

SUBSTANCES WHICH ARE HAZARDOUS, DANGEROUS, TOXIC, OR WHICH OTHERWISE VIOLATE ANY STATE, FEDERAL, OR LOCAL LAW, ORDINANCE, CODE, REGULATION, RULE, ORDER, OR STANDARD

AUTOCAD FILE

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WORKER SAFETY AND AHBL ASSUMES NO RESPONSIBILITY. ALL TRENCH SAFETY SYSTEMS SHALL MEET THE REQUIREMENTS OF THE WASHINGTON INDUSTRIAL SAFETY AND HEALTH ACT, CHAPTER 49.17 RCW.

UTILITY NOTE

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE APPROXIMATE ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES THAT INCUR DUE TO THE CONTRACTOR'S FAILURE TO LOCATE EXACTLY AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. AHBL ASSUMES NO LIABILITY FOR THE LOCATION OF UNDERGROUND UTILITIES.

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BAY STREET APARTMENTS SEC 26, TWP 24N, RGE 1E, W.M. PORT ORCHARD, WASHINGTON

DATUM PLANE	MLLW	NGVD	NAVD88
HIGHEST ESTIMATED TIDE	14.70 +/-	0.5	
MEAN HIGHER HIGH WATER	11.74	5.51	9.22
MEAN HIGH WATER	- 10.86	- 4.64	8.34
MEAN (HALF) TIDE LEVEL	6.85	0.62	4.33
NGVD	6.23	0.00	3.71
MEAN LOW WATER	- 2.85	-3.40	0.33
NORTH AMERICAN DATUM 8	2.52	-3.71	0.00
MEAN LOWER LOW WATER	- 0.00	-6.23	2.52
LOWEST ESTIMATED TIDE	4.50 +/- 0.	5	

APPROXIMATE CUT / FILL QUANTITIES

CUT: 258 CY FILL: 2,208 CY NET: 1.950 CY (FILL) NOTE:

THE ABOVE QUANTITIES ARE ESTIMATES ONLY INTENDED FOR THE PERMITTING PROCESS AND ARE NOT TO BE USED BY THE CONTRACTOR FOR BID PURPOSES. THE CONTRACTOR SHALL MAKE THEIR OWN EARTHWORK DETERMINATION.

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SITE ADDRESS

429 BAY STREET PORT ORCHARD, WA 98366

PARCEL

4044-001-001-0007

USE

MIXED USE, COMMERCIAL/MULTI FAMILY RESIDENTIAL

CONSTRUCTION TYPE V-A

BUILDING AREA

65,243 SQ. FT. (GROSS AREA)

EXISTING SITE VEGITATION

THE MAJORITY OF THE PARCEL IS PAVED WITH STREET TREES AND SHRUBS ALONG FRONTAGE AND AROUND SITE.

UTILITIES:

SEWER: WATER: FIRE: ELECTRIC: GAS: TELEPHONE: CABLE TV:

PORT ORCHARD PUBLIC WORKS PORT ORCHARD PUBLIC WORKS SOUTH KITSAP FIRE AND RESCUE PUGET SOUND ENERGY CASCADE NATURAL GAS CENTURYLINK COMCAST

SHEET INDEX

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C3.00	GRADING AND DRAINAGE PLAN
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C5.03	BAY STREET NOTES AND DETAILS
PFA-1	WSDOT PLAN FOR APPROVAL





