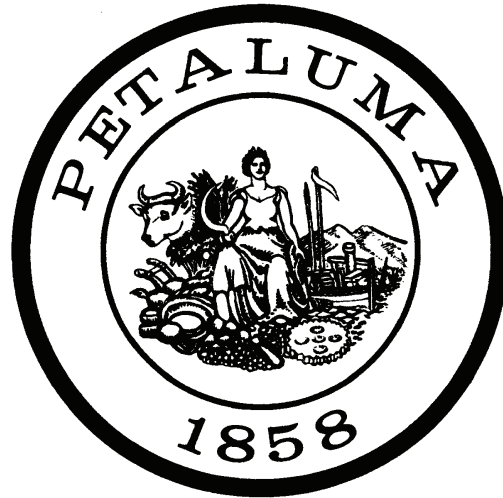


CONTACT INFORMATION

NAME	TITLE	PHONE	EMAIL
Alex Radovanovich	Engineer / Project Manager	707-284-8671	alex.radovanovich@meadhunt.com
Dan Cohen	Airport Manager	707-778-4404	dcohen@cityofpetaluma.org
Jonathan Sanglerat	Associate Civil Engineer (PW)	707-292-2772	jsanglerat@cityofpetaluma.org
Ken Eichstaedt	Senior Engineer (PW)	707-776-3672	keichstaedt@cityofpetaluma.org

NOTE: UPDATED CONTACT INFO WILL BE PROVIDED PRIOR TO CONSTRUCTION



MAYOR
Teresa Barrett

VICE MAYOR
Brian Barnacle

COUNCIL MEMBERS
D'Lynda Fischer
Dave King
Kevin McDonnell
Mike Healy
Dennis Pocekay

CITY MANAGER
Peggy Flynn

DIRECTOR OF PUBLIC
WORKS & UTILITIES
Christopher Bolt

	SIGNATURE	DATE
CITY ENGINEER		
ENGINEERING MANAGER		
FIRE MARSHAL		
PARKS		
PLANNING		
POLICE		
UTILITY MANAGER		

City of Petaluma, California

BASED AIRCRAFT APRON REHABILITATION

PETALUMA MUNICIPAL AIRPORT

AIP NO. 3-06-0186-029-2022

CITY PROJECT NO. C61502110

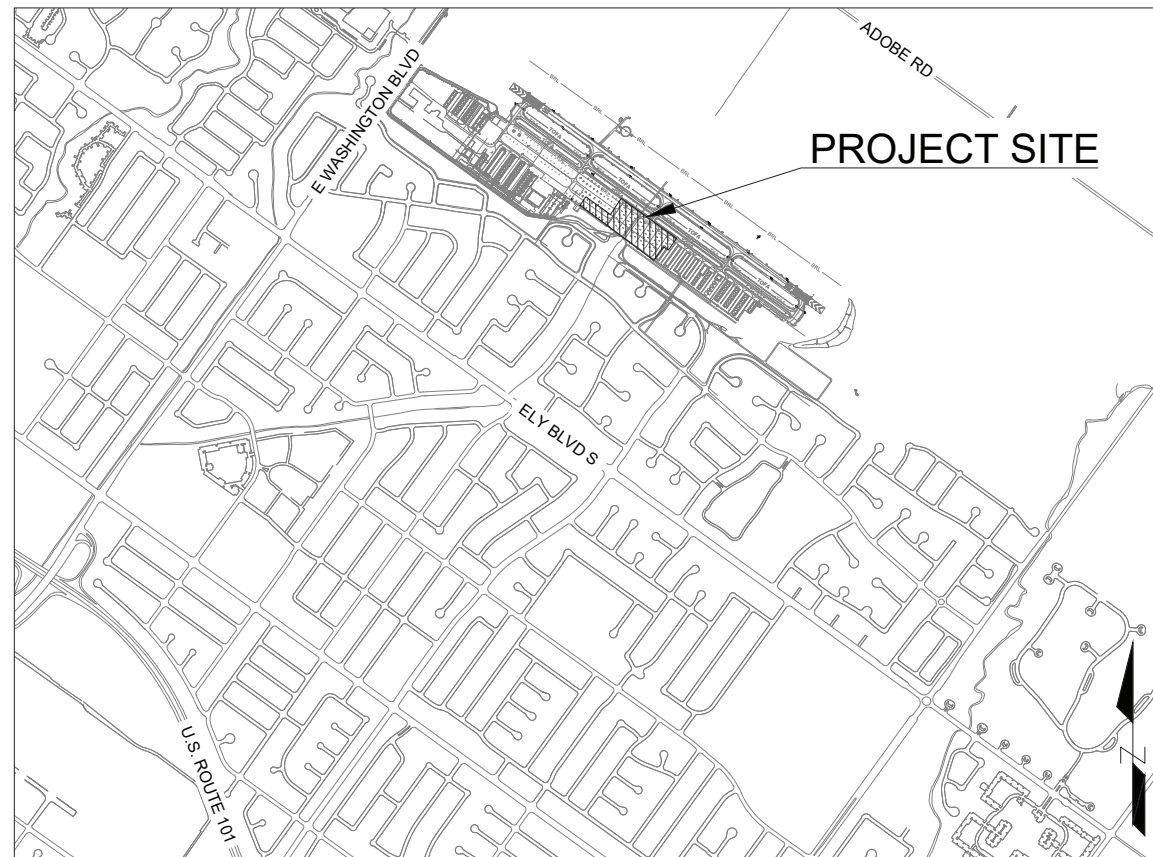
PROJECT DESCRIPTION:

Work Area A: Reconstruction

- Asphalt Concrete Pavement Removal
- Subgrade Excavation and Preparation
- Lime and Cement Treatment of Subgrade and Compaction
- Placement and Compaction of New Aggregate Base Material
- Paving of New Asphalt Surface Course
- Application of New Pavement Markings
- Tie-Down Installation
- Drainage Improvements, including New Valley Gutter, New Curb and Gutter, and Underdrain Replacement

Work Area B: Surface Treatment

- Surface Preparation
- Crack Repair and Isolated Pavement Repair (if needed)
- Double Application of Slurry Seal
- Application of New Pavement Marking
- Replacement of Tie-Down Chains



LOCATION MAP
SCALE: N.T.S.

RECORD PLAN

I _____ HEREBY STATE THAT THESE RECORD PLAN CHANGES ARE COMPLETE FROM INFORMATION FURNISHED BY THE PROJECT CONTRACTOR, SOILS ENGINEER AND MY OFFICE. I HEREBY STATE THAT TO THE BEST OF MY KNOWLEDGE THE WORK WAS DONE IN ACCORDANCE WITH THE FINAL APPROVED PLANS. THE ENGINEER AND THE CITY WILL NOT BE RESPONSIBLE FOR ANY ERRORS OR OMISSIONS WHICH HAVE BEEN INCORPORATED INTO THIS DOCUMENT AS A RESULT. FIELD VERIFICATION OF CRITICAL FACTS AND DATA SHOULD BE MADE IF THESE DOCUMENTS ARE TO BE USED AS A BASIS FOR FUTURE WORK. ENGINEER'S SIGNATURE _____ DATE: _____

SHEET INDEX

- 1 G-001 COVER SHEET
- 2 G-002 LEGEND & ABBREVIATIONS
- 3 G-021 PROJECT LAYOUT PLAN
- 4 G-081 CSPP
- 5 B-051 PLAN & LOG OF SOIL BORINGS
- 6 C-051 DEMOLITION PLAN
- 7 C-101 GRADING, PAVING & DRAINAGE PLAN
- 8 C-311 PAVEMENT DETAILS
- 9 C-651 MARKING PLAN - 1
- 10 C-652 MARKING PLAN - 2
- 11 C-901 CROSS SECTIONS

ALL PROJECT PLANS HAVE BEEN PREPARED AND REVIEWED TO COMPLY WITH CURRENT AMERICANS WITH DISABILITIES ACT (ADA) REQUIREMENTS AND/OR THE CALIFORNIA BUILDING STANDARDS CODE (CBCS). **N/A**

THESE PROJECT PLANS CONTAIN ELEMENT(S) THAT ARE NOT "TECHNICALLY FEASIBLE" AND/OR CAN'T MEET THE APPLICABLE CBCS BECAUSE IT WOULD CREATE AN "UNREASONABLE HARDSHIP." PLEASE SEE THE WRITTEN ANALYSIS SUPPORTING THIS DETERMINATION FILED UNDER THE PROJECT FILE.

DESIGNED BY Alex Radovanovich 1/28/22
SIGNATURE _____ DATE _____

APPROVED BY: Gina Benedetti-Petnic
GINA BENEDETTI-PETNIC P.E. C42778
ASSISTANT DIRECTOR - PUBLIC WORKS AND UTILITIES

DESIGNED BY: Alex Radovanovich
ALEX RADOVANOVICH P.E. C85082
CIVIL ENGINEER

BID SET

DATE: JANUARY 2022
DESIGNED BY: AR
DRAWN BY: ISB
CHECKED BY: SAS

PROJECT NO.
C61502110



SIGNED 1/28/22
CITY OF PETALUMA
PUBLIC WORKS & UTILITIES
202 N. McDowell Blvd., PETALUMA, CALIFORNIA, 94954
PH. 707-778-4546 FAX. 707-778-4508



BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT
COVER SHEET

SHEET
G-001

1 OF 11

RECORD DRAWING REFERENCE			
NO.	DATE	DESCRIPTION	BY

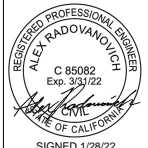
LEGEND:		
EXISTING	PROPOSED	
— ROFA —	N/A	RUNWAY OBJECT FREE AREA
— RSA —	N/A	RUNWAY SAFETY AREA
— TOFA —	N/A	TAXIWAY OBJECT FREE AREA
— TSA —	N/A	TAXIWAY SAFETY AREA
N/A	— — — — —	CONTRACTOR ACCESS ROUTE
N/A	·XXXXXXXXXXXXXX	LOW PROFILE BARRICADES
W O	N/A	RUNWAY EDGE LIGHT
G R	N/A	RUNWAY THRESHOLD LIGHT
R P	N/A	TAXIWAY ENTRANCE-EXIT LIGHTS
[]	N/A	RUNWAY HOLDING POSITION SIGN
[E]	N/A	ELECTRIC PULLBOX
[C]	N/A	JUNCTION CAN
— FNC —	N/A	FENCE
— E —	N/A	ELECTRICAL
— IRR —	N/A	IRRIGATION LINE
— G —	N/A	PG&E GAS LINE
— — — — —	N/A	PG&E EASEMENT BOUNDARY
— SD —	N/A	STORM DRAIN PIPE
— UD —	— UD —	UNDERDRAIN LINE
— T —	N/A	TELEPHONE LINE
— W —	N/A	WATER LINE
— SS —	N/A	SANITARY SEWER LINE
— FAA —	N/A	FAA LINE
+	N/A	BENCHMARK
N/A	[Solid Grey Box]	ASPHALT PAVEMENT RECONSTRUCTION
N/A	[Dotted Grey Box]	ASPHALT SURFACE TREATMENT
N/A	[Diagonal Lines Box]	ASPHALT PAVEMENT JOIN
N/A	[Cross-hatch Box]	ASPHALT PAVEMENT REMOVAL

