monument sign.

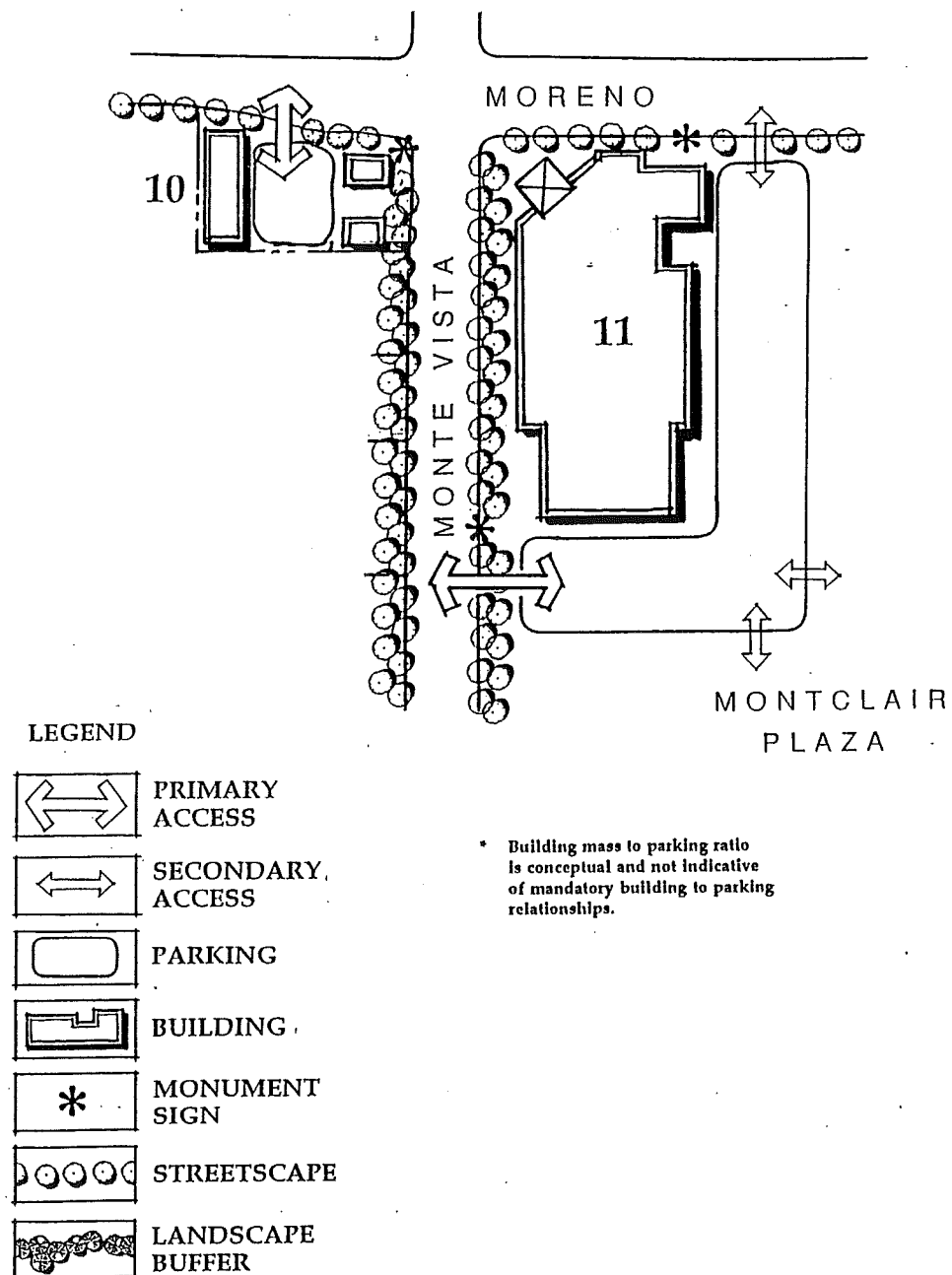
e. *Undesirable Uses*

- ▲ Industrial uses.
- ▲ Automotive sales, repair and service.
- ▲ Drive-thru restaurants.
- ▲ Residential uses.

Focus Area
Design Concepts

Area 11

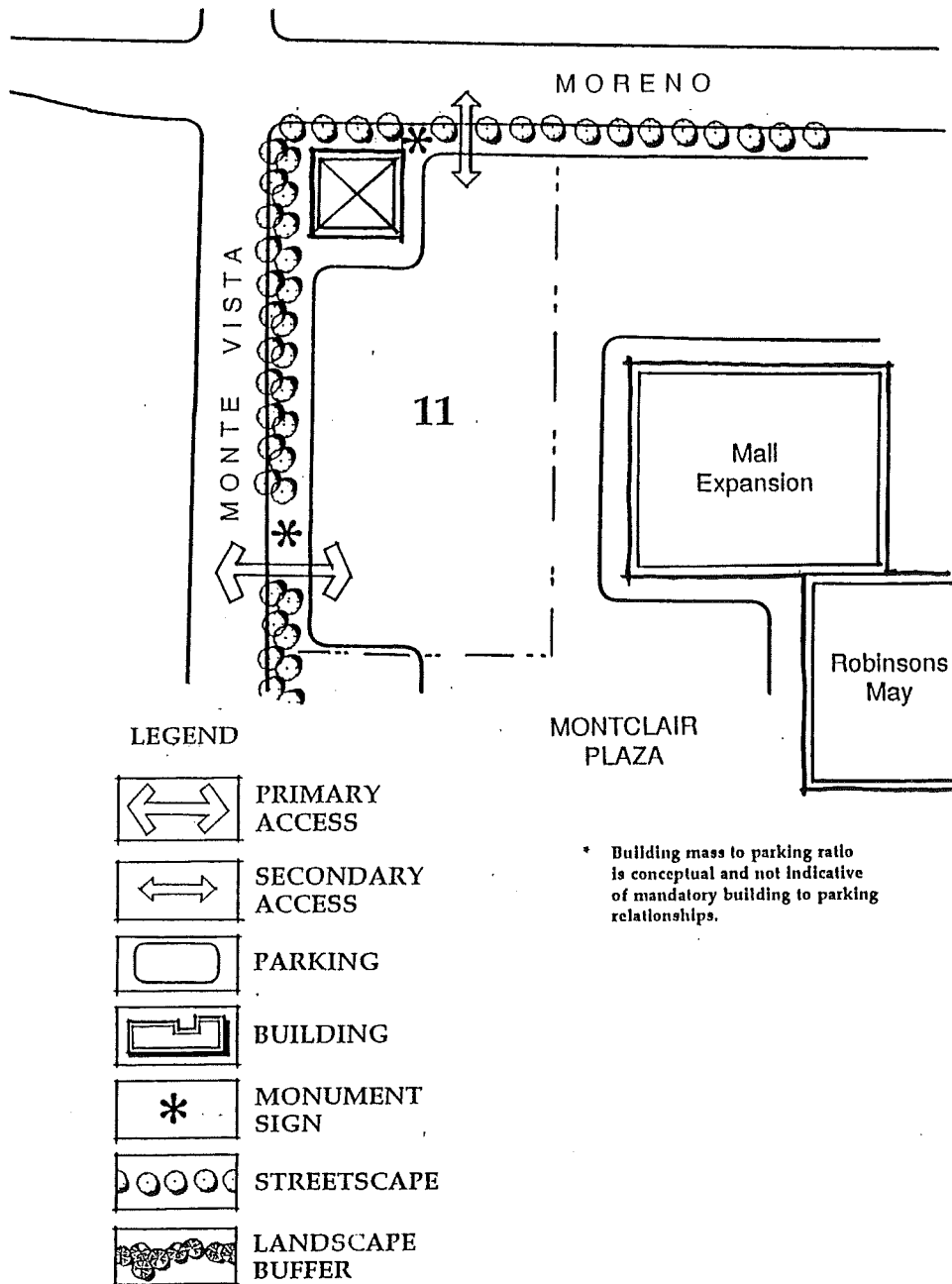
Alternative 1



Focus Area
Design Concepts

Area 11

Alternative 2



15. **Area 12**a. ***Site Profile***

- ▲ Located on Arrow Highway, just east of Central Avenue, this single parcel is currently vacant. The site is located in between a commercial shopping center on the west and a business park use on the east (4.5 acres approximately).

b. ***Land Use Concept***

- ▲ The primary land use concept for this area is a business park. Commercial uses may be located along the Arrow Highway frontage.

c. ***Development Criteria***

- ▲ The site's primary access shall be on Arrow Highway.
- ▲ A single monument sign is allowed on Arrow Highway.
- ▲ Building mass should be clustered in a campus format with only limited parking allowed between Arrow Highway and the front buildings.
- ▲ A landscape buffer should be developed adjacent to the railroad tracks.
- ▲ One monument sign allowed.

d. ***Front Parkway Design***

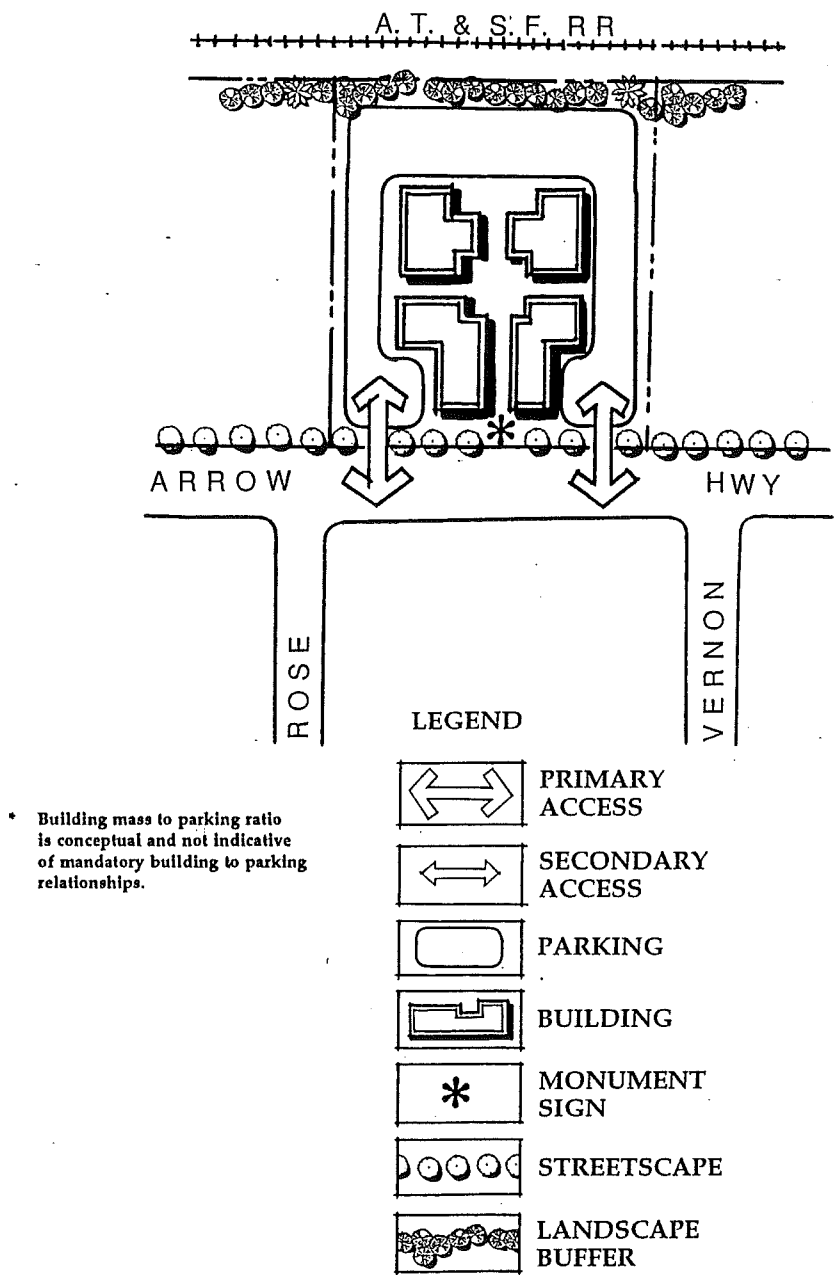
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Oak trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

e. ***Undesirable Uses***

- ▲ Heavy industrial uses.
- ▲ Residential uses.
- ▲ Quasi public uses.

Focus Area
Design Concepts

Area 12



16. Area 13**a. Site Profile**

This large multiple owner focus area is located on the south side of Arrow Highway and east of Vernon Street. The site is partially vacant. A water utility facility is also located here (14.14 acres approximately).

b. Land Use Concept

The land use concept for this area is for light industry or business park.

c. Development Criteria

- ▲ The site's primary access shall be from Arrow Highway.
- ▲ The site's secondary entries may be from Vernon Street and Olive Street.

d. Front Parkway Design

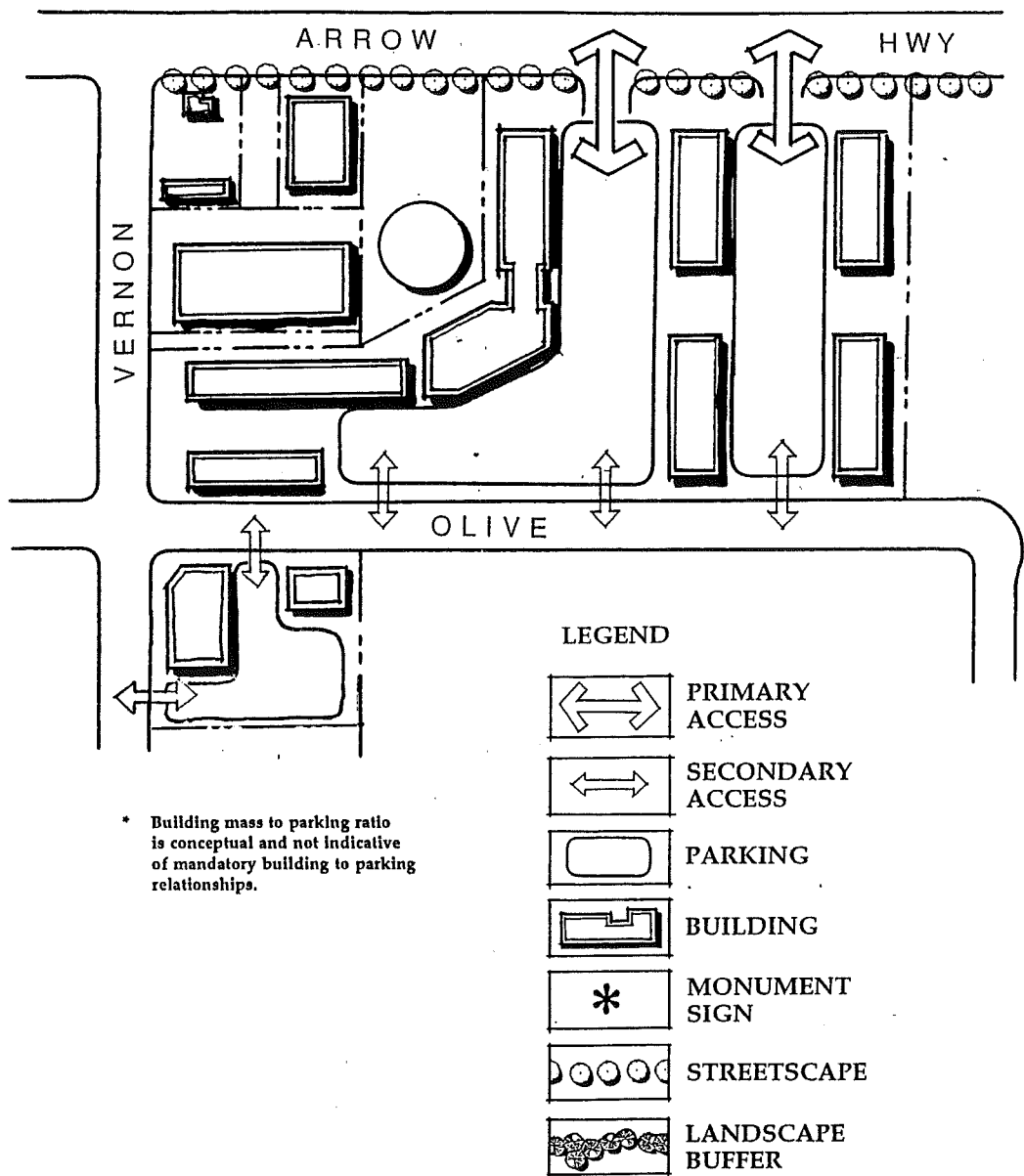
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Oak trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

e. Undesirable Uses

- ▲ Heavy industrial uses.

Focus Area
Design Concepts

Area 13



17. **Area 14**a. ***Site Profile***

This is a prominent focus area located in the southeast corner of the Specific Plan area. The site is positioned between the San Bernardino Freeway and Moreno Street west of Benson Avenue. A large portion of the site is the previous Grand Prix race car amusement site (13.73 acres approximately).

b. ***Land Use Concept***

The land use concept for this area is freeway oriented commercial or new auto sales.

c. ***Development Criteria***

- ▲ Building masses should vary between one, two and three story elements.
- ▲ An attractive landscape parkway should be located adjacent to the San Bernardino Freeway.
- ▲ A freeway oriented pylon sign may be justified if the site develops as a regional commercial use, subject to compliance with the Montclair Sign Ordinance.
- ▲ Two monument signs shall be allowed along Moreno Street.
- ▲ A single monument sign shall be allowed on the Benson Avenue.
- ▲ The majority of parking should be located between the buildings and the San Bernardino Freeway, not along Moreno Street.
- ▲ Through access shall be maintained to the Montclair East Shopping Center.

d. ***Front Parkway Design***

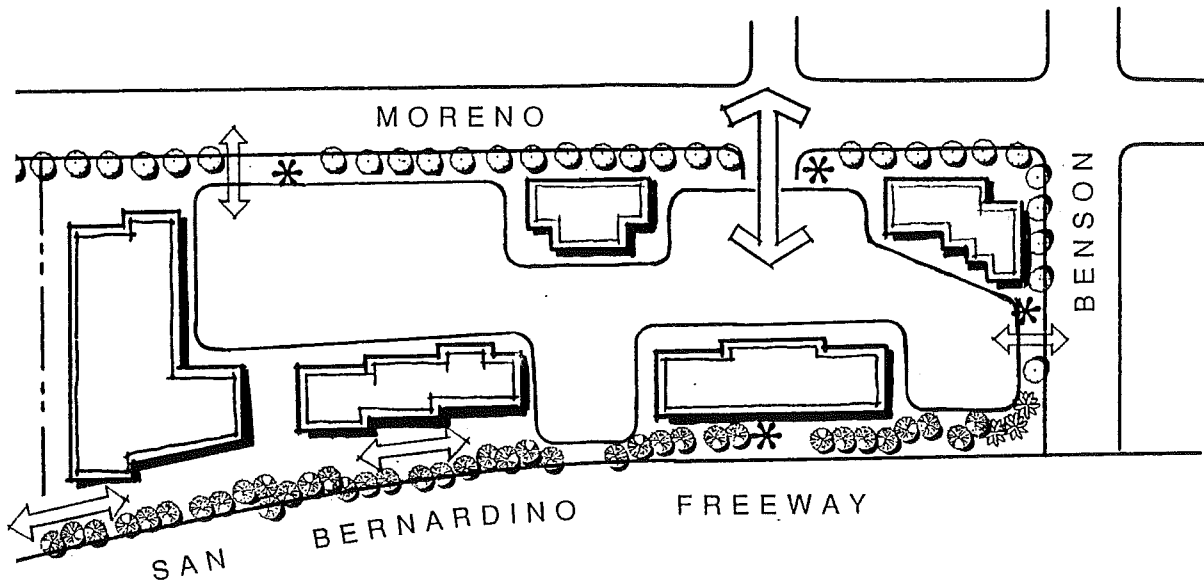
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 30' o.c. single row of Evergreen Pear trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument signs to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument signs.

e. ***Undesirable Uses***

- ▲ Industrial uses.
- ▲ Any use with outdoor storage, except new vehicles for sale.

Focus Area
Design Concepts

Area 14



LEGEND

* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

18. Area 15a and 15b**a. Site Profile**

Referred to as the "K-Mart site," Area 15a is fully developed with a multiple tenant shopping center and several smaller, individually owned parcels on the southwestern portion of the area. The area is located on the northwestern corner of the Central Avenue and Palo Verde Street intersection. Area 15b includes condominium units and a church. Together, focus areas 15a and 15b constitute (15.82 acres approximately).

b. Land Use Concept

The primary use for this area is commercial development (Alternative 1) or automobile dealership expansion (Alternative 2). If dealership expansion occurs, Carrillo Street may be abandoned by the City.

c. Development Criteria

- ▲ Primary site access shall be from Central Avenue with secondary entries along Palo Verde permissible.
- ▲ Additional satellite pad buildings may be approved along Central Avenue if the retail shopping center is to remain.
- ▲ The City may consider the abandonment of Carrillo Street as a development incentive.
- ▲ A consistent building design is required for all commercial buildings.
- ▲ Building mass should be articulated into smaller separate and district structures.
- ▲ A landscaped parkway adjacent to the freeway ramp is very desirable.
- ▲ Three monument signs are allowed on Central Avenue and 2 on Palo Verde Street.

d. Front Parkway Design

- ▲ 25' wide parkway landscape strip on Central; 20' wide parkway on Palo Verde.
- ▲ Formal, 40' o.c. single row of Canary Island Pine trees set back 7' from the sidewalk on Central; Carrotwood Trees 40' o.c., 4' from sidewalk on Palo Verde.
- ▲ Monument sign to contain a river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

e. Undesirable Uses

- ▲ Residential uses.
- ▲ Industrial uses.
- ▲ Public/quasi public uses.

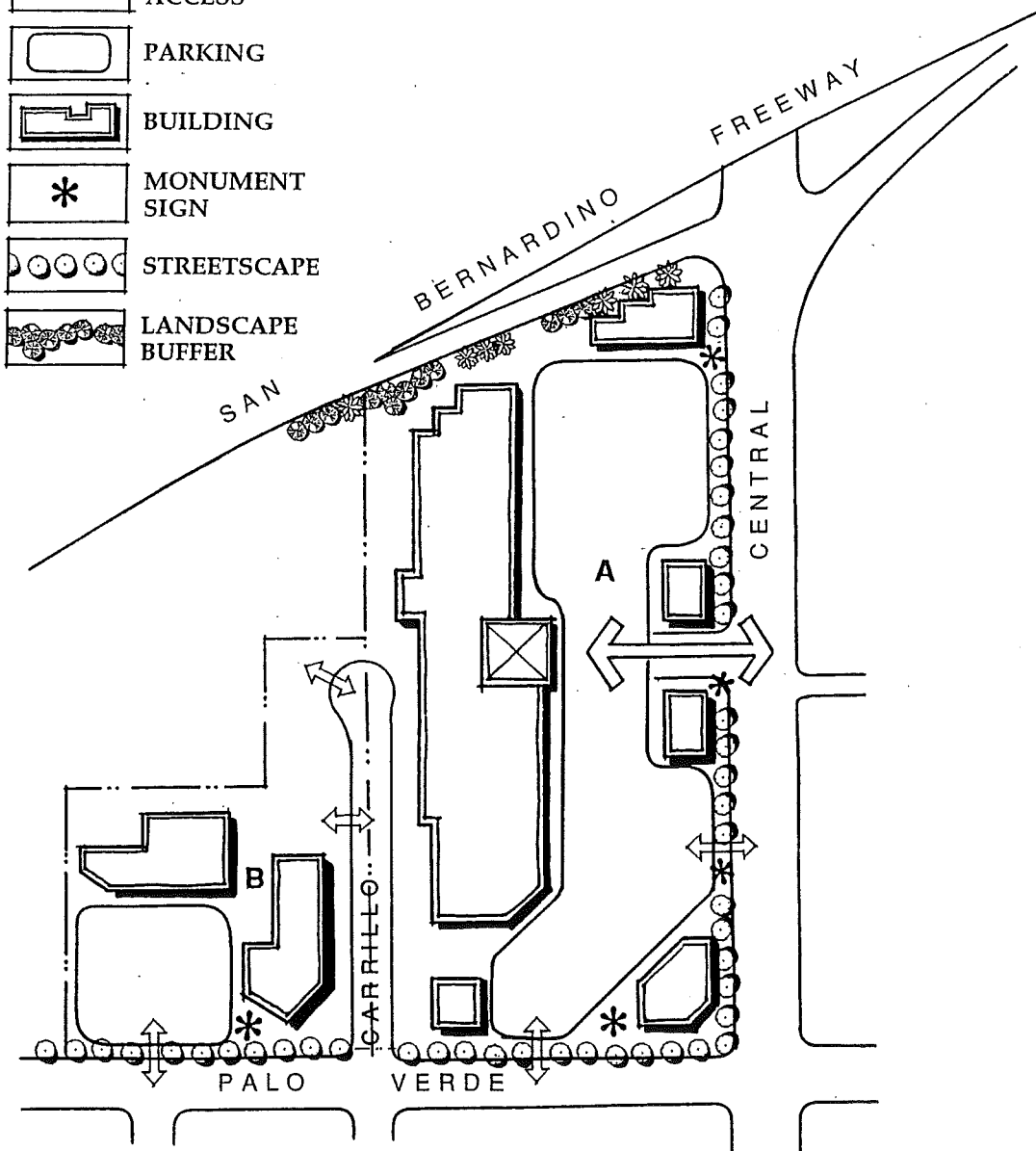
Focus Area Design Concepts

Area 15a, 15b

Alternative 1

LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER



* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

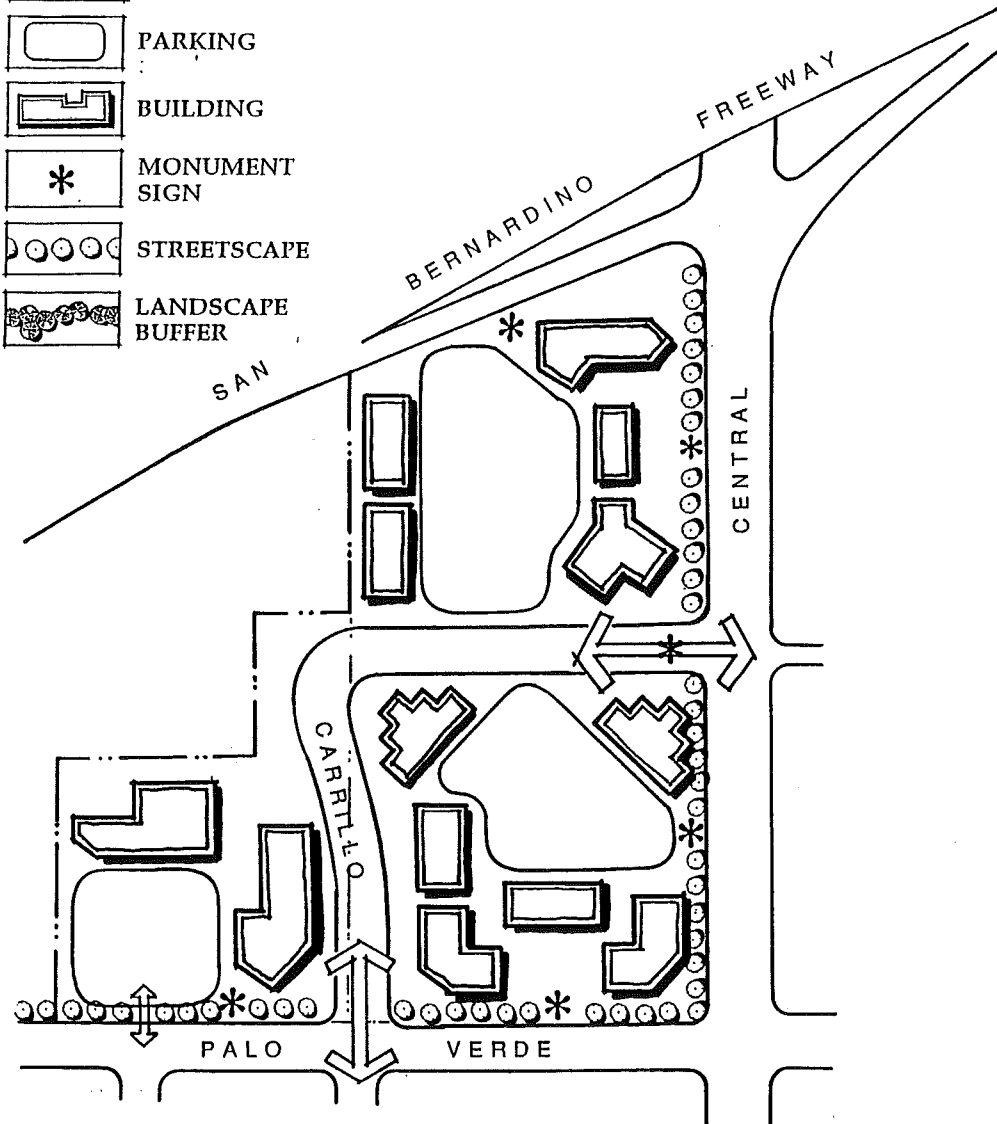
Focus Area Design Concepts

Area 15a, 15b

Alternative 2

LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER



* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

19. **Area 16**a. ***Site Profile***

This small parcel is a remnant site created by the freeway on-ramp construction at Monte Vista Avenue and Palo Verde Street. The site is bounded by the eastbound I-10 ramp and Palo Verde Avenue (2.83 acres approximately). The adjacent City-owned site to the west includes a public art sculpture and several "landmark" trees that will be preserved. This is a key entry point into North Montclair and should be appropriately landscaped to emphasize its importance.

b. ***Land Use Concept***

Due to size and access constraints on the site, it may develop as either commercial or office.

c. ***Development Criteria***

- ▲ One vehicular access point is allowed on Palo Verde Avenue.
- ▲ A landscape buffer adjacent to the freeway ramp shall be provided.
- ▲ A secondary access should be provided through a reciprocal access agreement with the adjacent property to the east.
- ▲ The adjacent site to the west may be able to accommodate a small parking lot in order to assist in the development of Area 16.
- ▲ A single monument sign is allowed on Palo Verde Avenue.

d. ***Front Parkway Design***

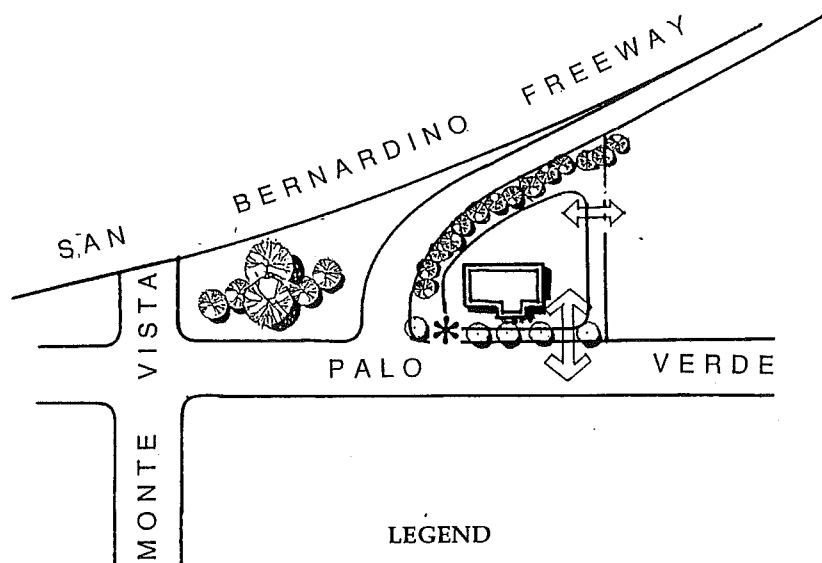
- ▲ 20' wide parkway landscape strip.
- ▲ Formal, 40' o.c. single row of Carrotwood trees set back 4' from back of sidewalk.
- ▲ Contemporary shoebox style parking lot lights.
- ▲ Monument sign to have opaque field/background if internally illuminated can-type monument sign.

e. ***Undesirable Uses***

- ▲ Industrial uses.
- ▲ Residential uses.

Focus Area Design Concepts

Area 16



LEGEND

	PRIMARY ACCESS
	SECONDARY ACCESS
	PARKING
	BUILDING
	MONUMENT SIGN
	STREETSCAPE
	LANDSCAPE BUFFER

* Building mass to parking ratio is conceptual and not indicative of mandatory building to parking relationships.

CHAPTER IV

DESIGN

GUIDELINES

Introduction

A. Purpose Of The Design Guidelines

The North Montclair design guidelines are intended to promote high quality design and ensure that new development fits within the context of surrounding properties and the North Montclair area as a whole. The design guidelines establish the City's design philosophy for the North Montclair area and are one of the main elements used in the evaluation of development projects that are subject to discretionary design review by the City.