DATE: June 17, 2020 FILENAME: Q:\2019\2190242\10_CIV\CAD\2190242-SH-TESC-DEMO.dwg

GENERAL NOTES

- 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH ALL CURRENTLY ADOPTED WSDOT AND APWA SPECIFICATIONS AND PLANS, AND THE CITY OF PORT ORCHARD MUNICIPAL CODE, THE CURRENTLY ADOPTED CITY OF PORT ORCHARD DEVELOPER'S HANDBOOK, THE CURRENTLY ADOPTED SURFACE WATER DESIGN MANUAL AND THE CONDITIONS OF PRELIMINARY SUBDIVISION APPROVAL. IT SHALL BE THE SOLE RESPONSIBILITY OF THE APPLICANT AND THE PROFESSIONAL CIVIL ENGINEER TO CORRECT ANY ERROR, OMISSION, OR VARIATION FROM THE ABOVE REQUIREMENTS FOUND IN THESE PLANS. ALL CORRECTIONS SHALL BE AT NO ADDITIONAL COST OR LIABILITY TO THE CITY OF PORT ORCHARD.
- 2. THE DESIGN ELEMENTS WITHIN THESE PLANS HAVE BEEN REVIEWED ACCORDING TO THE PORT ORCHARD DESIGN STANDARDS. SOME ELEMENTS MAY HAVE BEEN OVERLOOKED OR MISSED BY THE CITY OF PORT ORCHARD CITY ENGINEER. ANY DEVIATION FROM ADOPTED STANDARDS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY OF PORT ORCHARD CITY ENGINEER, PRIOR TO CONSTRUCTION.
- 3. APPROVAL OF THESE ENGINEERING PLANS SUCH AS FOR ROADS, GRADING, OR DRAINAGE DOES NOT CONSTITUTE AN APPROVAL OF ANY OTHER DESIGN (E.G., WATER, SEWER, GAS, ELECTRICAL, ETC.).
- 4. BEFORE ANY CONSTRUCTION OR DEVELOPMENT ACTIVITY, A PRECONSTRUCTION MEETING MUST BE HELD BETWEEN THE CITY OF PORT ORCHARD PUBLIC WORKS DEPARTMENT, THE APPLICANT AND THE APPLICANT'S CONSTRUCTION REPRESENTATIVE.
- 5. PROOF OF LIABILITY INSURANCE SHALL BE SUBMITTED TO THE CITY OF PORT ORCHARD PRIOR TO THE PRECONSTRUCTION MEETING.
- 6. A COPY OF THESE APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- 7. CONSTRUCTION NOISE SHALL COMPLY WITH THE CURRENT POMC SECTION 9.24.050.
- 8. IT SHALL BE THE APPLICANT /CONTRACTOR'S RESPONSIBILITY TO OBTAIN ALL RIGHT-OF-WAY PERMITS AND CONSTRUCTION EASEMENTS NECESSARY BEFORE INITIATING OFF-SITE WORK WITHIN A CITY OF PORT ORCHARD STREET RIGHT-OF-WAY.
- 9. FRANCHISED UTILITIES OR OTHER INSTALLATIONS THAT ARE NOT SHOWN ON THESE APPROVED PLANS SHALL NOT BE CONSTRUCTED UNLESS AN APPROVED SET OF PLANS IS SUBMITTED TO THE CITY OF PORT ORCHARD PRIOR TO CONSTRUCTION.
- 10. THE VERTICAL DATUM SHALL BE NAVD 1988 AND THE HORIZONTAL DATUM SHALL BE NAD 1983 HARN STATE PLANE WASHINGTON NORTH FIPS 4601 FEET.
- 11. GROUNDWATER SYSTEM CONSTRUCTION SHALL BE WITHIN A RIGHT-OF-WAY OR APPROPRIATE DRAINAGE EASEMENT, BUT NOT UNDERNEATH THE ROADWAY SECTION.
- 12. ALL UTILITY TRENCHES SHALL BE BACKFILLED AND COMPACTED IN ACCORDANCE WITH THE CITY OF PORT ORCHARD STANDARDS 13. ALL ROADWAY SUBGRADE SHALL BE BACKFILLED, COMPACTED TO 95% MAXIMUM DENSITY AND PREPARED FOR SURFACING IN
- ACCORDANCE WITH WSDOT STANDARD SPECIFICATION 2-06.3. 14. OPEN CUTTING OF EXISTING ROADWAYS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY OF PORT ORCHARD CITY ENGINEER AND NOTED ON THESE APPROVED PLANS. ANY OPEN CUT SHALL BE RESTORED IN ACCORDANCE WITH THE CITY OF PORT
- ORCHARD STANDARD SPECIFICATIONS. 15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACTOR. ANY WORK WITHIN THE TRAVELED RIGHT- OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE AT LEAST ONE FLAGGER FOR EACH LANE OF TRAFFIC AFFECTED. REFER TO "TRAFFIC CONTROL," OF THE WSDOT STANDARD SPECIFICATIONS SHALL APPLY IN ITS ENTIRETY. TRAFFIC CONTROL PLANS SHALL FOLLOW THE CURRENTLY ADOPTED MUTCD MANUAL AS APPLICABLE.
- TO PROTECT SIGNIFICANT TREES FROM THE IMPACTS OF THE PROPOSED DEVELOPMENT, THE APPLICANT SHALL PROVIDE THE BEST PROTECTION FOR SIGNIFICANT TREES PER THE REGULATIONS. AT A MINIMUM, ANY SIGNIFICANT TREES TO BE RETAINED SHALL BE FENCED TWO FEET OUTWARD FROM THE IDENTIFIED DRIP LINE. TREES THAT SUSTAIN DAMAGE DURING CONSTRUCTION SHALL BE
- REPLACED PURSUANT TO POMC. A REPRESENTATIVE OF THE CITY OF PORT ORCHARD DCD STAFF SHALL VERIFY PROTECTIVE FENCING PLACEMENT PER THIS CONDITION PRIOR TO ISSUANCE OF A NOTICE TO PROCEED FOR GRADING AND CLEARING. THE CITY SHALL INSPECT FOR COMPLIANCE WITH THE TREE PLAN PRIOR TO A FINAL INSPECTION. THE INSPECTION SHALL ALSO EVALUATE THE CONDITION OF RETAINED TREES AND ANY AND ALL CORRECTIONS WILL BE REQUIRED TO BE COMPLETED PRIOR TO A FINAL INSPECTION AND RELEASE OF ANY POST FINANCIAL GUARANTEES FOR THE SITE.