ABBREVIATIONS					
AB	AGGREGATE BASE	FL	FLOW LINE	PERF	PERFORATED
AC	ASPHALT CONCRETE	FNC	FENCE	PG&E	PACIFIC GAS & ELECTRIC
ALT	ALTERNATE	FT	FEET	PSI	POUNDS PER SQUARE INCH
AOA	AIRCRAFT OPERATIONS AREA	G	GAS LINE	PSIG	POUNDS PER SQUARE INCH GAUGE
APCH	APPROACH	GALV	GALVANIZED	PSF	POUNDS PER SQUARE FOOT
APPROX	APPROXIMATE	GND	GROUND	PVC	POLYVINYL CHLORIDE
ATCT	AIR TRAFFIC CONTROL TOWER	H	HEIGHT	QTY	QUANTITY
AWG	AMERICAN WIRE GAUGE	HDPE	HIGH DENSITY POLYETHYLENE	R	RADIUS
AWOS	AUTOMATED WEATHER OBSERVING SYSTEM	HIRL	HIGH INTENSITY RUNWAY LIGHT	RC	RELATIVE COMPACTION
BLDG	BUILDING	HORIZ	HORIZONTAL	RCP	REINFORCED CONCRETE PIPE
BM	BENCHMARK	HWY	HIGHWAY	REIL	RUNWAY END IDENTIFIER LIGHT
CB	CATCH BASIN	IN	INCHES	REQ	REQUIRED
CIP	CAST-IN-PLACE	INV	INVERT ELEVATION	ROW	RIGHT OF WAY
CKT	CIRCUIT	IRR	IRRIGATION LINE	RSA	RUNWAY SAFETY AREA
CL	CENTERLINE	KV	KILOVOLT	RWY	RUNWAY
CLR	CLEAR	L	LENGTH	SS	SANITARY SEWER LINE
CMP	CORRUGATED METAL PIPE	LBS	POUNDS	SF	SQUARE FOOT
CO	CLEANOUT	LED	LIGHT-EMITTING DIODE	SD	STORM DRAIN PIPE
CON	CONDUIT	LF	LINEAL FEET	STA	STATION
CONC	CONCRETE	MAOP	MAXIMUM ALLOWABLE OPERATING PRESSURE	STD	STANDARD
CONT	CONTINUOUS	MAX	MAXIMUM	T	TELEPHONE LINE
CP	CONTROL POINT	MID	MID POINT	TC	TOP OF CURB
DB	DIRECT BURIAL	MIN	MINIMUM	TG	TOP OF GRATE
DEG	DEGREE	MIRL	MEDIUM INTENSITY RUNWAY LIGHT	T/L	TAXILANE
DI	DROP INLET	MITL	MEDIUM INTENSITY TAXIWAY LIGHT	TOP	TOP OF PIPE
DIA	DIAMETER	MPH	MILES PER HOUR	TWY	TAXIWAY
DIM	DIMENSION	(N)	NEW	TYP	TYPICAL
DIP	DUCTILE IRON PIPE	NO. OR #	NUMBER	UG	UNDERGROUND
(E)	EXISTING	NOTAM	NOTICE TO AIRMAN	UON	UNLESS OTHERWISE NOTED
E	ELECTRICAL LINE	NTS	NOT TO SCALE	VERT	VERTICAL
EG	EXISTING GRADE	OC	ON CENTER	VIF	VERIFY IN FIELD
ELEV	ELEVATION	OFF	OFFSET	W	WATER LINE
EOP	EDGE OF PAVEMENT	OFZ	OBSTACLE FREE ZONE	W/	WITH
FAA	FEDERAL AVIATION ADMINISTRATION	OH	OVERHEAD	W/O	WITHOUT
FBO	FIXED BASE OPERATOR	PAPI	PRECISION APPROACH PATH INDICATOR	WSP	WELDED STEEL PIPE
FG	FINISHED GRADE	PB	PULL BOX	WV	WATER VALVE
FH	FIRE HYDRANT	PCC	PORTLAND CEMENT CONCRETE	WWM	WELDED WIRE MESH

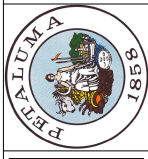
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DATE: JANUARY 2022
DESIGNED BY: AR
DRAWN BY: ISB
CHECKED BY: SAS

PROJECT NO.
C61502110

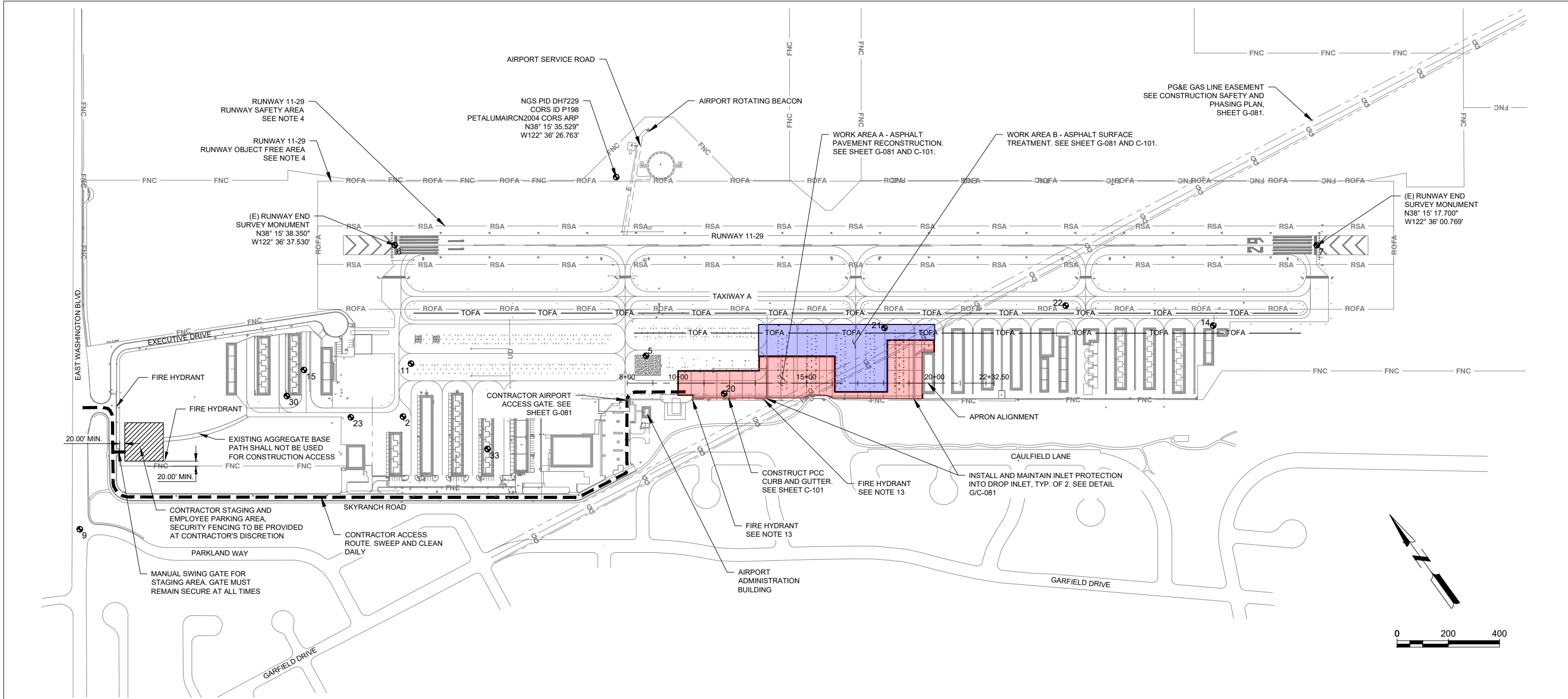


SIGNED 1/28/22
ALEX RADOVANOVICH
REGISTERED PROFESSIONAL ENGINEER
STATE OF CALIFORNIA
CITY OF PETALUMA
PUBLIC WORKS & UTILITIES
202 N. McDowell Blvd., PETALUMA, CALIFORNIA, 94954
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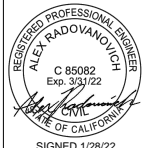
BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT
LEGEND & ABBREVIATIONS

SHEET
G-002



DATE: JANUARY 2022
 DESIGNED BY: AR
 DRAWN BY: ISB
 CHECKED BY: SAS

PROJECT NO.
 C61502110



CITY OF PETALUMA
 PUBLIC WORKS & UTILITIES
 202 N. McDowell Blvd., PETALUMA, CALIFORNIA, 94954
 PH. 707-778-4546 FAX. 707-778-4508



BASED AIRCRAFT APRON REHABILITATION
 PETALUMA MUNICIPAL AIRPORT
 PROJECT LAYOUT PLAN

SHEET
G-021
 3 OF 11

GENERAL CONSTRUCTION NOTES:

- ALL WORK SHALL COMPLY WITH THE FEDERAL AVIATION ADMINISTRATION ADVISORY CIRCULAR 150/5370-2, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND THE PROJECT SPECIFICATIONS.
- THE CONTRACTOR SHALL REMAIN WITHIN THE PROJECT LIMITS, ACCESS ROUTE, AND STAGING AREA SHOWN IN THE PLANS. ACCESS OUTSIDE THESE LIMITS SHALL NOT BE ALLOWED UNLESS APPROVED IN WRITING BY THE AIRPORT MANAGER. THE CONTRACTOR'S ACCESS ROUTES SHALL BE AS SHOWN ON THE PLANS UNLESS OTHERWISE APPROVED BY THE AIRPORT.
- HAUL ROUTES ON PAVEMENTS SHALL BE CLEANED DAILY BY VACUUM SWEEPER. HAUL ROUTES OVER GRAVEL/DIRT SHALL BE DISKED AND SMOOTH GRADED AT THE COMPLETION OF THE PROJECT AT THE CONTRACTORS EXPENSE. DUST CONTROL SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES..
- NO ACCESS TO THE RUNWAY OR ENCROACHMENT INTO THE RUNWAY OBJECT FREE AREA (ROFA) OR RUNWAY SAFETY AREA (RSA) SHALL BE PERMITTED. THE CONTRACTOR SHALL REMAIN INSIDE WORK AREA A OR B AT ALL TIMES. SEE THE PROJECT SPECIFICATIONS FOR SPECIFIC PHASING AND ACCESS REQUIREMENTS.
- THE CONTRACTOR SHALL PROVIDE AND PLACE LIGHTED BARRICADES, AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER, TO PREVENT AIRCRAFT TRAFFIC FROM ENTERING A CLOSED TAXILANE OR APRON WORK AREA DURING CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL CONSTRUCTION ACTIVITIES WITH THE AIRPORT AND SHALL SUBMIT A BASELINE SCHEDULE FOR REVIEW PRIOR TO ISSUANCE OF THE NOTICE TO PROCEED WITH CONSTRUCTION. THE CONTRACTOR SHALL ALSO MAINTAIN AN UP TO DATE PROGRESS SCHEDULE WITH A MINIMUM TWO WEEK LOOK AHEAD. THE CONTRACTOR WILL BE EXPECTED TO SUBMIT THE SCHEDULE AT THE WEEKLY CONSTRUCTION MEETINGS
- CONTRACTOR SHALL HYDROSEED ANY AREA DISTURBED BY CONTRACTOR OPERATIONS (STAGING AREAS, STOCKPILE AREA, ETC.), OUTSIDE PAVEMENT LIMITS.
- NO GUARANTEE IS EXPRESSED OR IMPLIED THAT ALL UNDERGROUND OBSTRUCTIONS ARE SHOWN ON THE PLANS. THOSE SHOWN ARE BASED ON THE BEST INFORMATION AVAILABLE AND THE CONTRACTOR IS CAUTIONED THAT THE ENGINEER AND THE CITY ASSUME NO RESPONSIBILITY FOR ANY OBSTRUCTION SHOWN OR NOT SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL NOT BEGIN EXCAVATION UNTIL ALL EXISTING UNDERGROUND FACILITIES WITHIN THE WORK AREA HAVE BEEN MARKED IN THE FIELD BY THE CONTRACTOR. THE CONTRACTOR SHALL POTHOLE AND VERIFY THE DEPTH OF ALL UTILITIES SHOWN INSIDE THE CONSTRUCTION ZONE BEFORE BEGINNING WORK. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE FOR "UNDERGROUND UTILITY INVESTIGATION AND POTHOLES" IN ACCORDANCE WITH SPECIFICATION ITEM SP-100.