Discretionary design review makes a careful examination of a project's quality of site planning, architecture, landscape design and important details such as signs and lighting. The purpose is to ensure that each new development carefully considers the community context in which it takes place and makes a conscientious effort to develop

a compatible relationship to neighboring properties and to the City's urban design goals/objectives.

The design guidelines are intended to implement the following Community Design objectives and policies contained in the General Plan:

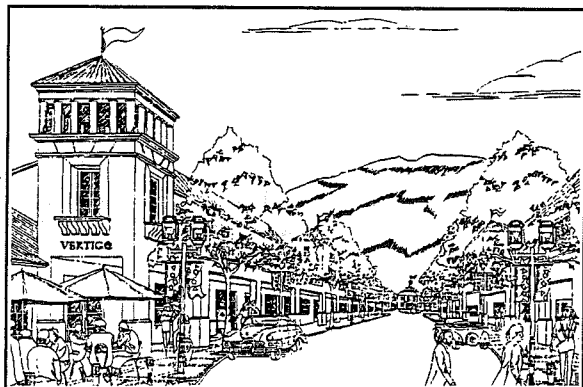
Objectives

- To promote community identity and community aesthetics as a means for creating a positive living and working environment as well as to maintain high economic stability.
- To develop procedures which will permit the City to require aesthetic treatment in all new development.

Policies

- Site planning, architectural and landscape architectural design should result in an attractive appearance and a harmonious relationship among the various elements of the development to blend with the image of the community.
- Continue the establishment of an individual and distinctive identity by encouraging the highest quality design in architecture, landscape architecture, sign graphics and in the design of street furniture and fixtures.
- The size, height, number and type of on-premises signs allowed should be the minimum necessary for identification. The design, materials, color, texture and location should relate to and be in harmony with the surrounding environment. Sign regulations should be based on the premise that the purpose of signing is for identification and not as means of advertising.

"Quality Development" as used within the context of this document means development that is in compliance with the development standards and design guidelines set forth in this Specific Plan and any other applicable development regulations of the City.



B. When Are The Guidelines Applied?

The design guidelines and the design review process are applicable whenever any of the following occur:

- Any new development for which a building permit is required;
- Any addition to or enlargement of an existing structure or use except additions to existing residential uses; or
- Any change of use or reuse where City approval is required and established after a vacancy of 6 months or more.

An applicant who is changing only a portion of an existing building need only comply with the guidelines related to the portion changed and to directly related portions. In the event that proposed modifications affect more than 60 percent of any facade visible to a public parking area or public right-of-way, or the Community Development Director determines that the proposed changes are significant, the applicant shall be required to comply with all portions of the design guidelines/standards for the entire facade and all landscaping, parking and signs on the site. In addition, activities not required to apply for a building permit (i.e. minor repairs, painting) are encouraged to follow the guidelines whenever possible.

Implementation of the design guidelines will be through the City's **Plan Review Process** as established under Chapter 5 of the Municipal Code. In making its findings on specific project applications, the Development Review Committee, Planning Commission and City Council shall utilize, and make reference to, the design guidelines contained in this section.

C. Organization Of The Guidelines

The design guidelines contained in this section are organized as follows:

Context And Compatibility Guidelines

- Compatible Design
- Project Types
- Edge Conditions

Commercial Development Design Guidelines

- Site Planning
- Architecture
- Lighting
- Building Equipment And Services
- Walls And Fences
- Design For Crime Prevention
- Parking, Circulation And Access

Business Park/Industrial Design Guidelines

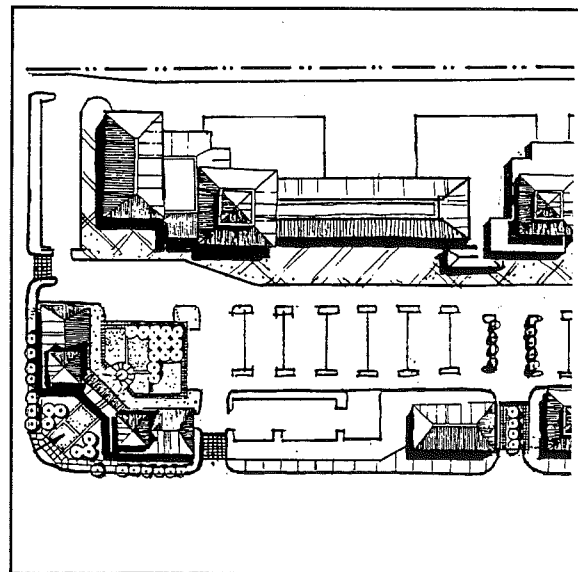
- Site Planning
- Architecture
- Roofs
- Lighting
- Parking And Circulation
- Loading Facilities
- Landscaping
- Walls And Fences
- Screening

Design Guidelines For Signs

- Introduction
- General Design Guidelines
- Facade Signs
- Freestanding Signs
- Banner Signs

Landscape Design Guidelines

- Landscape Concept
- Aesthetic Considerations
- General Landscaping Guidelines
- Plant Palette
- Hardscape Guidelines



Context And Compatibility Guidelines

A. Compatible Design

The concept of "compatible" design is one of the most important concepts to be aware of when using these design guidelines. Compatible designs do not seek to imitate neighboring buildings, but do reflect their surroundings in terms of design concepts - mass, scale, rhythm, color and building arrangement. Compatible designs will be in harmony with the best designs of surrounding developments.

In order to differentiate between types of compatibility issues and the relationship between buildings and the public right-of-way, two special terms or labels have been developed to cover the majority of situations found in the North Montclair project area: 1) project types, and 2) edge conditions.

B. Project Types

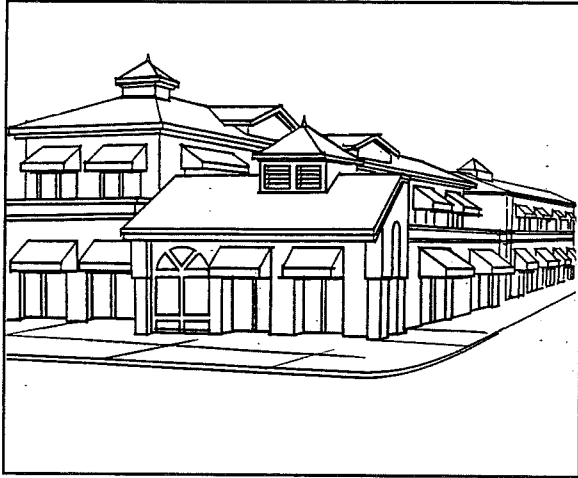
An important design consideration is the type of project being proposed. **Project Types** are used to differentiate specific building/site arrangements that are common to the North Montclair area and that should be considered as appropriate models for future development. The project types are:

1. Freestanding Commercial/Office Buildings

This category includes buildings housing from one to three uses, (generally only one) which are designed to stand apart from adjacent buildings. These buildings may range in size from the tiny fast food drive-in to a massive discount warehouse store.

Freestanding buildings have several design problems which are not shared by the other project types:

- Freestanding buildings generally are viewed from all sides so that landscaping and building materials must consider all sides of the building;



- Large freestanding buildings such as discount warehouses or "big box" users can easily overshadow smaller adjacent buildings. By themselves they can become large uninteresting boxes without appropriate elements designed to create a more human scale;
- Freestanding buildings have more opportunities for creative design since the design is directed toward a single use (e.g. a restaurant);
- Freestanding buildings have more opportunity for signs, but signs can more easily overwhelm the building; and
- A freestanding building usually has its own parking lot, typically with its own access point(s) and is usually segregated from parking lots on adjacent properties.

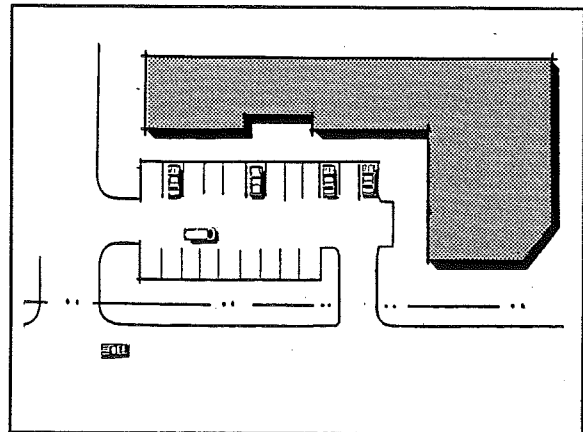
In particular, those buildings providing automotive sales or services have additional design problems, including:

- A need to provide show windows and display areas for large products (automobiles);
- A need to provide easy access for servicing vehicles and for service supplies, although these areas are often unsightly; and
- A perceived need for promotional advertising such as painted window signs, balloons, banners and temporary signs.

Special development regulations for automobile related uses are provided in Chapter III, page III-37.

2. Strip Commercial Developments

This development type includes buildings housing more than three uses/tenants, which are designed as a single unit oriented to a shared parking area. In responding to the needs of individual tenants, these buildings often have the greatest problems with maintaining consistency in materials and signage.

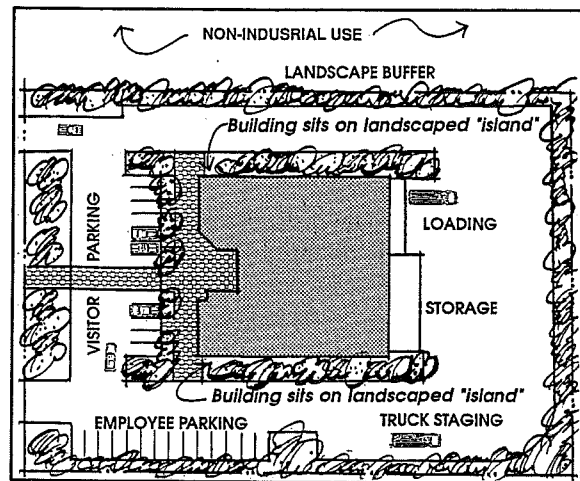


Strip developments exhibit several problems which are not shared by other project types:

- A need to provide storefront visibility and access for a number of tenants;
- A need to provide signage identifying a number of different tenants;
- A perceived need by tenants for individuality in storefront designs or signage;
- A need to provide landscaping to soften the building bulk without hiding signage or storefronts; and
- A need to provide convenient shared parking, often located between the street and the primary building facade.

3. Business/Industrial Parks

Like strip commercial developments, this development type also includes buildings housing three or more uses/tenants, but since their uses are primarily light industrial, as opposed to retail, the issues associated with business/industrial parks are different. To accommodate light industrial-type uses these buildings typically provide truck access and loading doors for each tenant space. Parking lots usually serve employees, although some easily accessible customer parking is also required.



Typical problems associated with business/industrial park developments include:

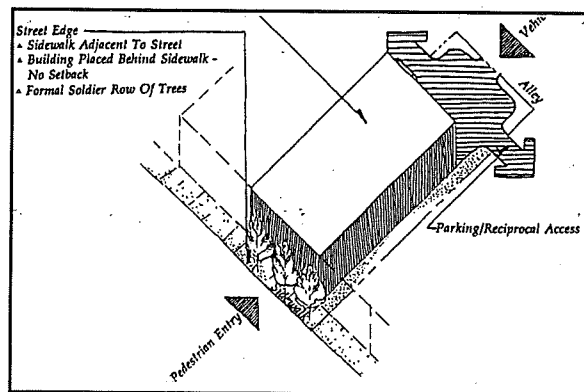
- A need to screen loading and storage areas from public view;
- A need to accommodate a large number of truck deliveries;
- A need to avoid typical box-like architecture by providing wall articulation, varied roof forms and the use of landscaping to buffer bulky building shapes and to screen loading areas;
- A need to provide separate parking areas for long-term employee parking and more convenient short-term customer parking; and
- A need to provide clear circulation routes with adequate directional signs for truck deliveries.

C. Edge Conditions

The "edge condition" of a particular development (existing or proposed) is defined by the relationship of the building(s) and parking to the street or front property line. In the North Montclair area there are essentially four edge conditions that are appropriate for new development (see below and page IV-10, Edge Condition Reference Map).

1. Street Adjacent Buildings - Pedestrian Orientation (A)

This condition is the only one that does not presently exist in the North Montclair area. It is only appropriate along the Fremont Street "pedestrian precinct" between the Montclair Plaza and the Montclair Transcenter. The requirements of this special design area are discussed in the "key sites" chapter following the design guidelines.

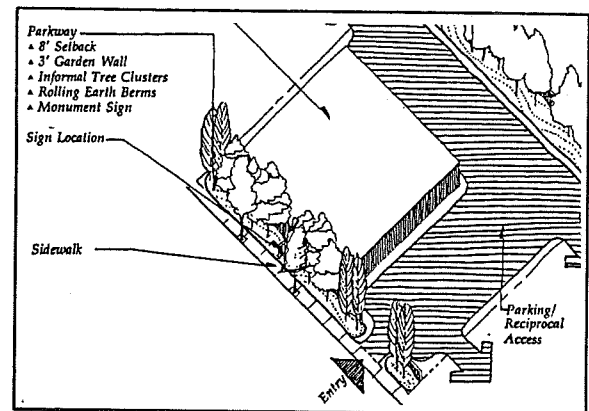


2. Semi-Street Adjacent Buildings - Landscaped Setback (B)

This condition requires that buildings be set back from the street with a landscaped yard in the setback area along all front and side street property lines. Parking is appropriate on the side or rear of the building away from the street.

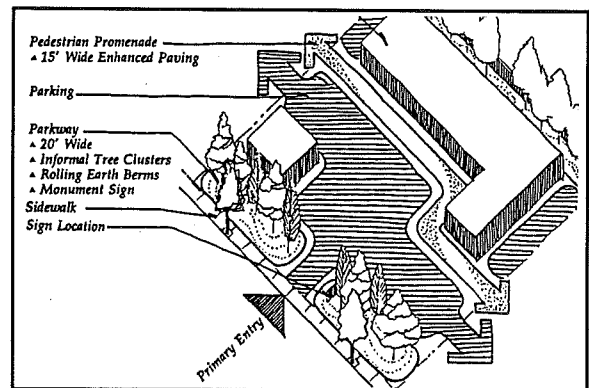
The depth of the landscaped setback is determined by the development standards for

the particular zoning district (see Table 3-2 on page III-14).



3. Buildings Set Back From Street - Limited Parking (c)

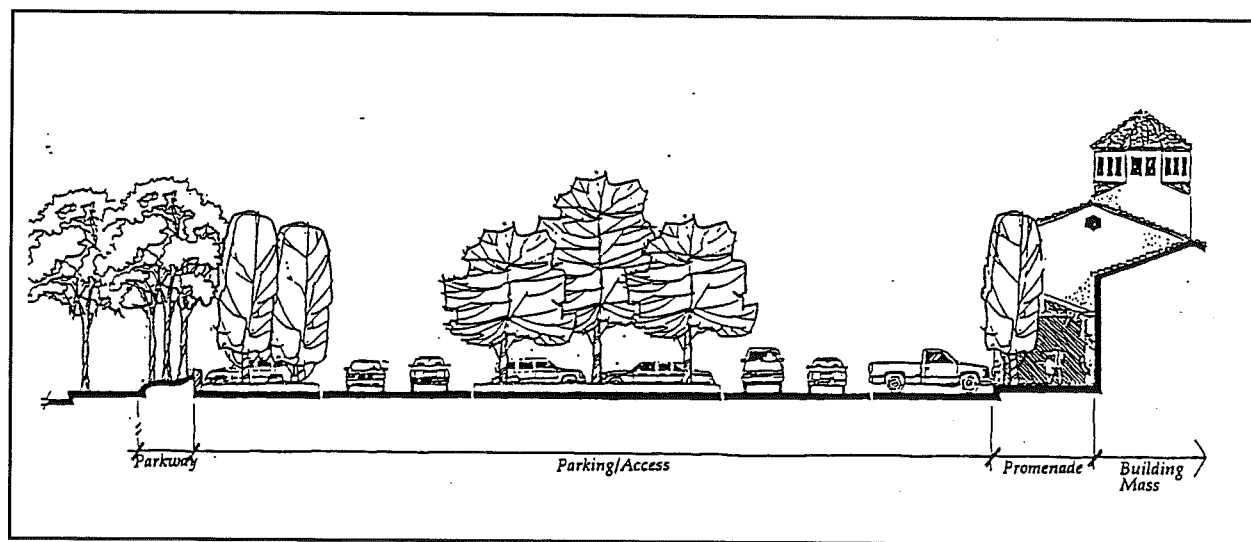
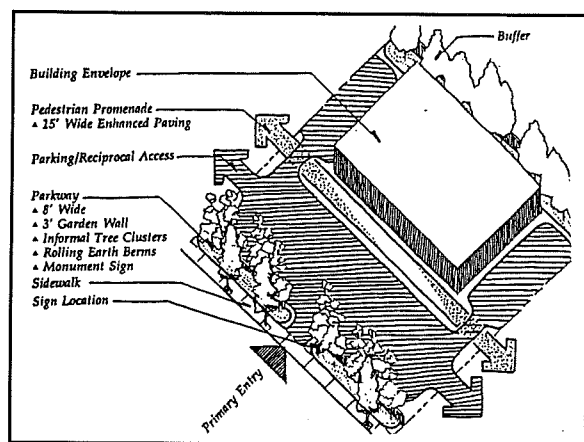
This condition allows one bay of parking (two rows plus aisle) between the building and street. A landscaped setback is required between the street and parking lot. The depth of the landscaped setback is determined by conditions within the design zone unless a different setback is specified in the development standards. If additional parking is required it should be provided at the rear of the building.



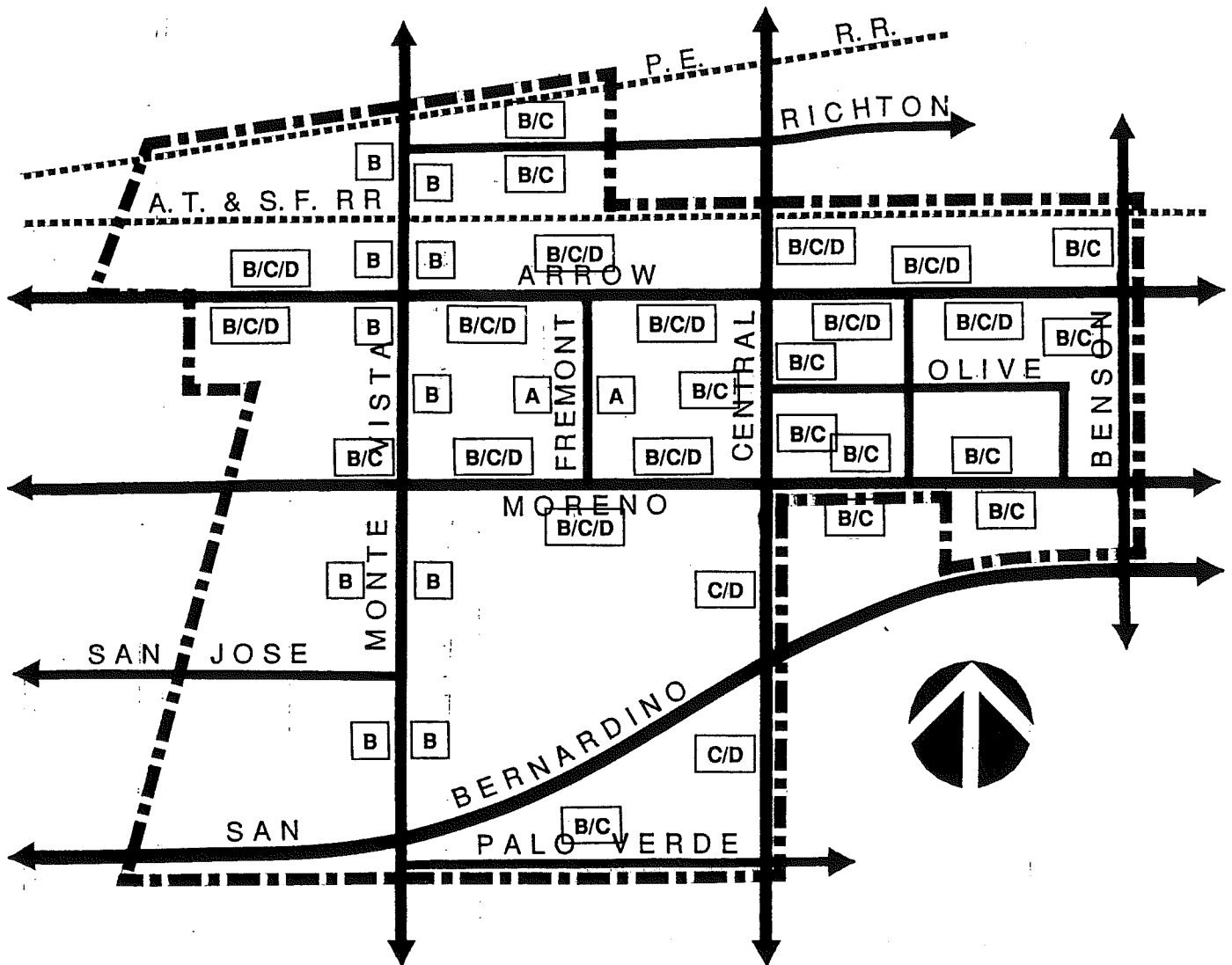
4. Building Set Back From Street - Parking In Front (D)

This condition allows the building to be set back from the street an unlimited distance with the majority of parking placed between the building and street. A landscaped setback is required between the street and parking lot. The depth of the setback will be determined by the development standards.

The specific guidelines for how projects are to be developed using the "edge condition" determinant are provided in the *Commercial Development Guidelines* on page IV-11.



EDGE CONDITION REFERENCE MAP



Example A: Street Adjacent Buildings - Pedestrian Orientation

Provide corner "cut-offs" for buildings on prominent intersections.

Place ground level front elevation of the building on the front property line and at the sidewalk edge to maintain the continuity of the "street wall."

Avoid setbacks from the sidewalk edge.

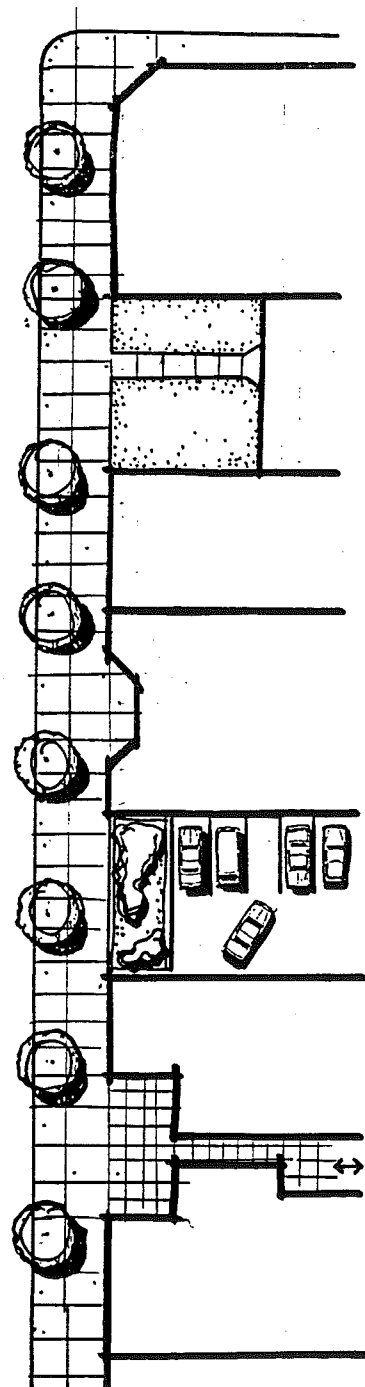
Create continuous pedestrian activity in an uninterrupted sequence by minimizing gaps between buildings.

Avoid parking lots that interrupt commercial space.

Avoid blank walls and other "dead" spaces at the ground level.

Create pedestrian paseos to parking lots at the rear of buildings.

Use building indentations to create small pedestrian plazas along the street wall.



Example B: Semi-Street Adjacent Buildings - Landscaped Setback

The setback area shall be fully landscaped, interrupted only by pedestrian areas and sidewalks. No Parking shall be located within any required front yard setback.

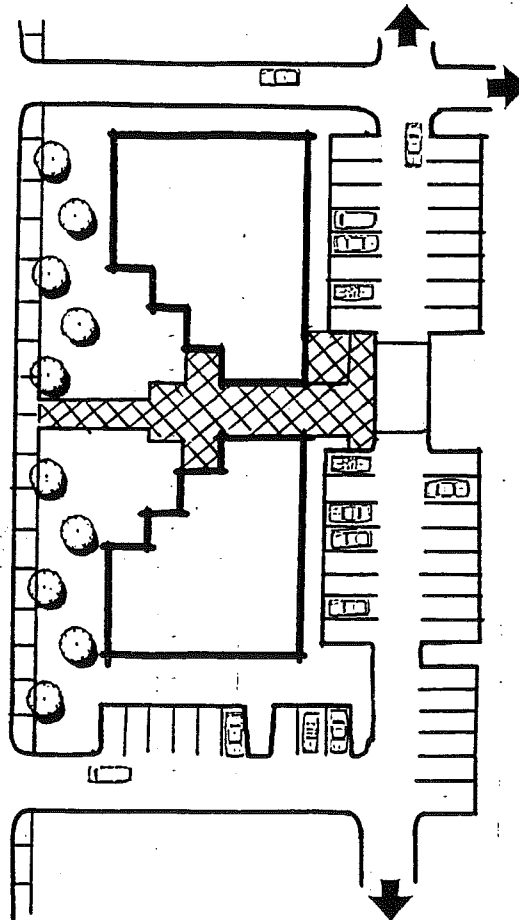
Parking is not to be located in the setback space.

Parking at rear of building is preferred.

If parking lots are located at the sides of buildings or elsewhere on the site where they may be visible from the street, they must be screened from street view by low walls and/or shrubs at least 30 inches high.

Driveways should be kept to a minimal number and width as necessary for safety.

Provide access to adjoining parcels whenever possible.



Example C: Buildings Set Back From Street - Limited Parking

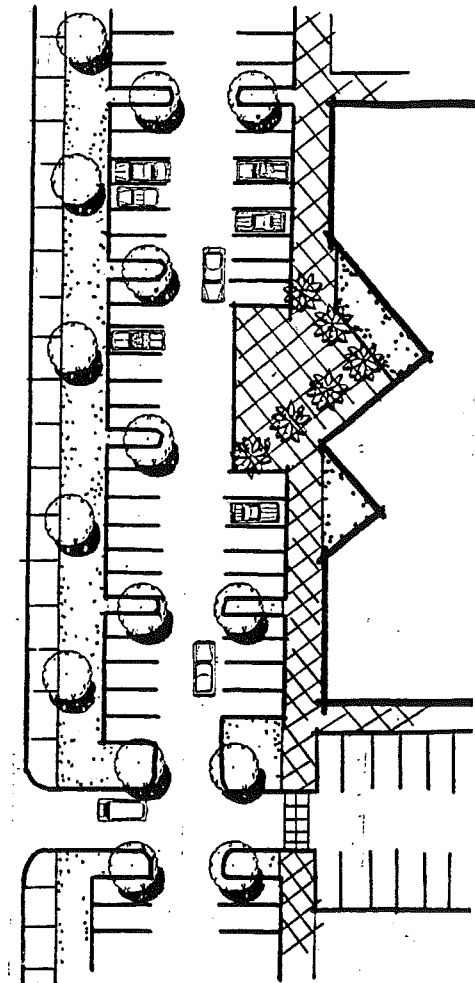
Only one bay of parking (two rows plus aisle) are permitted. Parking lot landscaping shall comply with development standards.

Provide landscaped pedestrian areas at primary building entrances.

Trees, shrubs and/or low walls at least 30 inches high should be used to screen the parking area from street view.

Provide accent landscaping and enhanced paving at project entries.

Provide vehicular access and pedestrian connections to adjoining parcels whenever possible.



Example D: Buildings Set Back From Street - Parking In Front

Buildings may be setback from street any distance with parking between street and building(s).

Trees, shrubs and/or low walls at least 30 inches high shall be used to screen parking areas from street view.

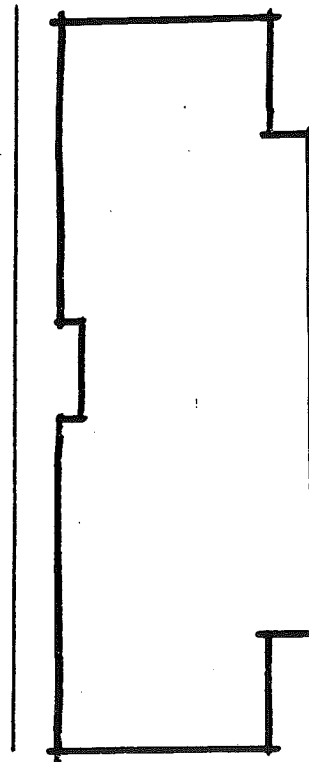
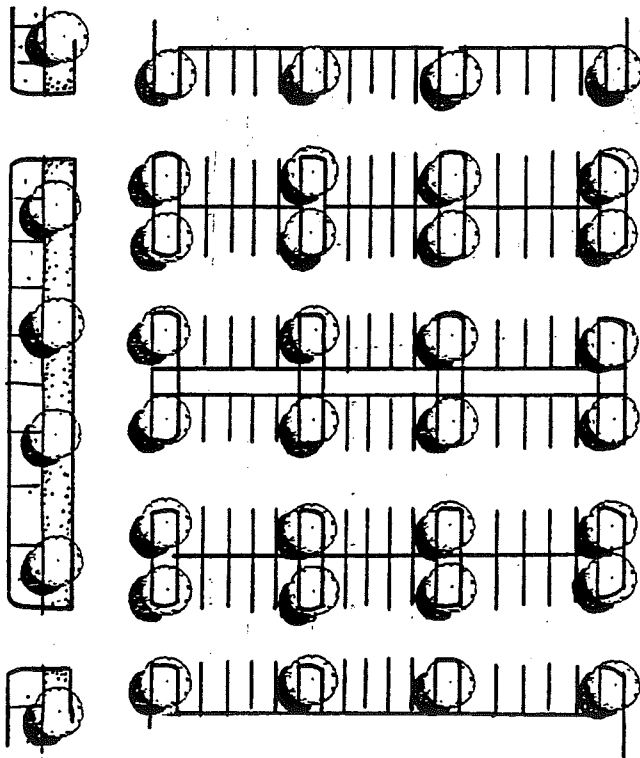
Provide fully landscaped buffer at the street edge of all parking areas.

Provide accent landscaping and enhanced paving.

Provide landscaped pedestrian areas at primary building entrances.

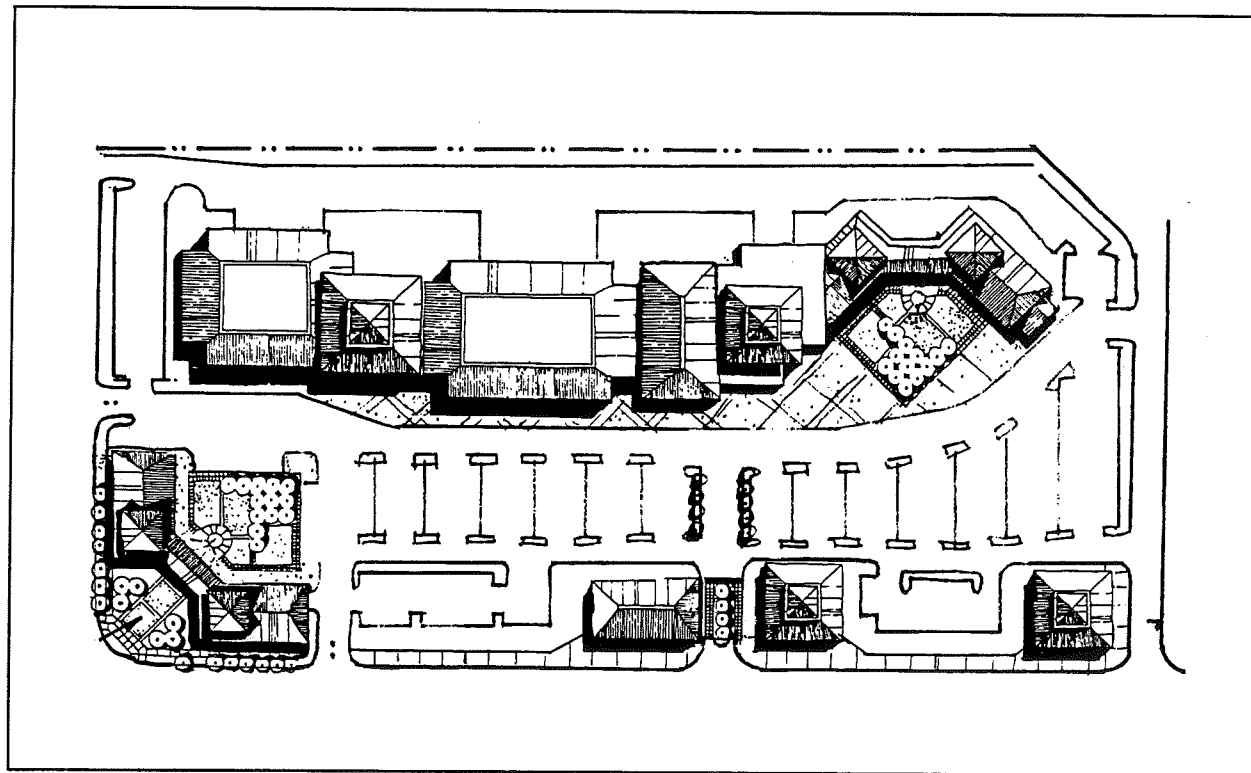
Provide access to adjoining parcels whenever possible.