DRAINAGE NOTES

DRAINAGE NOTES (ALSO REFER TO CHAPTER 9 SURFACE WATER DRAINAGE):

- 16. ALL STORM PIPE AND APPURTENANCES SHALL BE LAID IN ACCORDANCE WITH CITY OF PORT ORCHARD DESIGN AND CONSTRUCTION STANDARDS. THIS SHALL INCLUDE LEVELING AND COMPACTING THE TRENCH BOTTOM, THE TOP OF THE FOUNDATION MATERIAL AND ANY REQUIRED BEDDING TO A UNIFORM GRADE SO THAT THE ENTIRE DRAINAGE FACILITY IS SUPPORTED BY A UNIFORMLY DENSE UNYIELDING BASE.
- 17. ALL STORM PIPE SHALL BE SUBJECT TO A LOW-PRESSURE AIR TEST IN ACCORDANCE WITH WSDOT STANDARD SPECIFICATION 7-04.3(1)F AND A VIDEO INSPECTION IN ACCORDANCE WITH THE PORT ORCHARD DESIGN STANDARDS.
- 18. STORM PIPE COVER, MEASURED FROM THE FINISHED GRADE ELEVATION TO THE TOP OF THE OUTSIDE SURFACE OF THE PIPE, SHALL BE 2 FEET MINIMUM, UNLESS AUTHORIZED BY THE CITY OF PORT ORCHARD CITY ENGINEER UNDER THE FOLLOWING CIRCUMSTANCES:
- a. UNDER DRIVEWAYS THE PIPE COVER MAY BE REDUCED TO 1 FOOT MINIMUM IF THE 2- FEET CANNOT BE ACHIEVED AND THE COVER IS CONSISTENT WITH THE PIPE MANUFACTURER'S RECOMMENDATIONS.
- b. IN AREAS NOT SUBJECT TO VEHICULAR LOADS, SUCH AS LANDSCAPE PLANTERS AND YARDS, THE PIPE COVER MAY BE REDUCED TO 1 FOOT MINIMUM.
- c. IF DUCTILE IRON PIPE OR C900 PIPE IS USED, THE PIPE COVER MAY BE REDUCED TO 1 FOOT MINIMUM.
- 19. STEEL PIPE SHALL BE GALVANIZED AND HAVE ASPHALT TREATMENT #1 OR BETTER INSIDE AND OUT (WSDOT STANDARD SPECIFICATION 9-05.4(3)).
- 20. ANY DRAINAGE STRUCTURE, SUCH AS A CATCH BASIN OR A MANHOLE, NOT RECEIVING SURFACE RUNOFF AND NOT LOCATED WITHIN A TRAVELED ROADWAY OR SIDEWALK SHALL HAVE A SOLID LOCKING LID. ANY DRAINAGE STRUCTURE ASSOCIATED WITH A PERMANENT RETENTION/DETENTION FACILITY, NOT RECEIVING SURFACE RUNOFF, SHALL HAVE A SOLID LOCKING LID.
- 21. ALL CATCH BASIN GRATES SHALL CONFORM TO THE CURRENTLY ADOPTED STORMWATER MANAGEMENT MANUAL AND THE WSDOT STANDARD PLANS WHEN LOCATED WITHIN THE RIGHT- OF-WAY. AND SHALL INCLUDE A COMBINATION INLET FRAME (OPEN-CURB-FACE FRAME), WHEN LOCATED IN A SUMP CONDITION OR BEFORE AN INTERSECTION WITH A 4% GRADE OR ABOVE. A HERRINGBONE GRATE MAY BE USED OUTSIDE THE RIGHT-OF-WAY. ALL CATCH BASINS WITHIN THE GUTTER LINE SHALL BE INSTALLED IN ACCORDANCE WITH THE CITY OF PORT ORCHARD STANDARD
- DETAILS AS APPLICABLE. MAXIMUM CATCH BASIN HEIGHT FROM FINISHED GRADE TO PIPE INVERT SHALL BE PER THE APPLICABLE DETAIL.
- 22. FOR ANY CURB GRADE LESS THAN 0.8% (0.0080 FT/FT), INCLUDING CURB RETURNS, A PROFESSIONAL LAND SURVEYOR, CURRENTLY LICENSED IN THE STATE OF WASHINGTON, SHALL VERIFY THAT THE CURB FORMS OR STRING LINES ARE AT THE GRADES NOTED ON THE APPROVED PLANS PRIOR TO PLACEMENT OF CONCRETE. THE CONTRACTOR IS RESPONSIBLE FOR SURVEY COORDINATION AND COSTS.
- 23. FOR ANY DRAINAGE PIPE GRADE LESS THAN 0.5% (0.0050 FT/FT), A PROFESSIONAL LAND SURVEYOR, CURRENTLY LICENSED IN THE STATE OF WASHINGTON, SHALL VERIFY THAT THE AS-BUILT PIPE MATCHES THE GRADES NOTED ON THE APPROVED PLANS PRIOR TO COMPLETION OF SUBGRADE. THE CONTRACTOR IS RESPONSIBLE FOR SURVEY COORDINATION AND COSTS.
- 24. NOT USED.

26. NOT USED.

25. ROCK FOR EROSION PROTECTION OF DITCHES, WHERE REQUIRED MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF ONE FOOT (1'), AND MUST MEET THE FOLLOWING SPECIFICATIONS: 100% MUST PASS THE 8" SIEVE, 40% MAXIMUM CAN PASS THE 3" SIEVE AND 10% MAXIMUM CAN PASS THE 3/4" SIEVE.