- THE CONTRACTOR SHALL MAINTAIN AIRPORT SECURITY AT ALL TIMES AT ACCESS GATES UNDER THEIR CONTROL. THIS SHALL INCLUDE LOCKING ACCESS GATES OR PROVIDING PERSONNEL TO MONITOR THE GATES AND PREVENT UNAUTHORIZED ACCESS DURING CONSTRUCTION. ANY BREACH OF SECURITY SHALL BE REPORTED IMMEDIATELY TO THE AIRPORT STAFF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGES RESULTING FROM FAILURE TO MAINTAIN AIRPORT SECURITY AT LOCATIONS UNDER CONTRACTOR CONTROL.
- THE CONTRACTOR SHALL ENSURE ALL EMPLOYEES, INCLUDING SUBCONTRACTORS, WORKING ON THE SITE ARE AWARE OF AND FOLLOW THE REQUIRED SAFETY MEASURES AND PROJECT CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) WHEN ON THE AIRPORT.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL PREPARE AND IMPLEMENT A STORM WATER POLLUTION PREVENTION PLAN (SWPPP). THE SWPPP SHALL BE KEPT ON SITE AT ALL TIMES AND UPDATED REGULARLY. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE FOR "SWPPP PREPARATION, MANAGEMENT, AND MONITORING."
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND OBTAINING PERMITS AND METERS FOR CONSTRUCTION WATER.
- THE CONTRACTOR SHALL NOTE ALL APPROVED FIELD CHANGES AND OTHER OCCURENCES AND SUBMIT A FULL SIZE COMPLETE CONSTRUCTION "RECORD DRAWING" SET NOTED AND DATED ON THE DRAWINGS TO THE RESIDENT PROJECT REPRESENTATIVE (RPR) PRIOR TO ACCEPTANCE OF THE WORK.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO EHSURE ALL MATERIAL AND WORKMANSHIP FULLY CONFORMS TO THE SPECIFICATIONS, STANDARDS AND ORDINANCES OF THE CITY OF PETALUMA.
- ALL EROSION AND SEDIMENT CONTROL MATERIALS AND METHODS SHALL COMPLY WITH THE CALIFORNIA REGIONAL WATER QUALITY CONTROL BOARD, SAN FRANCISCO BAY REGION, EROSION AND SEDIMENT CONTROL MANUAL.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY EXISTING SURVEY CONTROL, AS WELL AS EXISTING GRADES AT PAVEMENT JOIN LOCATIONS. ANY DISCREPANCIES SHALL BE BROUGHT TO THE RPR IMMEDIATELY. IF THERE ARE DISCREPANCIES, THE ENGINEER MAY NEED TO RE-EVALUATE THE PROPOSED GRADES. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE FOR "EXISTING SURVEY VERIFICATION" IN ACCORDANCE WITH SPECIFICATION ITEM SP-100.

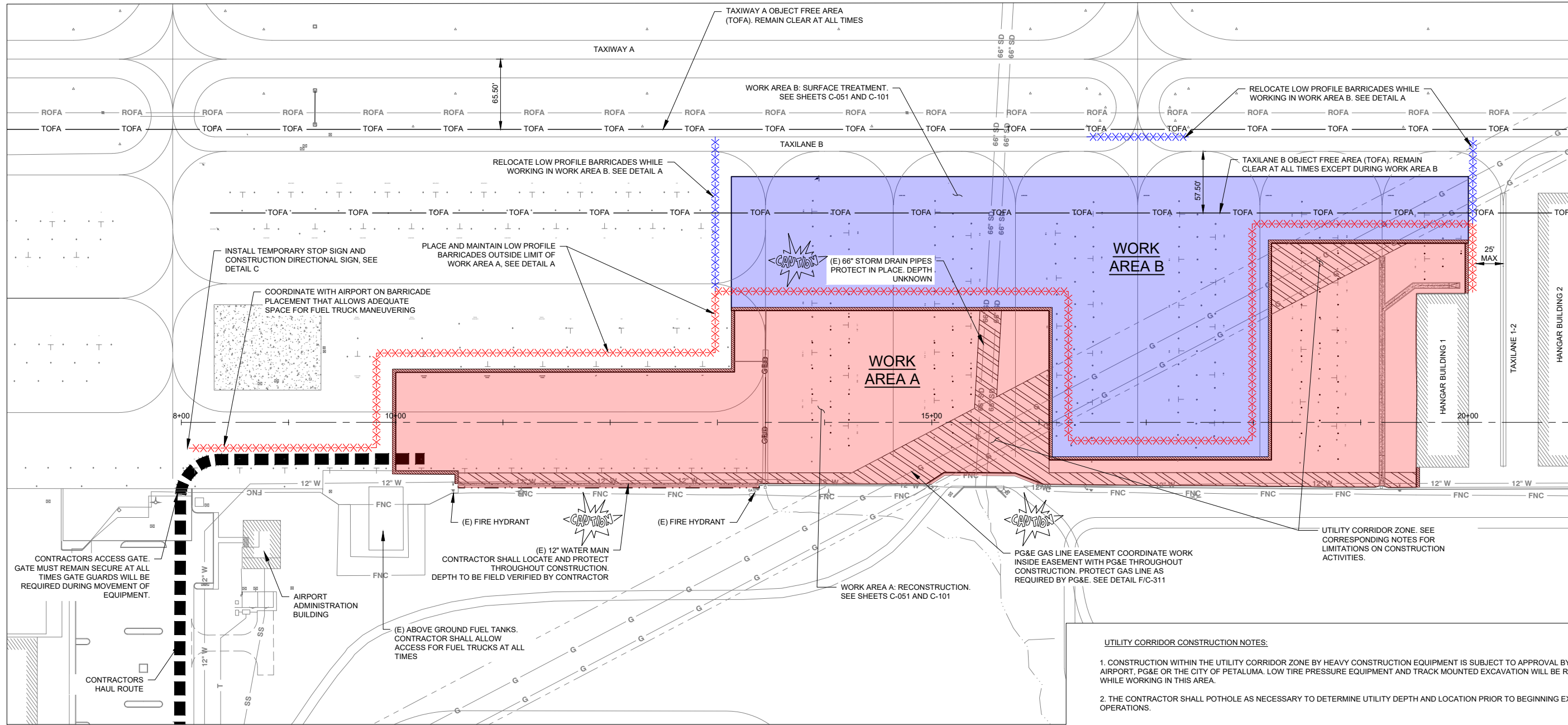
APRON ALIGNMENT TABLE

TANGENT DATA	PT STATION	NORTHING	EASTING	LENGTH	COURSE
START	8+00.000	19051.889	20435.311	1432.50	S 54° 08' 54.86" E
END	22+32.50	18212.895	21596.408		

SURVEY BENCH MARKS

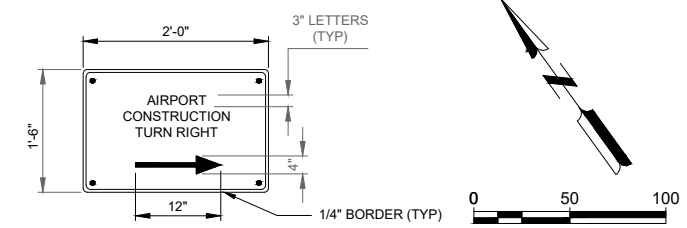
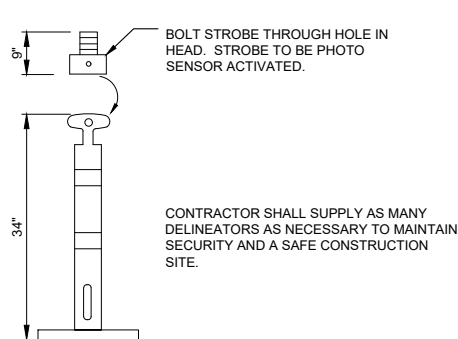
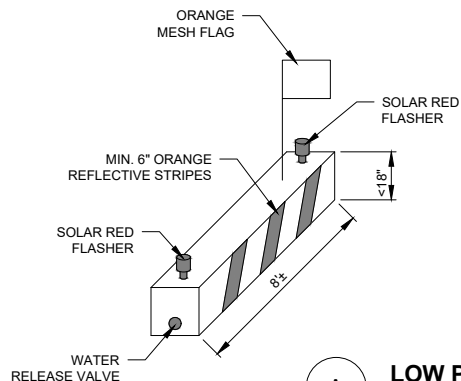
NO.	NORTHING	EASTING	ELEVATION	DESCRIPTION
2	19458.835	19649.111	78.45	CP PK
5	19096.014	20556.575	74.43	CP MON
7	17911.832	22932.917	74.70	CP PK
9	19841.154	18368.860	78.86	CP MON
11	19607.844	19795.604	80.68	CP PK
14	17894.553	22420.654	73.97	CP PK
15	19832.951	19442.212	81.92	CP PK
20	18797.278	20718.052	71.88	CP MAG
21	18638.604	21375.637	73.33	CP MAG
22	18294.908	21999.956	73.30	CP 100D
33	19162.998	19842.774	73.43	PK NAIL

BASIS OF SURVEY DATA IS NAD 83 HORIZONTAL AND NAVD 88 VERTICAL.



CONSTRUCTION BARRICADE NOTES

- THE CONTRACTOR SHALL PROVIDE 150 LOW PROFILE BARRICADES FOR USE DURING THE PROJECT. FOR EACH BARRICADE, THE CONTRACTOR SHALL PROVIDE 2 RED OMNI-DIRECTIONAL SOLAR POWERED CONSTRUCTION FLASHERS AND AN ORANGE MESH FLAG. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING AND MAINTAINING THE LOW PROFILE BARRICADES THROUGHOUT CONSTRUCTION. BARRICADES SHALL BE FILLED WITH WATER AS NECESSARY.
- MAXIMUM SPACING BETWEEN LOW PROFILE BARRICADES IS 4 FEET.
- CONTRACTOR SHALL COORDINATE WITH AIRPORT ON EXACT BARRICADE LOCATION. DURING WORK AREA B, BARRICADES BETWEEN WORK AREAS A AND B CAN BE RELOCATED OUTSIDE THE LIMIT OF WORK AREA B, AS SHOWN.
- CONTRACTOR SHALL COORDINATE WITH AIRPORT WHILE WORKING ADJACENT TO TAXILANE 1-2. CONTRACTOR SHALL CLEAR TAXILANE 1-2 PRIOR TO AIRCRAFT TAXING.

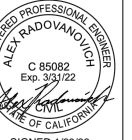


- NOTES**
- THE TEMPORARY STOP SIGN AND CONSTRUCTION DIRECTIONAL SIGN SHALL BE INSTALLED TO MEET THE REQUIREMENTS OF MUTCD AND FAA ADVISORY CIRCULAR 150/5220-23. CONTRACTOR SHALL SUBMIT PROPOSED INSTALLATION METHOD TO RPR FOR APPROVAL DURING THE MOBILIZATION ELEMENT.