Arrange parking bays so that pedestrians walk parallel to drive aisles.



3. Internal Site Organization

- Locate buildings and on-site circulation systems to minimize pedestrian/vehicle conflicts where possible. Link buildings to the public sidewalk where possible with textured paving, landscaping, trellises, etc.
- Larger projects with multiple buildings should consider clustering buildings around areas of useable open space. This provides opportunities to create plazas or pedestrian malls and prevents long "barrack-like" rows of buildings. When clustering is impractical, a visual link between separate buildings should be established. This link can be accomplished through the use of an arcade system, trellis or other open structure and textured walkways.
- Recognize the importance of spaces between buildings as "outdoor rooms" on the site. Outdoor spaces should have clear, recognizable shapes that reflect careful planning and are not simply "left over" areas between buildings. Open spaces should provide pedestrian amenities such as shade, benches, fountains, etc.
- Organize landscaped areas to define outdoor spaces and to buffer the visual impact of buildings and parking lots.
- Loading facilities, equipment and service areas and refuse storage enclosures should be located at the rear of buildings or areas where they will be the least visible. All such facilities shall be screened from public view by combinations of dense landscaping, walls and berms.



- Carefully consider the location of all property line walls/fences. Design plans should show a detail of the property line wall and how it corresponds with existing, adjacent building walls. All roof overhangs, etc., should be shown in project drawings.

B. Architectural Guidelines

1. Building Elevations

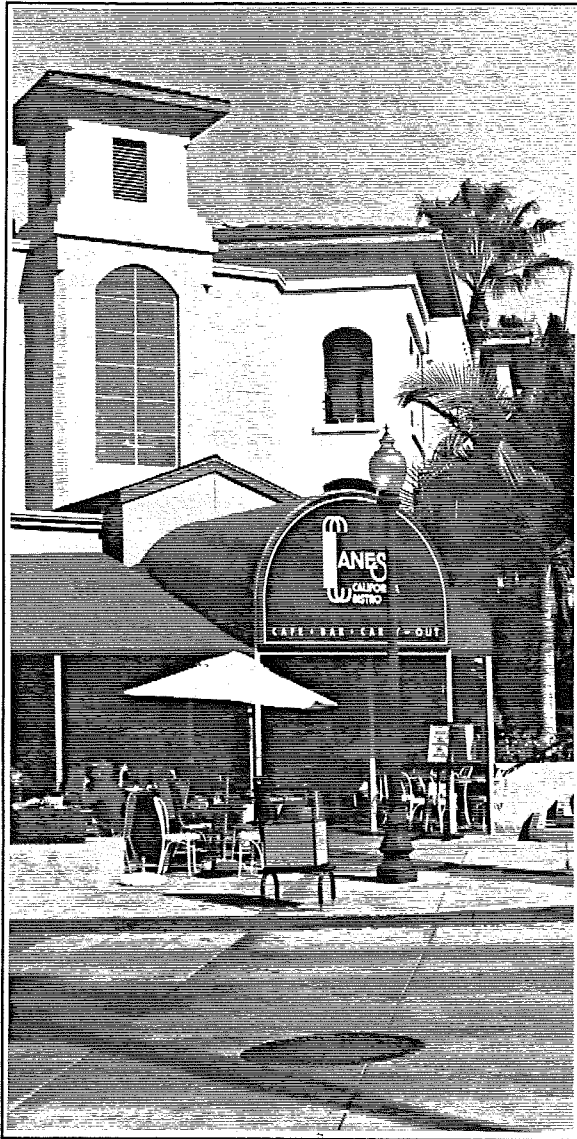
- All building elevations should be given careful design consideration. This is particularly true for freestanding buildings which are potentially viewed from all elevations. Each wall surface that is accessible to the public should be treated as a primary facade.
- Buildings should be designed to avoid a "box-like" appearance. This may be accomplished in a variety of ways:
 - *Vary the planes of the exterior walls in depth and/or direction. Wall planes should not run in one continuous direction for more than fifty (50) feet without an offset;*
 - *Vary the height of the building so that it appears to be divided into distinct massing elements;*
 - *Articulate the different parts of a building's facade by use of color, arrangement of facade elements or a change in materials;*
 - *Use landscaping and architectural detailing at the ground level to lessen the impact of an otherwise bulky building; and*

- *Avoid blank walls at the ground floor levels. Utilize windows, trellises, wall articulation, arcades, change in materials or other features.*

- Additions to existing buildings should be designed to be integrated with the existing building. The new addition should match the original in terms of scale, window/door styles and openings, roofline materials, color and other aspects of design.

2. Building Height And Scale

- Apparent building height and bulk should be reduced by dividing larger building masses into smaller-scale parts.
- The height and scale of new development should be compatible with that of surrounding development. New development height should "transition" from the height of adjacent development to the maximum height of the proposed building. This is especially applicable where buildings are located very close to each other. It is often possible to adjust the height of a wall cornice or parapet to match that of an adjacent building. Similar design linkages can be achieved to adjust apparent height by placing window lines, belt courses and other horizontal elements in a pattern that reflects the same elements on neighboring buildings.
- Emphasis of building elements should be used with restraint. A major element of emphasis, such as an entry or theme tower, should not overshadow other design elements of the building. The various parts of the building should create a visual balance.



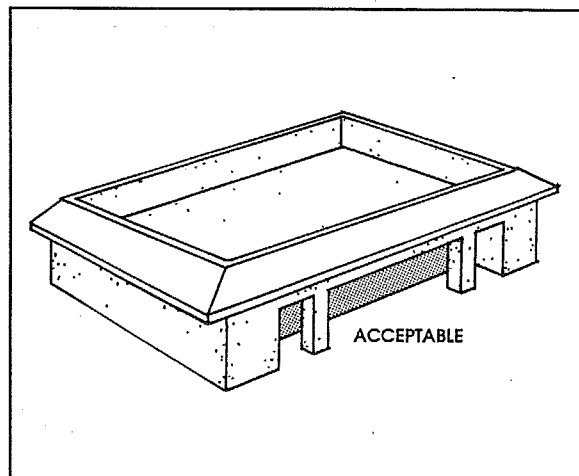
- A traditional principle which is often helpful in reducing building bulk and improving scale is to divide the mass of the structure into distinct horizontal parts that express a sense of base, mid-section and top. This is especially helpful for buildings of three or more stories.
- Buildings over two stories high should set back their upper story street-facing facades to reduce apparent height.

3. Architectural Design Theme

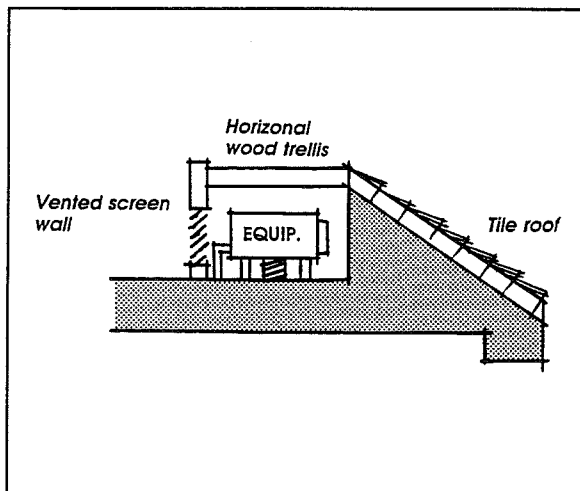
There is no particular architectural design theme specified. The appropriate architectural style/character for North Montclair is based upon the relationship of a building to neighboring buildings, the suitability of a building for its purpose and its relationship of proportion and materials.

4. Roofs And Parapet Walls

- Parapet walls should be treated as an integral part of the building design. Such walls should not appear as unrelated visual elements. Unnecessary height and bulk should be avoided. Where mansard roofs are incorporated into parapet design, views from above the building onto the flat roof area must be considered.
- Piecemeal mansard roofs (used on a portion of the building perimeter only) are discouraged. Mansard roofs, if utilized on commercial structures should wrap around the entire building perimeter where feasible.



- Non-architectural roofing materials (e.g., roll roofing) should not be visible from any direction when viewed from ground level.
- Changes in roof orientation should be accompanied by offsets in building elevation. Similarly, abrupt changes in roof heights require offsets in elevation to distinguish building forms.



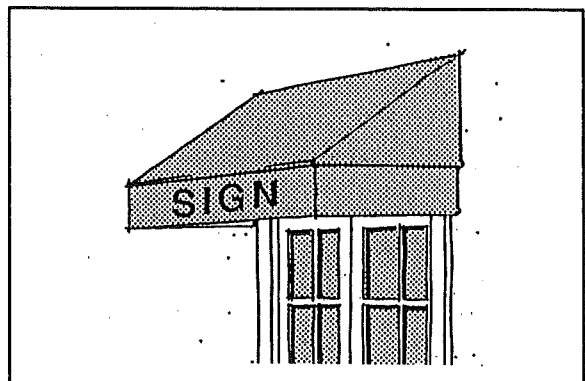
- Integrate the rooftop equipment into the design of the roof. It is often possible to create a "well" within the structure so that the equipment is surrounded by pitched roof forms. Roof plans should designate equipment zones on roof tops. View shed analysis should be provided where appropriate.
- The following roof materials are discouraged:
 - corrugated metal
 - high contrast or brightly colored glazed tile
 - highly reflective surfaces
 - illuminated roofing
 - wood shingles and shakes

5. Awnings And Canopies

- The use of awnings and canopies on new buildings and on buildings undergoing remodeling is a way to add color and architectural interest to an otherwise plain facade. Awnings should be considered in areas of pedestrian activity to provide shelter from the elements.
- When used on a row of contiguous storefronts, awnings should be of the same form, color and material.



- Signs on awning should be painted on the valance or end panels only. Within a shopping center, the placement of signs on awnings shall be consistent with an approved sign program.



- Canvas, treated canvas, matte finish vinyl and fabric awnings are encouraged. Plexiglas, metal and glossy vinyl illuminated awnings are prohibited.
- Internally illuminated awnings are discouraged; however, a consistent design awning program for a center will be reviewed on a case by case basis. Signs may be allowed on awnings in this case.

6. Materials And Colors

- Building materials and colors should be compatible and blend with the existing materials and colors of surrounding developments.
- Cleaning up a storefront and repainting it can enliven the identity of each individual building as well as harmonize separate structures into a cohesive and attractive environment. Color selection is a major design consideration; therefore, it is important that colors blend well with the environment and are not overly bright or garish.
- Base colors (primary building color) should be in the light pastel or earth tone range. Small buildings should range from off-white to medium earth tones. Large buildings should range in the medium toned colors. Slightly darker tones can make large, dull buildings look smaller. Large areas of intense white color should be avoided on all buildings.
- The number of colors appearing on a building exterior should be kept to a minimum. Small buildings should use no more than three colors.

- Primary colors (red, blue, orange) should only be used to accent building elements, such as door and window frames and architectural details.
- Architectural detailing should be painted to complement the facade and tie in with adjacent buildings.
- The following materials are strongly discouraged as primary exterior wall materials:

- *unfinished/unpainted concrete and concrete block*
- *corrugated metal or plastic*
- *reflective mirror-type glass*
- *standing seam metal walls*
- *plywood (painted or otherwise)*
- *imitation "rock work" veneers*
- *corrugated fiberglass*
- *asphalt shingles*
- *illuminated sidings and awnings*
- *plastic laminate*
- *unmilled, bare aluminum*
- *synthetic materials made to resemble masonry*
- *high contrast color glazed masonry or tile, except in small areas of detail*

C. Lighting

- Lighting should be adequate to provide illumination for the security and safety of on-site areas such as parking, loading, shipping and receiving, pathways and working areas. Average maintained footcandles should be a minimum of 1.5 - 2 over the surface area.
- The use of exterior accent lighting on buildings and landscaping is encouraged.

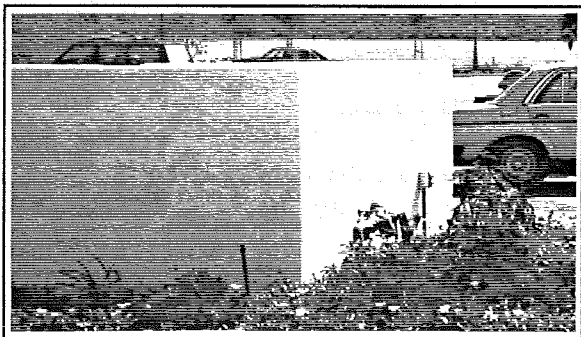
- The design of light fixtures and their structural support should be architecturally compatible with main buildings on-site.
- Along walkways, low-level lighting in the form of bollards or fixtures mounted on short posts or low walls is encouraged. Fixtures should be placed to minimize glare. Posts should be located to avoid hazards for pedestrians or vehicles.



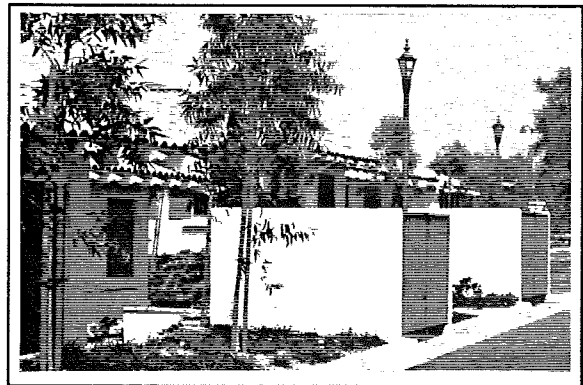
- All lighting must be shielded to confine light spread within the site boundaries.

D. Building Equipment And Services

- Outdoor storage areas, trash containers and utility equipment present a negative visual image, especially in commercial areas, and should be concealed from public view to the greatest degree possible.



- Roof-mounted equipment should be screened from view from adjacent streets, properties and pedestrian areas. Give special attention to buildings whose roofs are viewed from higher elevations. Integrate roof-mounted equipment into the design of the roof.
- Roof-mounted equipment and vents should be painted an appropriate color to minimize their visibility. Skylights should blend with roofing material colors.
- Trash containers and outdoor storage areas shall be screened from public streets, pedestrian areas and neighboring properties. The method of screening should be designed to be compatible with the architectural character of the development and be of durable materials. Trash bin enclosures shall be provided in compliance with Standard #CD 106 - Trash Bin Enclosures.
- Landscaping should be used in conjunction with building materials to enhance ground level screening devices.



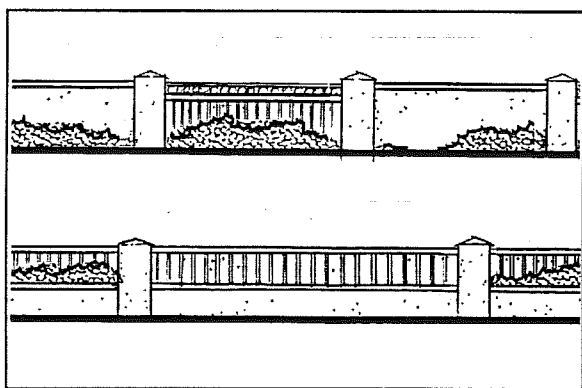
- When feasible in larger commercial developments, separate service and loading areas from main circulation and parking areas.

- Utility meters, electrical transformers, telephone junction boxes and other utility equipment should be screened and located to reduce their visual impact. Utility services are required to be placed underground.

E. Walls And Fences

Walls and fences are generally used for security purposes and to screen areas from public view. If they are not required for a specific purpose they should not be utilized.

- Walls should be kept as low as possible while performing their screening and security functions.
- Walls viewed from the street or parking lots should be designed to blend with the architectural character of the development.
- Long expanses of wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided.



- Whenever possible, vines should be maintained on walls and plant material utilized immediately adjacent to walls to soften blank surfaces and to discourage graffiti.

- Chainlink fencing is not permitted in commercial zoning districts. When security fencing is required, it should be a combination of solid walls with pillars and decorative view ports, or a combination of short solid wall segments with wrought iron grille work on top.

F. Design For Crime Prevention

The concept of crime prevention through environmental design (CPTED) includes a wide variety of strategies aimed at reducing opportunities for crime through the proper design and effective use of the built environment. Key components of CPTED include: natural access control, natural surveillance and territorial reinforcement. CPTED involves design of physical space in the context of the needs of bona fide users of the space, the activity planned for the space and the predictable behavior of bona fide users and criminal offenders.

The following guidelines focus primarily on one major topic - the ability to provide as much "visual penetration" into the building sites as possible.

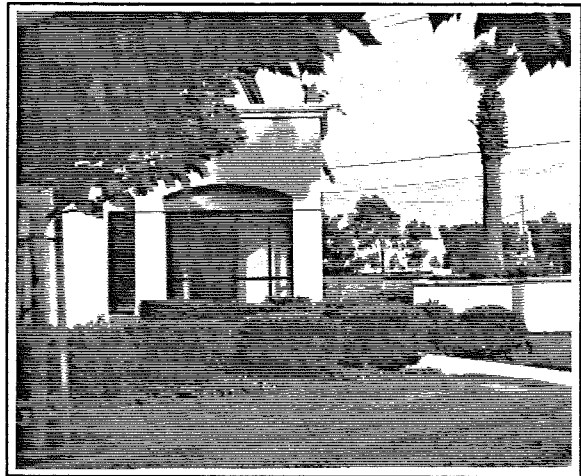
- Use plants and low fencing to direct movement and restrict, symbolically, areas where people should not enter.
- Minimize the height of parking lot screening (shrubs or low walls) to allow a visible "window" above 3 feet and below 7 feet.
- Restrict the number of entry points into a development.

- Secure areas behind buildings through use of:
 - *adequate security lighting*
 - *limited access (walls, fences, gates, shrubs)*
 - *surveillance through windows or with cameras*
- Limit the depth of the retail commercial building setbacks so that visual access into storefronts is possible from the street.
- Use security fencing/walls with view ports or sections of wrought iron grille work to allow views into the development while restricting access.

G. Landscape Design

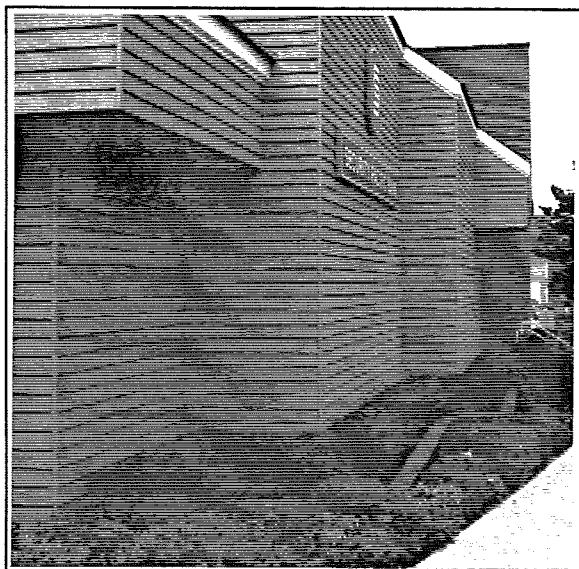
Landscaping for commercial uses is important to define specific areas by helping to focus on building entrances, parking lots, defining the edges of various land uses, providing transition between neighboring properties (buffering) and providing screening for loading and storage areas. Landscaping should also be used as a unifying element within a project to obtain a cohesive appearance and to help achieve compatibility of a new project with surrounding development.

In addition to bringing a cohesive appearance to a project, another important objective of landscaping in the North Montclair area is to help unify the appearance of the adjacent street corridors. The basic design concept is to utilize the required on-site landscaped setback area to provide consistent tree plantings along designated street segments. A list of required trees and their locations is provided in Section __, Streetscape Design.



- Landscaped areas should generally incorporate plantings utilizing a three tier system; 1) grasses and ground-covers, 2) shrubs, and 3) trees.
- The following are common planting design concepts that should be used whenever appropriate:
 - *specimen trees used in informal groupings and rows at major focal points*
 - *use of flowering vines both on walls and arbors*
 - *use of planting to create shadow and patterns against walls*
 - *trees to create canopy and shade, especially in parking areas*
 - *berms, plantings and walls to screen outdoor areas from wind*
- On-site landscaping along the street frontage should contribute to the overall design harmony along the street corridor. In particular, consider the use of plant materials utilized within other neighboring properties to provide compatibility and harmony.

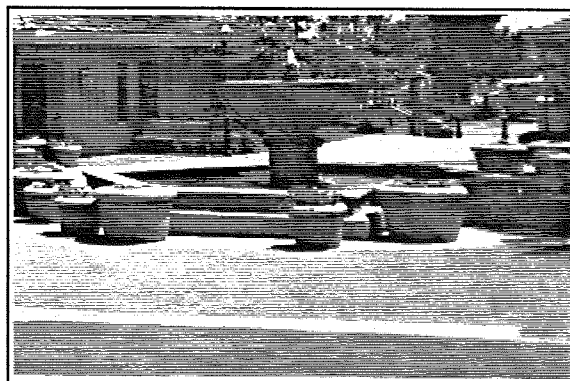
- Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended goals.
- Landscaping around the entire base of buildings is encouraged, whenever practical, to soften the edge between the parking lot and the building. This should be strongly accented at entrances to provide focus.



- Landscaping should be protected from vehicular and pedestrian encroachment by raised planting surfaces, depressed walks or the use of curbs. Concrete mow-strips separating turf and shrub areas should be provided.
- The use of inorganic ground-cover (gravel, crushed rock, etc.) is discouraged. If used in small areas, it should be in combination with live plants and should be limited to a special accent feature.

H. Site Furnishings And Special Features

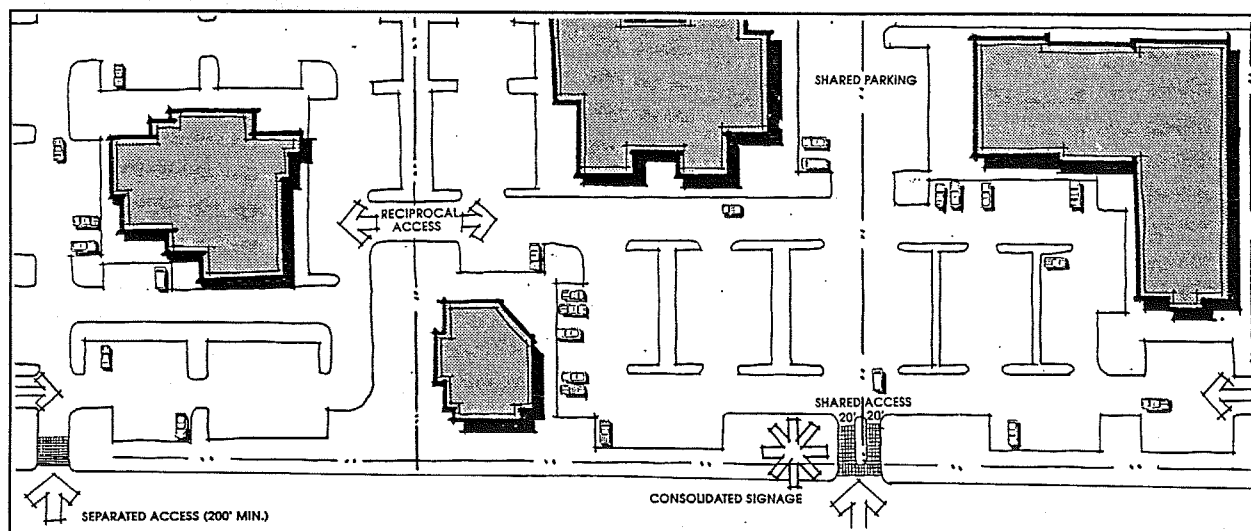
- Permanent outdoor seating is recommended in all publicly accessible ways and spaces. Seating should be either:
 - Incorporated as part of the design of the building base;
 - Custom Designed in a style related to the architecture of the building; or
 - Catalog Items using materials appropriate to the architectural character of the building(s).
- Fountains are encouraged in plazas and courtyards to provide relief in hot weather. The design should be appropriate to the architectural character of the building(s).



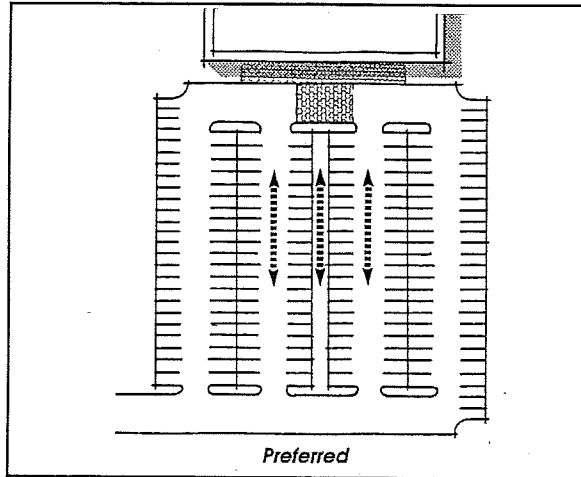
- The use of special features such as arcades, trellises, arbors, columns, railings and decorative walls is encouraged in order to enhance the visual interest of a project. The use of these elements and their design should be consistent with the architectural character of the building(s).

I. Parking, Circulation And Access

- The primary considerations for planning parking lot layouts and site access are safety and efficiency, and minimizing the visual impact of large paved areas. Specifically, a developer should analyze the following factors:
 - *ingress and egress with consideration to possible conflicts with street traffic*
 - *pedestrian and vehicular conflicts*
 - *on-site circulation and service vehicle zones*
 - *the overall configuration and appearance of the parking area*
 - *potentials to connect parking lot with neighboring properties*
- Separate vehicular and pedestrian circulation systems should be provided. Pedestrian linkages between uses in commercial developments should be emphasized, including distinct pedestrian access from parking areas in large commercial developments, such as shopping centers.
- Parking aisles should be separated from primary vehicle circulation routes whenever possible.
- Primary entrances/driveways should incorporate special paving treatments and enhanced landscaping.
- To encourage the use of long term and compact spaces by employees, employee parking will be located adjacent to exclusive employee entrances.
- Common driveways which provide vehicular access to more than one site are strongly encouraged. Reciprocal access agreements may be required for some development proposals.
- Shared parking between adjacent businesses and/or developments is strongly encouraged whenever practical.
- Where parking areas are connected, interior circulation should allow for a similar direction of travel and parking spaces in all areas to reduce conflict at points of connection.

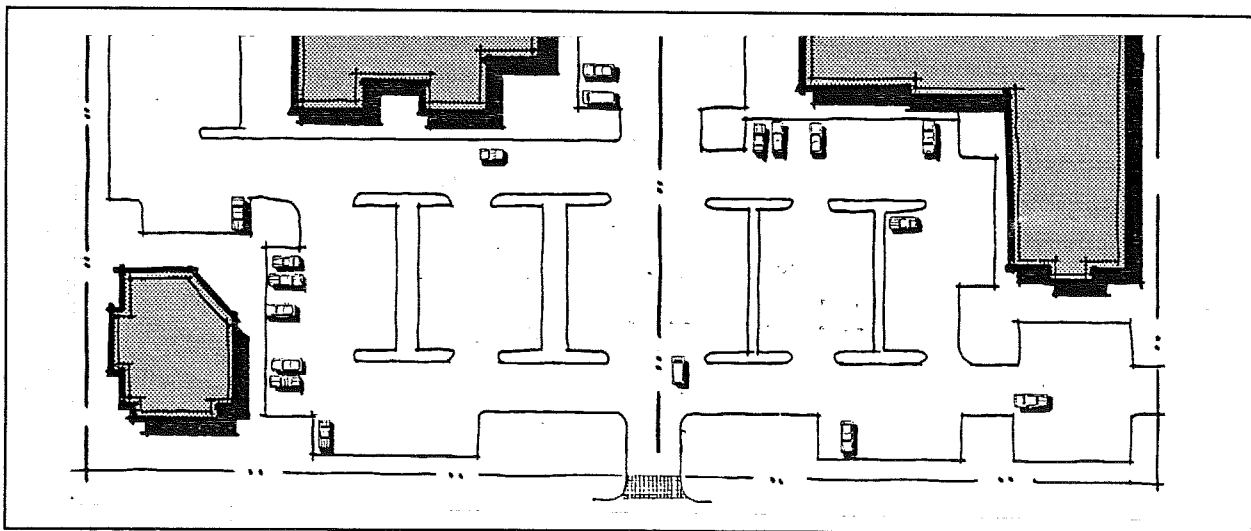
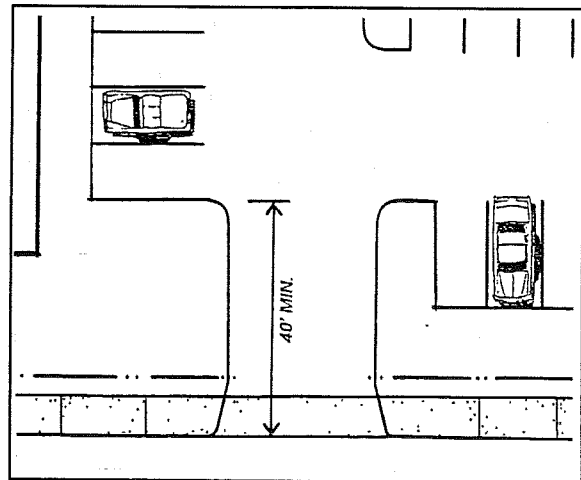


- The layout of parking areas should be designed so that pedestrians walk parallel to moving cars. Minimize the need for the pedestrian to walk between parked cars or to cross parking aisles and landscape areas.

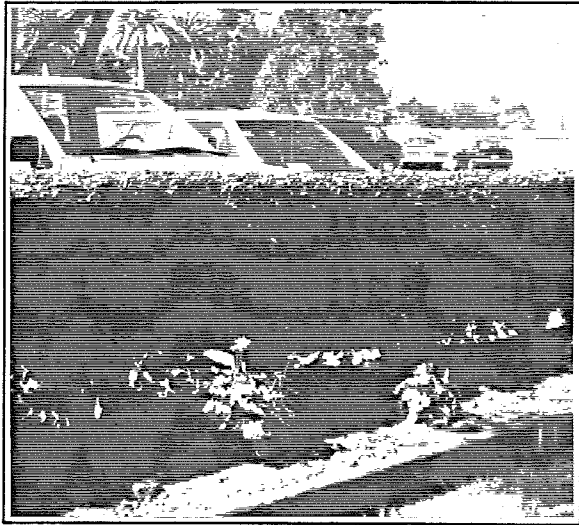


- Parking areas which accommodate a significant number of vehicles should be divided into a series of connected smaller lots. Landscaping and offsetting portions of the lot are effective in reducing the visual impact of large parking areas.

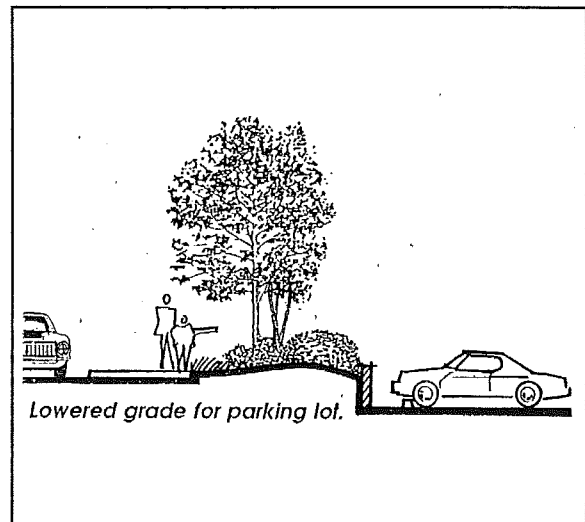
- Limit curb cuts for driveways opening to public streets. Locate access driveways for corner properties with more than one street frontage on the street with the least traffic volume, as determined by the City Engineer.
- The first parking stall which is perpendicular to an access driveway or first aisle juncture should be at least 40 feet back from the curb. With larger commercial centers, significantly more setback area may be required for vehicle stacking.



- Access for service vehicles, trash collection and storage areas should be located on alleys where alleys exist. When no alley exists, the access should be located on the street with least the traffic volume.