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EROSION AND SEDIMENT CONTROL NOTES

EROSION AND SEDIMENT CONTROL NOTES (ALSO REFER TO

- CHAPTER 9 SURFACE WATER DRAINAGE) 27. APPROVAL OF THESE TEMPORARY EROSION AND SEDIMENT CONTROL (TESC) PLANS DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
- 28. THE IMPLEMENTATION OF THESE TESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE TESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CESCL UNTIL ALL CONSTRUCTION IS APPROVED.
- 29. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THESE PLANS SHALL BE CLEARLY FLAGGED BY A CONTINUOUS LENGTH OF SURVEY TAPE (OR FENCING, IF REQUIRED) PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE APPLICANT/CESCL FOR THE DURATION OF CONSTRUCTION.
- 30. STABILIZED CONSTRUCTION ENTRANCES, IN ACCORDANCE WITH STANDARD DETAILS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS CONSTRUCTED WHEEL WASH SYSTEMS OR WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN AND TRACK-OUT TO STREET RIGHT-OF- WAY DOES NOT OCCUR FOR THE DURATION OF THE PROJECT.
- 31. THE TESC FACILITIES SHOWN ON THESE PLANS MUST BE CONSTRUCTED PRIOR TO ALL CLEARING AND GRADING TO ENSURE THAT THE TRANSPORT OF SEDIMENT TO SURFACE WATERS, DRAINAGE SYSTEMS, AND ADJACENT PROPERTIES IS REDUCED TO REQUIRED LEVELS.
- 32. THE TESC FACILITIES SHOWN ON THESE PLANS ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE TESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND MODIFIED TO ACCOUNT FOR CHANGING SITE CONDITIONS (E.G., ADDITIONAL COVER MEASURES, ADDITIONAL SUMP PUMPS, RELOCATION OF DITCHES AND SILT FENCES, ADDITIONAL PERIMETER PROTECTION, ETC.), AS DIRECTED BY THE CITY ENGINEER.
- 33. THE TESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CESCL AND MAINTAINED TO ENSURE CONTINUED PROPER FUNCTIONING. WRITTEN RECORDS SHALL BE KEPT OF WEEKLY REVIEWS OF THE TESC FACILITIES AND OF SAMPLES TAKEN DURING THE WET SEASON (OCTOBER 1 TO APRIL 30 AND OF MONTHLY REVIEWS DURING THE DRY SEASON (MAY 1 TO SEPTEMBER 30).
- 34. ANY AREAS OF EXPOSED SOILS, INCLUDING ROADWAY EMBANKMENTS. THAT WILL NOT BE DISTURBED FOR TWO DAYS DURING THE WET SEASON OR SEVEN DAYS DURING THE DRY SEASON SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVE TESC METHODS (E.G., SEEDING, MULCHING, PLASTIC COVERING, FTC.)
- 35. ANY AREA NEEDING TESC MEASURES NOT REQUIRING IMMEDIATE ATTENTION SHALL BE ADDRESSED WITHIN SEVEN (7) DAYS. 36. THE TESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN
- TWENTY-FOUR (24) HOURS FOLLOWING A STORM EVENT. 37. AT NO TIME SHALL MORE THAN ONE (1) FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN, ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO FINAL INSPECTION. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO A DOWNSTREAM SYSTEM.
- 38. ANY PERMANENT FLOW CONTROL FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION SYSTEM, THE TEMPORARY FACILITY MUST BE GRADED SO THAT THE BOTTOM AND SIDES ARE AT LEAST THREE (3) FEET ABOVE THE FINAL GRADE OF THE PERMANENT FACILITY.
- 39. WHERE STRAW MULCH FOR TEMPORARY EROSION CONTROL IS REQUIRED, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF 2 TO 3 INCHES.
- 40. PRIOR TO THE BEGINNING OF THE WET SEASON (OCTOBER 1), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH AREAS CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. DISTURBED AREAS SHALL BE SEEDED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON.
- -A SKETCH MAP OF THOSE AREAS TO BE SEEDED AND THOSE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE CITY OF PORT ORCHARD CITY ENGINEER. THE INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT
- PROPERTIES, OR DRAINAGE FACILITIES.