X:\16195000\210165.01\TECH\CAD\DRAWINGS\SHEETS\G-081 CSPP.DWG
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DATE: JANUARY 2022
DESIGNED BY: AR
DRAWN BY: ISB
CHECKED BY: SAS

PROJECT NO.
C61502110

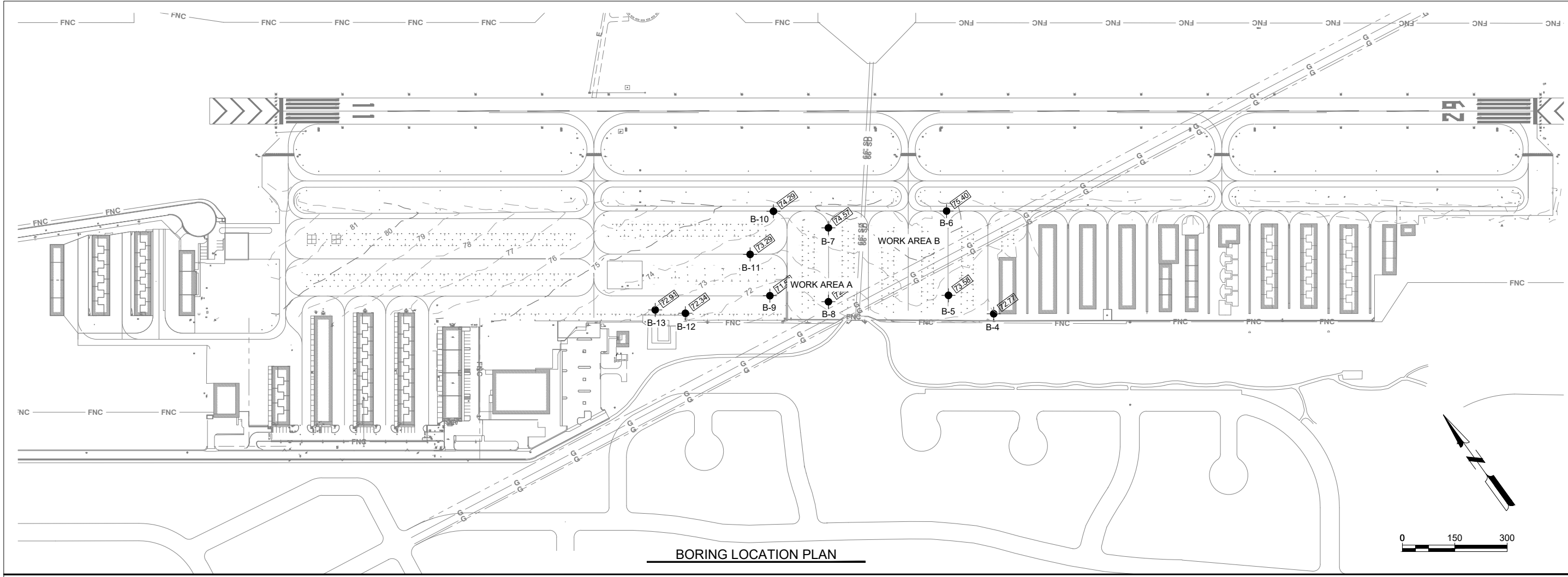


SIGNED 1/28/22
ALEX RADOVANOVICH
REGISTERED PROFESSIONAL ENGINEER
STATE OF CALIFORNIA
C 85082
Exp. 3/31/22
CITY OF PETALUMA
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202 N. McDowell Blvd., PETALUMA, CALIFORNIA, 94954
PH. 707-778-4546 FAX. 707-778-4508



**BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT**

SHEET
G-081



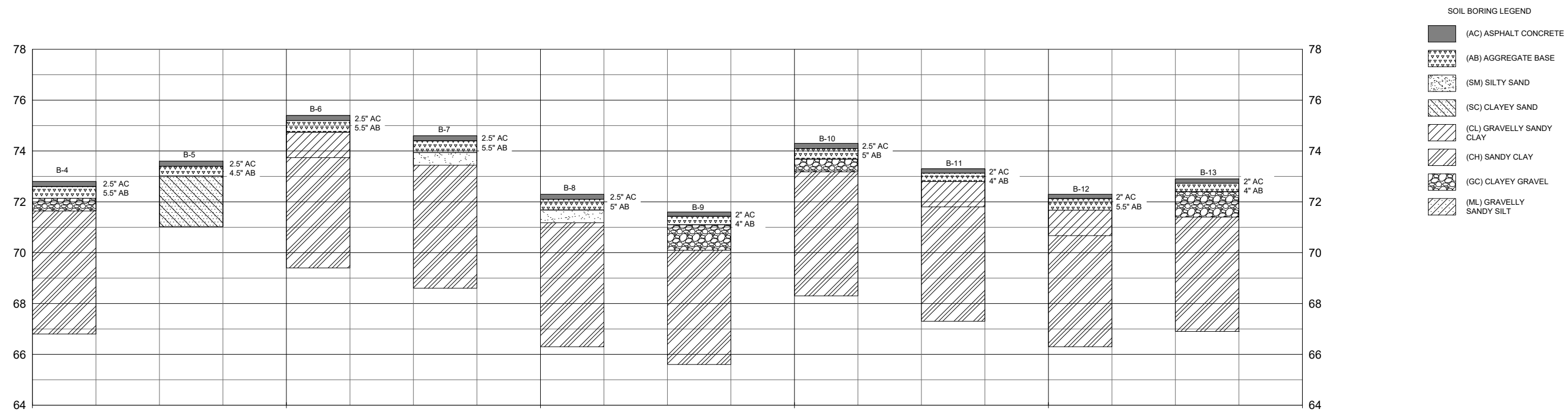
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PROJECT NO.
 C61502110

Mead & Hunt

REGISTERED PROFESSIONAL ENGINEER
 ALEX RADOVANOVICH
 C 85082
 Exp. 3/31/22
 STATE OF CALIFORNIA
 SIGNED 1/28/22

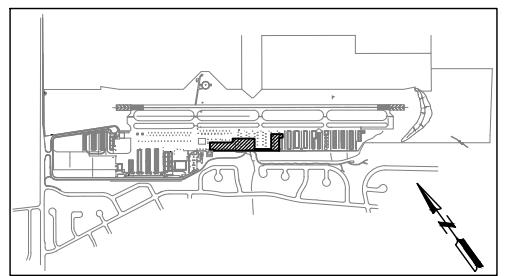
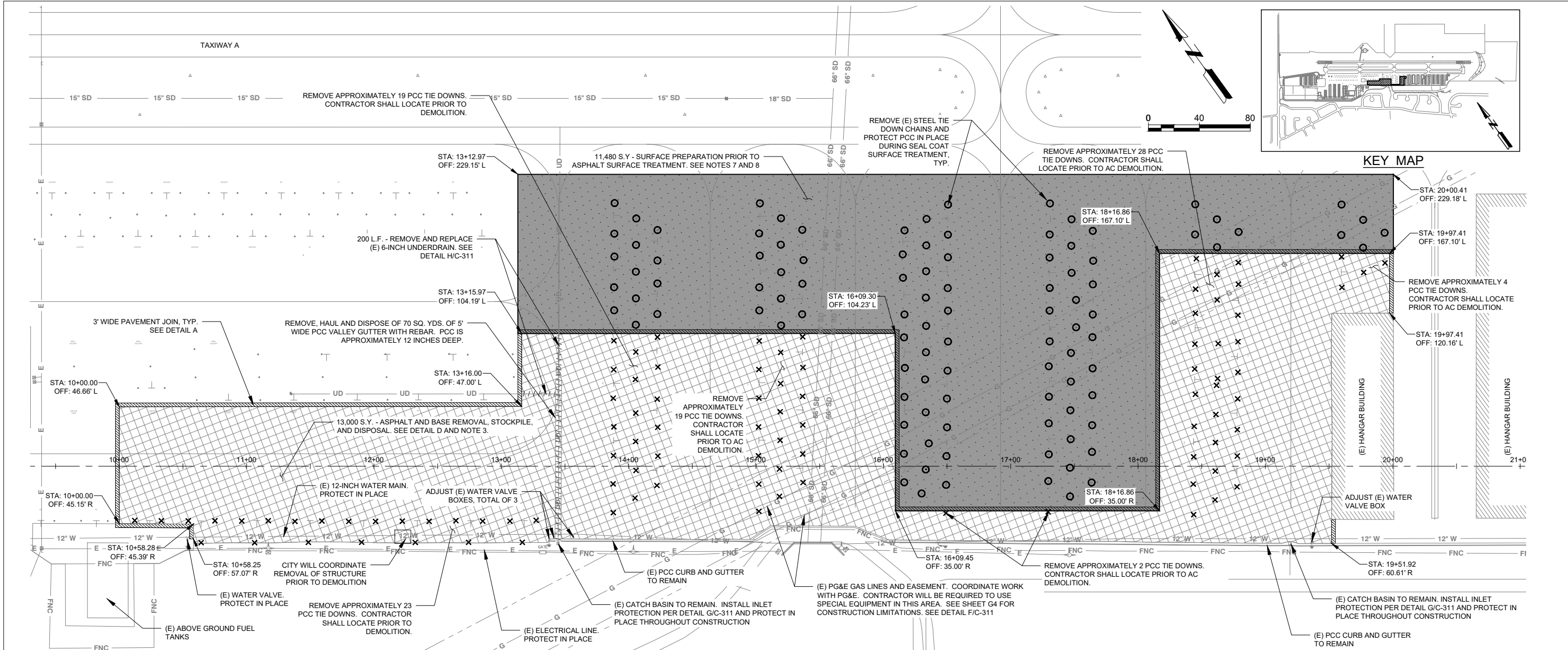
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X:\16195000\210165.01\TECH\CAD\DRAWINGS\SHEETS\B-051 PLAN & LOG OF SOIL BORINGS.DWG
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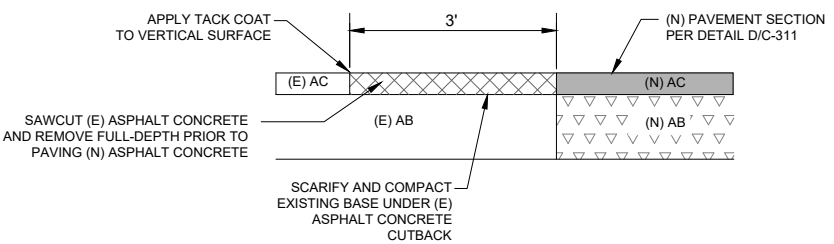
BASED AIRCRAFT APRON REHABILITATION
 PETALUMA MUNICIPAL AIRPORT
 PLAN & LOG OF SOIL BORINGS