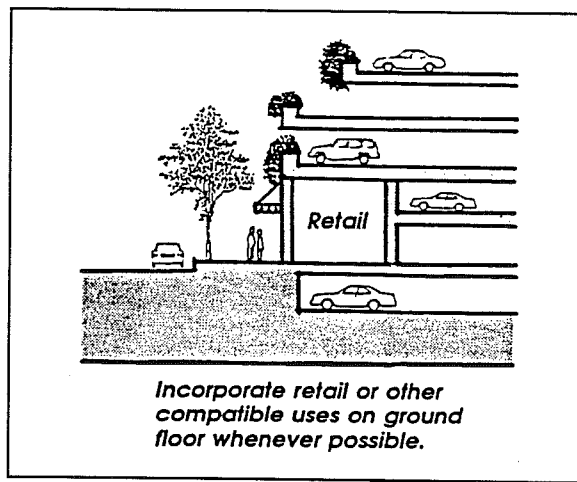
- Visually screen parking lots from street view to a height of 30 inches by planting, or a combination of planting and low walls or earth berms.
- If a wall is used to create a parking lot screen, it should not be greater than 42 inches high if located within the front yard setback. If shrubs are used, the shrubs should be a minimum of 30 inches in height after two years growth. Space shrubs in massed plantings so that branches intertwine. Solid walls used for screening should be accompanied by a minimum 5-foot wide landscaped edge between the property line and the wall, facing the street.



- Where practical, lowering the grade of the parking lot from existing elevations may aid in obscuring views of automobiles while promoting views of architectural elements and interior activities.
- Both perimeter and interior landscaping should use canopy-type trees. The location and spacing of trees is dependent on the type of tree used, but the overall effect should be of a relatively consistent tree cover which will shade the pavement and vehicles. One tree for each 5 parking spaces is the required minimum.
- Interior parking lot trees may be provided in either 4 foot wide planter breaks or in a row located in the center of the parking bay.

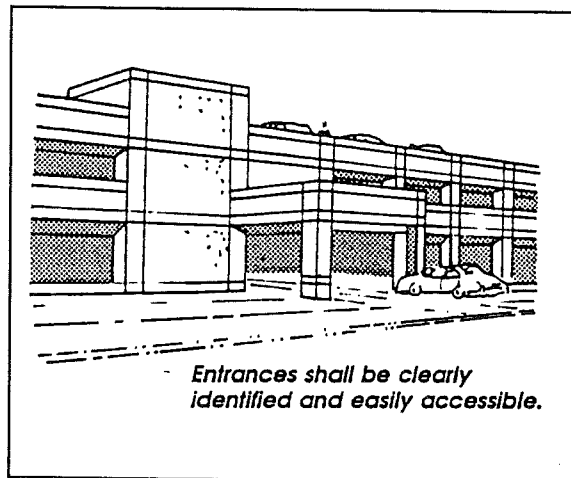
J. Structured Parking

- Structured parking is encouraged. If not feasible in the immediate development program for the site, consideration should be given to a longer term master plan for the site that would eventually convert surface parking areas to structured parking.



- Minimize the visual impact of parking structures by locating them at the rear or interior portions of the property when possible.
- Parking structures which must be located on public street frontages should:
 - *minimize the street frontage of the structure by placing its short dimension along the street edge when possible*
 - *develop activities such as shops, offices or other commercial space along the ground level of street frontage*
 - *when this is not possible, provide a minimum 30 foot landscaped setback area*

- Pedestrian and vehicular entrances must be clearly identified and easily accessible to minimize pedestrian/vehicle conflict. The use of enhanced paving to distinguish entrances is strongly encouraged.
- The use of special architectural features and details is encouraged to provide the entrance with character and protection from weather.



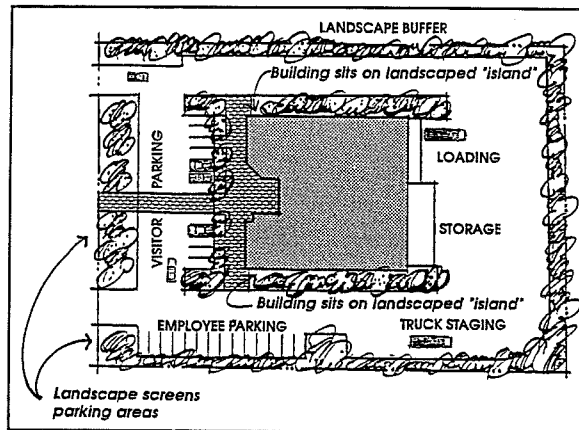
Business Park / Industrial Guidelines

The following design guidelines are applicable to all business park/industrial park projects within the MIP (Manufacturing Industrial Park) zoning district. Business park projects which incorporate a commercial retail component should review the design guidelines for commercial development in the previous section.

A. Site Planning

- The main elements of sound business park/industrial site design include the following:
 - *controlled site access*
 - *service areas located at the sides and rear of buildings*
 - *convenient access, visitor parking and on-site circulation*
 - *screening of outdoor storage, work areas and equipment*
 - *emphasis on the main building entry and landscaping*
 - *placement of buildings to provide plazas and courtyards*
 - *landscaped open space*
- A variety of building setbacks should be provided in order to avoid long monotonous building facades and to create diversity throughout the project. However, this should be accomplished with consideration for existing setback conditions on neighboring property developments.
- A project's relationship to the street (its street edge condition) is determined by setback requirements in the development standards section and by which "edge conditions" are appropriate in that area. The development standards provide the minimum setback requirements for buildings and parking, while "edge conditions" determine the maximum setbacks and the appropriate location of parking and front yard landscaping.

- Structures should be located on "turf islands," where the main entrance of the building does not directly abut paved parking areas. A minimum 5 foot landscape strip should be provided between parking areas and the portion of the building where public parking is provided.



- Placement of structures which create opportunities for plazas, courts or gardens is encouraged. Setback areas can often be used to provide space for patio areas.

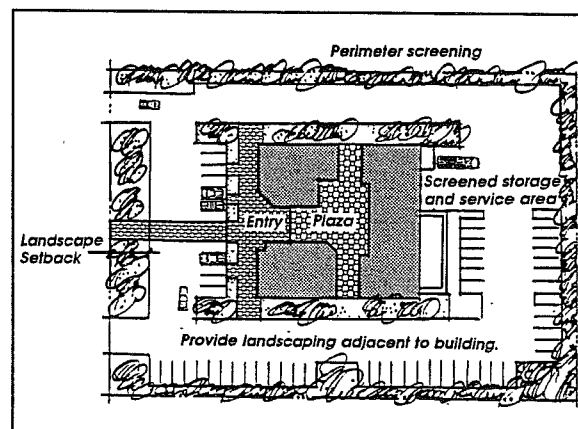
B. Parking And Circulation

- Parking lots should not be the dominant visual elements of the site. Large expansive paved areas located between the street and the building are to be avoided in favor of smaller multiple lots separated by landscaping and buildings (Edge Condition "C").
- Site access and internal circulation should be designed in a straight forward manner which emphasizes safety and efficiency. The circulation system should be designed to reduce conflicts between vehicular and pedestrian traffic. Circulation routes and parking areas should be separated wherever possible.

- Adjacent projects should be connected whenever possible to promote ease of access by pedestrians and automobiles and reduce traffic movements on adjacent streets. In some cases only a sidewalk connection may be necessary, while in other cases the provision of shared vehicle access and parking will be appropriate.

- Entrances and exits to parking and loading facilities should be clearly marked with appropriate directional signage where multiple access points are provided.

- Parking lots adjacent to and visible from public streets must be adequately screened from view through the use of rolling earth berms, low screen walls, changes in elevation, landscaping or combinations thereof.



C. Loading Facilities

- To alleviate the unsightly appearance of loading facilities for industrial uses, these areas should not be located at the front of buildings where it is difficult to adequately screen them from view. Such facilities are more appropriate at the rear of the building where special screening may not be required.

- When it is not possible to locate loading facilities at the rear of the building because of circumstances unique to the site, loading docks and doors may be located at the side of the building but must be screened from view by a combination of screen walls, ornamental landscaping and/or portions of the building. Gates should be located so as not to allow views from the public sight-of-way into loading areas.

D. Architectural Guidelines

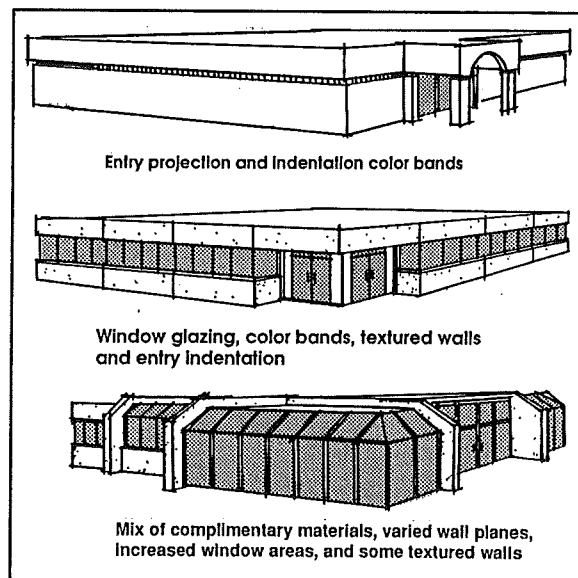
The architectural style of buildings in the business park/industrial category should be one that is contemporary, with clean simple lines. Buildings should project an image of high quality through the use of appropriate materials and settings which reflect a well landscaped park-like environment.

Because these types of structures and uses have a greater potential to impose more heavily on surrounding uses due to their overall larger size, these guidelines are intended to decrease this impact as much as possible.

- As a category of structure type, typically bland industrial buildings often present unattractive "box-like" forms. A variety of design techniques should be used to help overcome this situation and to direct development into a cohesive design statement. For example:
 - *Employ variety in structure forms to create visual character and interest;*

- *Avoid long "unarticulated" facades. Facades with varied front setbacks are strongly encouraged. Wall planes should not run in one continuous direction for more than 50 feet without an offset;*
- *Avoid blank front and side wall elevations on street frontages;*
- *Use entries to structures to portray a quality office appearance while ensuring that they are architecturally tied into the overall building composition and scale; and*
- *Use sensitive alteration of colors and textures to produce visual interest and enhance architectural forms.*

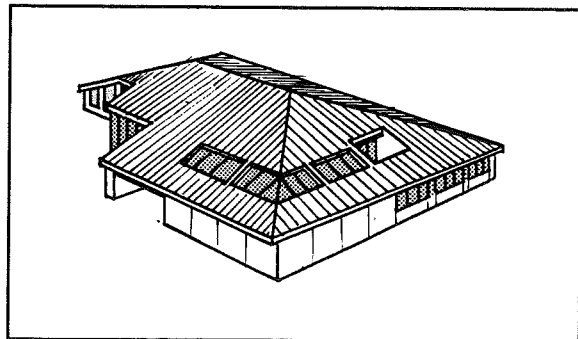
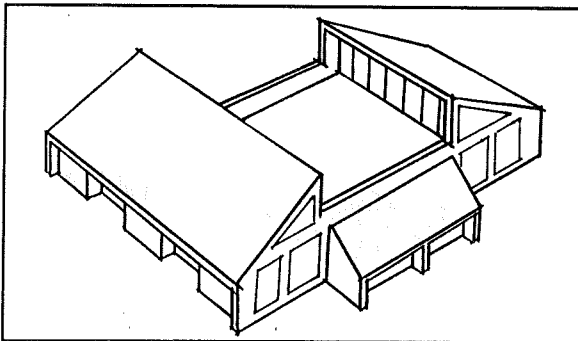
- There is no particular design theme required. The appropriate architectural style/character for North Montclair is based upon the relationship of a building to neighboring buildings, the suitability of a building for its purpose and its relationship of proportion and materials.



- Design elements which are undesirable and should be avoided include:
 - *highly reflective surfaces*
 - *large blank, unarticulated wall surfaces*
 - *exposed, untreated precision block walls*
 - *chain link fence and barbed wire*
 - *false fronts*
 - *"stuck on" mansard roofs*
 - *materials with high maintenance such as stained wood, shingles or metal siding*
- Office buildings should avoid the "glass box" look. Building walls should not be more than 60% transparent.
- Reflective glass should not be the "mirror look" highly reflective type. The reflective co-efficient of any glass used should be less than 30. Where tinted glass is used, the transmittance co-efficient should be greater than 30.
- Piecemeal mansard roofs (used on a portion of the building perimeter only) should be avoided. Mansard roofs should wrap around the entire perimeter of the structure where feasible.
- Rooftop equipment must not be visible from adjacent streets. Mechanical equipment, including ducts and pipes, must be contained within rooftop penthouses, or opaque screening must be provided to conceal all rooftop equipment.
- Roof screens should be sheathed in a matching or complimentary material and color to the exterior building material and color. All roof screens should be solid and continuous. Equipment must be covered by continuous grills or louvers. Picket fence screening is not permitted.
- The following roof materials should not be used:
 - *corrugated metal (standing rib metal roofs are permitted)*
 - *high reflective surfaces*
 - *illuminated roofing*

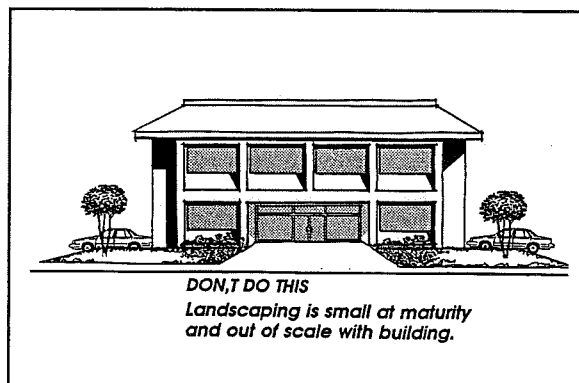
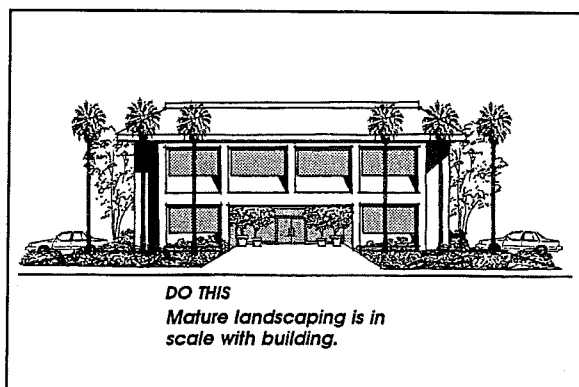
E. Roofs

- Non-architectural roofing materials (e.g., roll roofing) should not be visible from any direction when viewed from ground level.
- Unless roofing materials are a part of the design element (shingles, tiles, etc.), the ridge line elevation should not exceed the parapet elevation.



F. Landscaping

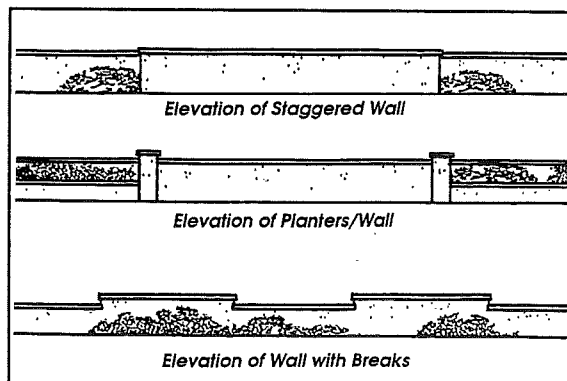
- Landscaping should be used to define entrances to buildings, screen parking lots and loading areas, and provide transition between neighboring properties (buffering).
- Landscaping should be in scale with adjacent buildings and be of appropriate size at maturity to accomplish its intended purpose.
- Landscaping around the entire base of buildings is recommended to soften the edge between the parking lot and the structure. Landscaping should be accented at building entrances to provide focus.



- Trees should be located throughout the parking lot and not simply at the ends of parking aisles. In order to be considered within the parking lots, trees should be located in planters that are bounded on at least 3 sides by parking area paving.

G. Walls And Fences

- If walls are not required for a specific screening or security purpose they should not be used. The intent is to keep walls as low as possible while still performing their screening and security functions.
- Where walls are used at property frontages or screen walls are used to conceal storage and equipment areas, they should be designed to blend with the site's architecture. Landscaping should be used in combination with such walls whenever possible.
- Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets should be provided along the wall.
- When security fencing is required, it should be a combination of solid pillars, or short solid wall segments and wrought iron grille work.



H. Screening

- Screening for outdoor storage should be a minimum of 6 feet and a maximum of 8 feet high.
- Where screening is required, a combination of elements should be used including solid masonry walls, berms and landscaping. Chainlink fencing with wood or metal slatting is an acceptable screening material only for areas not visible from a public street or parking lot.
- Any equipment, whether on the roof, side of the building or ground, must be screened. The method of screening shall be architecturally integrated with the building exterior in terms of materials, color shape and size. Where individual equipment is provided close together, a continuous screen is desirable versus several smaller screens.

I. Design For Crime Prevention

The concept of crime prevention through environmental design (CPTED) includes a wide variety of strategies aimed at reducing opportunities for crime through the proper design and effective use of the built environment. Key components of CPTED include: natural access control, natural surveillance and territorial reinforcement. CPTED involves design of physical space in the context of the needs of bona fide users of the space, the activity planned for the space and the predictable behavior of bona fide users and criminal offenders.

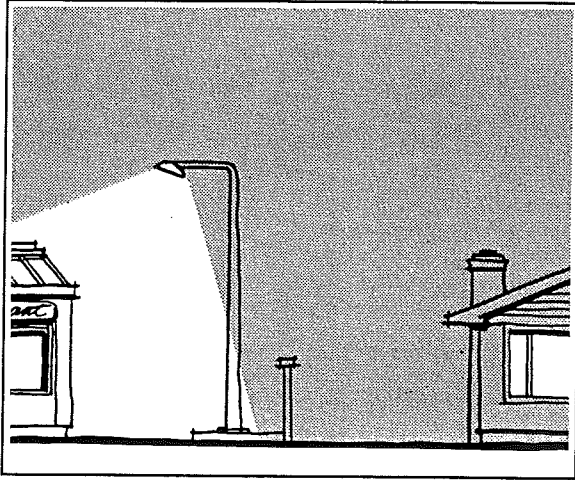
The following guidelines focus primarily on one major topic — the ability to provide as much "visual penetration" into the building sites as possible.

- Use plants and low fencing to direct movement and restrict, symbolically, areas where people should not enter.
- Minimize the height of parking lot screening (shrubs or low walls) to allow a visible "window" above 3 feet and below 7 feet.
- Restrict the number of entry points into a development.
- Secure areas behind buildings through use of:
 - *adequate security lighting*
 - *limited access (walls, fences, gates, shrubs)*
 - *surveillance through windows or with cameras*
- Use security fencing/walls with view ports or sections of wrought iron grille work to allow views into the development while restricting access.

J. Lighting

- Lighting should be used to provide illumination for the security and safety of on-site areas such as parking, loading, pathways and working areas.
- The design of light fixtures and their structural support should be architecturally compatible with main buildings on-site. Illuminators should be integrated within the architectural design for the buildings.
- As a security device, lighting should be adequate but not overly bright. All building entrances should be well lighted.

- All lighting should be shielded to confine light spread within the site boundaries.



- One footcandle evenly distributed across a parking lot is the suggested minimum. At entrances and loading areas, up to 2 footcandles may be appropriate.

Sign Design Guidelines

A. Introduction

A city's image can be greatly affected by the design, character and quality of commercial signing. Poorly crafted signs can make a street ugly and uninviting, while a well designed, unobtrusive sign can provide identification of a business without being aesthetically offensive or overbearing. Signs communicate information about the business and the quality of the sign can communicate something of the quality of the business. A well designed sign greatly contributes to the character of the building facade and can enhance the total image of the surrounding area.

Sign concepts should be considered during the design of buildings so that signs are architecturally incorporated into the buildings. Size, height, location and material should strongly relate to the building's design. Signs should have graphic balance and be an integral part of the overall storefront or building facade.

These design guidelines are intended to provide direction regarding the type and appearance of signs in North Montclair. Specific regulations regarding the size and number of signs permitted for each business/property are contained in the City's adopted Sign Ordinance.

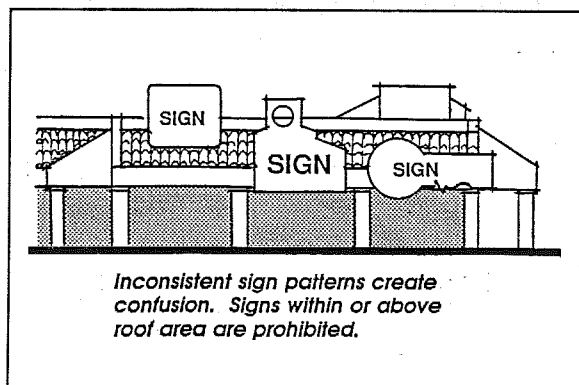
B. General Guidelines

Every structure should be designed with a precise concept for adequate signing. Provisions for sign placement, sign proportion in relation to building, and the readability of the sign should be considered in developing the overall signing concept. All signs should be highly compatible with the structure and site design relative to color, material, size and placement.

1. Size and Shape

- The size of a sign should be in context with the building it identifies.
- A sign's size should be compatible with other signs nearby.

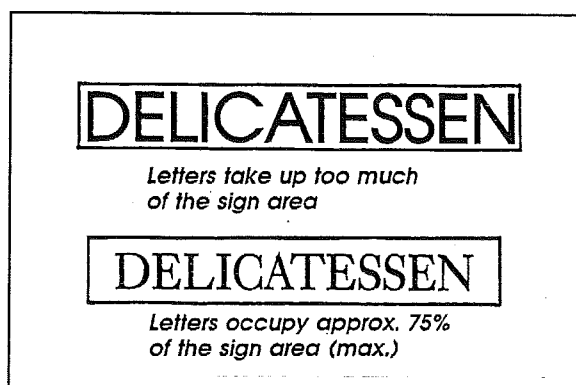
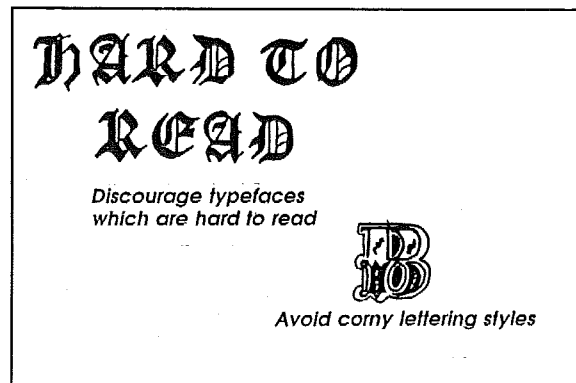
- Pedestrian oriented signs should be much smaller than vehicle oriented signs. Make them easily read from 15 to 20 feet.
- Signs are generally approved if they are within a range of 1 to 1.5 square feet of total area per linear foot of primary building frontage. Signing areas exceeding this range may be considered in order to accommodate the needs of multi-tenant centers and freestanding pad-type buildings.
- Avoid strange or awkward shapes which can restrict legibility.



2. Lettering and Graphics

- Use a brief message, making the sign easier to read. The fewer the words, the more efficient the sign. When the name of the business alone is not sufficient, simple generic copy should be used.
- Avoid overly intricate and difficult to read typefaces.

- Avoid faddish or bizarre typefaces which may soon go out of style and convey the image of an out of date or unfashionable business.



- Do not allow lettering to occupy more than 75% of the sign.
- The use of graphics consistent with the nature of the business is encouraged, such as a hammer symbol for a hardware store or a mortar and pestle for a drug store.

- Signing should be dimensional, using individual letters, whenever feasible. Painted on signing, such as on walls, is not permitted.



3. Color and Materials

- Select colors carefully, avoiding too many colors on one sign and the use of day-glow or garish color paint.
- Provide sufficient contrast between the color of a sign, as well as colors used on the building and other development features.
- Consider interior neon signs which can be used to attract attention and create a special ambiance. Such signs must fall within the maximum 25% window allowance and may not flash or include other animation. All wiring and other hardware must be concealed from view.

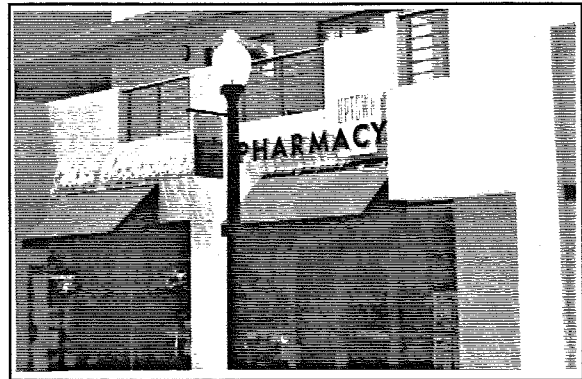
4. Location

- Signs should be placed near the entrance to a building to indicate the most direct access.

C. Facade Signs

1. Description

- A facade sign is one which is attached to or part of a building.



2. Size

- The maximum length of any sign should not exceed 60% of the total length of the front facade.

3. Lighting

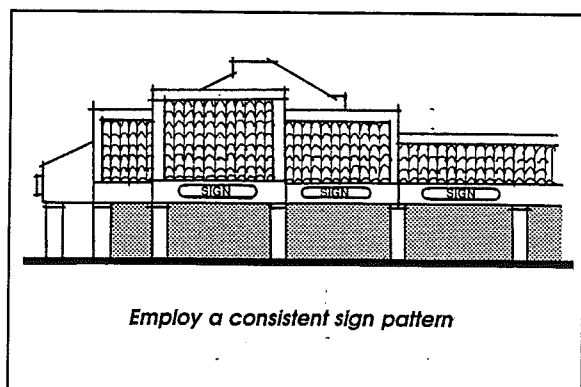
- Internal sign lighting is allowed provided it is not harsh or unnecessary.
- The use of backlit individually cut letter signs is encouraged over can-type box signs with translucent backlit panels. If this type of sign is used, panels must be opaque.

4. Compatibility

- Facade signs should be compatible with the predominant visual elements of the building.

- When there is more than one sign, such as in a shopping center, all signs should be compatible with each other in the following ways:

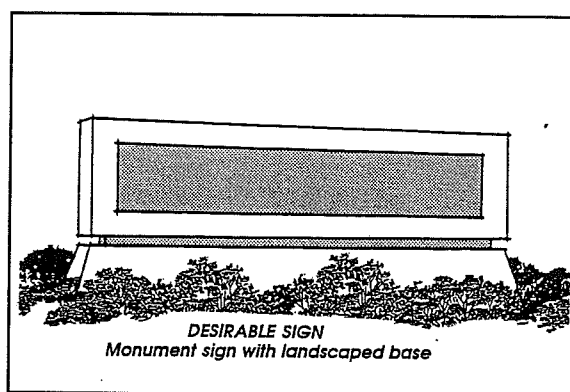
- *type of construction materials*
- *letter size and field color*
- *method used for supporting sign*
- *shape of sign*



D. Freestanding Signs

1. Description

- A freestanding sign is any which is located separately from the building it identifies. Monument signs, which sit low to the ground and are not elevated by a pole, are generally the only form of freestanding signs approved by the City of Montclair.



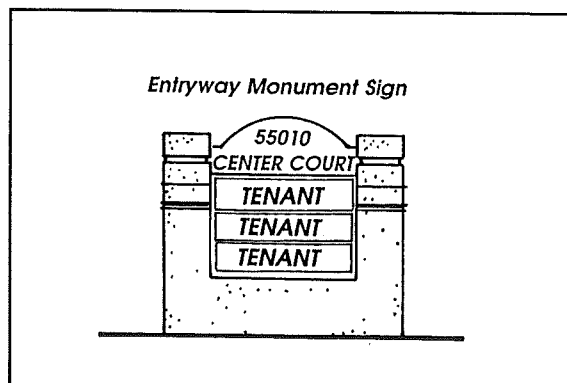
5. Addresses

- Addresses shall be indicated by 6 inch high numbers placed on a background of contrasting color and must be easily visible from the street. The numbers should, if possible, be placed above the entrance doorway or near it. Any business with rear vehicular access shall also display the same address on the rear of the building.

6. Windows

- No window signing, either permanent or temporary, should obscure more than 25% of the total glass area.

- Freestanding signs should provide street addresses and identification for the building or commercial center as a whole and may include the names of up to three major tenants.



2. Placement

- Signs shall be placed perpendicular to approaching vehicular traffic, in raised planters when possible.
- Each monument sign shall be located within a landscaped area consisting of 4 square feet of planted space for each square foot of sign area (one side only).

3. Height

- Monument signs are generally approved within a 6'0" to 8'0" height range.

4. Size

- All individual tenant signs should be of uniform size and limited to the width of the architectural features of the center sign.
- At least 10% of a freestanding sign for a large development should be devoted to identification of the center by name and/or address.

E. Banner Signs**1. Time Limits**

- Each business address may obtain a banner permit for either two 15 day periods or one 45 day period per calendar year.
- Banner permits may not be granted consecutively, but must be separated by a period of at least 30 days.

2. Size

- The size of a banner should be in proportion to the size of the building.

3. Placement

- Banner signs shall only be mounted on the building facade of the business they identify.

F. Prohibited Signs

The following signs are prohibited:

- Roof signs.
- Flashing signs.
- Projecting signs (signs which are affixed to a building and extend outward from it), except for commercial buildings on Fremont Street.
- Animated signs (those which move or rotate in any way).
- Temporary signs (except permitted banners) including, but not limited to, balloons and other inflatables, A-board signs, portable signs, animated characters and vehicle signs which are not an integral part of an operable vehicle.

Landscape Design Guidelines

A. Introduction

Landscaping will play an important role in achieving the urban design goals for the North Montclair Specific Plan area. On-site landscaping (located on the project site) will blend with and reinforce landscaping provided within the public street right-of-way. In addition, projects will be expected to consider the character (type, location, materials) of landscaping on adjacent sites.

The primary objective for landscaping within the Specific Plan area is to act as a unifying element in order to create a cohesive visual image within North Montclair and to increase the overall quality of the area's environment.

B. Landscape Concept

- The relationship between the street and immediately adjacent on-site landscaped setback areas has been established by the design standards in the Streetscape Design chapter. These setback areas will contain trees, shrubs, groundcover and annual color.

- On-site landscaping should provide a consistent design theme throughout the project and be compatible with adjacent properties when appropriate. In larger developments, different landscape themes may be utilized where it will help to heighten the distinction between spaces and to strengthen a sense of movement or place.

C. General Guidelines

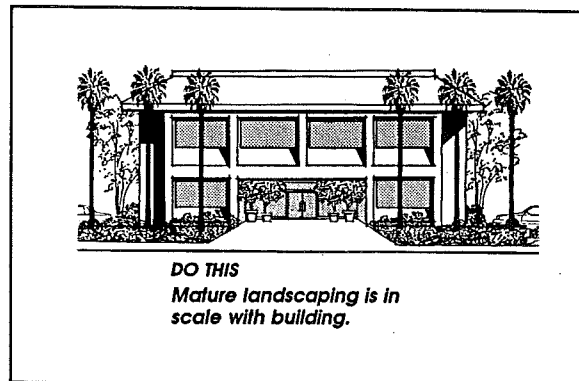
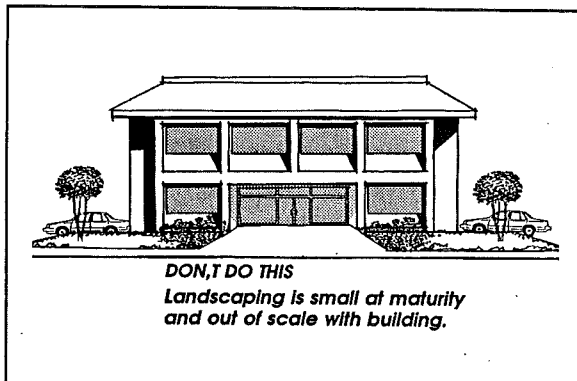
- Landscape designs should exhibit the basic design principles that are used in building design: harmony, balance, rhythm and contrast.
- Landscaping should use a combination of trees, shrubs and groundcover. Groundcover alone will not be enough. Landscape designs should utilize a three tier system of plantings that resembles the construction of outdoor rooms with floors, walls and ceilings.

Floors: Groundcover can be used to define space and function; decrease erosion; and lessen glare and reflection.

Walls: Shrubs can be grouped together to form dense hedges and walls to help define spaces and screen unsightly views. The location and height of landscape walls should take into consideration the security aspects of site design and not be located so that natural surveillance is restricted.

Ceilings: Moderate to very full trees can be used singly or in groups as an umbrella or canopy to form landscape ceilings. These can serve to provide privacy, climate control and define a hierarchy of spatial organization and function.