TESC NOTES

- 1. ALL LIMITS OF CLEARING AND AREAS OF VEGETATION PRESERVATION AS PRESCRIBED ON THESE PLANS SHALL BE CLEARLY FLAGGED IN THE FIELD AND OBSERVED DURING CONSTRUCTION.
- 2. ALL TEMPORARY SEDIMENTATION AND EROSION CONTROL MEASURES, AND PROTECTIVE MEASURES FOR CRITICAL AREAS AND SIGNIFICANT TREES SHALL BE INSTALLED PRIOR TO INITIATING ANY CONSTRUCTION ACTIVITIES.
- 3. ALL REQUIRED SEDIMENTATION AND EROSION CONTROL FACILITIES MUST BE CONSTRUCTED AND IN OPERATION PRIOR TO ANY LAND CLEARING AND/OR OTHER CONSTRUCTION ACTIVITIES TO ENSURE THAT SEDIMENT LADEN WATER DOES NOT ENTER ANY EXISTING DRAINAGE SYSTEM. THE CONTRACTOR SHALL SCHEDULE AN INSPECTION OF THE EROSION CONTROL FACILITIES PRIOR TO ANY LAND CLEARING AND/OR OTHER CONSTRUCTION. ALL EROSION AND SEDIMENT CONTROL FACILITIES SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL SUCH TIME THAT CLEARING AND/OR CONSTRUCTION IS COMPLETED AND THE POTENTIAL FOR ON-SITE EROSION HAS PASSED. THE IMPLEMENTATION. MAINTENANCE, REPLACEMENT, AND ADDITIONS TO THE EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 4. THE EROSION AND SEDIMENTATION CONTROL SYSTEM MEASURES DEPICTED ON THESE PLANS ARE INTENDED TO BE MINIMUM REQUIREMENTS TO MEET ANTICIPATED SITE CONDITIONS. AS CONSTRUCTION PROGRESSES AND UNEXPECTED OR SEASONAL CONDITIONS DICTATE, ADDITIONAL MEASURES MAY BE NECESSARY TO ENSURE COMPLETE SILTATION CONTROL ON THE SITE. DURING THE COURSE OF CONSTRUCTION, IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY CONSTRUCTION RELATED ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES, OVER AND ABOVE THE MINIMUM REQUIREMENTS, AS MAY BE NEEDED TO PROTECT ADJACENT SITES, SENSITIVE AREAS, NATURAL WATER COURSES, AND/OR STORM DRAINAGE SYSTEMS.
- ANY DISTURBED AREA WHICH HAS BEEN STRIPPED OF VEGETATION SHALL BE IMMEDIATELY STABILIZED WITH MULCHING, GRASS PLANTING, OR OTHER APPROVED EROSION CONTROL TREATMENT APPLICABLE TO THE TIME OF YEAR IN QUESTION. GRASS SEEDING ALONE WILL BE ACCEPTABLE ONLY DURING THE MONTHS OF MAY THROUGH SEPTEMBER INCLUSIVE. SEEDING MAY PROCEED OUTSIDE THE SPECIFIED TIME PERIOD WHENEVER IT IS IN THE INTEREST OF THE CONTRACTOR BUT SHALL BE AUGMENTED WITH MULCHING, NETTING, OR OTHER TREATMENT.
- NO TEMPORARY OR PERMANENT STOCKPILING OF MATERIALS OR EQUIPMENT SHALL OCCUR WITHIN CRITICAL AREAS OR ASSOCIATED BUFFERS, OR THE CRITICAL ROOT ZONE FOR VEGETATION PROPOSED FOR RETENTION.
- WATER SITE AS NECESSARY TO CONTROL DUST.
- 8. TOPSOIL SHALL NOT BE PLACED WHILE SUBGRADE IS IN A FROZEN OR MUDDY CONDITION, WHEN THE SUBGRADE IS EXCESSIVELY WET, OR WHEN CONDITIONS EXIST THAT MAY OTHERWISE BE DETRIMENTAL TO PROPER GRADING OR PROPOSED SODDING OR SEEDING.
- ON-SITE EROSION CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY PROBLEMS OCCURRING BEFORE FINAL STABILIZATION OF THE SITE SHALL BE CORRECTED BY THE CONTRACTOR.
- 10. IF EROSION OR SEDIMENTATION OCCURS ON ADJACENT SITES, ALL CONSTRUCTION WORK WITHIN THE PROJECT LIMITS THAT WILL AGGRAVATE THE SITUATION MUST CEASE AND THE CONTRACTOR SHALL IMMEDIATELY COMMENCE RESTORATION OR MITIGATION MEASURES. RESTORATION ACTIVITY SHALL CONTINUE UNTIL SUCH TIME AS THE PROBLEM IS RECTIFIED.
- 11. ALL EROSION AND SEDIMENTATION CONTROL MEASURES SHOWN ON THESE DRAWINGS SHALL BE INSTALLED PRIOR TO, OR AS THE FIRST STAGE OF SITE PREPARATION.
- 12. SHOULD THE TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES SHOWN ON THESE DRAWINGS NOT PROVE ADEQUATE TO CONTROL EROSION AND SEDIMENTATION, THE CONTRACTOR SHALL INSTALL ADDITIONAL FACILITIES AS NECESSARY TO PROTECT ADJACENT SITES, SENSITIVE AREAS, NATURAL WATER COURSES, AND/OR STORM DRAINAGE SYSTEMS.
- 13. ALL NECESSARY FACILITIES SHALL BE MAINTAINED ON SITE TO PREVENT DEBRIS, DUST, AND MUD FROM ACCUMULATING ON ADJACENT ROADWAYS.
- 14. THE CONTRACTOR SHALL BE REQUIRED TO CALL 1-800-424-5555 OR "811" A MINIMUM OF TWO WORKING DAYS PRIOR TO COMMENCING ANY EXCAVATION ACTIVITIES TO DETERMINE FIELD LOCATIONS OF ALL UNDERGROUND UTILITIES.
- 15. RUNOFF SHALL NOT BE ALLOWED TO DRAIN INTO THE STORM DRAINAGE SYSTEM DURING CONSTRUCTION. PROVIDE TEMPORARY PROTECTION FOR YARD DRAINS AS REQUIRED TO PREVENT RUNOFF FROM ENTERING THE STORM DRAINAGE SYSTEM.
- 16. THE TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL CONFORM TO THE CONSTRUCTION STORMWATER POLLUTION PREVENTION PLAN AND THE CONSTRUCTION STORMWATER NPDES AND STATE WASTE DISCHARGE GENERAL PERMIT. UPDATES MEASURES AS NECESSARY TO CONFORM TO SITE CONDITIONS.
- 17. ALL TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE COMPLETELY REMOVED PRIOR TO PROJECT COMPLETION.
- 18. REFER TO THE CONSTRUCTION STORMWATER POLLUTION PREVENTION PLAN AND NPDES PERMIT FOR ADDITIONAL REQUIREMENTS.