SHEET
B-051
 5 OF 11

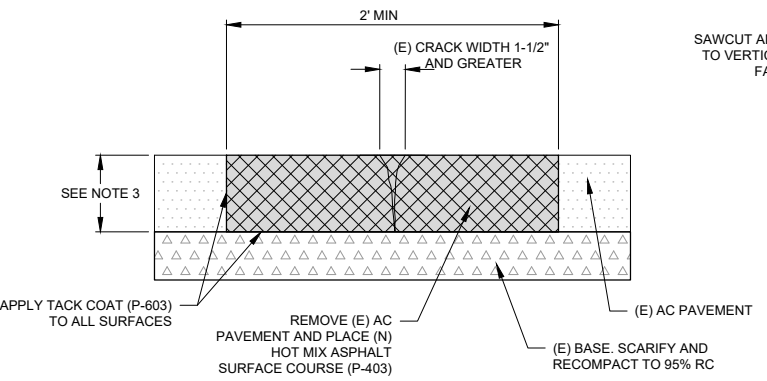


- DEMOLITION AND SURFACE PREPARATION NOTES:**
- LOCATE AND REMOVE (E) CONCRETE, STEEL TIE DOWN ANCHORS, AND CHAINS. LOCATIONS AND TYPE SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING THE (E) ANCHORS IN THE PROJECT WORK LIMITS. SOME CONCRETE ANCHORS MAY BE LOCATED UNDER ASPHALT CONCRETE.
 - THE CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES IN EACH PHASING ZONE PRIOR TO DEMOLITION AND PROTECT THROUGHOUT ENTIRE PROJECT.
 - CONTRACTOR HAS THE OPTION TO COLD MILL THE EXISTING ASPHALT AND BASE AND USE THE MILLINGS FOR RECYCLED BASE MEETING THE REQUIREMENTS OF SPECIFICATION ITEM SP-100. MILLINGS SHALL BE STOCKPILED AT THE STAGING AREA SHOWN ON SHEET G-021, FOR USE AS NECESSARY BY THE CONTRACTOR FOR STABILIZING UNEXPECTED SUBGRADE ISSUES. REMAINING SUBGRADE SHALL BE HAULED AND DISPOSED OF OFF-SITE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING A DISPOSAL SITE AND ALL MATERIAL TESTING REQUIRED FOR THE DISPOSAL SITE SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
 - UPON COMPLETION OF THE PROJECT, THE REMAINING MILLINGS SHALL BE HAULED OFF-SITE. THE CONTRACTOR STAGING/STOCKPILE AREA SHALL BE RETURNED TO ITS ORIGINAL CONDITION IMMEDIATELY AFTER USE.
 - STATIONS AND OFFSETS FOR PAVEMENT REMOVAL ARE SHOWN AT THE LIMITS OF FULL DEPTH RECONSTRUCTION (3-FT-WIDE PAVEMENT JOIN IS OUTSIDE THESE STATIONS AND OFFSETS).
 - SAWCUT AND REMOVE PAVEMENT JOIN AFTER PLACEMENT AND FINAL COMPACTION OF AGGREGATE BASE COURSE.
 - SURFACE PREPARATION SHALL BE PERFORMED ON ALL PAVEMENTS TO BE SEALED. SEE SPECIFICATION ITEM P-101 AND P-626 FOR SURFACE PREPARATION REQUIREMENTS.
 - CONTRACTOR SHALL PERFORM SITE INVESTIGATION WITH AIRPORT AND RPR TO IDENTIFY AREAS THAT MAY NEED CRACK REPAIR (OVER 1-1/2 INCH WIDE) OR ISOLATED PAVEMENT REPAIR. NO PAYMENT WILL BE MADE FOR THESE ITEMS UNLESS APPROVED IN ADVANCE BY THE CITY. IF PERFORMED, CRACK REPAIR AND PAVEMENT REPAIR SHALL BE COMPLETED PRIOR TO APPLICATION OF THE INITIAL COAT OF SLURRY SEAL.

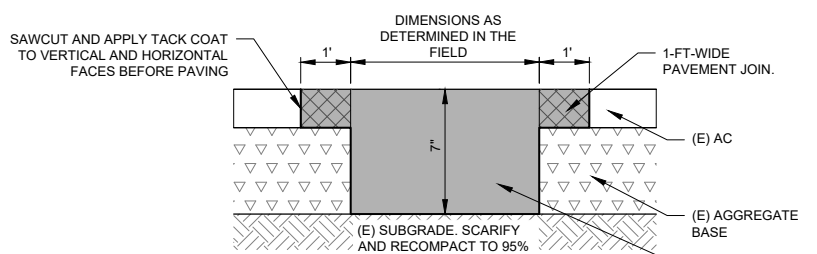
- LEGEND**
- WORK AREA A - ASPHALT AND BASE REMOVAL, STOCKPILE, AND DISPOSAL. SEE DETAIL D AND NOTE 3
 - WORK AREA B - SURFACE PREPARATION PRIOR TO ASPHALT SURFACE TREATMENT. SEE NOTES 7 AND 8
 - WORK AREA A - PCC TIE DOWN, STEEL ANCHOR AND CHAIN REMOVAL
 - WORK AREA B - REMOVE AND REPLACE STEEL CHAIN AND PROTECT PRIOR TO SEAL COAT SURFACE TREATMENT



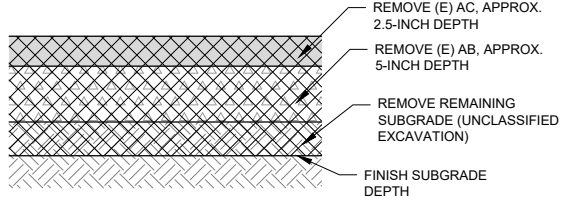
A AC PAVEMENT JOIN
NO SCALE



B CRACK REPAIR (OVER 1-1/2 INCH WIDE)
NO SCALE



C ISOLATED PAVEMENT REPAIR DETAIL
NO SCALE

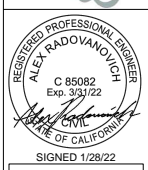


D ASPHALT AND BASE REMOVAL
NO SCALE

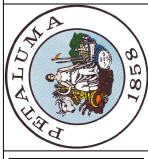
- NOTES:**
- THE FINISHED PAVEMENT REPAIR SURFACE SHALL NOT VARY MORE THAN 1/4 INCH FROM THE EXISTING SURFACE WHEN EVALUATED WITH A 12-FOOT CONTRACTOR-PROVIDED STRAIGHTEDGE. IF THIS TOLERANCE IS EXCEEDED, THE SURFACE SHALL BE CORRECTED WITH DIAMOND GRINDING PER ITEM P-403, OR BY REMOVING AND REPLACING FULL DEPTH OF HMA.
 - MINIMUM REMOVAL WIDTH IS 2 FEET. CONTRACTOR MAY REMOVE A LARGER WIDTH OF PAVEMENT AS NECESSARY TO PERFORM THE CONTRACTOR'S PREFERRED REMOVAL METHOD, SUCH AS MILLING. NO ADDITIONAL PAYMENT WILL BE MADE FOR INCREASED REMOVAL WIDTH.
 - REPLACEMENT SHALL BE THE FULL DEPTH OF EXISTING ASPHALT. EXISTING ASPHALT THICKNESS IS ANTICIPATED TO VARY FROM 2 INCHES TO 3 INCHES.

DATE: JANUARY 2022
DESIGNED BY: AR
DRAWN BY: ISB
CHECKED BY: SAS

PROJECT NO.
C61502110



SIGNED 1/28/22
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BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT
DEMOLITION PLAN





SHEET
C-051

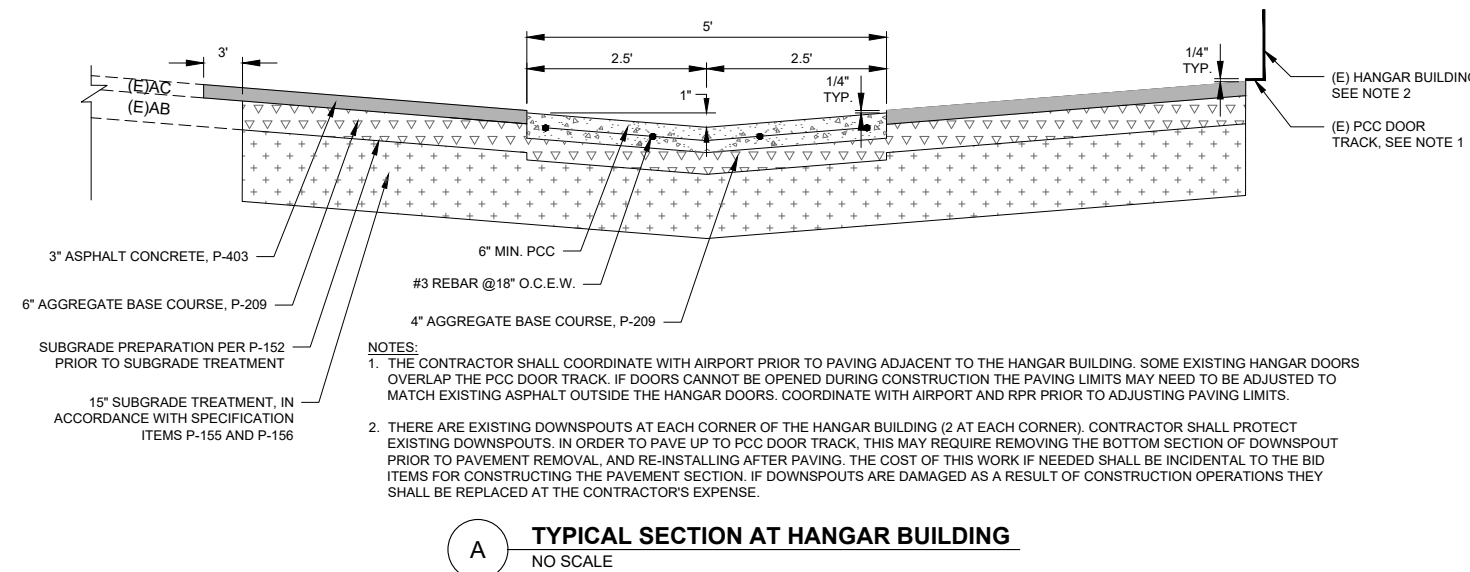
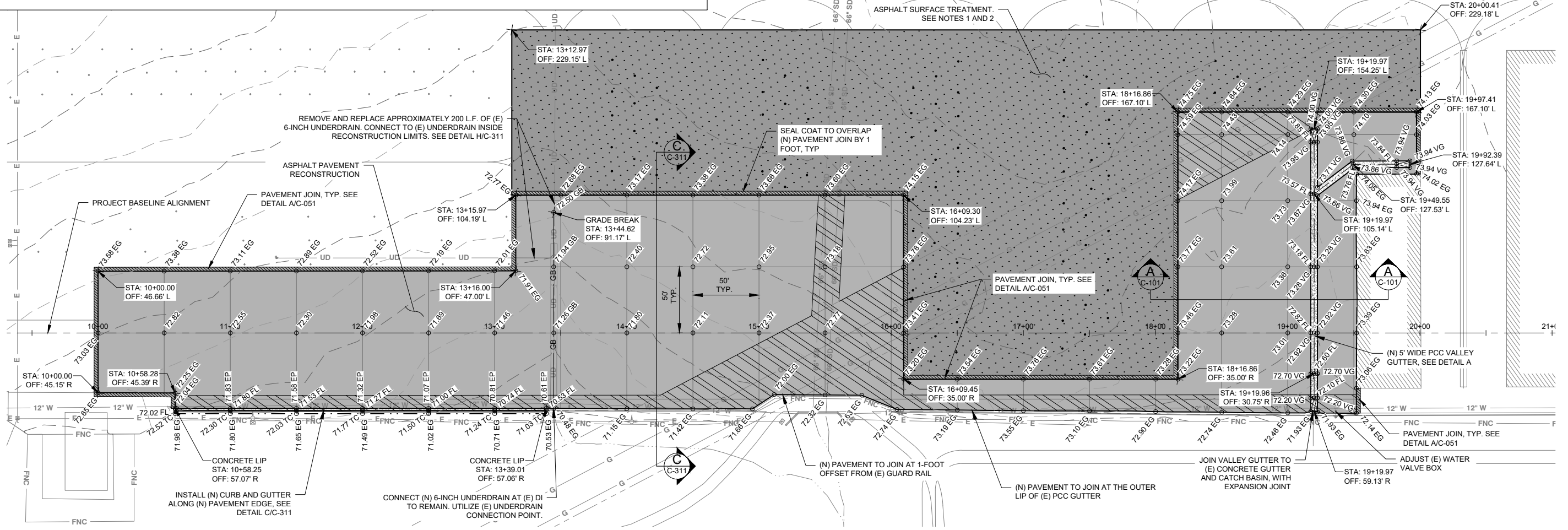
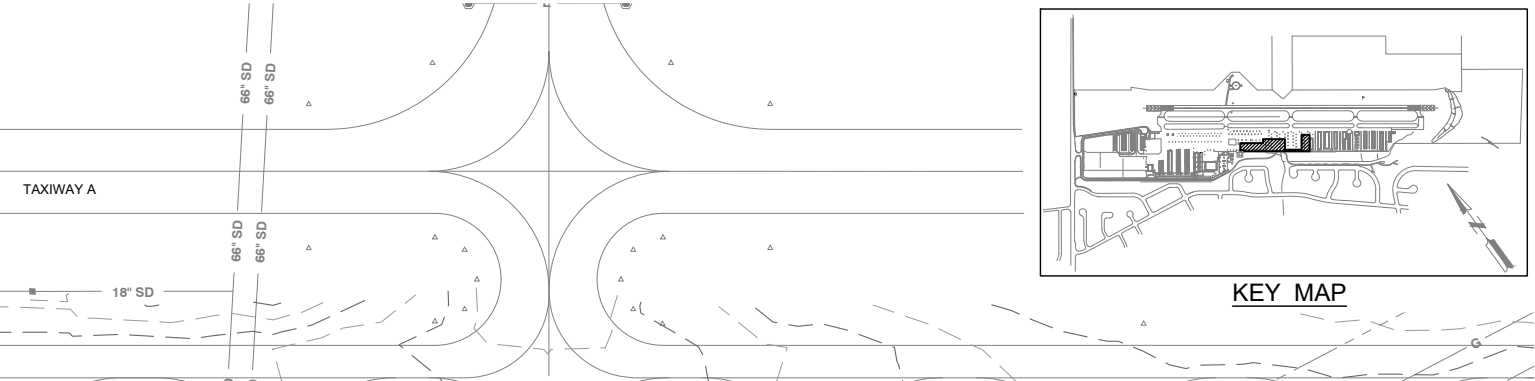
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RECONSTRUCTION AND SURFACE TREATMENT NOTES:

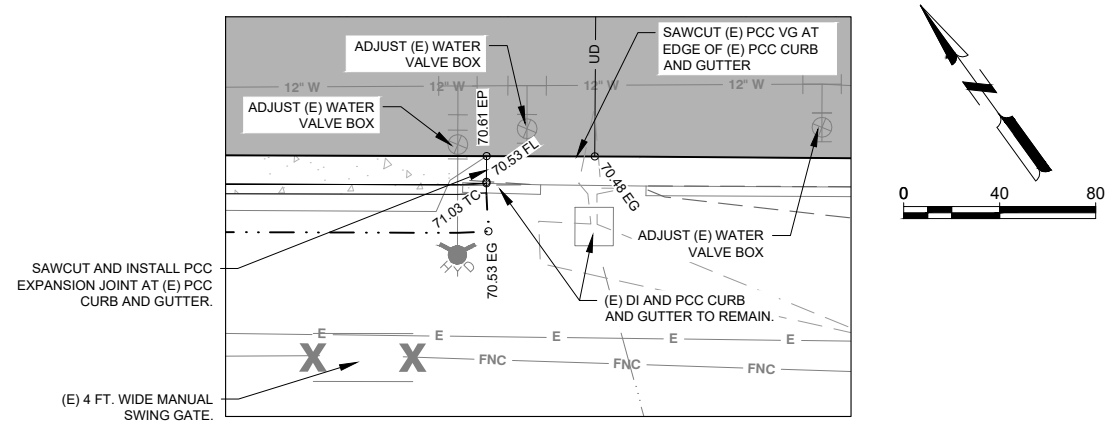
- PRIOR TO REMOVING PAVEMENT, CONTRACTOR SHALL VERIFY THE EXISTING GRADES SHOWN ON THESE PLANS AT THE PAVEMENT LIMITS. ANY DISCREPANCIES SHALL BE BROUGHT TO THE ENGINEER IMMEDIATELY. IF THERE ARE DISCREPANCIES, THE ENGINEER MAY NEED TO RE-EVALUATE THE PROPOSED GRADES.
- SURFACE TREATMENT SHALL CONSIST OF A DOUBLE APPLICATION OF SLURRY SEAL. THE INITIAL COAT SHALL FILL CRACKS LESS THAN 1-1/2 WIDE IN ACCORDANCE WITH SPECIFICATION ITEM SP-100. THE FINAL COAT SHALL NOT BE APPLIED UNTIL THE FIRST COAT HAS CURED. THE DURATION SHALL BE ACCORDING TO WEATHER CONDITIONS AND MANUFACTURER RECOMMENDATIONS.
- THE SLURRY SEAL SHALL BE ROLLED IN ACCORDANCE WITH SPECIFICATION ITEM P-626.

LEGEND

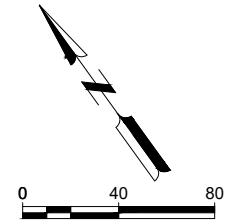
-  ASPHALT PAVEMENT RECONSTRUCTION
-  UTILITY CORRIDOR ZONE. SEE SHEET G-081
-  ASPHALT SURFACE TREATMENT
-  3' WIDE ASPHALT PAVEMENT JOIN SEE DETAIL A, SHEET C-051



A TYPICAL SECTION AT HANGAR BUILDING
NO SCALE



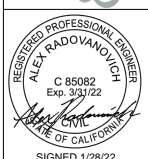
B INSTALLATION OF NEW CURB AND GUTTER
SCALE 1" = 5'



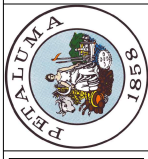
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PROJECT NO.
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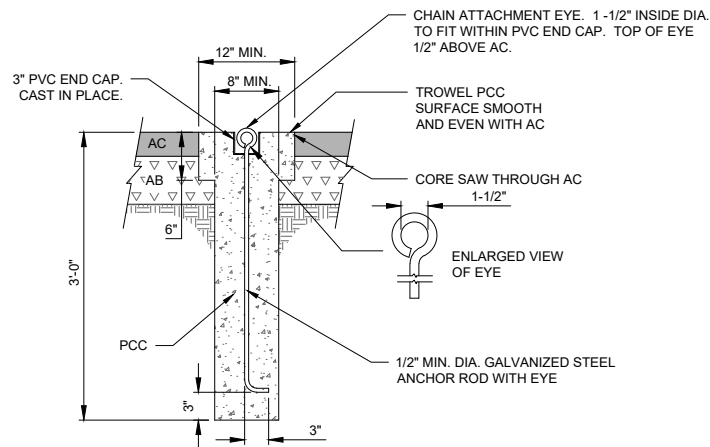


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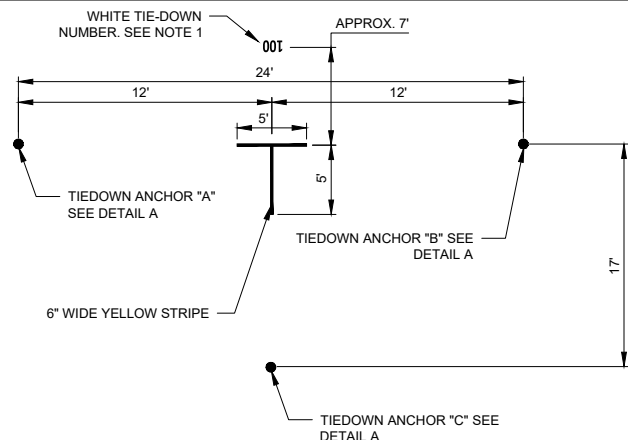
BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT
GRADING, PAVING & DRAINAGE PLAN

SHEET
C-101
7 OF 11



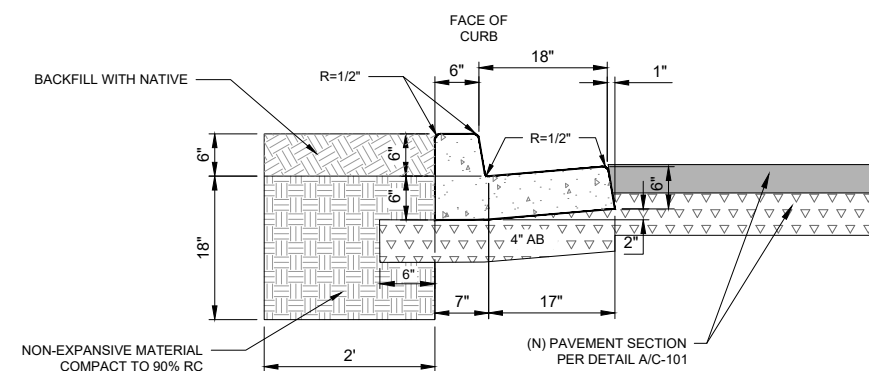
NOTE: ONE 7' TAIL CHAIN AND TWO 10' WING CHAINS PLUS HOOKS REQUIRED PER AIRCRAFT PARKING POSITION. SEE SPECIFICATIONS FOR CHAIN & HOOK DESCRIPTION.

A TYPICAL TIE DOWN ANCHOR
NO SCALE



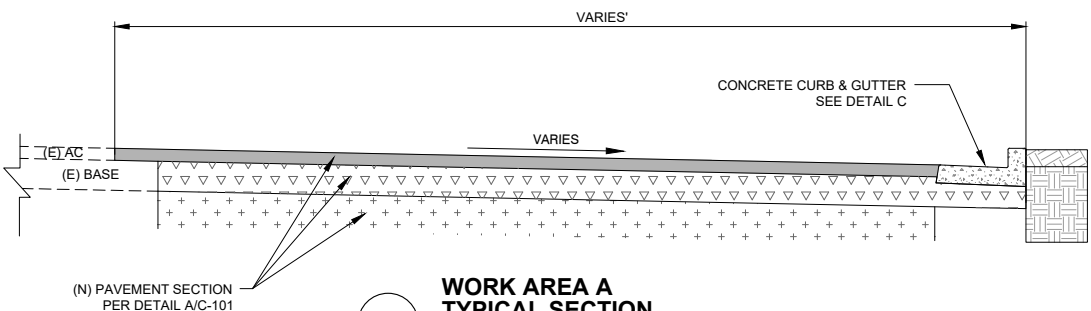
NOTES:
1. EXISTING TIE-DOWNS ARE NUMBERED AS SHOWN ON SHEETS C-651 AND C-652. PRIOR TO PERFORMING ANY REMOVALS, THE CONTRACTOR SHALL CONFIRM THE LAYOUT AND SIZE OF THE PAINTED NUMBERS WITH THE AIRPORT. THE NEW NUMBERS SHALL MATCH THE LAYOUT OF EXISTING, UNLESS OTHERWISE DIRECTED BY AIRPORT.
2. FOR NEW TIE-DOWN MARKINGS IN WORK AREA B, SOME OF THE EXISTING TIE-DOWN ANCHORS DO NOT HAVE THE SAME DIMENSIONS AS SHOWN IN THIS DETAIL. PRIOR TO PERFORMING ANY REMOVALS, THE CONTRACTOR SHALL REVIEW THE EXISTING TIE-DOWN MARKINGS IN WORK AREA B, AND RE-STRIPE THE NEW TIE-DOWN MARKINGS IN THE SAME LOCATION AFTER SURFACE TREATMENT, UNLESS OTHERWISE DIRECTED BY THE AIRPORT.