- The scale and nature of landscape materials should be appropriate to the site and/or structure. Large-scale buildings should be complemented by landscaping that is of the same scale and proportion.
- Landscape design should accent the overall architectural design theme through the use of structures such as arbors and trellises which are appropriate to the particular architectural style of adjacent buildings.
- Landscaping should not be utilized to screen or hide an otherwise unacceptable building. Building architecture should stand on its own, with landscape incorporated as an integral element of the overall project design, not as a substitution for poor design.
- Contiguous parcels or areas reserved for future development should provide landscaping adjacent to the street at the time the initial phase of development occurs.



D. Selection Of Materials

- Healthy, well-maintained plant materials should be used in all landscaped areas. Thickness, height, color, seasonal characteristics and ultimate growth should always be considered.
- Plant materials should be "long-lived" varieties. "Short-lived" materials such as flowering annuals may be utilized to accent the "longer-lived" elements which form the landscape framework.
- Landscape materials should be selected to minimize maintenance. A limited palette of landscape materials is recommended.
- Drought tolerant plant species are highly desirable and may be used in place of any materials on the plant palette provided later in this section.
- Groundcover should be of live plant materials. Gravel, colored rock, bark and similar materials are generally not accepted as groundcover. In some cases, *hardscape* plans using materials such as brick or other decorative paving materials may be approved in lieu of plant materials (particularly on Fremont Street).
- Plant materials that exhibit annual or seasonal color are recommended to highlight special locations (e.g. flanking main building entries and driveways).

E. Size And Location

- Sizes of plant materials should generally conform to the following mix to create a sense of establishment and variety:

Trees

50 percent, 24 in box
40 percent, 15 gallon
10 percent, 5 gallon

Shrubs

90 percent, 5 gallon
10 percent, 1 gallon

Groundcover

100 percent coverage within 1 year

- Where planting is intended to perform a function such as screening or shading, its initial size and spacing must be selected to achieve its purpose within two years.
- Plant material should generally conform to the following spacing standards:
 - a minimum of twenty feet from the property corner at a street intersection to the center of the first tree or large shrub
 - a minimum of fifteen feet between center of trees and large shrubs to light standards
 - a minimum of fifteen feet between center of trees or large shrubs and fire hydrants
 - a minimum of ten feet between center of trees or large shrubs and edge of driveway

- Plant materials should be spaced so that they do not interfere with the adequate lighting of the premises or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Proper spacing should ensure unobstructed visibility for vehicles and pedestrians at intersections.

F. Parking Areas

- All parking areas shall be landscaped in compliance with the Development Standards of this Specific Plan. Landscaping will include perimeter screening and interior shading. Landscaping should be provided at suitable intervals throughout the lot and at the ends of parking rows, and should screen parked cars from adjacent streets.
- Parking lots should be landscaped with the following objectives in mind:
 - *utilize uniform tree species and container size in the interior of the lot*
 - *visually break up large paved areas with landscaping*
 - *maximize distribution of landscaping*
 - *create tree groves in orderly patterns*
 - *shade 50 percent of the asphalt area within five years from time of installation*
- Parking lots adjacent to a public street shall be landscaped to screen vehicles from view. Screening should consist of a combination of low walls (thirty-six inch maximum height), berms and landscape materials. The objective is to screen automobiles up to the height of

the windshield. Where practical, lowering the grade of the parking lot from the adjacent street elevation can also help to obscure views of auto-mobles while allowing views to the buildings beyond.



- Parking lot designs should include walkways and planting that help direct pedestrians comfortably and safely to their destinations.
- Planting areas adjacent to parking areas, drives or walks must be protected by concrete curbing where necessary.

G. Plant Palette

The following trees and shrubs have been selected for their abilities to thrive in urban conditions (i.e. heat, smog, compaction) and for their relative tolerance to drought.

The trees on this list are to be used in locations other than in the landscape setback areas adjacent to the street as defined in the Streetscape chapter of the Specific Plan. The Urban Design Streetscape Program requires specific trees for parkways on each of the major streets in the Specific Plan area.

Developments adjacent to minor streets that are not included in the Streetscape Design Plan should utilize trees from the list below:

<u>Botanic Name</u>	<u>Common Name</u>
Trees	
Brachychiton populneus	Bottle Tree
Calodendrum capense	Cape Chestnut
Cedrus deodara	Deodar Cedar
Cinnamomum camphora	Camphor Tree*
Cupaniopsis anacardioides	Carrot Wood
Ficus benjamina	Benjamin Fig*
Ficus r. "Nitida"	Indian Laurel*
Geijera parviflora	Australian Willow
Harpephyllum caffrum	Kaffir Plum
Hymenosporum flavum	Sweet Shade
Lagerstroemia indica	Crape Myrtle
Magnolia g. "Majestic Beauty"	Majestic Beauty
Magnolia g. "Samuel Sommer"	Samuel Sommer
Magnolia g. "Russet"	Russet Magnolia
Magnolia g. "St. Mary"	St. Mary Magnolia
Metrosideros tomentosus	New Zealand Christmas Tree
Pinus canariensis	Canary Island Pine
Pinus halepensis	Aleppo Pine
Schinus terebinthifolius	Brazilian Pepper
Tristania conferta	Brisbane Box

<u>Botanic Name</u>	<u>Common Name</u>
Shrubs	
Hibiscus "White Wings"	
Raphiolepis l. "Enchantress"	
Pittosporum t. "Variegata"	
Buxus j. "Green Beauty"	
Camellia Sasanqua	

* These trees should be planted in root barriers.

NOTE: All of the trees are to be planted in root barriers when located within 4 feet of a public sidewalk.

H. Hardscape Guidelines

- For purposes of these Design Guidelines, hardscape elements include all paving material on private property including sidewalks, paths, walkways, courtyards and plazas.
- Pedestrian materials should be:
 - *textured enough to avoid slipping when wet*
 - *flat enough to avoid ankle sprains*
 - *durable enough to go unmaintained for a number of years*
 - *pleasing to look at*
 - *fade resistant*
 - *non reflective*
- Paving materials that should be avoided include:
 - *"turf block"*
 - *loose gravel*
 - *bomanite or paver with a cobblestone or rounded appearance*
 - *white or very light colored paving*
 - *decomposed granite*
- Asphalt is not a suitable paving material for pedestrian walkways.

CHAPTER V

URBAN DESIGN

STREETSCAPE

Introduction

A. Purpose Of The Urban Design Streetscape Program

The purpose of the Urban Design Streetscape Program is to establish design standards for streetscape improvements within the public rights-of-way and to provide a specific design context for adjacent private outdoor spaces. The Urban Design Streetscape Program identifies and coordinates the public/private streetscape design elements of paving, street tree plantings, median plantings, lighting, parkway width and design, and other unique design features within the North Montclair Specific Plan area.

The Urban Design Streetscape Program will be used as a planning tool for public and private projects in conjunction with the development standards of this Specific Plan and the City of Montclair Zoning Code.

This chapter contains information, design parameters, graphic materials and conceptual designs which can result in immediate or near term implementation of the recommendations for public area improvements. It should be noted, however, that certain additional actions will be necessary, or may be determined to be desirable, to continue moving toward the implementation of the Urban Design Streetscape Program in an expeditious manner. These actions include:

1. Preparation of final design and working drawings for public area improvements;
2. Preparation of financial package to pay for construction and maintenance of the public area improvements;

3. Development of a new unique logo for North Montclair to be used on flags, banners and signs proposed in this plan; and
4. Initiation of public workshops through which property and business owners are apprised of improvement plans and their role in the upgrading of the area.

B. Image And Identity

A person's mental image of North Montclair is formed over time through direct experiences and information gained from personal observations, media coverage and from other people. Opinions vary among individuals, whether they are residents, merchants, visitors or people who have never visited the area, but nonetheless have formed impressions. It is reflected in the ways people patronize business, negotiate traffic, visit the mall or off-price retail centers, use the Transcenter, choose restaurants or decide where to live. Impressions of the area can be measured in objective criteria such as retail sales activity, job information and assessed valuation. Impressions can also be evaluated in more qualitative or perceptual terms such as clarity, distinctiveness, intensity of activity, openness, accessibility and attractiveness.



As described here, image and identity are considered from the perspective of the way people will experience the North Montclair area -- driving through it, observing its unique qualities, the character of its streets, walking through commercial areas of the Montclair Plaza and the Fremont Avenue shops, and visiting specific destinations.

The best commercial districts have memorable qualities:

- *A clear sense of arrival, through a distinct change in landscape, built areas or special entrance features;*
- *A commercial or cultural "heart" which says something about activities, history, commerce or natural features which the community values. In some cities, it is a college campus; in others, an old plaza with an ancient tree, a cluster of financial institutions, a shopping street, a historic district or a distinctive commercial intersection. In Montclair, one of the objectives is to link the Transcenter with Montclair Plaza, creating a unique pedestrian zone along Fremont Avenue;*
- *A clear organization of streets and districts which gives people a sense of direction and orientation, in being able to find one's destination easily and safely. The extent of confusion in traffic circulation, and the amount of congestion increasingly figures in people's perceptions of particular places; and*
- *A sense of uniqueness. This quality can derive from a single feature, an areawide design theme or an event which is held in a special place each year.*

So, where is North Montclair?

North Montclair is not a clearly identifiable area, even though its boundaries are easily delineated by City boundaries, various streets and the San Antonio Wash which form its edges. The Montclair Plaza provides a strong focus for the area, but outside the immediate plaza property the area quickly loses its identity.

Conclusions regarding the image and identity of North Montclair include:

- *North Montclair does not have a readily distinguishable boundary;*
- *North Montclair's central focus is the Plaza; however, commercial land use patterns within the area are inconsistent and spread out;*
- *North Montclair is dominated by the automobile with few pedestrian amenities; and*
- *Similar to other areas, landownership is fragmented.*

What became increasingly apparent in preparation of the urban design concept was the need to focus resources on specific road segments. The urban design plan that evolved acknowledges this fact.



NORTH MONTCLAIR SPECIFIC PLAN

Image Framework Plan

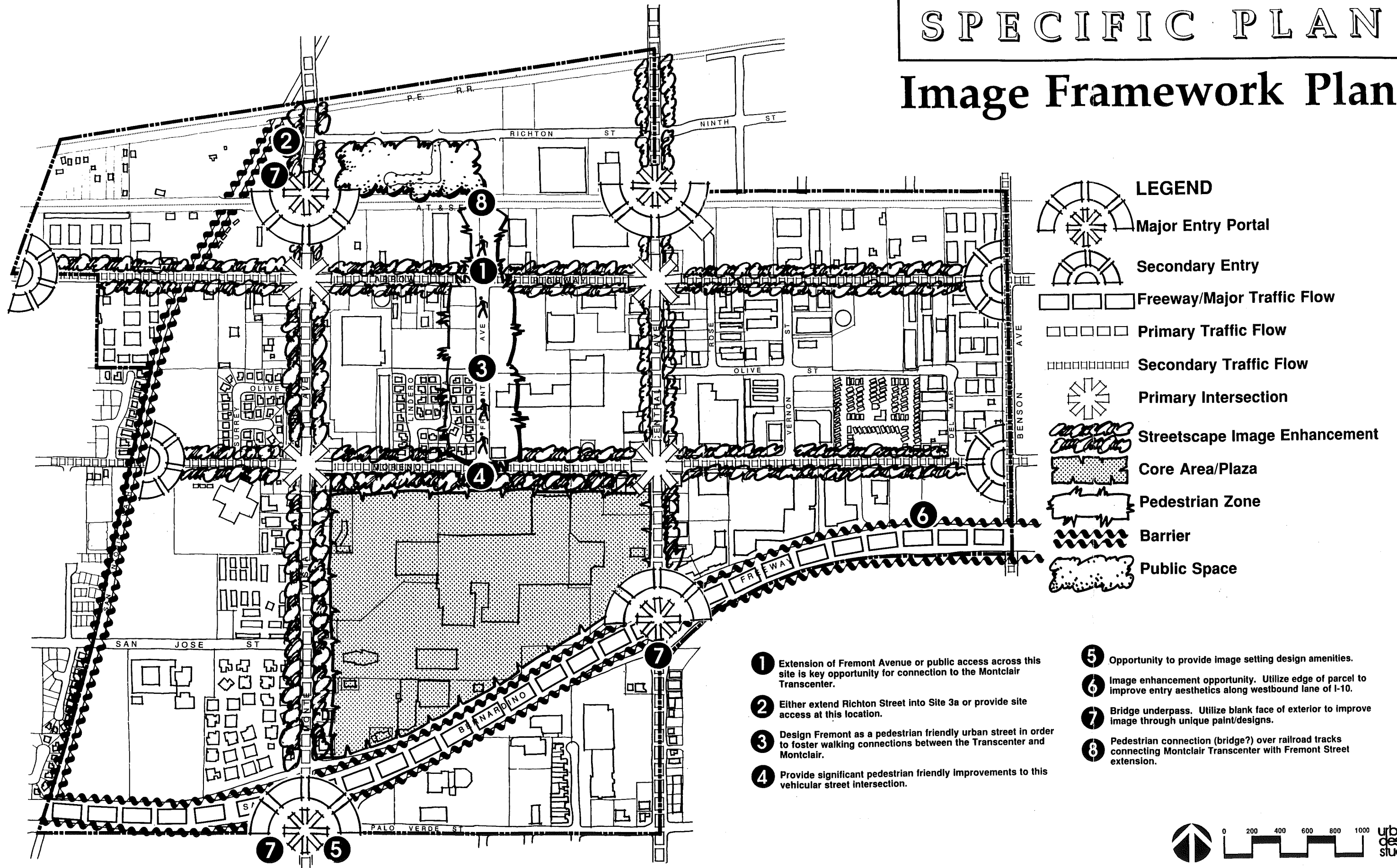


Image Framework / Design Concepts

A. Introduction

The Image Framework plan identifies and evaluates special conditions and planning opportunities that are addressed in the North Montclair Urban Design Streetscape Program. It includes findings from technical reviews, field investigations and discussions with City representatives. This product is the single most important source of background information in defining the direction of the urban design concept.

B. Planning Considerations

The analysis considered the following items:

- *Primary and secondary traffic flows;*
 - *Existing parkway conditions, street trees intersection land uses and on-street parking;*
 - *Opportunities for defining new urban design concepts;*
 - *Prominent public spaces; and*
 - *Logical opportunities for major entry portals, major intersections, secondary entry nodes and secondary commercial intersections.*
- *Physical location/configuration of existing streets, buildings and open spaces.;*
 - *New opportunities for maximizing efficient, pleasant and safe pedestrian movement and connections;*

All findings were recorded on the Image Framework Plan shown on the following page.

NORTH MONTCLAIR SPECIFIC PLAN

Urban Design Streetscape Concepts

Legend



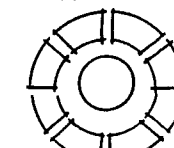
Primary Entry



Central Avenue/Major Traffic Flow



Fremont Avenue/Major Pedestrian Flow



Major Focal Intersection



Secondary Intersection



Entry Portal



High Image Streetscape
Montclair Plaza Area



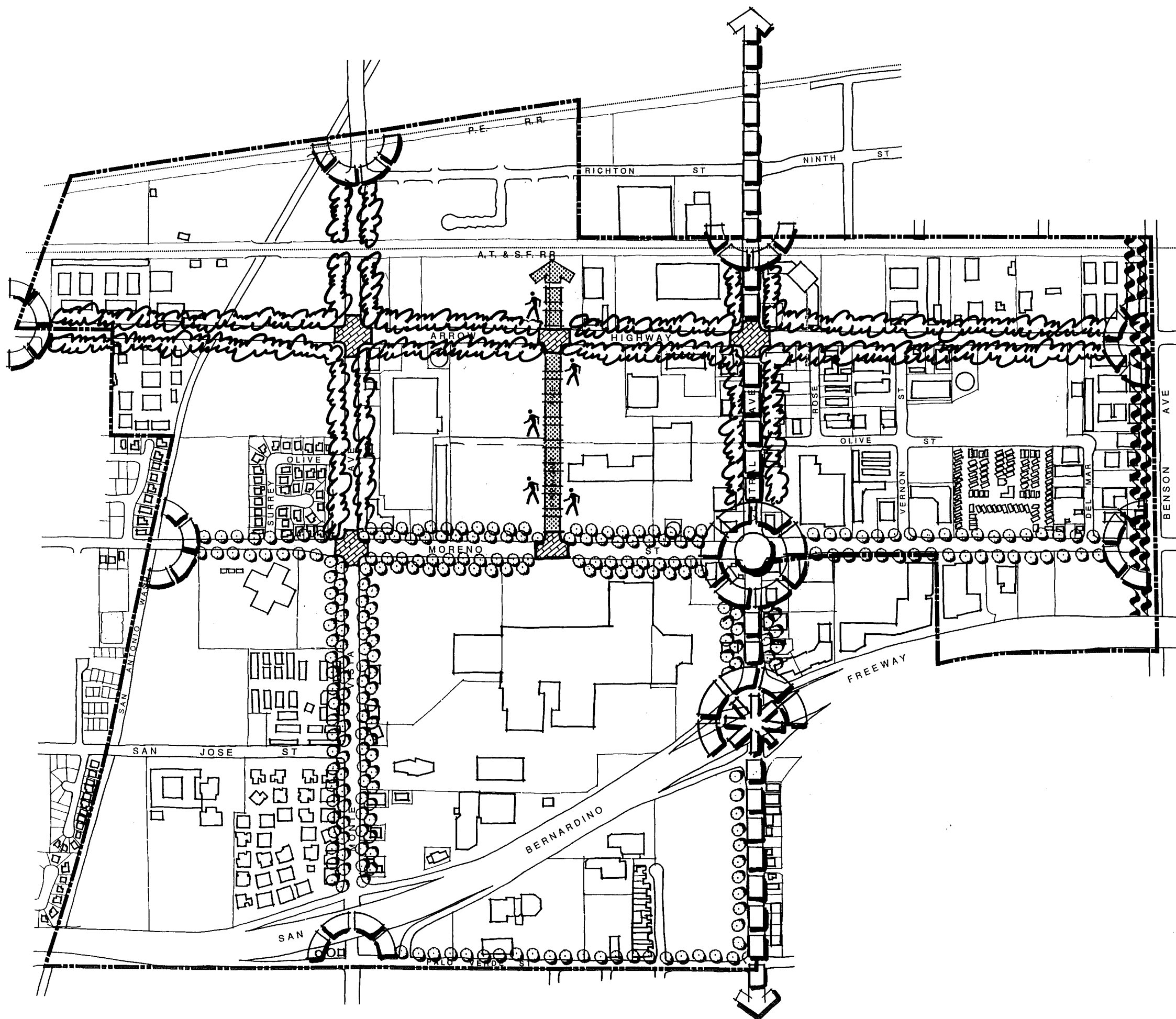
Primary Streetscape Enhancement



Secondary Streetscape Enhancement



Community Separator
Benson Avenue



C. Urban Design Streetscape Concepts

The following urban design concepts are based in part upon the preceding Image Framework Plan, goals and objectives of the Specific Plan and input from City staff and the public.

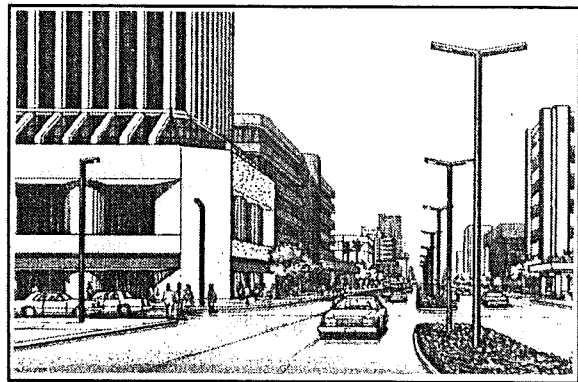
The purpose of this subsection is to provide a basis upon which particular improvements to public streets, sidewalks, medians and parkways can be recommended.

The key features of the urban design concept for North Montclair are:

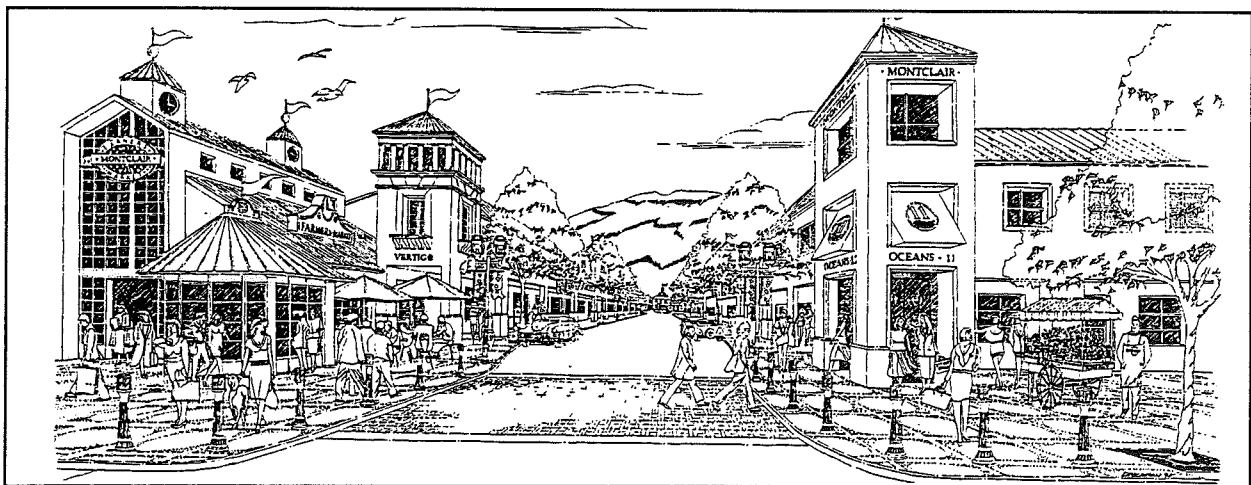
Central Avenue as the major entry street serving local and regional commercial uses: Provides excellent access to the heart of North Montclair.

Fremont Avenue as the major pedestrian street and connection of the Plaza and Transcenter: Will serve as the major north-south access link between the Transcenter and Montclair Plaza, which denotes the "heart" of North Montclair. Utilize urban streetscape materials which compliment regionally oriented transit and commercial development.

Montclair Plaza border streets as the "high image" urban streetscapes: Segments of Monte Vista Avenue, Moreno Street and Central Avenue which are adjacent to Montclair Plaza will have the primary and exclusive urban streetscapes depicting the most elaborate design features.



Private parkways as an integral component of the Urban Design Streetscape Program: Provide consistency within private front setbacks to add to the unified image of each street segment. Utilize consistent trees, signs, planting arrangements and parking lot lighting.

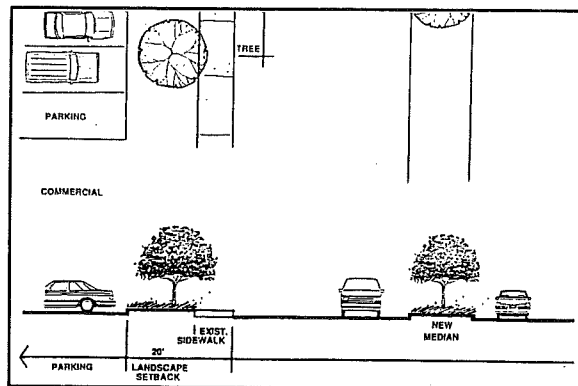


Consistently designed entry portals at major intersections: Each intersection would acknowledge the nuances of the immediate commercial environment and varying sidewalk/parkway depths. For intersections on the edge of the Specific Plan area, only the two corners on the North Montclair side should receive special treatment to emphasize the district edge/entry.



Retain quality streetscape elements: The urban design concept is to emphasize existing streetscape elements versus extensive and expensive revised streetscape designs. New materials are recommended for certain streets as new private development of rehabilitation occurs.

Arrow Highway as the high quality suburban vehicular "through-street": Arrow Highway will serve as the primary east-west through street in the area, linking the adjoining communities of Claremont and Upland.

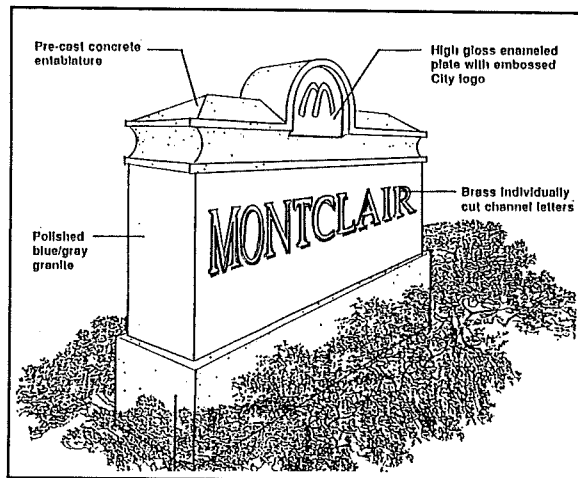


Monte Vista Avenue as the high volume traffic link from the foothill communities to the Transcenter, Montclair Plaza and the freeway: Provides a high quality, multi image streetscene through the heart of North Montclair.

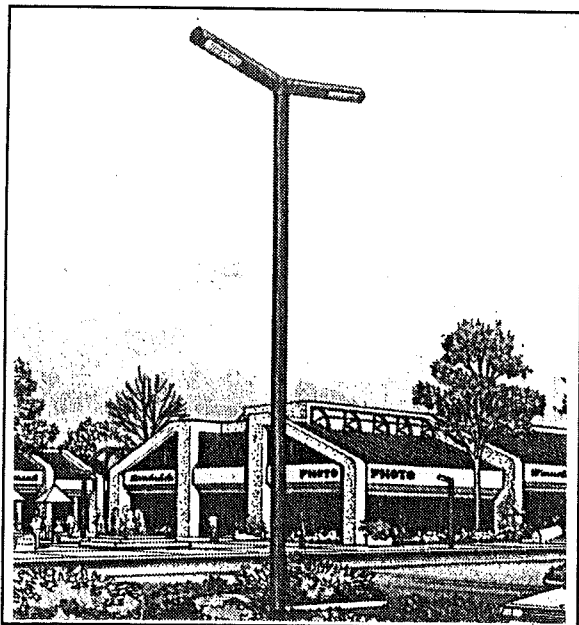
Color as a design unifier: Due to the variety of streetscapes in North Montclair, a single color, cobalt blue, will be utilized on all public right-of-way streetscape hardware.

Benson Avenue as the strong City "edge" identifier: Utilize median design and Montclair private parkways to differentiate between the two communities.

Parkway monument sign materials as urban/suburban identifiers: Monument sign bases of suburban river cobble design or urban theme of polished stone veneer.



High mast contemporary street lighting as "high image" streetscape trademark: Segments of Monte Vista, Moreno and Central which are adjacent to Montclair Plaza will receive special street lighting. Existing cobra head street lights to be removed and relocated.

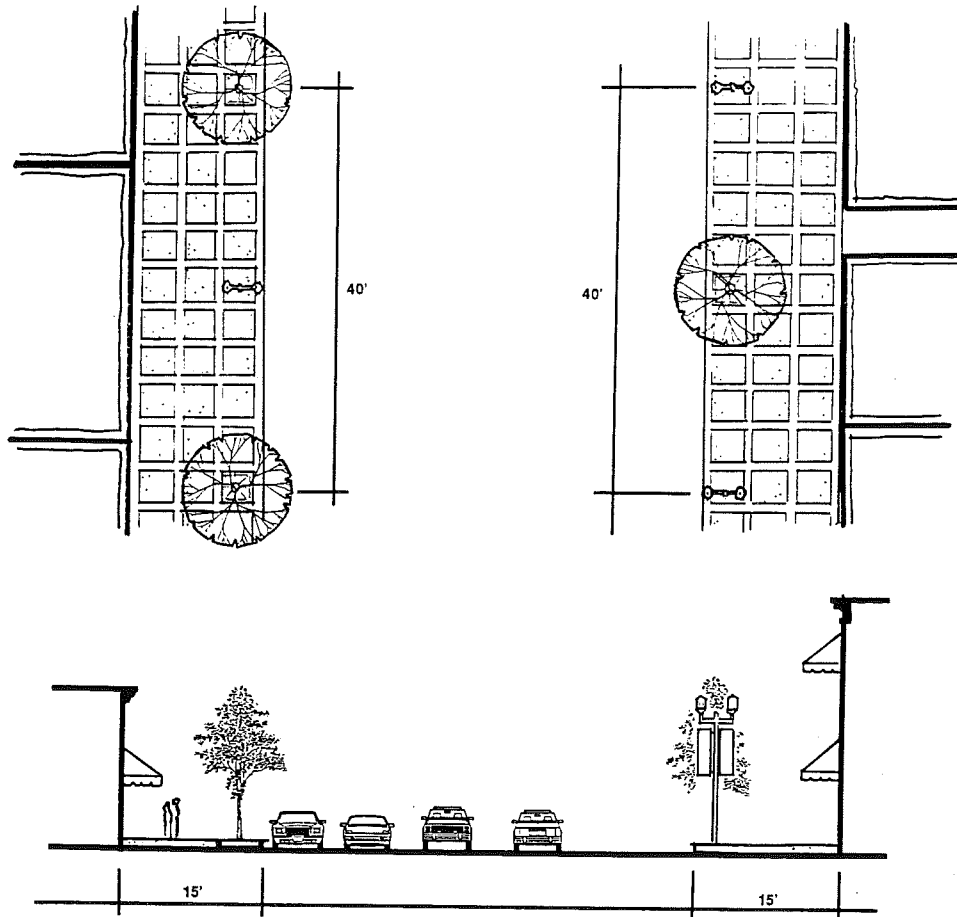


D. Urban Design Streetscape Program

The following pages depict the Urban Design Streetscape Program on a street segment by street segment basis.

Immediately following the street segment depictions, are the streetscape furniture selections. These include the theme color (cobalt blue), paving example, street lights, entry monument design, bench example, planters, trash receptacles, tree grates, and bollards.

Fremont Avenue - Moreno to Arrow



Public R.O.W. Parameters

Ultimate No. of Lanes: Four (two preferred)

On Street Parking: Yes (may be diagonal, subject to traffic analysis)

Median: No

Sidewalk Location/Width: 15' wide curb adjacent.

Tree Specie #1: London Plane

Tree Specie #2:

Spacing: 40' on center

Arrangement: Formal single row, curb adjacent.

Tree Grate: 4' Square

Street Lights: "Duluth" style (FXDL30) double fixture on W209 pole (by Western Lighting); 40' on center between street trees.

Crosswalk Paving: Basketweave pattern of blue and red brick pavers.

Banner Poles: Banners attached to new light poles.

Uplights: Yes, in tree grates.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: No

Parkway Design: N/A

Allowable Tree Species: N/A

Spacing: N/A

Arrangement: N/A

Building Location: Sidewalk adjacent.

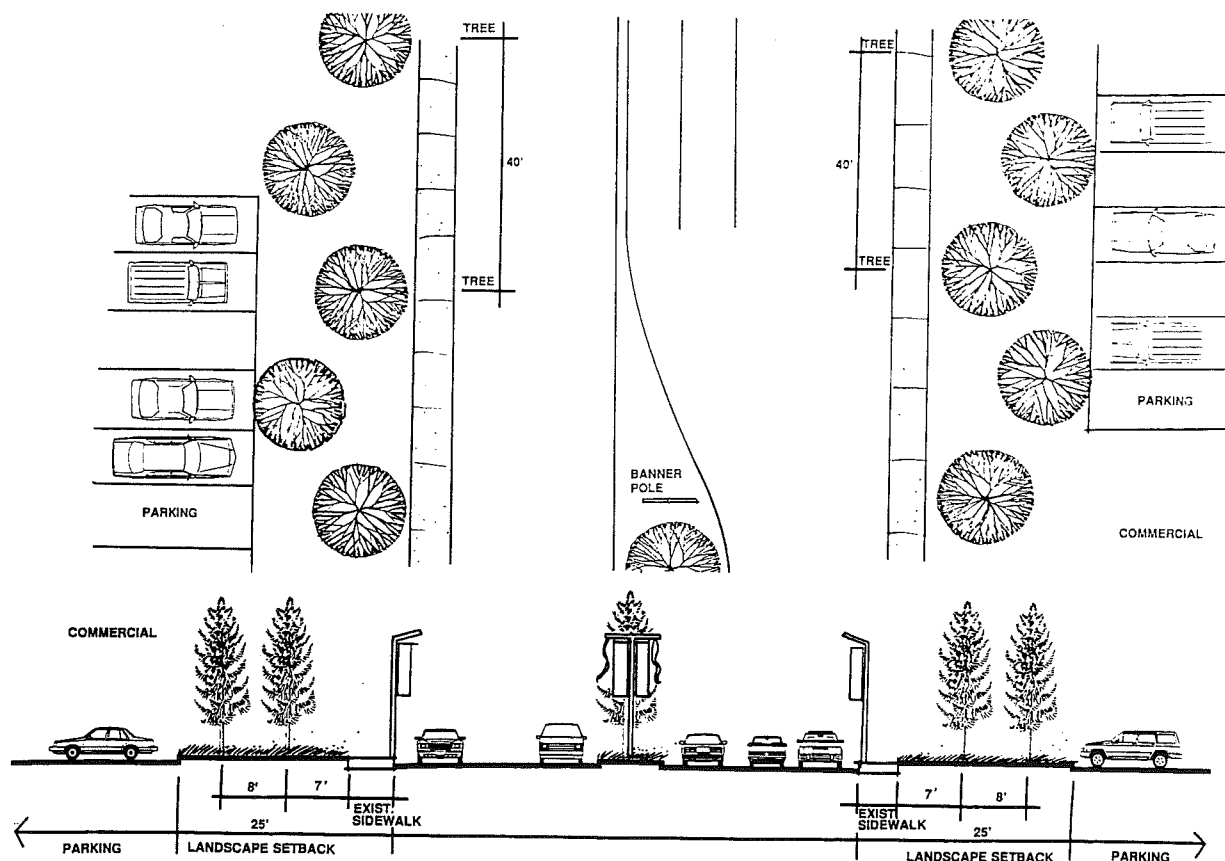
Parking Lot Lights: To match street lights. (height & spacing vary)

Driveway Entry Paving: Basketweave pattern of blue and red brick pavers.