CONSTRUCTION SEQUENCE

- WORKS DEPARTMENT.

- 50. NOT USED.

- DAYS.
- ESTABLISHED.

DEMOLITION NOTES

- OTHER AREAS.

- SEE BMP C140.

RECOMMENDED CONSTRUCTION SEQUENCE: 44. CONDUCT A PRE-CONSTRUCTION MEETING WITH THE PUBLIC

45. POST "NOTICE OF CONSTRUCTION ACTIVITY" SIGN WITH NAME AND PHONE NUMBER OF THE CESCL.

46. FLAG OR FENCE CLEARING LIMITS AND SIGNIFICANT TREES.

47. INSTALL CATCH BASIN PROTECTION, IF REQUIRED. 48. GRADE AND INSTALL CONSTRUCTION ENTRANCE(S)

49. INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).

51. GRADE AND STABILIZE CONSTRUCTION ROADS.

52. CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DITCHES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.

53. MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE CITY OF PORT ORCHARD STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.

54. RELOCATE SURFACE WATER CONTROLS AND EROSION CONTROL MEASURES, OR INSTALL NEW MEASURES TO ENSURE THAT AS SITE CONDITIONS CHANGE THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CITY OF PORT ORCHARD EROSION AND SEDIMENT CONTROL STANDARDS.

55. COVER ALL AREAS THAT WILL BE IDLE FOR MORE THAN SEVEN DAYS DURING THE DRY SEASON OR TWO DAYS DURING THE WET SEASON WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING, OR EQUIVALENT.

56. STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN SEVEN

57. SEED OR SOD ANY AREAS TO REMAIN IDLE UNTIL SEED OR SOD IS

58. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES REMOVED, IF APPROPRIATE.

1. DEMOLITION: IT IS THE INTENT OF THE WORK UNDER THIS CONTRACT TO INCLUDE THE DEMOLITION OF ALL EXISTING BUILDINGS, PAVING, UTILITIES AND OTHER EXISTING SITE IMPROVEMENTS INCLUDING THEIR UNDERGROUND COMPONENTS AS INDICATED BY THIS DRAWING AND AS REQUIRED FOR NEW CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FULLY REVIEW THE SITE CONDITIONS AND TO CORRELATE THESE OBSERVATIONS WITH THE PROJECT WORK AND INCLUDE ALL NECESSAR` DEMOLITION, WHETHER SHOWN OR NOT, AND INCLUDE ALL SUCH COSTS IN THE BASE BID.

2. CLEARING: IT IS THE INTENT OF THE WORK UNDER THIS CONTRACT TO CONDUCT ALL CLEARING NECESSARY TO BE ABLE TO COMPLETE ALL THE WORK OF THIS PROJECT.

3. CONTRACTOR SHALL LEGALLY DISPOSE OF THE OWNER'S PROPERTY, ALL DEMOLISHED AND REMOVED MATERIALS, UNLESS INDICATED OTHERWISE.

4. IT IS THE RESPONSIBILITY OF THE CONTRACTOR FOR ENSURING THAT ALL UTILITIES HAVE BEEN DISCONNECTED PRIOR TO COMMENCING DEMOLITION. CONTRACTOR SHALL COORDINATE UTILITY DEMOLITION WITH NEW CONSTRUCTION.

5. COORDINATE POWER DEMOLITION WITH UTILITY PURVEYOR AND ELECTRICAL PLANS. CONTRACTOR TO ENSURE THAT DEMOLITION OF LINES WILL NOT COMPROMISE POWER TO

6. GAS DISCONNECTION BY UTILITY PURVEYOR CONTRACTOR TO COORDINATE WORK WITH P.S.E. CONTRACTOR TO DEMOLISH ABANDONED SERVICE. CONTRACTOR TO ENSURE THAT DEMOLITION OF GAS LINES WILL NOT IMPACT GAS SERVICE TO OTHER AREAS. NO DISRUPTION IN SERVICE ALLOWED.