B TIEDOWN MARKING AND LAYOUT
NO SCALE

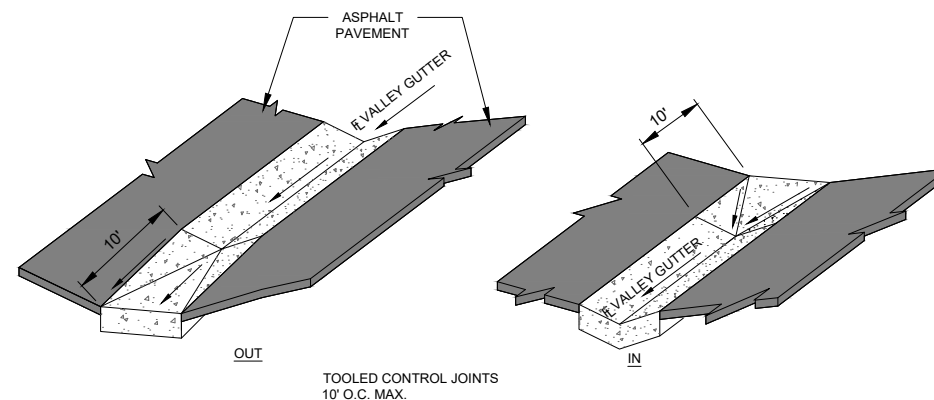


NOTES:
1. CONCRETE SHALL MEET THE REQUIREMENTS OF FAA SPECIFICATION ITEM P-610.
2. EXPANSION JOINTS, 1/4 INCH WIDE. SHALL BE INSTALLED AT EACH SIDE OF STRUCTURES, AT ENDS OF CURB RETURNS AND AT THE TOP OF DRIVEWAY TAPERS.
3. EXPANSION JOINTS SHALL BE INSTALLED AT 48 FOOT INTERVALS WITH WEAKENED PLANE JOINTS EVERY 16 FEET.
4. IF EXTRUSION MACHINE IS USED EXPANSION JOINTS SHALL BE DEEP SCORE 1/3 THE THICKNESS.
5. ALL MATERIALS SHOWN IN THIS DETAIL ARE INCLUDED IN THE LINEAR FOOT PRICE FOR PCC CURB AND GUTTER. REFER TO SPECIFICATION ITEM D-754.

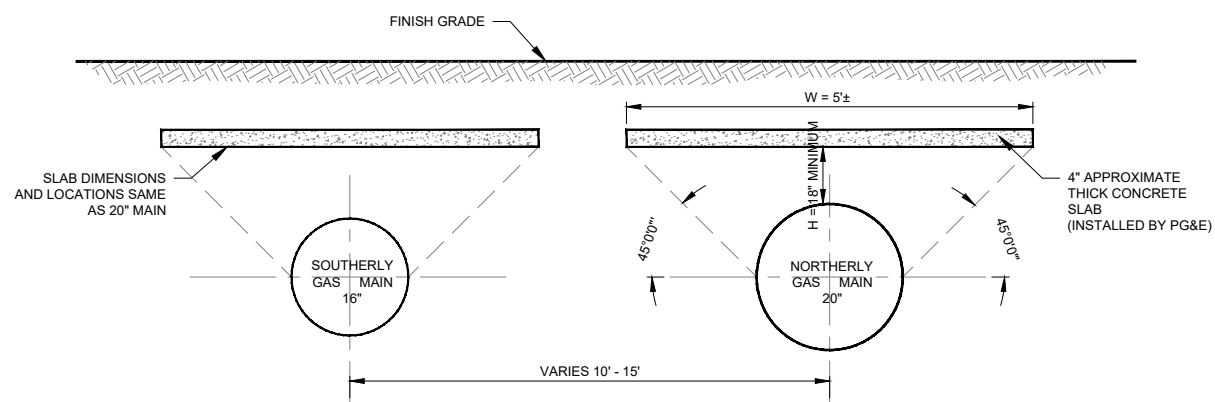
C CURB AND GUTTER TYPICAL SECTION
NO SCALE



D WORK AREA A TYPICAL SECTION
NO SCALE

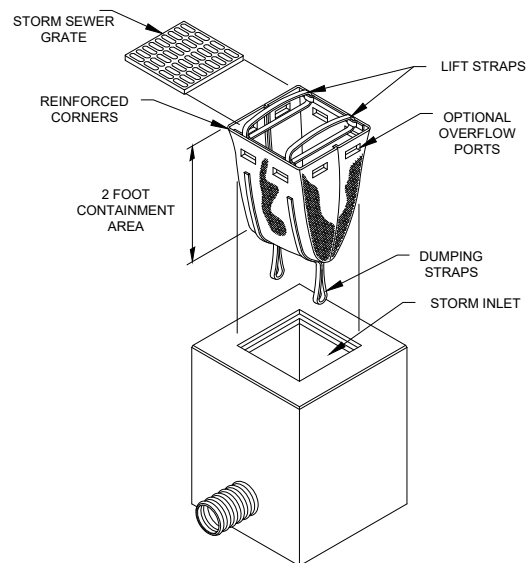


E VALLEY GUTTER TRANSITION
NO SCALE

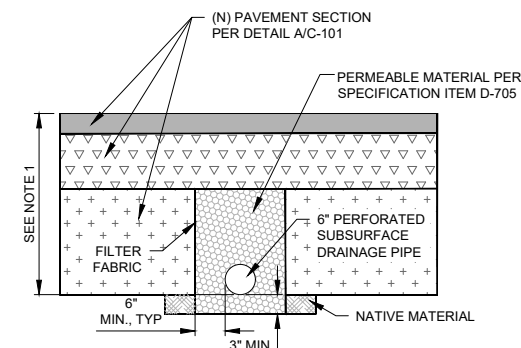


NOTES:
1. COORDINATE ALL WORK ADJACENT TO GAS LINE WITH PG&E

F EXISTING PG&E GAS MAIN DETAIL
NO SCALE



G INLET PROTECTION
NO SCALE

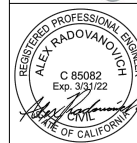


NOTES:
1. BASED ON AVAILABLE RECORD DATA, EXISTING UNDERDRAIN PIPE INVERT IS ESTIMATED TO BE APPROXIMATELY 24 INCHES BELOW EXISTING GROUND SURFACE. EXISTING PIPE, PERMEABLE MATERIAL, AND FILTER FABRIC SHALL BE REMOVED PRIOR TO SUBGRADE TREATMENT. NEW FILTER FABRIC, PIPE, AND PERMEABLE MATERIAL SHALL BE INSTALLED AFTER SUBGRADE TREATMENT. NEW PIPE SHALL TIE INTO EXISTING PIPE AT THE LOCATIONS SHOWN ON SHEET C-101, AT THE SAME INVERT ELEVATION AS EXISTING.

H PERFORATED UNDERDRAIN TRENCH DETAIL
NO SCALE

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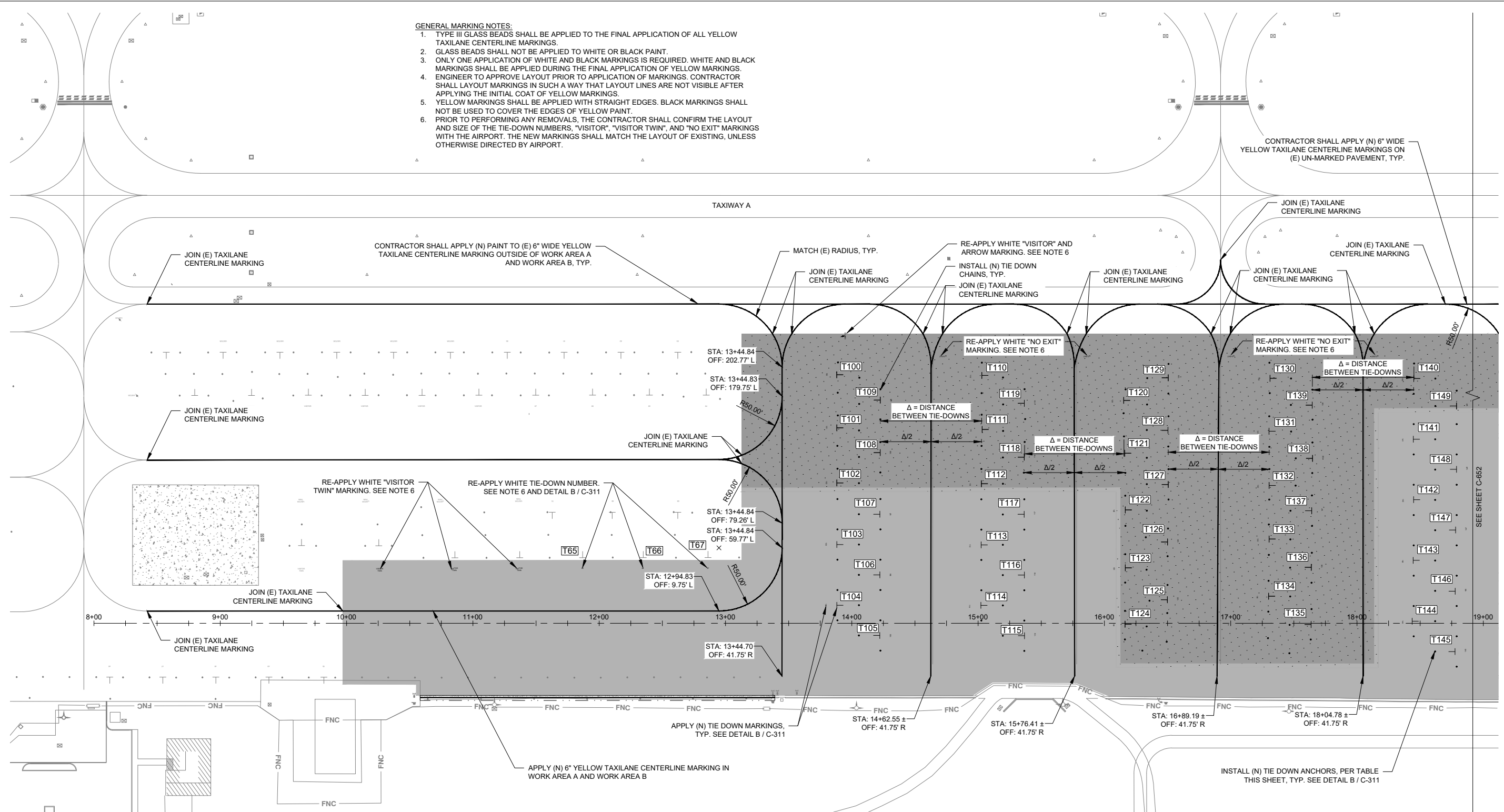
BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT
PAVEMENT DETAILS

SHEET
C-311

8 OF 11

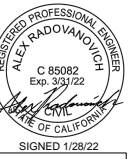
GENERAL MARKING NOTES:

- TYPE III GLASS BEADS SHALL BE APPLIED TO THE FINAL APPLICATION OF ALL YELLOW TAXILANE CENTERLINE MARKINGS.
- GLASS BEADS SHALL NOT BE APPLIED TO WHITE OR BLACK PAINT.
- ONLY ONE APPLICATION OF WHITE AND BLACK MARKINGS IS REQUIRED. WHITE AND BLACK MARKINGS SHALL BE APPLIED DURING THE FINAL APPLICATION OF YELLOW MARKINGS.
- ENGINEER TO APPROVE LAYOUT PRIOR TO APPLICATION OF MARKINGS. CONTRACTOR SHALL LAYOUT MARKINGS IN SUCH A WAY THAT LAYOUT LINES ARE NOT VISIBLE AFTER APPLYING THE INITIAL COAT OF YELLOW MARKINGS.
- YELLOW MARKINGS SHALL BE APPLIED WITH STRAIGHT EDGES. BLACK MARKINGS SHALL NOT BE USED TO COVER THE EDGES OF YELLOW PAINT.
- PRIOR TO PERFORMING ANY REMOVALS, THE CONTRACTOR SHALL CONFIRM THE LAYOUT AND SIZE OF THE TIE-DOWN NUMBERS, "VISITOR", "VISITOR TWIN", AND "NO EXIT" MARKINGS WITH THE AIRPORT. THE NEW MARKINGS SHALL MATCH THE LAYOUT OF EXISTING, UNLESS OTHERWISE DIRECTED BY AIRPORT.