Parkway Sign Parameters: No freestanding signs allowed

Color Theme: Cobalt Blue

Central Avenue - I-10 to Moreno



Public R.O.W. Parameters

Ultimate No. of Lanes: Six (2 left turn lanes northbound)

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Median Tree Specie #1: Canary Island Pine (existing)

Tree Specie #2: N/A

Spacing: 40' on center

Arrangement: Single row

Tree Grate: No

Street Lights: KIM Lighting O.T.S.; high pressure sodium, height, wattage, spacing to be determined.

Crosswalk Paving: Basketweave pattern of blue and red brick pavers, between 12" wide concrete ribbon curb.

Banner Poles: On light standards and at end of tree rows in medians.

Uplights: In medians on banners only.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 25' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Canary Island Pine.

Spacing: Each row 40' on center; one row 12' from sidewalk; second 24' from sidewalk.

Arrangement: Double row in an alternating pattern.

Building Location: Adjacent to parkway preferred; behind parking allowed.

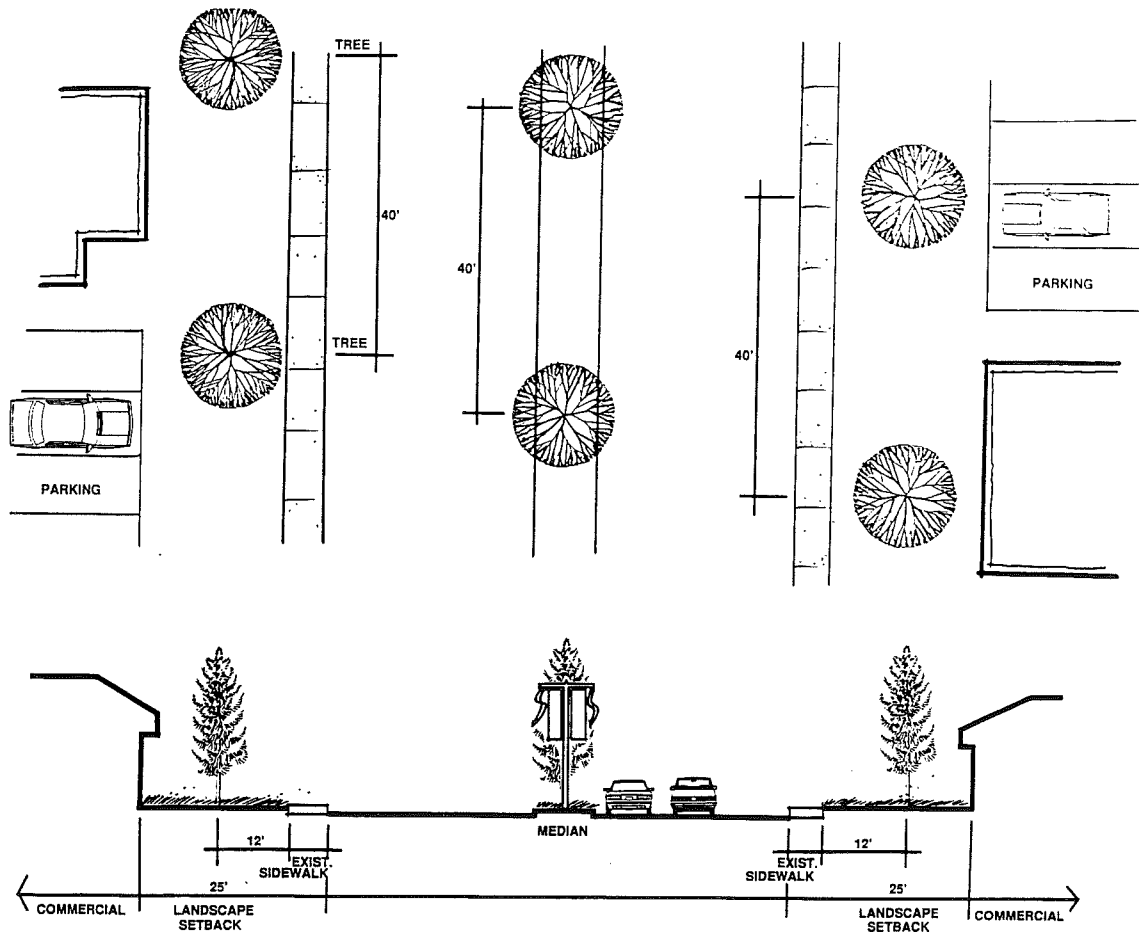
Parking Lot Lights: To match street lights.

Driveway Entry Paving: Basketweave pattern of blue and red brick pavers.

Parkway Sign Parameters: "Ground Hugging" monument sign with polished stone veneer. Opaque field/background required on internally illuminated can-type signs.

Color Theme: Cobalt Blue

Monte Vista Avenue - Moreno to Richton



Public R.O.W. Parameters

Ultimate No. of Lanes: Six

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walk design.

Median Tree Specie #1: Canary Island Pine

Parkway Tree Specie #2: Canary Island Pine

Spacing: 40' on center

Arrangement: Single row

Tree Grate: N/A

Street Lights: Existing

Crosswalk Paving: Brick pavers.

Banner Poles: In medians, double banner poles, 16' high, theme color; and at ends of tree rows.

Uplights: In medians on banners only.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 25' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Canary Island Pine; Brisbane Box.

Spacing: 40' on center

Arrangement: Single row; 12' from sidewalk.

Building Location: Adjacent to parkway is preferable; behind parking allowed.

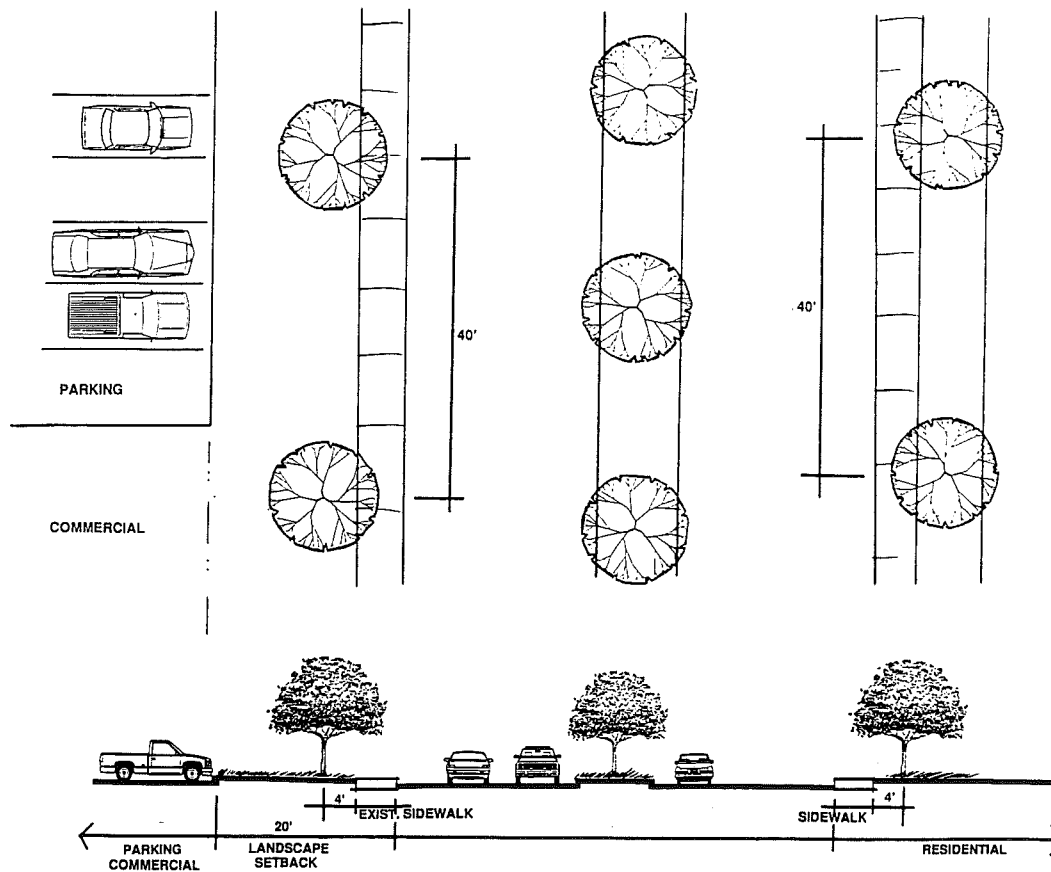
Parking Lot Lights: Contemporary shoebox style.

Driveway Entry Paving: Colored or stamped and colored concrete, or brick pavers.

Parkway Sign Parameters: "Ground-Hugging" monument sign with river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Palo Verde Street - Monte Vista to Central



Public R.O.W. Parameters

Ultimate No. of Lanes: Four

On Street Parking: Yes

Median: Existing

Sidewalk Location/Width: 6' wide curb adjacent concrete.

Median Tree Specie #1: Existing

Tree Specie #2: N/A

Spacing: Existing

Arrangement: Existing

Tree Grate: N/A

Street Lights: Existing

Crosswalk Paving: None

Banner Poles: No

Uplights: No

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: North side (commercial) only; 20' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Carrotwood

Spacing: 40' on center; 4' from sidewalk.

Arrangement: Single row

Building Location: Adjacent to parkway preferred; behind parking allowable.

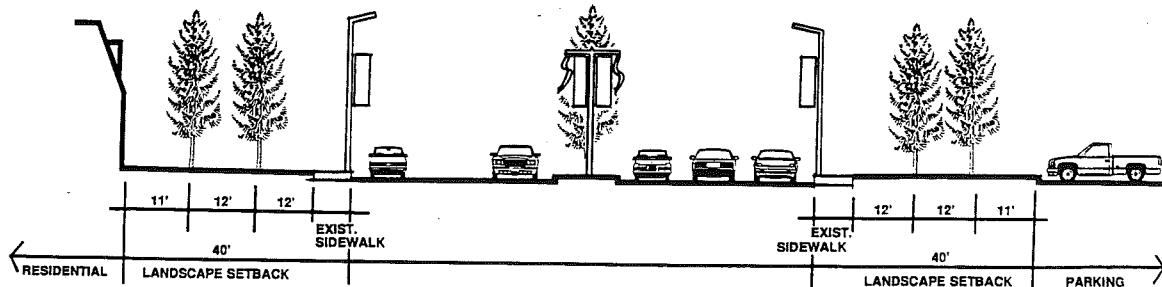
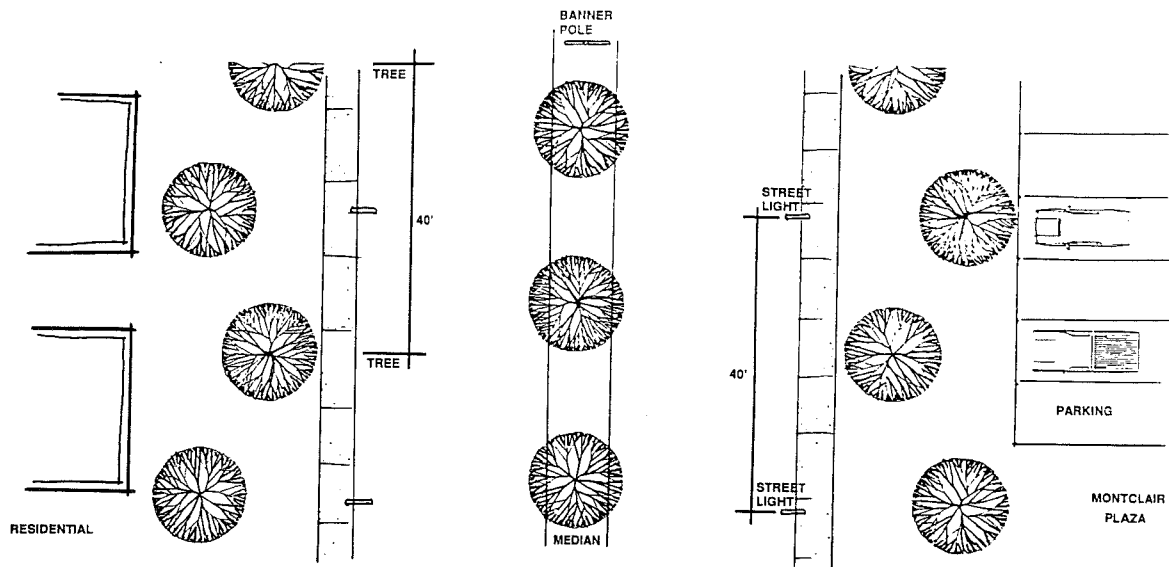
Parking Lot Lights: Contemporary shoebox style (N. side only)

Driveway Entry Paving: Not required

Parkway Sign Parameters: "Ground-Hugging" monument sign. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Monte Vista Avenue - I-10 to Moreno



Public R.O.W. Parameters

Ultimate No. of Lanes: Six

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Median Tree Specie #1: Canary Island Pine

Tree Specie #2:

Spacing: 40' on center

Arrangement: Single row

Tree Grate: N/A

Street Lights: KIM Lighting O.T.S.; high pressure sodium, height, wattage, spacing to be determined.

Crosswalk Paving: Basketweave pattern of blue and red brick pavers between 12" wide concrete ribbon curb.

Banner Poles: On light standards and at end of tree rows in medians.

Uplights: In medians on banners only.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 40' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Canary Island Pine; Brisbane Box.

Spacing: Each row 40' on center; one row 12' from sidewalk; second 24' from sidewalk.

Arrangement: Double row in an alternating pattern.

Building Location: Adjacent to parkway preferred; behind parking allowed.

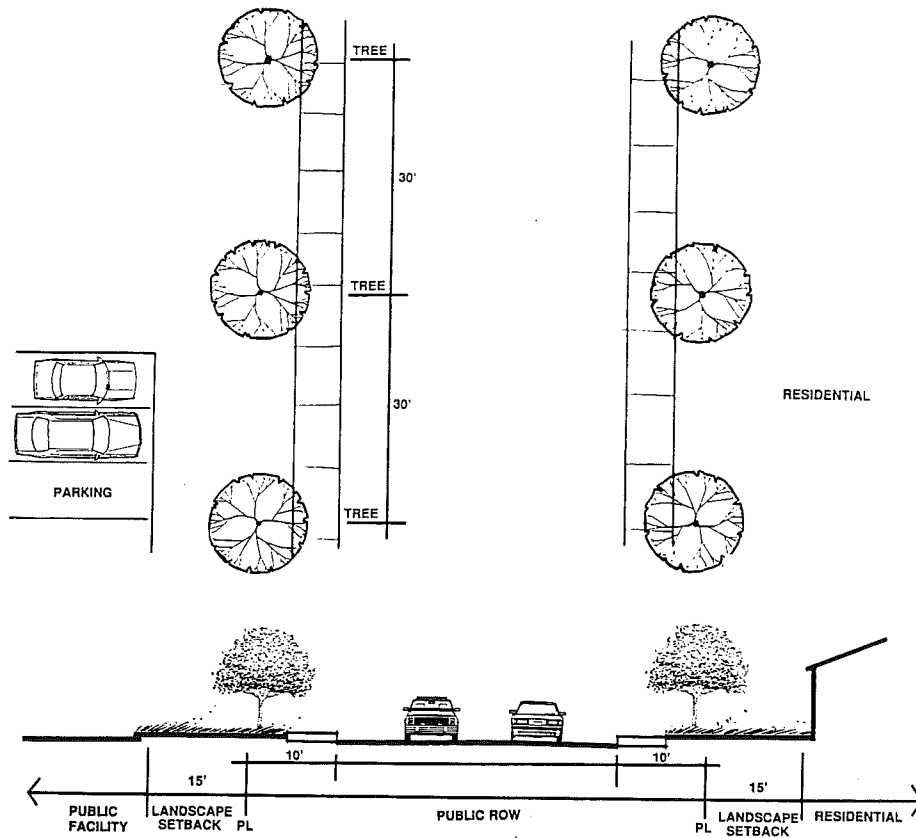
Parking Lot Lights: To match street lights.(E. side only)

Driveway Entry Paving: Basketweave pattern of blue and red brick pavers.

Parkway Sign Parameters: "Ground Hugging" monument sign with polished stone veneer. Opaque field/background required on internally illuminated can-type signs.

Color Theme: Cobalt Blue

Moreno Street - San Antonio Wash to Monte Vista



Public R.O.W. Parameters

Ultimate No. of Lanes: Two

On Street Parking: Yes

Median: No

Sidewalk Location/Width: 6' wide curb adjacent concrete sidewalk.

Parkway Tree Specie #1: Evergreen Pear

Tree Specie #2: N/A

Spacing: 40' on center; 2' from sidewalk.

Arrangement: Single row

Tree Grate: No

Street Lights: Existing

Crosswalk Paving: Basketweave pattern of blue and red brick pavers at Moreno and Monte Vista.

Banner Poles: No

Uplights: No

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 15' wide

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Personal selection by property owner, subject to landscape plan approval.

Spacing: Unrestricted

Arrangement: Unrestricted

Building Location: Per setback requirement

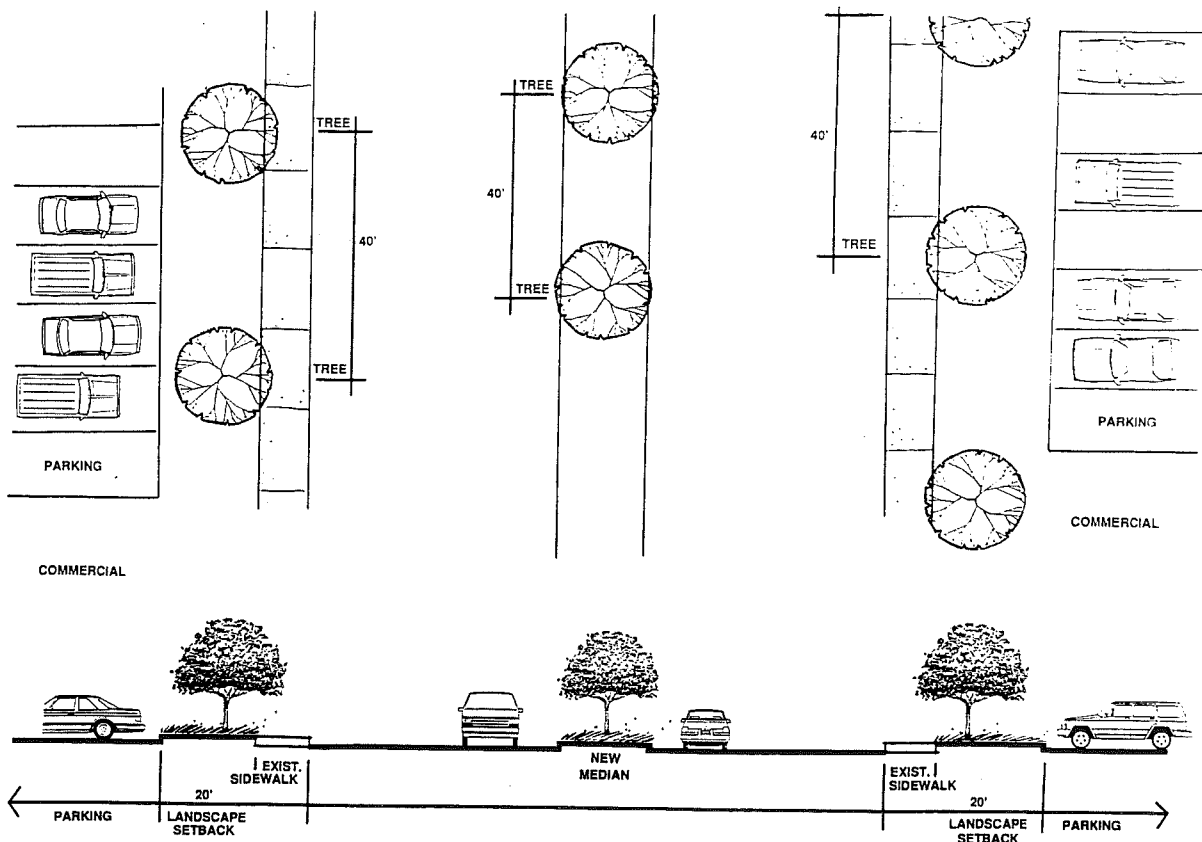
Parking Lot Lights: N/A

Driveway Entry Paving: Not required

Parkway Sign Parameters: No parkway signs allowed

Color Theme: None

Arrow Highway - City Limits to City Limits



Public R.O.W. Parameters

Ultimate No. of Lanes: Six

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Curb adjacent; 5' concrete sidewalk.

Median Tree Species #1: Sycamore

Tree Species #2: Coast Live Oak

Spacing: 40' on center

Arrangement: Single Row

Tree Grate: N/A

Street Lights: Existing

Crosswalk Paving: Basketweave pattern of blue and red brick pavers at intersection of Fremont only, between 12" wide concrete ribbon curb.

Banner Poles: No

Uplights: No

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 20' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Coast Live Oak

Spacing: 40' on center; 4' from sidewalk.

Arrangement: Single row

Building Location: Behind parking area preferred; behind landscape setback allowed.

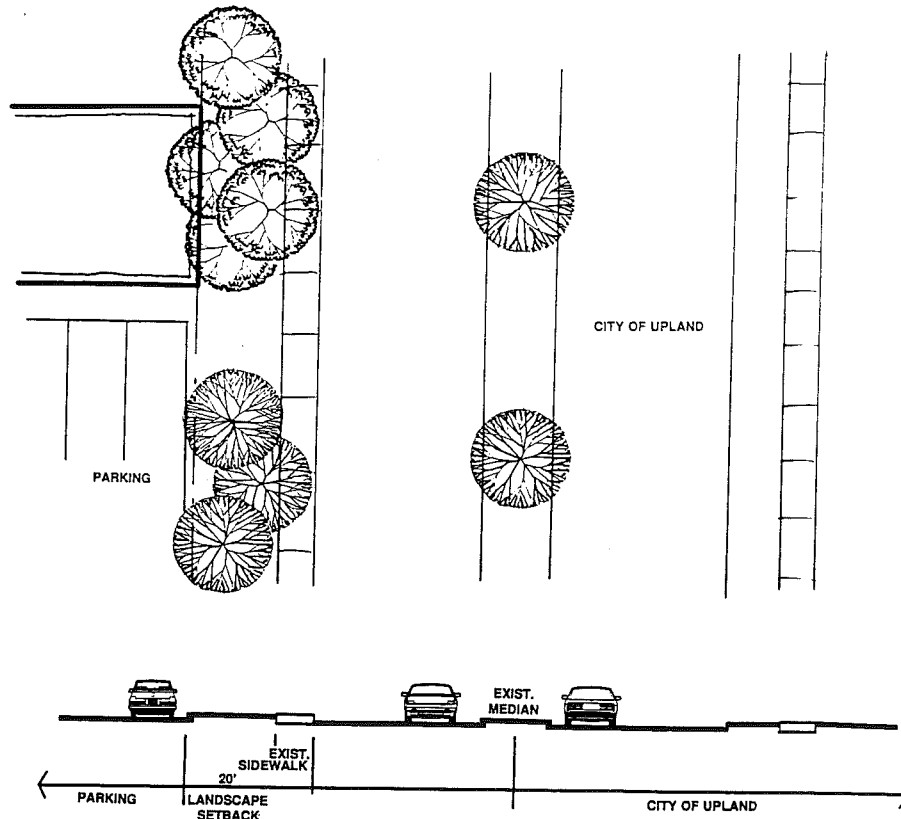
Parking Lot Lights: Contemporary shoebox style

Driveway Entry Paving: Not required

Parkway Sign Parameters: "Ground-Hugging" monument sign with river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Benson Avenue - I-10 to Arrow Highway



Public R.O.W. Parameters

Ultimate No. of Lanes: Four

On Street Parking: Yes

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Median Tree Specie #1: Canary Island Pine

Tree Specie #2:

Spacing: 30' on center

Arrangement: Single row

Tree Grate: N/A

Street Lights: Existing

Crosswalk Paving: None

Banner Poles: No

Uplights: No

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 20' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Canary Island Pine; Brisbane Box.

Spacing: N/A

Arrangement: Alternating groupings of 3-5-2-3.

Building Location: Behind parkway; adjacent parking area.

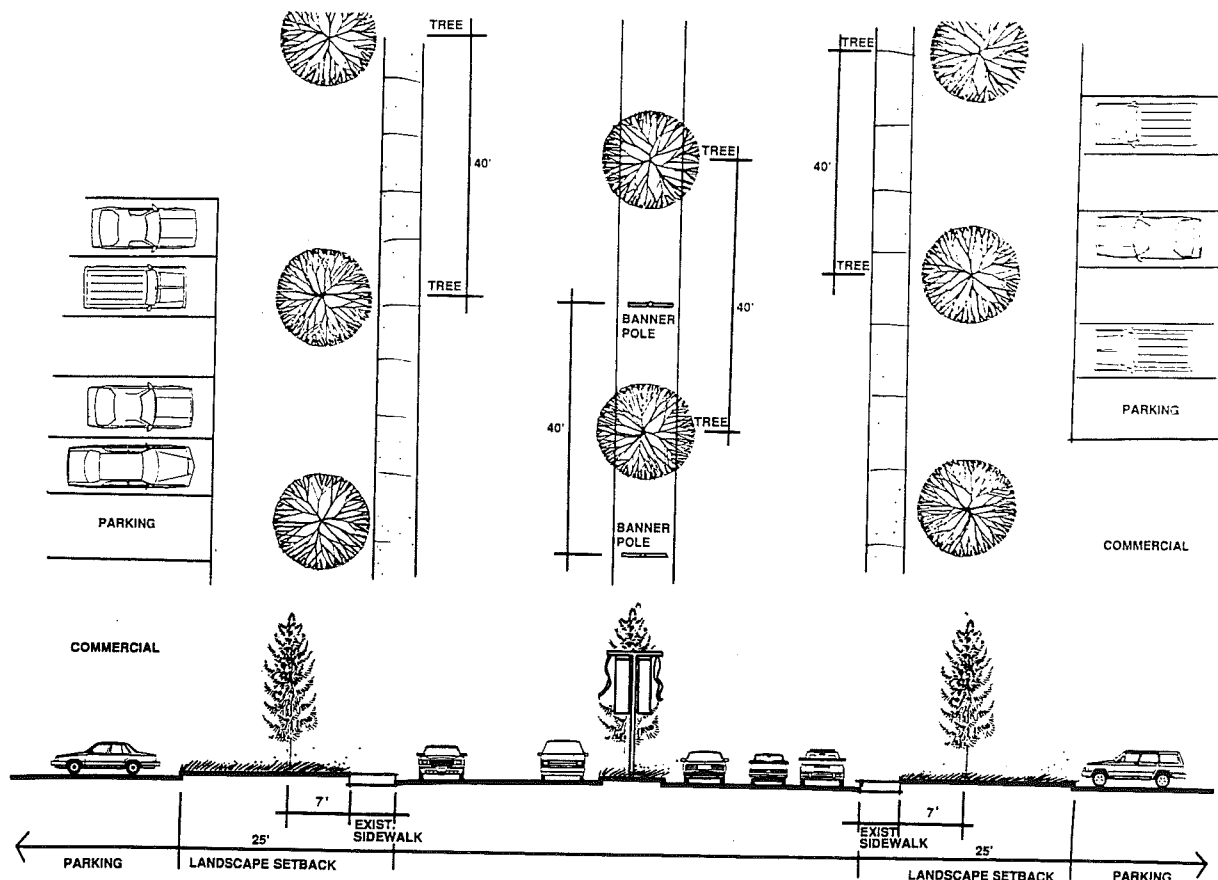
Parking Lot Lights: Contemporary shoebox style

Driveway Entry Paving: Not required

Parkway Sign Parameters: "Ground-Hugging" monument sign with river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Central Avenue - Palo Verde to I-10 and Moreno to Richton



Public R.O.W. Parameters

Ultimate No. of Lanes: Six

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Tree Specie #1: Canary Island Pine (existing)

Tree Specie #2:

Spacing: Match existing

Arrangement: Match existing

Tree Grate: No

Street Lights: Existing

Crosswalk Paving: Brick pavers.

Banner Poles: In medians; double banner poles, 16" high, theme color, located at ends of tree rows.

Uplights: In medians on banners only; locate at end of tree rows where planted with Pines.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 25' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Canary Island Pine.

Spacing: 40' on center; 7' from sidewalk.

Arrangement: Single row; 12' from R.O.W.

Building Location: Adjacent to parkway is preferable; behind parking allowed.

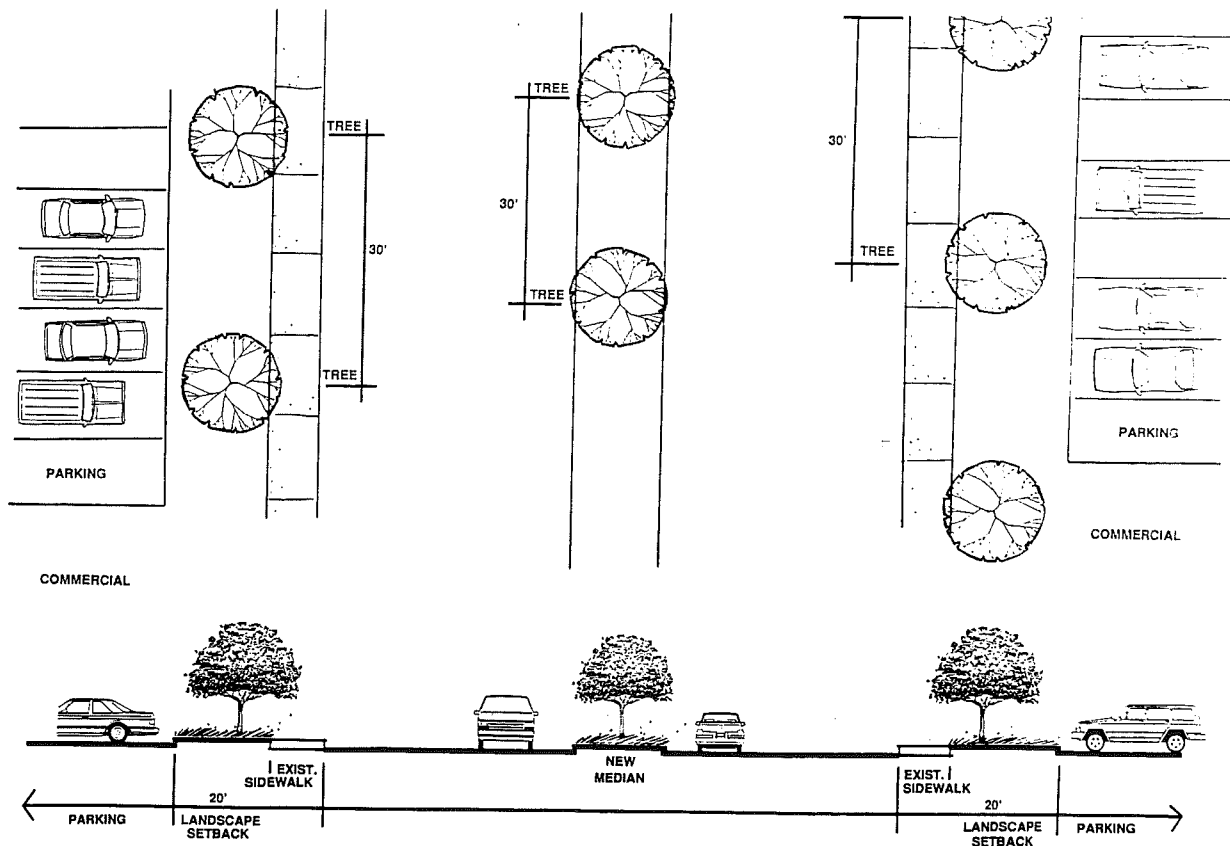
Parking Lot Lights: NA

Driveway Entry Paving: Colored and scored (or stamped) concrete or brick pavers.