7. ANY ABANDONED SIDE SEWER(S) SHALL BE PLUGGED OR CAPPED AS SHOWN OR AT PROPERTY LINE.

8. UTILITIES TO BE ABANDONED SHALL BE CAPPED. CONTRACTOR SHALL AS-BUILT LOCATION.

9. ALL MONUMENTS DISTURBED AS A RESULT OF CONSTRUCTION ACTIVITIES SHALL BE REPLACED PER WAC 332-120 AT NO ADDITIONAL COST TO THE OWNER. CONTRACTOR SHALL APPLY FOR PERMIT FROM THE DEPARTMENT OF NATURAL RESOURCES AS NECESSARY.

10. CONTRACTOR SHALL ENSURE THAT ADEQUATE FIRE PROTECTION ACCESS IS PROVIDED TO SITE AND EXISTING STRUCTURES TO REMAIN. CONTRACTOR SHALL ALSO PROVIDE ADEQUATE FIRE PROTECTION FOR THE NEW BUILDING.

11. COORDINATE DEMOLITION OF ASPHALT WITH TESC PLAN. MAINTAIN PAVEMENT AS A WORKING SURFACE AS LONG AS FEASIBLE. COORDINATE ACCESS ALONG SOUTH SIDE OF PROJECT LIMITS WITH CITY - PROVIDE A SAFE PEDESTRIAN ACCESS MEETING ADA REQUIREMENTS.

12. CONTRACTOR SHALL MINIMIZE DUST GENERATION ONSITE BY SPRINKLING THE SITE WITH WATER UNTIL SURFACE IS WET.

13. SAWCUT CONCRETE SIDEWALK AT THE NEAREST JOINT.



Project Title:

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET

TACOMA, WA 98402

<u>Project No.</u>

2190242.11

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<u>Revisions:</u>

Sheet Title:

NOTES AND DETAILS

<u>Designed by:</u> <u>Drawn by:</u> AMT AMT

<u>Checked by:</u> WJF

PW20-037

PW20-038

<u>Sheet No.</u>

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BAY STREET APARTMENTS SEC 26, TWP 24N, RGE 1E, W.M. PORT ORCHARD, WASHINGTON

- 2. SIZE THE BELOW-INLET GRATE DEVICE (BIGD) FOR THE STORMWATER STRUCTURE IT WILL SERVE.
- 4. THE RETRIEVAL SYSTEM MUST ALLOW FOR THE REMOVAL OF THE BIGD WITHOUT SPILLING THE
- 5. PERFORM MAINTENANCE IN ACCORDANCE WITH WSDOT STANDARD SPECIFICATION 8-01.3(5).
- 6. FILTERS SHALL BE INSPECTED AFTER EACH STORM EVENT AND CLEANED OR REPLACED WHEN 1/3 FULL
- 7. ALL FILTERS MUST BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.

NOTES:

- 1. THE FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL CUT TO THE LENGTH OF THE BARRIER TO AVOID USE OF JOINTS. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPLICED TOGETHER ONLY AT A SUPPORT POST WITH A MINIMUM SIX-INCH OVERLAP, AND BOTH ENDS SECURELY FASTENED TO THE POST.
- 2. THE FILTER FABRIC FENCE SHALL BE INSTALLED TO FOLLOW THE CONTOURS (WHERE FEASIBLE). THE FENCE POSTS SHALL BE SPACED A MAXIMUM OF SIX FEET APART AND DRIVEN SECURELY INTO THE GROUND (MINIMUM OF 30")
- 3. A TRENCH SHALL BE EXCAVATED, ROUGHLY EIGHT INCHES WIDE AND TWELVE INCHES DEEP. UPSLOPE AND ADJACENT TO THE WOOD POST TO ALLOW THE FILTER FABRIC TO BE BURIED.
- 4. WHEN STANDARD STRENGTH FILTER FABRIC IS USED, A WIRE MESH SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY-DUTY WIRE STAPLES AT LEAST ONE INCH LONG, TIE WIRES, OR HOG RINGS. THE WIRE SHALL EXTEND INTO THE TRENCH A MINIMUM OF FOUR INCHES AND SHALL NOT EXTEND MORE THAN THIRTY SIX INCHES ABOVE THE ORIGINAL GROUND SURFACE.
- 5. THE STANDARD STRENGTH FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE, AND TWENTY INCHES OF THE FABRIC SHALL BE EXTENDED INTO THE TRENCH. THE FABRIC SHALL NOT EXTEND MORE THAN THIRTY SIX INCHES ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING TREES.
- 6. WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS WITH ALL OTHER PROVISIONS OF STANDARD NOTE (S) APPLYING.
- 7. THE TRENCH SHALL BE BACKFILL WITH 3/4 INCH MINIMUM DIAMETER WASHED GRAVEL
- 8. FILTER FABRIC FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, BUT NOT BEFORE THE UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED.
- 9. FILTER FABRIC FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
- 10. CONTRIBUTING LENGTH TO FENCE SHALL NOT BE MORE THAN 100 FEET.
- 11. DO NOT INSTALL BELOW AN OUTLET PIPE OR WEIR.
- 12. DO NOT DRIVE OVER OR FILL OVER FILTER FABRIC FENCE.