DATE: JANUARY 2022
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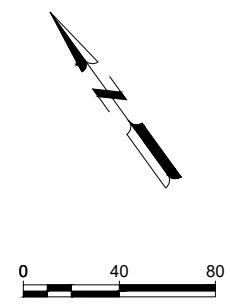
BASED AIRCRAFT APRON REHABILITATION
 PETALUMA MUNICIPAL AIRPORT
 MARKING PLAN - 1

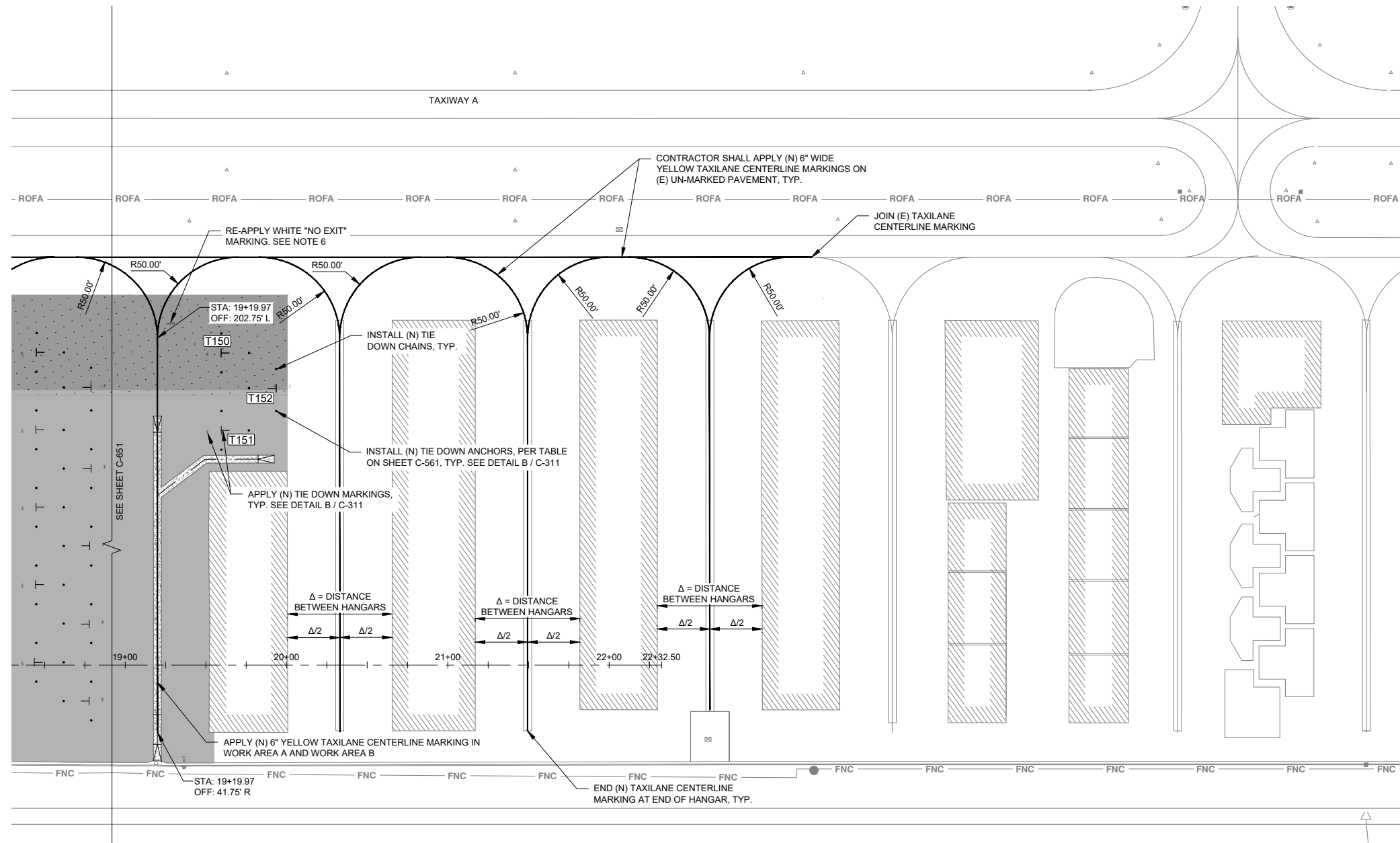
SHEET
C-651
 9 OF 11

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TIE DOWN ANCHOR INSTALLATION LOCATIONS															
TIE DOWN SET	ANCHOR	STATION	OFFSET	TIE DOWN SET	ANCHOR	STATION	OFFSET	TIE DOWN SET	ANCHOR	STATION	OFFSET	TIE DOWN SET	ANCHOR	STATION	OFFSET
T102	A	13+88.62	98.34 LT.	T105	A	14+22.62	02.34 LT.	T114	A	15+02.53	02.18 LT.	T142	A	18+44.86	85.70 LT.
	B	EXISTING	EXISTING		B	14+22.62	21.66 RT.		B	15+02.53	26.18 LT.		B	18+44.86	109.70 LT.
	C	EXISTING	EXISTING		C	14+05.62	09.66 RT.		C	15+19.53	14.18 LT.		C	18+61.86	97.70 LT.
T107	A	14+22.62	98.34 LT.	T112	A	15+02.53	98.18 LT.	T149	A	15+36.53	02.18 LT.	T147	A	18+78.86	85.70 LT.
	B	14+22.62	74.34 LT.		B	EXISTING	EXISTING		B	15+36.53	21.82 RT.		B	18+78.86	61.70 LT.
	C	14+05.62	86.34 LT.		C	EXISTING	EXISTING		C	15+19.53	09.82 RT.		C	18+61.86	73.70 LT.
T103	A	13+88.62	50.34 LT.	T117	A	15+36.53	98.18 LT.	T148	A	EXISTING	EXISTING	T152	A	18+44.86	37.70 LT.
	B	13+88.62	74.34 LT.		B	15+36.53	74.18 LT.		B	18+78.86	157.70 LT.		B	18+44.86	61.70 LT.
	C	14+05.62	62.34 LT.		C	15+19.53	86.18 LT.		C	EXISTING	EXISTING		C	18+61.86	49.70 LT.
T106	A	14+22.62	50.34 LT.	T113	A	15+02.53	50.18 LT.	T141	A	18+44.86	133.70 LT.	T146	A	18+78.86	37.70 LT.
	B	14+22.62	26.34 LT.		B	15+02.53	74.18 LT.		B	18+44.86	157.70 LT.		B	18+78.86	13.70 LT.
	C	14+05.62	38.34 LT.		C	15+19.53	62.18 LT.		C	18+61.86	145.70 LT.		C	18+61.86	25.70 LT.
T104	A	13+88.62	02.34 LT.	T116	A	15+36.53	50.18 LT.	T148	A	18+78.86	133.70 LT.	T144	A	18+44.86	10.30 RT.
	B	13+88.62	26.34 LT.		B	15+36.53	26.18 LT.		B	18+78.86	109.70 LT.		B	18+44.86	13.70 LT.
	C	14+05.62	14.34 LT.		C	15+19.53	38.18 LT.		C	18+61.86	121.70 LT.		C	18+61.86	01.70 LT.

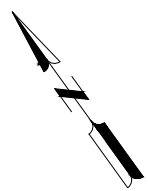
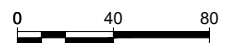
T100 = TIE DOWN SET IDENTIFIER





GENERAL MARKING NOTES:

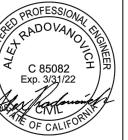
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DATE: JANUARY 2022
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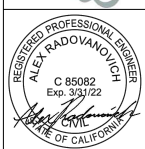
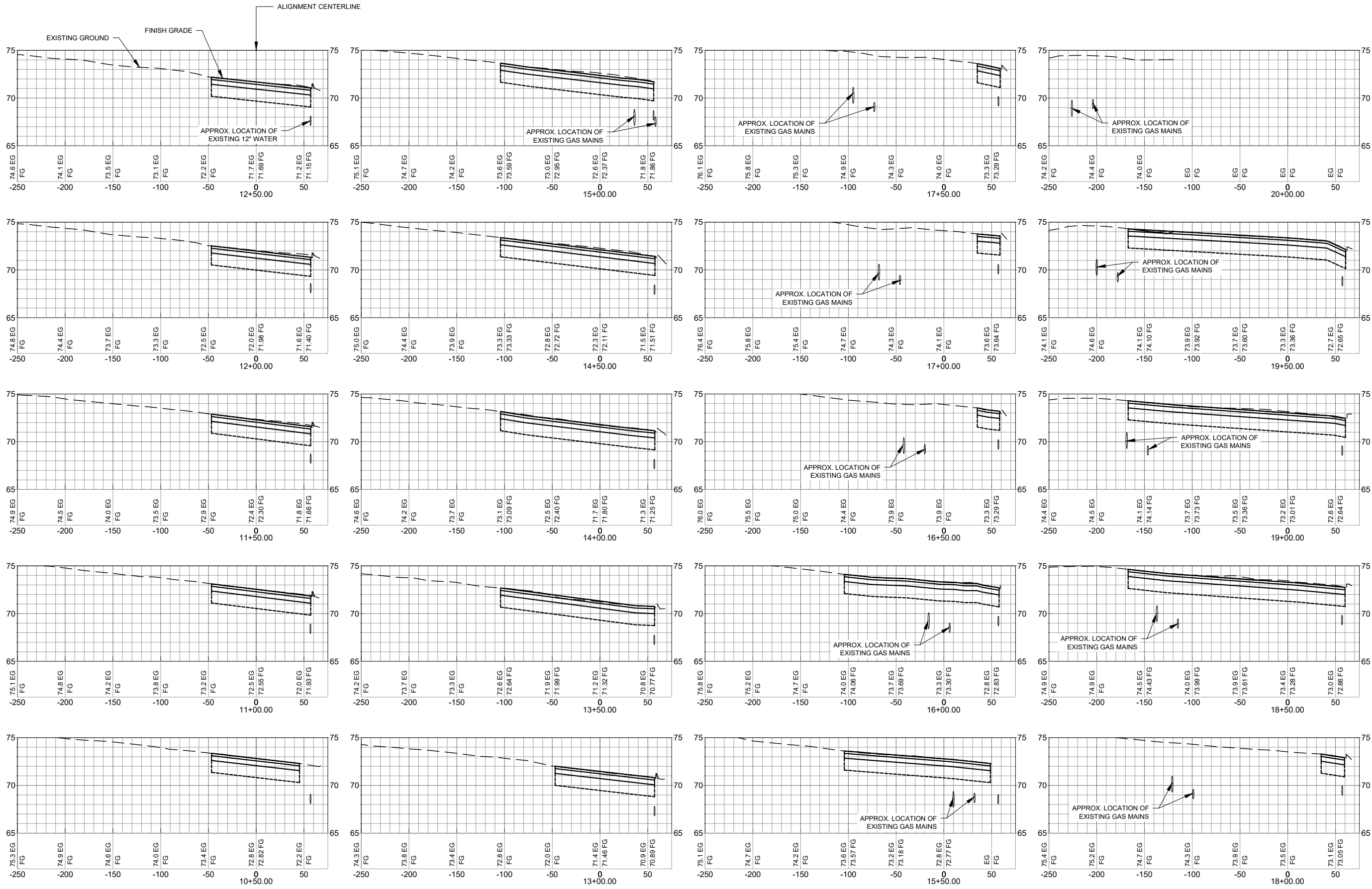
BASED AIRCRAFT APRON REHABILITATION
PETALUMA MUNICIPAL AIRPORT

MARKING PLAN - 2

SHEET

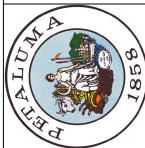
C-652

10 OF 11



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