Parkway Sign Parameters: "Ground-Hugging" monument sign with river rock base. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Moreno Street - Central to Benson



Public R.O.W. Parameters

Ultimate No. of Lanes: Four

On Street Parking: No

Median: Yes

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Median Tree Specie #1: Evergreen Pear

Tree Specie #2: N/A

Spacing: 30' on center

Arrangement: Single row

Tree Grate: N/A

Street Lights: Existing

Crosswalk Paving: Basketweave pattern of blue and red brick pavers at Moreno and Central.

Banner Poles: No

Uplights: No

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 20' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Evergreen Pear.

Spacing: 30' on center; 4' from sidewalk.

Arrangement: Single row.

Building Location: Adjacent to parkway preferable at corner locations; otherwise behind parking area.

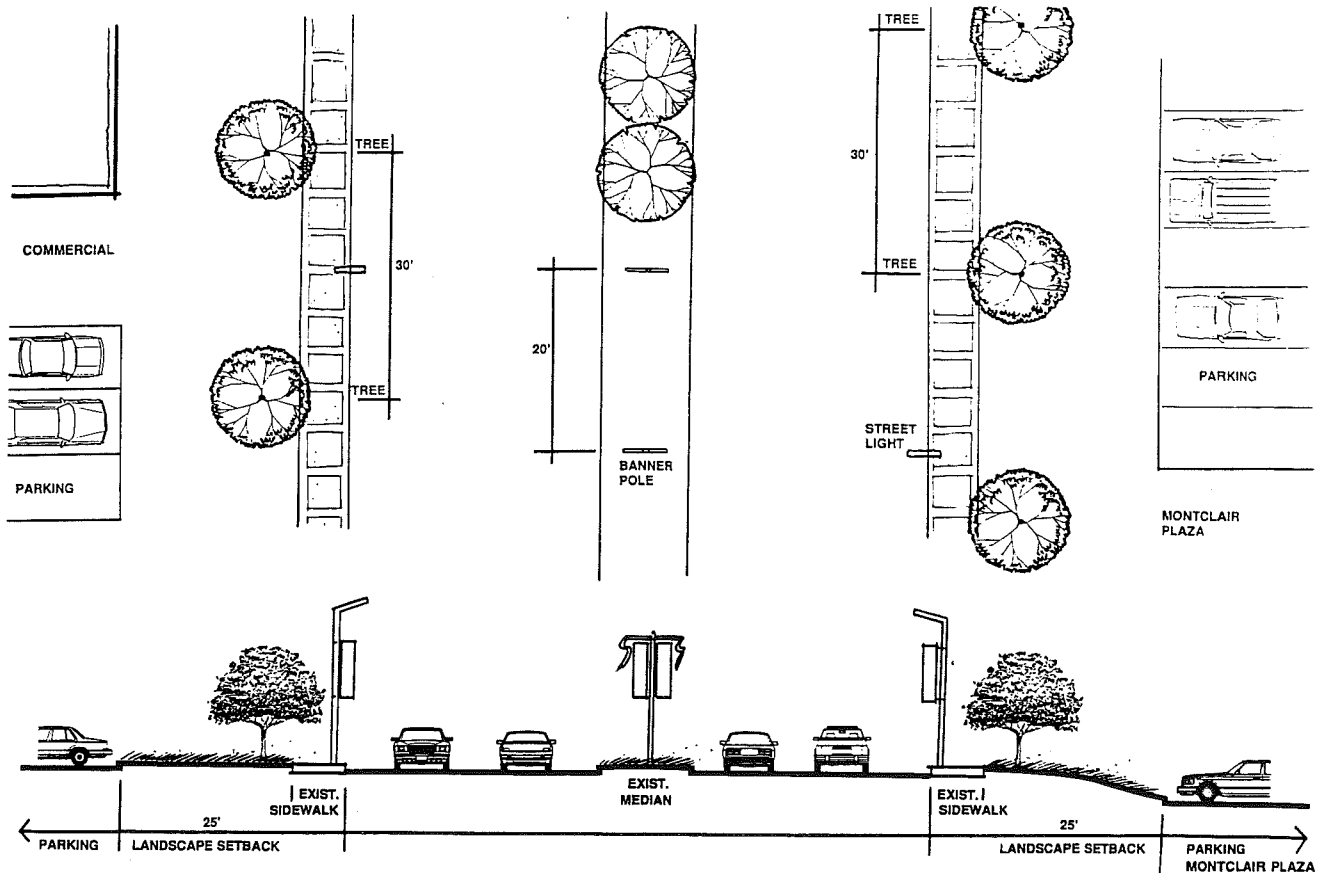
Parking Lot Lights: Contemporary shoebox style.

Driveway Entry Paving: Colored and scored (or stamped) concrete.

Parkway Sign Parameters: "Ground-Hugging" monument sign with river cobble base. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

Moreno Street - Monte Vista to Central



Public R.O.W. Parameters

Ultimate No. of Lanes: Four

On Street Parking: No

Median: Existing

Sidewalk Location/Width: Retain existing curb adjacent concrete walks.

Median Tree Specie #1: Existing in median.

Tree Specie #2: N/A

Spacing: N/A

Arrangement: N/A

Tree Grate: No

Street Lights: KIM lighting O.T.S.; high pressure sodium, height, wattage, spacing to be determined.

Crosswalk Paving: Basketweave pattern of blue and red brick pavers.

Banner Poles: In median; double banner poles; 16' high; theme color; 20' on center.

Uplights: In medians on banners only.

Color Theme: Cobalt Blue

Private Property Parameters

Parkway Landscape Strip and Width: 25' wide.

Parkway Design: Formal tree arrangement; low water turf.

Allowable Tree Species: Evergreen Pear.

Spacing: 40' on center; 4' behind sidewalk.

Arrangement: Single row

Building Location: North side: adjacent to parkway preferred; behind parking allowable. South side: Parkway adjacent preferred at corners only. Others part of Montclair Plaza.

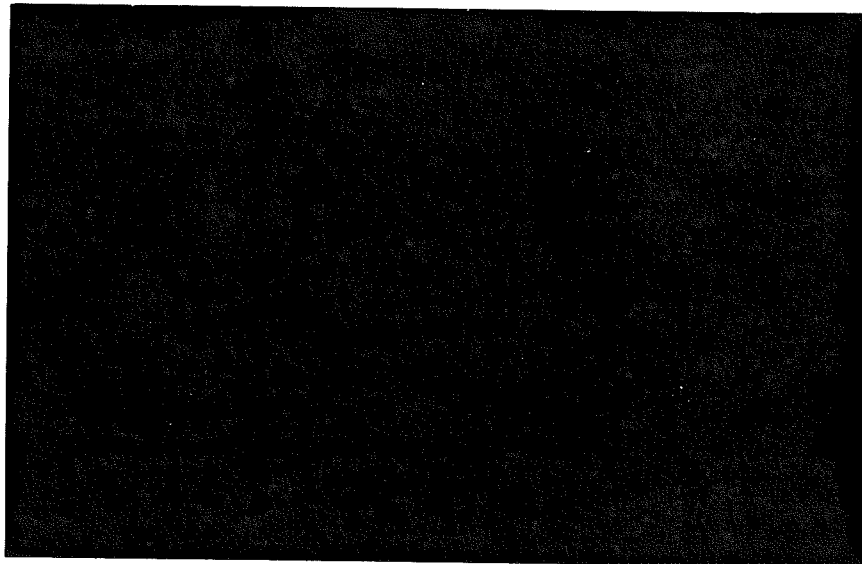
Parking Lot Lights: To match street lights.

Driveway Entry Paving: Basketweave pattern of blue and red brick pavers.

Parkway Sign Parameters: "Ground-Hugging" monument sign with polished stone veneer. Opaque field/background required on internally illuminated can-type monument sign.

Color Theme: Cobalt Blue

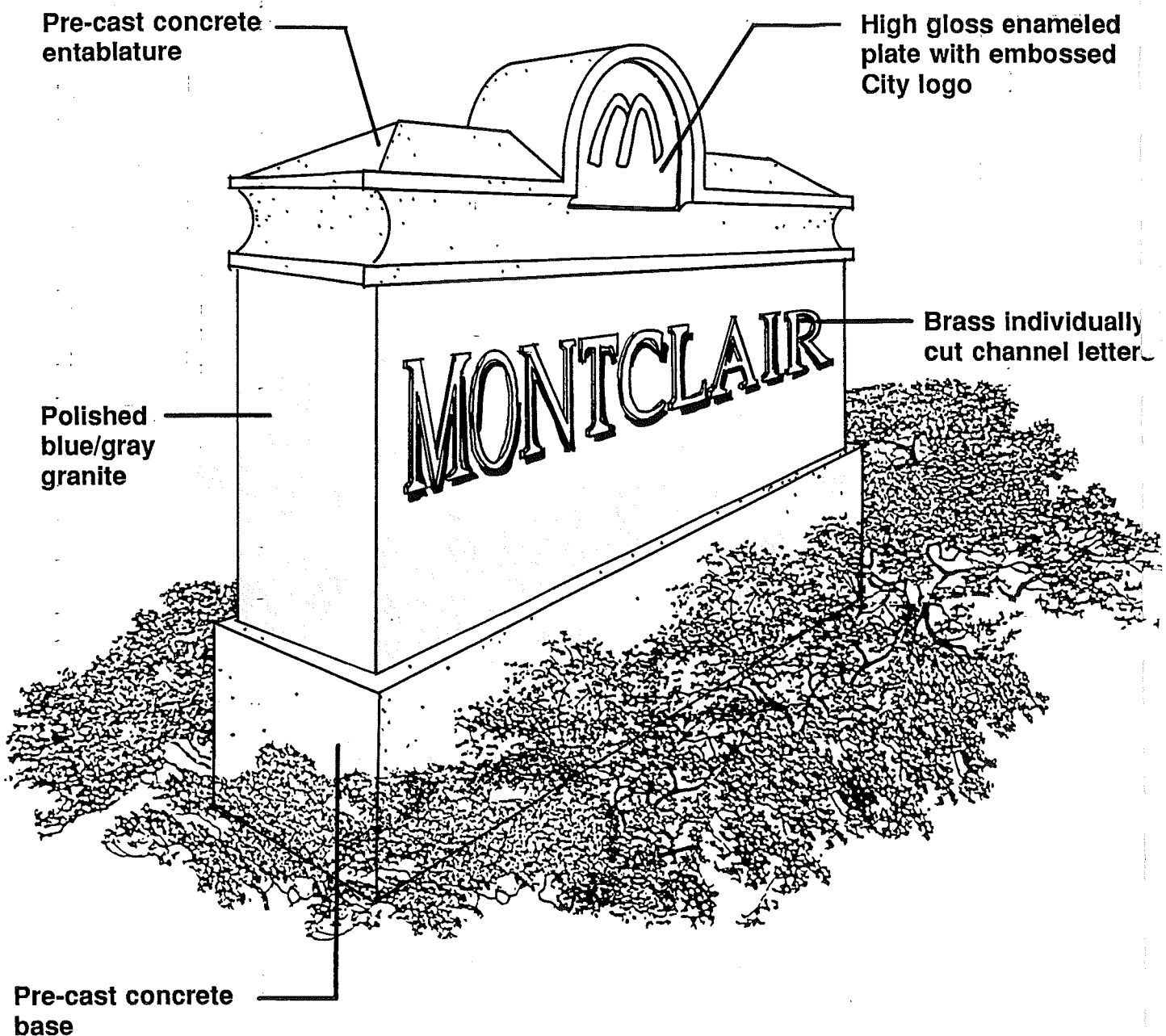
This color chip represents the integral theme color for all powder-coat painted street furniture including; lights, bollards, trash receptacles, and any other metal-surfaced street furniture.



Pantone Matching System; PMS # 300

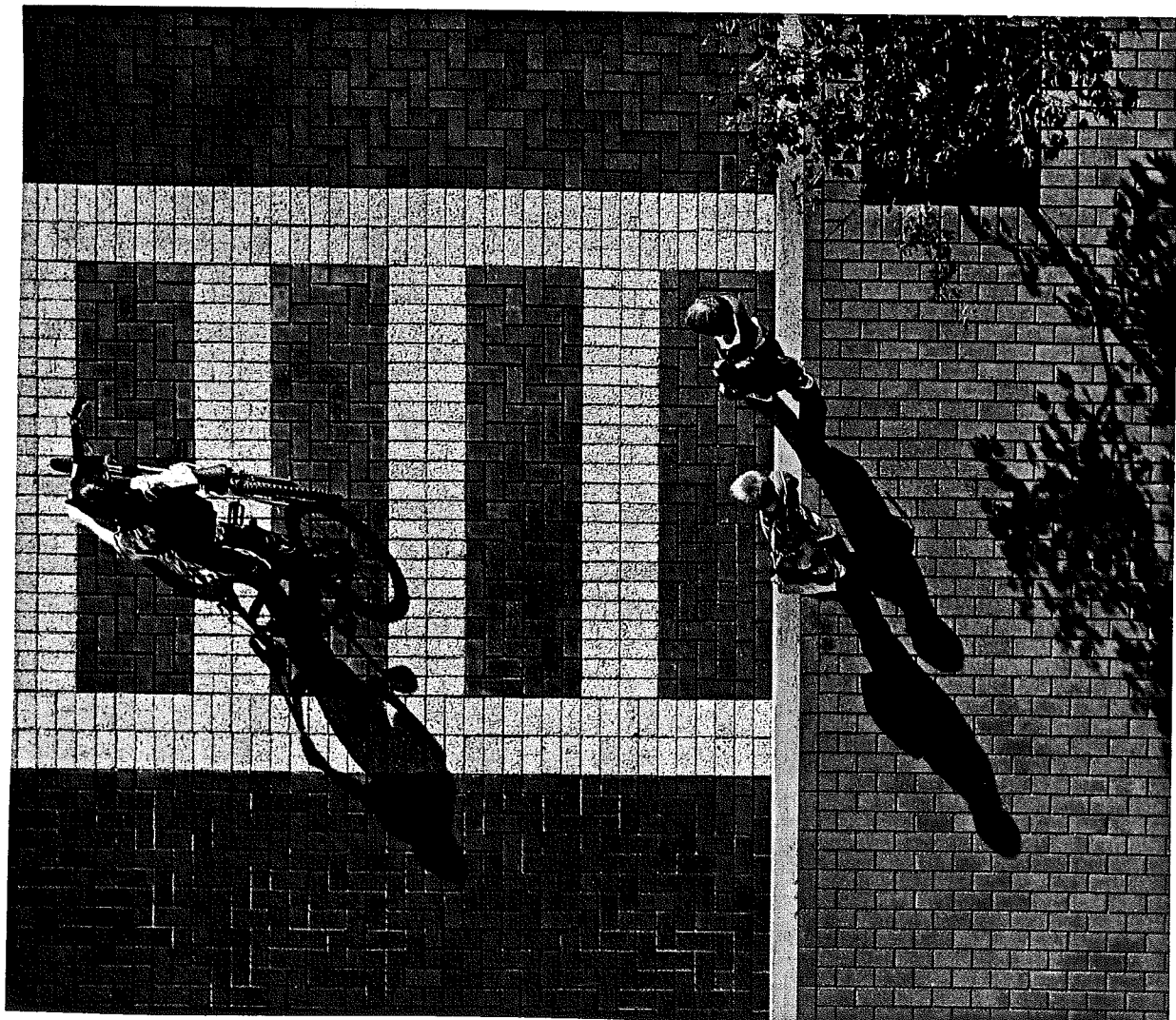
INTEGRAL THEME COLOR

Entry Monument Design

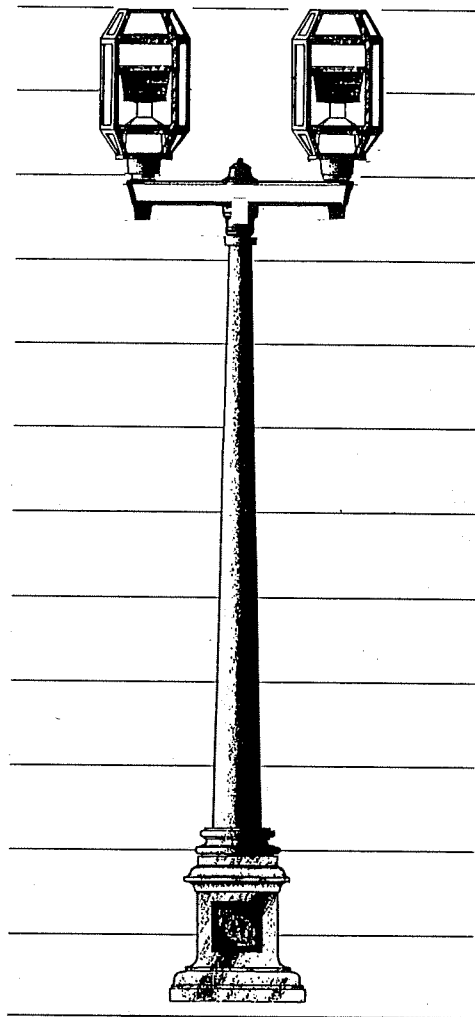




Concrete pavers(brick-style)
by Acker-Stone, Corona CA
or PermaConcrete, Moreno Valley CA
Basketweave pattern with color
variations of red,gray and blue.



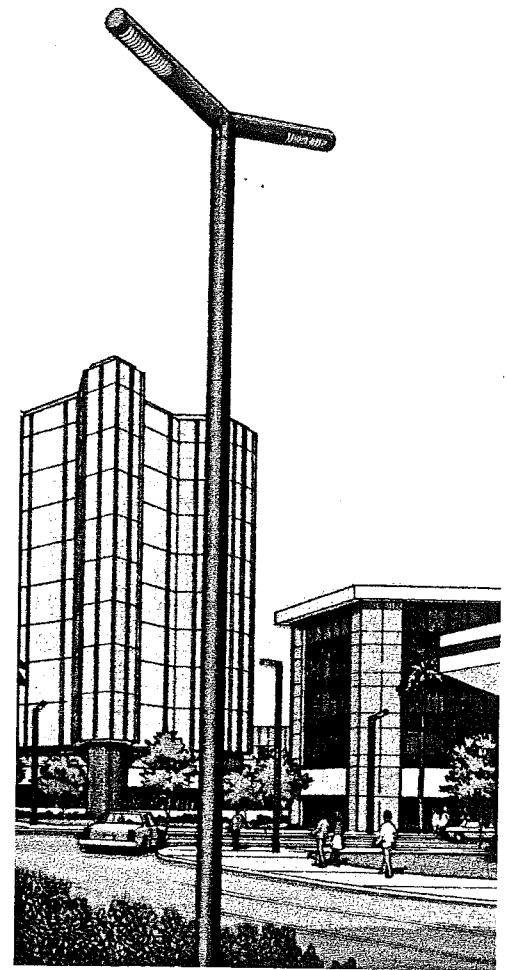
PAVING EXAMPLE



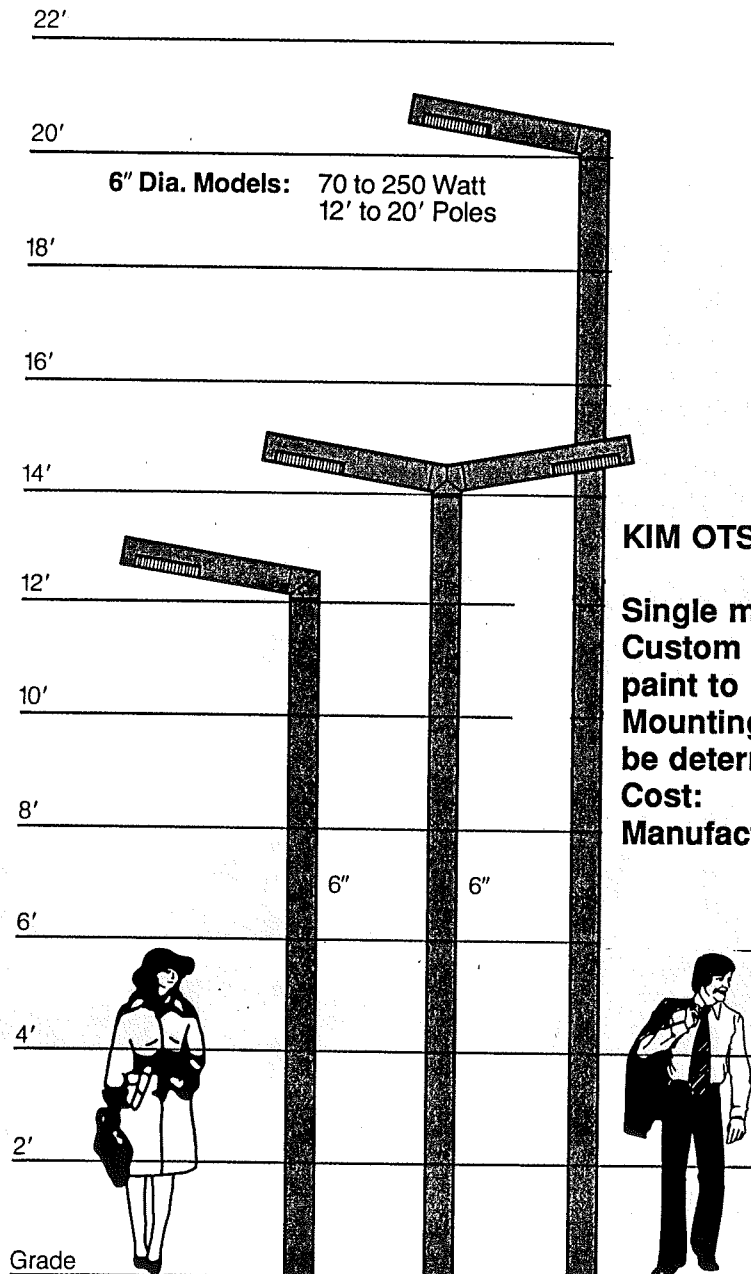
FXDL30
VT4-10

**Duluth
with double arm
fixture by
Western Lighting**

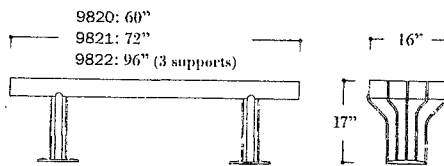
**ACCENT
STREET LIGHT**



**Montclair Plaza
Perimeter Only**



**NOTE: All prices are approximate costs
and will vary with quantity orders.**

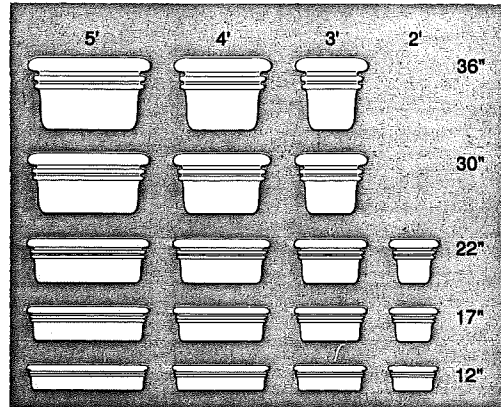
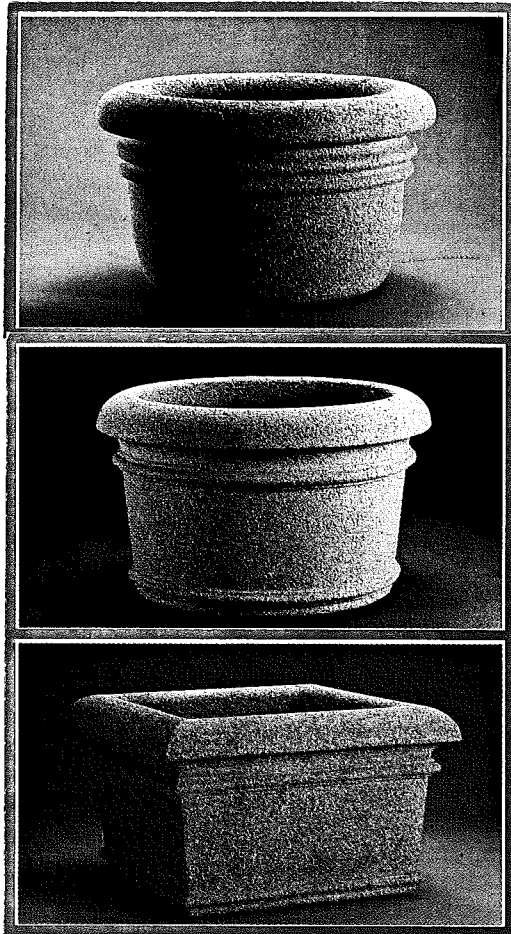


**Bench
by Recycle Design
Balance Bench
5' Length \$310.**

*Balance Bench with grey
Durawood slats and white
aluminum support*



BENCH EXAMPLE



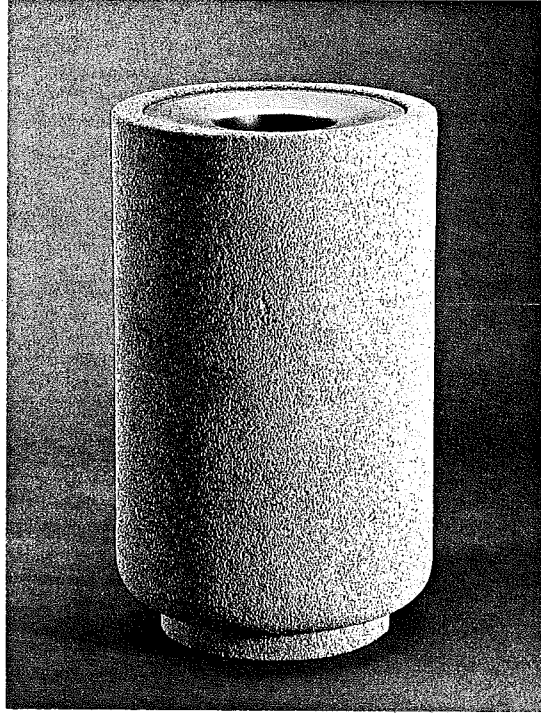
**Cast Stone Planters
by Dura Art Stone**

Styles: V (round)
X (round)
Y (square)

**Average cost: \$350. and up
for 3'dia. X 22\"**

Manufacturer: Dura Art Stone
11010 Live Oak
Fontana, CA 92335

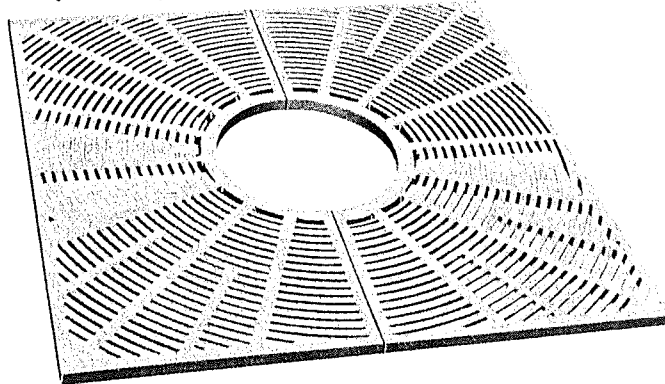
**NOTE: All prices are approximate costs
and will vary with quantity orders.**



**Cast Stone Trash Receptacle
Style TR-D3 by Dura Art Stone
Cost: \$276 (650 lbs)
Manufacturer: Dura Art Stone
11010 Live Oak
Fontana, CA 92335**

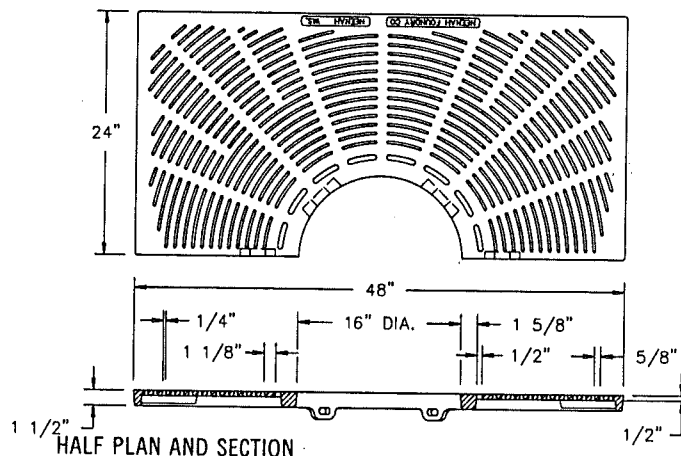
R-8708 180° SQUARE

(Formerly R-8647)



Note $\frac{1}{4}$ " slot openings for special pedestrian requirements. Two pieces per set with expandable tree opening. Available with cast iron angle frame, if required. Also available with matching extension pieces, making length 108" (See R-8812.) Order as R-8708-2.

Weight per set – 290 pounds.



Iron Tree Grate # R-8708

by Neenah Foundry Co.

Local Rep: Phoenix AZ 85034

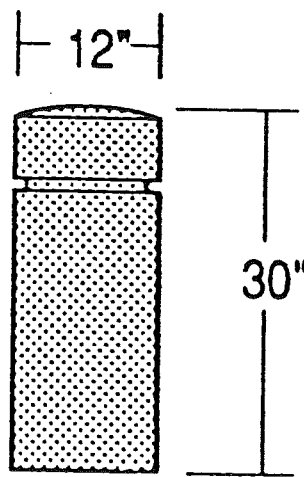
602-2259801

Cost: \$400.(Grate & frame)

***This is estimate of 100 qty.**

A larger price break may be available for larger orders.

TREE GRATE

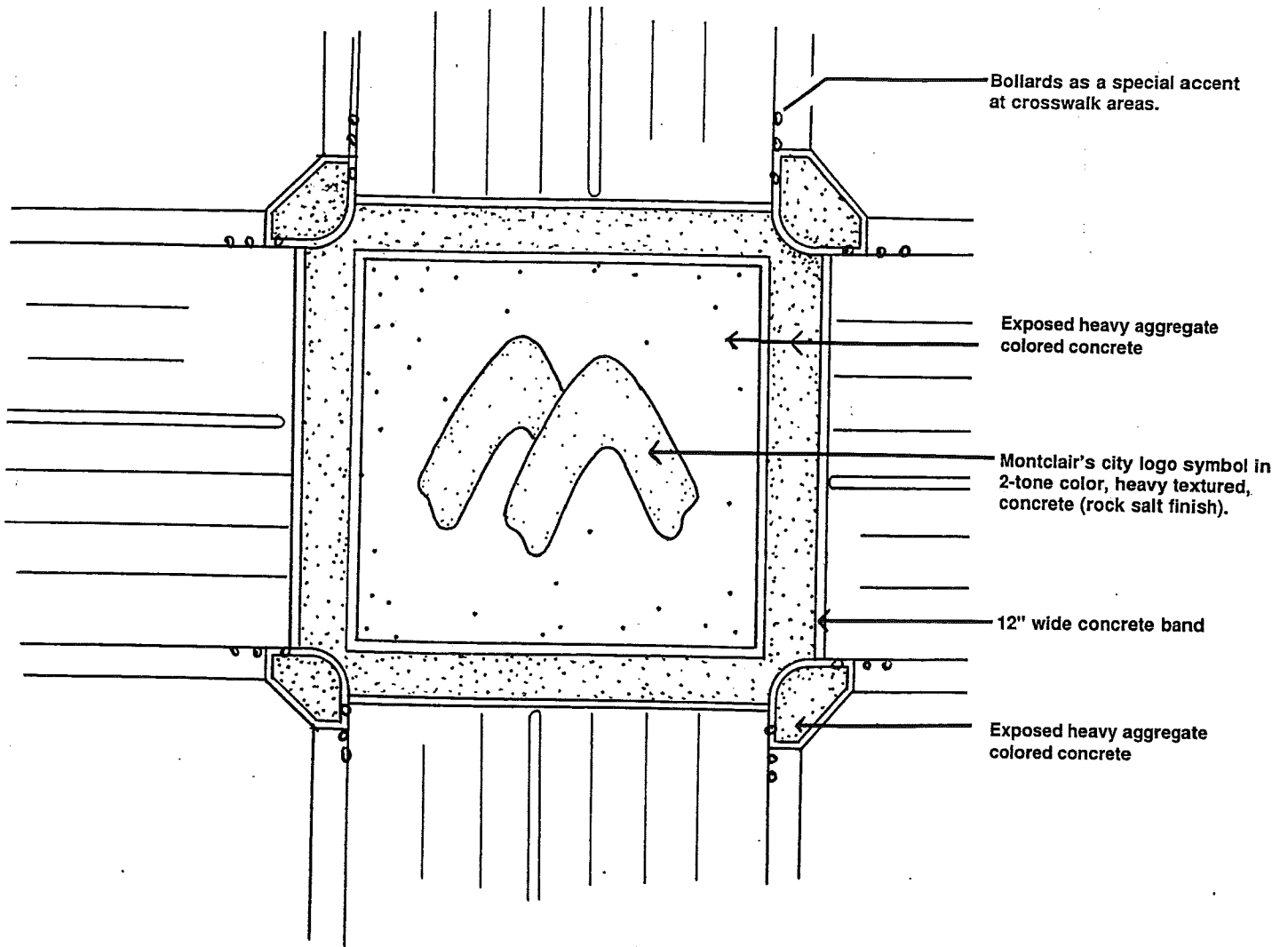


Concrete Bollard
Dura Art Stone PB-H4 round
12" dia. 30" high
Cost: \$179
Manufacturer: Dura Art Stone
Fontana, CA
(909) 350 9000

NOTE: All prices are approximate costs
and will vary with quantity orders.

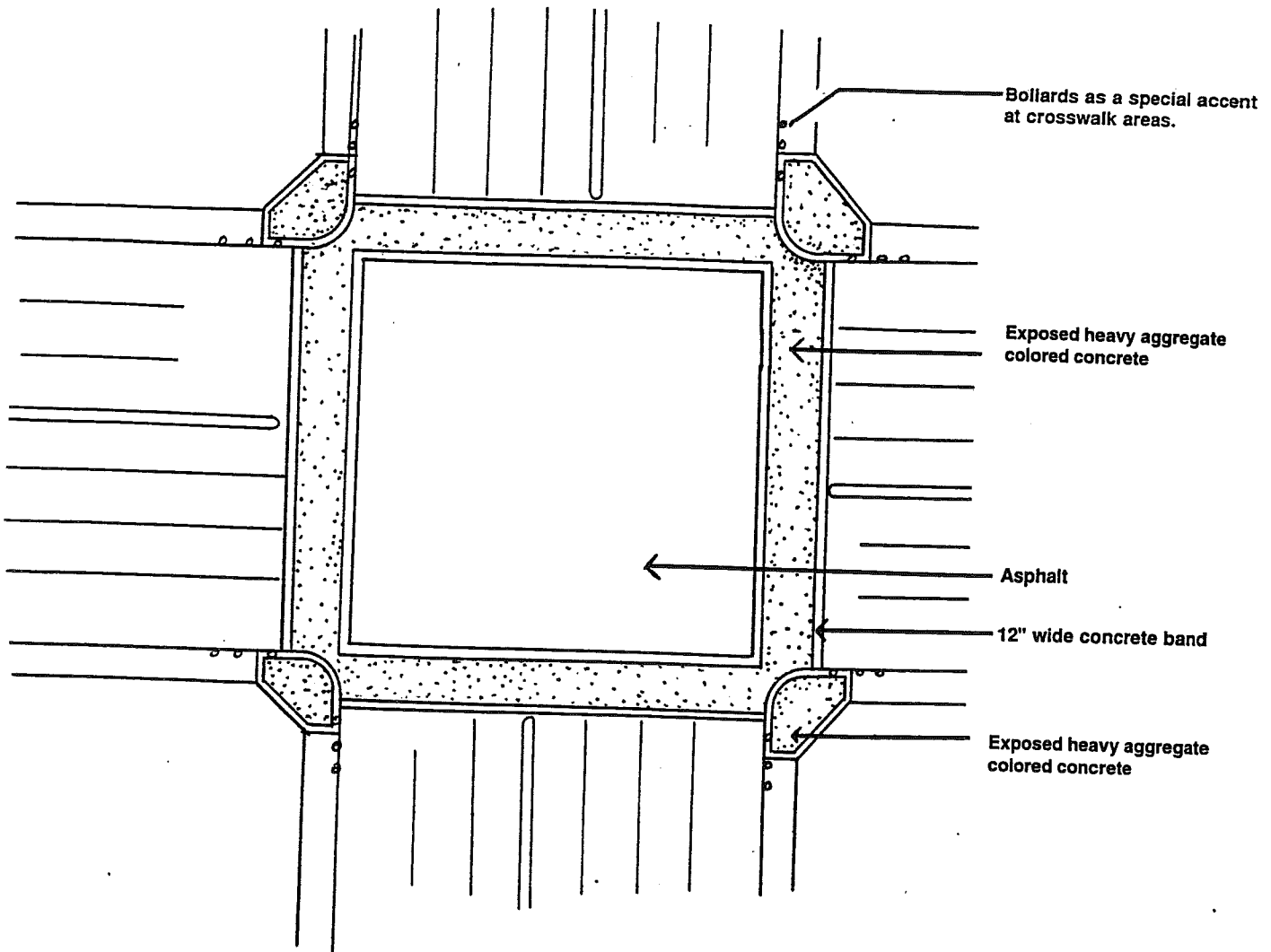
NORTH MONTCLAIR SPECIFIC PLAN

Major Focal Intersection



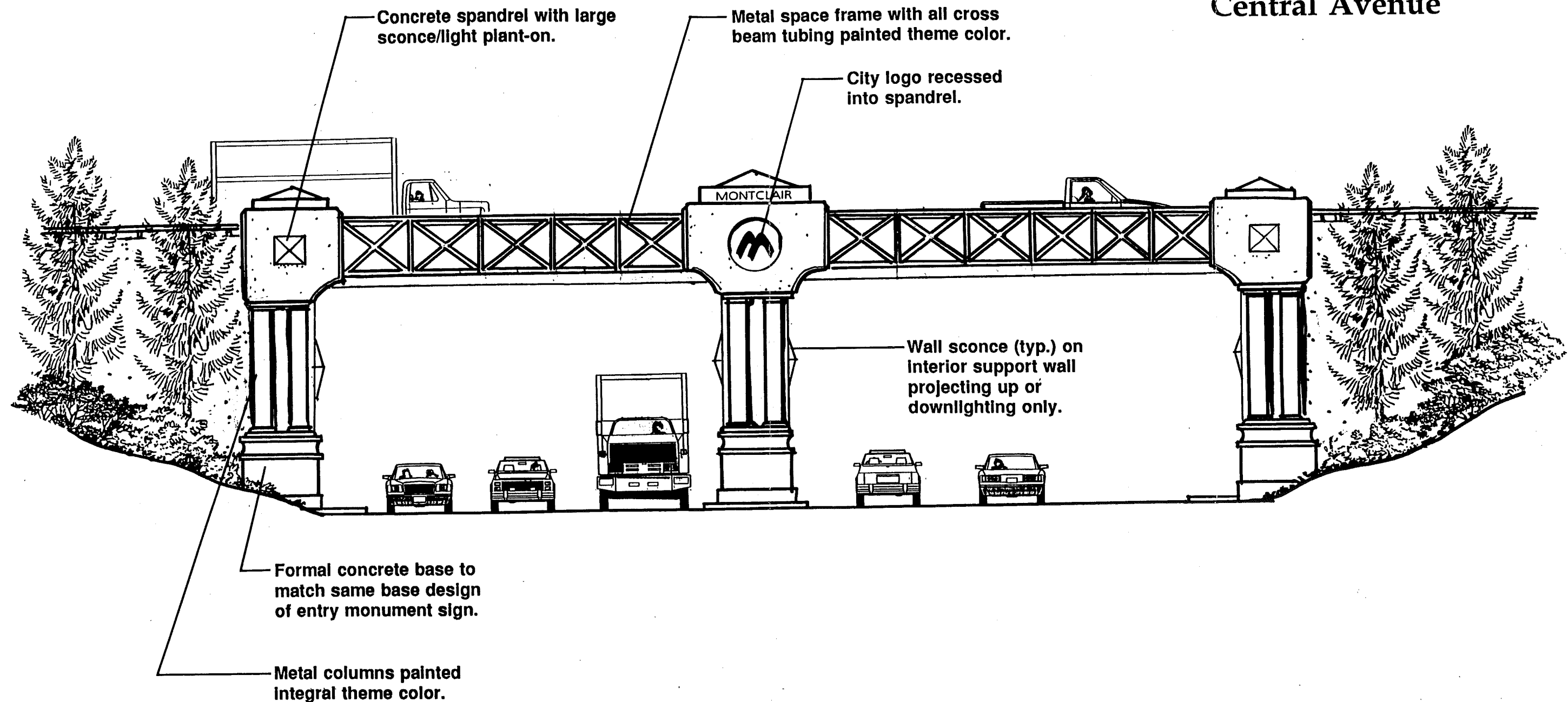
NORTH MONTCLAIR SPECIFIC PLAN

Secondary Intersection



**NORTH MONTCLAIR
SPECIFIC PLAN**

**Underpass Elevation Design
Central Avenue**



The design shown is a freestanding structure, in front of the bridge facade, which will appear as a part of the overcrossing itself. The same design could be used on any of the four overcrossings in Montclair.

This design is for illustrative purposes only, to be used to formulate alternative design concepts for the bridge/overcrossing of the San Bernardino Freeway at Central Avenue.

CHAPTER VI

IMPLEMENTATION AND ADMINISTRATION

Introduction

A. Purpose

This chapter provides recommendations for the administrative, financing and regulatory approaches that will effectively implement the North Montclair Specific Plan. In addition, this chapter also contains the required analysis to determine consistency between the Specific Plan and the City of Montclair General Plan.

B. Chapter Organization

This chapter contains the following sections:

1. Regulatory Framework and Consistency

This Section provides the background of regulatory actions that have preceded the preparation of this Specific Plan (i.e. General

Plan, Zoning Ordinance, Redevelopment Plans, etc.) and provides an analysis for determining consistency between the Specific Plan and other policy and regulatory documents.

2. Specific Plan Administration

This section outlines the administrative procedures that are necessary to implement this Specific Plan.

3. Public Improvements

This section identifies the public improvements recommended to implement this Specific Plan and provides rough order of magnitude cost estimates for the various improvements.

4. Potential Funding Mechanisms

This section provides information on a variety of funding sources and mechanisms to help finance the recommended public improvements.

5. Seven Year Capital Improvement Program

This section provides a year-by-year summary breakdown of capital expenditures over a seven year program.

6. Implementation Responsibilities Matrix

The chart in this section identifies the various City department that will be responsible for carrying out actions that will implement this Specific Plan.

Consistency With Other Plans And Ordinances

A. General Plan Consistency

California State law requires a specific plan to be consistent with the General Plan of the adopting locality. To ensure consistency with the City of Montclair General Plan, a review of the existing General Plan for all policies that are relevant to the North Montclair Specific Plan was made.

1. Land Use Policies

The Montclair General Plan Land Use chapter contains a number of both general and specific land use policies for commercial and industrial land uses that generally represent opportunities for the development of the North Montclair Specific Plan area. The applicable general land use policy states "...the preparation of specific plans for super block areas (specifically mentioned for Central and

Moreno) to promote the efficient utilization and consolidation of land," which indicates that, overall, the most appropriate method to approach the North Montclair area is the preparation of a specific plan.

There are many applicable land use policies germane to the North Montclair Specific Plan. Two policies that are particularly important state "recognize the importance of retaining the economic viability of Montclair Plaza..." and "Consider the establishment of new complementary uses around the Plaza perimeter..." These policies indicate the importance and value of the plaza and the surrounding area to Montclair as a whole.

Other applicable land use policies include:

- Promote the unitization and consolidation of smaller parcels, both commercial and residential uses, into larger, more usable properties.
- Consolidate access and parking areas.
- Promote the development of commercial centers rather than strip commercial.
- Provide adequate land in proper locations for the various types of commercial activities in order to realize optimum benefits for the residents of the community.
- Provide, through public and private investments, for the development of commercial properties designed to complement existing developments.
- Encourage the design of these properties to create an enjoyable environment for shopping by promoting improved architectural appearance of buildings, excellent landscaping and appropriately regulated signing, parking and traffic circulation.
- Promote the development of commercial centers which have distinctive and unique character or appearance relating to Montclair's community design objective.
- Prevent intrusion of incompatible uses in commercial areas.

Several policies related to industrial land uses are applicable in a more general way to the North Montclair Specific Plan in that they encourage industrial development, particularly the industrial park concept, and ensure that they present a positive visual image.

2. Economic Development

In the Economic Development Element, the importance of the Montclair Plaza area is demonstrated by the establishment of seven policies specifically designed to strengthen and improve the economic base of this area. These policies include "promote a special land use and design study for the Plaza area..." which represents a specific opportunity for the development of the North Montclair Specific Plan.

3. Community Design

The Community Design Element is a statement by the City of its intention to create a special design character throughout the community. Many of the objectives and policies of the element have specific implications for the North Montclair Specific Plan area.

Applicable objectives and policies include:

- To develop parkway improvement programs to enhance scenic qualities.
- To encourage the design of road and street improvements that protect or enhance the scenic values along the city's roadsides.
- To continue to develop and reexamine policies and programs regulating public and private improvement as they relate to enhancing the community aesthetic image.
- To promote the maintenance of compatible land uses and mitigate existing land use conflicts through redevelopment and/or incorporating the design principles and concepts contained in this element.

- To promote community identify and community aesthetics as a means for creating a positive living and working environment as well as to maintain high economic stability.
- To develop procedures which will permit the City to require aesthetic treatment in all new development.
- Continue the establishment of an individual and distinctive identity by encouraging the highest quality design in architecture, landscape architecture, sign graphics and in the design of street furniture and fixtures.
- Devise development standards that will fully integrate the regional shopping center with commercial development on Central Avenue and the Civic Center. This coordination will obtain the maximum benefit from both private and public investments.
- Establish a complete program for developing and landscaping the center island from city limit to city limit on all major circulation arteries.
- Existing or indispensable conflicting land uses should be effectively screened from view from the roadway. Effective screening can be accomplished by proper use of plantings, grading or attractive fencing.
- Site planning, architectural and landscape architectural design should result in an attractive appearance and a harmonious relationship among the various elements of the development to blend with the image of the community.

4. Housing Element

The Montclair 1990 Housing Element Review and Update contains a description of the current housing conditions in Montclair, an assessment of the housing constraints in the City and the City's housing related goals, objectives and policies. Three areas are affected by land use changes where amendments from residential use to commercial use will be required. These changes are further described in the following section.

The establishment of land use regulations, site development standards, development criteria for key opportunity sites, provision of areawide design guidelines and creation of a streetscape enhancement program within the North Montclair Specific Plan all combine to effectively implement the applicable policies of the General Plan. Implementation of the Specific Plan will not require amendment of the General Plan text.

B. General Plan Map Amendments

The General Plan Land Use Map designates a variety of land use categories within the North Montclair Specific Plan area. In implementing the goals and policies of the Specific Plan, the following amendments to the General Plan Land Use Map will be required in order to maintain consistency with the Specific Plan Land Use Plan:

- Northwest corner of Arrow and Monte Vista; change from industrial park to general commercial (C3);
- Southwest corner of Moreno and Monte Vista; change from preferred land uses of multiple family residential and regional commercial to neighborhood commercial (C2);

- West side of Monte Vista, approximately 150 feet south of Moreno; change from preferred land uses of multiple family residential (7-14 du./ac.) and regional commercial to institutional (I);
- Southwest corner of Arrow and Fremont, approximately 600 feet of frontage on each; change from preferred land uses of office professional and general commercial (C3) to only general commercial with a mixed use overlay;
- Southeast corner of Arrow and Fremont, approximately 400 feet of frontage on Arrow and 600 feet on Fremont; change from preferred land uses of office professional and general commercial to only general commercial (C3) with a mixed use overlay;
- Northwest corner of Moreno and Fremont, approximately 600 feet of frontage on each; change from preferred land uses of multiple family residential (7-14 du. ac.) and regional commercial to general commercial (C3) with a mixed use overlay;
- Northeast corner of Olive and Vernon, approximately 200 feet of frontage on Vernon and 250 feet of frontage on Olive; change from general commercial (C3) to industrial park (MIP); and
- Northwest corner of Palo Verde and Cabrillo, approximately 600 feet of frontage on Cabrillo and 500 feet of frontage on Palo Verde; change from multiple family residential (7-14 du./ac) to general commercial (C3).

C. Zoning Ordinance And Map Amendments

Existing zoning districts within the Specific Plan boundary should be repealed and the zoning map amended to indicate a new designation of "North Montclair Specific Plan." All development criteria, incentive programs and design guidelines of this Specific Plan should apply. For development standards and regulations that are not amended by this Specific Plan (e.g. sign standards) the regulations of the Zoning Ordinance should apply.

D. Redevelopment Plan Consistency

Redevelopment Plan - Project Area No. III provides the direction to "redevelop, rehabilitate and revitalize" much of the area located north of the I-10 freeway to the city boundary at the Southern Pacific railroad, and between Monte Vista Avenue and Benson Avenue. The land uses proposed in this redevelopment plan are essentially those put forth in the Montclair General Plan, and thus no substantial changes from the current and proposed land uses are anticipated. Section (330) of the redevelopment plan outlines the development standards for the area, which states "All construction in the project Area shall comply with and meet or exceed all applicable state and local laws in effect ... including ... fire, building, electrical, zoning, plumbing, sign and subdivision codes of the City of Montclair." Thus, Redevelopment Plan No. III does not contain any constraints for the North Montclair Specific Plan. The Specific Plan is consistent with the Redevelopment Plan for Project Area No. III.

E. Consistency with Other Specific Plans

Two existing specific plans currently fall within the boundaries of the North Montclair Specific Plan. The following discussion is for general background information and demonstrates consistency with prior planning actions.

Specific Plan No. 73-4 covers approximately 3.8 acres (6 lots) at the southeast corner of Monte Vista Avenue and Moreno Street adjacent to Montclair Plaza. The regional commercial uses proposed in the Plan have not been implemented (a small multi-tenant center was developed on the corner lot), and the existing church and vacant residential parcels currently remain.

The Specific Plan indicates that the subject properties shall be "regional/ commercial in nature and compatible with the existing regional land use" (i.e., Montclair Plaza). In addition, development of one lot at a time (rather than the entire area all at once) is allowable if a development plan is prepared for the entire area and if all of the property owners in the area agree with the provisions of the plan. These concepts have been carried forward in the North Montclair Specific Plan and, therefore, Specific Plan No. 73-4 can be repealed.

Specific Plan 81-2 covers approximately 10 acres between Olive and Moreno streets and from 600 feet east of Monte Vista Avenue to Fremont Avenue. The uses proposed in the Plan have not been implemented and the existing uses (40 single family residences) currently remain on the site.

Specific Plan 81-2 indicates that the proposed uses for the site are commercial, office and medium density residential, which is also consistent with the "preferred" land use designation on the General Plan Land Use Map. The North Montclair Specific Plan incorporates opportunities for these same land uses by providing for a mixed use overlay district which will allow multiple family residential uses and office uses in combination with general commercial uses. Therefore, Specific Plan 81-2 can be repealed.

Specific Plan Implementation

A. Introduction

This section describes the procedures required for the timely implementation of development within the Specific Plan area.

B. Specific Plan Implementation

The North Montclair Specific Plan establishes City policies with regard to future land use and development decisions within the boundaries of the Plan area. The adoption of this Specific Plan should set into motion a series of General Plan, Zoning Ordinance, and Zoning Map amendments that are necessary to implement the recommendations of the Plan.

It is the policy of the City to continually work toward the effective implementation of the North Montclair Specific Plan. Therefore, decisions with regard to discretionary permit applications should be guided, to the greatest extent feasible, by the recommendations of this Specific Plan, including the recommended land

use plan, development criteria, design guidelines, and urban design concepts.

C. Specific Plan Amendments

Amendments to this Specific Plan shall be processed and acted upon as follows:

1. Amendments to land use designations and development criteria require both Planning Commission and City Council approval by resolution following a public hearing; and
2. Amendments to focus area plans, design guidelines and streetscape designs require only Planning Commission approval by resolution following a public hearing.

D. Implementation Responsibilities

City of Montclair departmental responsibilities for the implementation of the North Montclair Specific Plan are indicated in the chart on the following page.

**TABLE 6-1
IMPLEMENTATION RESPONSIBILITY MATRIX**

Topic	City Department						
	CD-P	CD-B	PW-E	PW-S	AS	PD	FD
1. Public Improvements							
Streetscape Concept	X		X				
Signage			X	X			
Vehicular			X				
2. Development Fees/Financing	X	X	X		X		X
3. Development Regulations							
Building Design	X	X				X	
Development Standards	X		X				X
On-Site Landscaping	X					X	
Parking/Circulation	X		X				X
Permitted Uses	X		X				
Site Design Criteria	X		X				X
4. Development Review Process	X	X	X			X	X

KEY:

CD-P Community Development Department - Planning Division
 CD-B Community Development Department - Building Division
 PW-E Public Works Department - Engineering Division
 PW-S Public Works Department - Services Division
 AS Administrative Services Department
 PD Police Department
 FD Fire Department

Public Improvements

A. Introduction

In providing public improvements in the North Montclair Specific Plan area, the City will be making a visible economic commitment to realizing the goals and objectives of the Plan. These improvements will add value to the area, thereby laying a foundation for future private sector investment in the form of building renovations and new development. With development standards and guidelines in place, new private sector investment will combine with public improvements to fulfill the City's vision for the future of the North Montclair area.

B. Improvement Projects

The following Table 6-2, Improvement Projects summarizes the initial estimate of probable costs for those improvement projects recommended within the North Montclair Specific Plan area. Preliminary estimates are rough order of magnitude for budgeting purposes only. More detailed cost estimates should be prepared prior to requesting bids for a project.

**TABLE 6-2
IMPROVEMENT PROJECTS**

Improvement	Description	Budget Estimate
1. Circulation		
a. Extension of Richton Street	Extend Richton to the west to align with First.	
b. Palo Verde Street Realignment	Realign Palo Verde west of Central.	
c. Olive Street Extension	Complete Olive between Vernon and Del Mar.	
d. Freeway Underpasses	Widen underpasses at Central and Monte Vista.	
e. Freeway Underpasses	Enhanced landscaping, lighting and painting.	
2. Streetscape		
a. Benson Avenue	Paint all hardware.	7,500
b. Central Avenue	Add median links; paint all hardware; replace some street lights; add median banner poles/uplights.	275,000
c. Monte Vista Avenue	Add median links; paint all hardware; replace some street lights; add median banner poles/uplights.	275,000
d. Palo Verde Street	Paint all hardware.	7,500
e. Moreno Street	Paint all hardware; replace some street lights; add median banner poles/uplights.	75,000
g. Arrow Highway	Add median links; paint all hardware.	180,000
3. Intersection Enhancement		
a. Central and Moreno	Improve to "major" intersection status.	150,000
b. Central and Arrow	Improve to "minor" intersection status.	25,000
c. Fremont and Arrow	Improve to "minor" intersection status.	25,000
d. Fremont and Moreno	Improve to "minor" intersection status.	25,000

**TABLE 6-2
IMPROVEMENT PROJECTS (CONTINUED)**

Improvement	Description	Budget Estimate
e. Monte Vista and Moreno	Improve to "minor" intersection status.	25,000
f. Monte Vista and Arrow	Improve to "minor" intersection status.	25,000
g. Freeway Ramp and Central	Improve to "major" intersection status.	150,000
4. Fremont Street Improvements	Add street lights, trees, curb/gutter, sidewalks, furniture; paint hardware.	Allow 200,000
5. Sign Program		
a. Street Signs	Provide new street name signs.	70,000
b. City Entry Signs	Provide entry signs at Central Avenue, Monte Vista Avenue and Arrow Highway.	Allow 50,000
6. Pedestrian Crossing At Transcenter	Provide an at-grade pedestrian crossing from Transcenter to Montclair Plaza.	
7. Underground Utility Lines	Place existing aboveground utilities underground at Central Avenue, Monte Vista Avenue and Arrow Highway.	100 to 200 / ft.

Potential Funding Mechanisms

In order for the Specific Plan area to develop consistent with the goals and objectives of this plan, the identified public improvements will need to be funded. This section summarizes a variety of potential methods for funding improvement projects listed in the previous section. This listing is not intended to exclude any other available funding source nor does it require the use of any source listed.

A. Conditions Of Approval For Individual Projects

Under this method of financing individual developers are required to construct infrastructure and other facilities as part of their project approval when such improvements are directly related to their project.

Construction of facilities by individual developers is the easiest financing method to implement and should be employed whenever possible. This method allows developers to move ahead with their projects subject to their own timing without the potential delays which might accrue in the creation of more elaborate financing

schemes. However, issues of equity preclude developer construction for features of area wide or general benefit unless reimbursement is involved. In requiring construction of facilities by developers, even if there is to be reimbursement, care must be taken to avoid overloading front end development costs to the point that on-site project amenities be sacrificed in favor of up-front costs for off-site facilities/improvements.

B. Special Districts And Fees

Special fee districts are frequently established by local agencies to address specific infrastructure needs such as sewer, water or drainage. In most instances fees are collected upon the development of land within the fee district, based on a master plan for the specific facility. Fees are usually collected on per acre or per unit basis.

Impact fees are monetary exactions (other than taxes or special assessments) that are charged by local agencies in conjunction with approval of a development project. Impact fees are levied for the purpose of

defraying all or a portion of the costs of any public improvements or amenities which benefit the project. The collection of impact fees does not require formation of a special district.

Impact fees are paid by builders or developers, typically at the time a building permit is issued. The public facilities funded by impact fees must be specifically identified. There must be a reasonable relationship, pursuant to Assembly Bill 1600, between the type of development project, the need for the facilities, the cost of the facilities and the need to impose a fee.

While developer fees cannot typically be leveraged (i.e. provide security for bonds or other debt instruments), fees can be used in conjunction with debt financing to help retire bonds secured by other means (e.g. land). In this case, developer fees can generate supplemental revenues to reduce future special taxes or assessments, or free up tax increment or other revenues for alternative uses. Developer fees can also be used to generate reimbursement revenues to property owners or public agencies who have previously paid more than their fair share of public improvement costs.

C. Redevelopment

Portions of the North Montclair Specific Plan area are presently contained within two redevelopment project areas. The existence of a Redevelopment Agency and project area creates a strong centralized authority with broad powers relative to development administration and financing.

State Community Redevelopment Law allows communities to utilize tax increment financing to carry out redevelopment activities, by applying tax increments obtained in the project area to finance planning, administrative, acquisition and

improvement activities. Redevelopment Law permits the Agency to finance land acquisition for public purposes, construction of public facilities, such as roads, parks and sewers, and administrative, legal, planning and engineering costs related to the project.

The Redevelopment Agency can issue bonds to finance project area improvements and administrative cost, and can apply the tax increments derived in the project area to pay the debt service on the bonds. However, this financing method should be used cautiously. As long as the Redevelopment Agency has incurred debt affecting the project area, tax increments must be used to retire that debt. While increments can and are passed through to outside agencies supplying essential services to the project area, funds cannot be passed through to the City itself. The use of tax increments should, therefore, be viewed of as a municipal investment to be used on projects that will repay themselves through increased revenue to the City.

D. Special Assessment Districts (1911, 1913, 1915 Act)

California law provides procedures to levy assessments against benefitting properties and issue tax exempt bonds to finance public facilities and infrastructure improvements. Assessment districts, also known as improvement districts, are initiated by the legislative body (e.g. city), subject to majority protest of property owners or registered voters. Assessments are distributed in proportion to the benefits received by each property, and represent a lien against property. The assessments are fixed dollar amounts and may be prepaid. Only improvements with property-specific benefits (e.g. roads, sewer and water improvements) may be financed with assessments.

E. Area Of Benefit Fees

Area of benefit fees may be enacted by the legislative body (i.e. city) through adoption of an ordinance, without voter approval. The fee must be directly related to benefit received. It does not create a lien against property, but must be paid in full as a condition of approval. Its principle use is for encumbering properties that will not voluntarily enter into an assessment of CFD, so that they pay their fair share at the time they are ready to be developed. Proceeds may be used to reimburse property owners who pay up-front cost for facilities benefitting other properties. Benefitting properties may be given the option to finance the fees by entering into an assessment district (1913/1911 Act) or Mello-Roos (CFD).

F. Infrastructure Financing Districts (SB 308)

Senate Bill (SB) 308 authorizes cities to form infrastructure financing districts, in accordance with a prescribed procedure, to finance public capital facilities (including, but not limited to, arterial streets, transit facilities, drainage and sewer facilities, child care facilities, libraries, and parks/ recreational facilities) utilizing a method of tax increment financing, but tax revenues of county offices of education, school districts or community college districts would be subject to diversion under this bill.

SB 308 permits a district to only finance the purchase of facilities for which construction has been completed and provides that these facilities need not be physically located within the boundaries of the district. The bill prohibits infrastructure financing districts from overlapping redevelopment project areas. The bill would require any district that constructs dwelling units to set aside not less than 20 percent of these units to increase and improve

the city's supply of low- and moderate-income housing available at an affordable housing cost, as defined, to persons of low and moderate income as defined. The bill would require approval by the district's landowners or voters, as specified, of district formation and of bonds to be issued by the district to finance public capital facilities that provide significant benefits to an area larger than the area of the district.

G. Mello-Roos Community Facilities Districts

The Mello-Roos Community Facilities Act of 1982 allows for the creation of special districts authorized to levy a special tax and issue tax exempt bonds to finance public facilities and services. A Community Facilities District (CFD) may be initiated by the legislative body or by property owner petition and must be approved by a 2/3 majority of either property owners or registered voters (if there are more than 12 registered voters living in the area).

H. Landscaping And Lighting Districts

Landscaping and Lighting Districts (LLD) may be used for installation, maintenance and servicing of landscaping and lighting, through annual assessments on benefitting properties. LLD's also may provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities. They also may be used to fund and maintain parks above normal park standards maintained from general fund revenues.

I. Community Development Block Grant (CDBG) Funds

The federal government provides limited funding for local community development programs under the CDBG program. Funds have historically been made available for housing and business improvement and revitalization, job training and economic development. The emphasis for targeting of CDBG funds is generally towards benefitting groups and individuals of low and moderate income.

Subject to the availability of funds, CDBG could be utilized to develop minor public improvements (i.e. curbs, gutters, sidewalks) to rehabilitate housing, and to make cosmetic improvements to business facades in low-income areas. Like several of the other programs described, CDBG funds likely will not provide a significant basis for implementing the project's objectives, but can combine with other measures as part of the overall program.

J. Southern California Edison Undergrounding Funds

Utility companies are required to budget funds each year for undergrounding. These budgets are approved by the Public Utilities Commission and assigned to specific projects in each area based on priorities developed by local government.

Capital Improvement Program

A. Introduction

This section provides an annual capital improvement program with the intent of prioritizing projects into a logical sequence for implementation over a 7 year period.

B. Seven Year Capital Improvement Program

Year Two

PROJECTS

COSTS

- | | |
|-----------------------------------|-------------|
| 1. Monte Vista Streetscape | 275,000 |
| 2. Central at Arrow Intersection | 25,000 |
| 3. City Entry Signs | 50,000 |
| 4. Utility Undergrounding-Central | 100,000 |
| 5. Palo Verde Realignment | Unavailable |

YEAR TWO TOTAL

450,000

Year One

PROJECTS

COSTS

- | | |
|-----------------------------------|---------|
| 1. Central Avenue Streetscape | 275,000 |
| 2. Central at Moreno Intersection | 150,000 |
| 3. Street Signs | 70,000 |
| 4. Utility Undergrounding-Central | 100,000 |

YEAR ONE TOTAL

595,000

Year Three

PROJECTS

COSTS

- | | |
|---------------------------------------|---------|
| 1. Arrow Hwy & Benson Ave Streetscape | 200,000 |
| 2. Fremont Arrow/Monet Vista at Arrow | 50,000 |
| 3. Fremont at Moreno | 25,000 |
| 4. Utility Undergrounding-Monte Vista | 100,000 |

YEAR THREE TOTAL

415,000

Year FourPROJECTS

1. Extension of Richton Street
2. Exemption of Olive Street

Unavailable
Unavailable

YEAR FOUR TOTAL

Unavailable

Year SixPROJECTS

- 1.
2. Freeway Rampat Central Int. Enh. 150,000
3. Utility Undergrounding 100,000
4. Freeway Underpass Enhancement 70,000
- 5.

YEAR SIX TOTAL320,000Year FivePROJECTS

1. Fremont Street Improvements 200,000
2. Utility Undergrounding 100,000
3. Monte Vista at Moreno Intersection 25,000
4. Pedestrian Crossing at Transcenter Unavailable
- 5.

YEAR FIVE TOTAL325,000Year SevenPROJECTS

1. Moreno Street Streetscape 75,000
2. Palo Verde Street Streetscape 7,500
3. City Property Improvements 15,000
- 4.
5. Utility Undergrounding 100,000

YEAR SEVEN TOTAL197,500