CONSTRUCTION ENTRANCE NOTES

- MATERIAL SHALL BE 4-INCH TO 8-INCH QUARRY SPALLS AND MAY BE TOP-DRESSED WITH 1-INCH TO 3-INCH ROCK. (STATE STANDARD SPECIFICATIONS, SECTION 8-15.)
- 2. THE ROCK PAD SHALL BE AT LEAST 12 INCHES THICK AND 100 FEET LONG. WIDTH SHALL BE THE FULL WIDTH OF THE VEHICLE INGRESS AND EGRESS AREA.
- 3. ADDITIONAL ROCK SHALL BE ADDED PERIODICALLY TO MAINTAIN PROPER FUNCTION OF THE PAD.
- 4. IF THE PAD DOES NOT ADEQUATELY REMOVE THE MUD FROM THE VEHICLE WHEELS, THE WHEELS SHALL BE HOSED OFF BEFORE THE VEHICLE ENTERS A PAVED STREET. THE WASHING SHALL BE DONE ON AN AREA COVERED WITH CRUSHED ROCK AND WASH WATER SHALL DRAIN TO A SEDIMENT RETENTION FACILITY OR THROUGH A SILT FENCE.
- 5. GEOTEXTILE SHALL MEET THE FOLLOWING: GRAB TENSILE STRENGTH 200 SI MIN. GRAB TENSILE ELONGATION 30% MAX. MULLEN BURST STRENGTH 400 PSI MIN. AOS 2-45(U.S. STANDARD SIEVE)
- * USE OF EXISTING PAVEMENT IS PERMISSIBLE UNTIL PAVEMENT FAILS. ANY FAILED PAVEMENT MUST BE REPLACED PER UTILITY AND SURFACING SERIES C5.X.

PLASTIC COVERING NOTES

- 9-14.5(3).

- PERIOD.

HYDROSEEDING NOTES

- OTHERWISE.

- PLASTIC COVERING).

- SHOULD BE MINIMIZED, ESPECIALLY ADJACENT TO WATER BODIES AND WETLANDS.

1. PLASTIC SHEETING SHALL HAVE A MINIMUM THICKNESS OF 6 MILS AND SHALL MEET THE REQUIREMENTS OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION

2. COVERING SHALL BE INSTALLED AND MAINTAINED TIGHTLY IN PLACE BY USING SANDBAGS OR TIRES ON ROPES WITH A MAXIMUM 10-FOOT GRID SPACING IN ALL DIRECTIONS. ALL SEAMS SHALL BE TAPED OR WEIGHTED DOWN FULL LENGTH. SEAMS SHALL HAVE AT LEAST A 12 INCH OVERLAP.

3. CLEAR PLASTIC COVERING SHALL BE INSTALLED IMMEDIATELY ON AREAS SEEDED BETWEEN NOVEMBER 2 AND MARCH 31 AND REMAIN UNTIL VEGETATION IS FIRMLY ESTABLISHED.

4. WHEN COVERING IS USED ON UN-SEEDED SLOPES, IT SHALL BE KEPT IN PLACE UNTIL THE NEXT SEEDING

5. PLASTIC COVERING SHALL BE BURIED TWO FEET AT THE TOP OF SLOPES IN ORDER TO PREVENT SURFACE WATER FLOW BENEATH SHEETS.

6. PROPER MAINTENANCE OF COVERING INCLUDES, BUT IS NOT NECESSARILY LIMITED TO, REGULAR CHECKS FOR AND REPAIRS OF RIPS AND DISLODGED ENDS.

HYDROSEEDING SHALL BE APPLIED TO ALL DISTURBED NON-PAVED SURFACES, UNLESS NOTED

2. HYDROSEEDING SHALL CONSIST OF BENTGRASS AND SHALL BE APPLIED AT A RATE OF 120 LBS/ACRE. HYDROSEED SHALL INCLUDE A MINIMUM OF 1500 LBS/ACRE OF MULCH WITH 3% TACKIFIER. IT SHALL ALSO INCLUDE A 10-4-6 FERTILIZER AT A RATE OF 90 LBS/ACRE.

4. SEED BEDS PLANTED BETWEEN MAY 1 AND OCTOBER 31 WILL REQUIRE IRRIGATION AND OTHER MAINTENANCE AS NECESSARY TO FOSTER AND PROTECT THE ROOT STRUCTURE. FOR SEED BEDS PLANTED BETWEEN OCTOBER 31 AND APRIL 30. ARMORING OF THE SEED BED WILL BE NECESSARY TO FOSTER AND PROTECT THE ROOT STRUCTURE (E.G. GEOTEXTILES, JUTE MAT, CLEAR

BEFORE SEEDING, INSTALL ALL REQUIRED SURFACE RUNOFF CONTROL MEASURES SUCH AS GRADIENT TERRACES, INTERCEPTOR DIKES, SWALES, LEVEL SPREADERS, AND SEDIMENTATION BASINS. 7. THE SEED BED SHALL BE FIRM WITH A FAIRLY FINE SURFACE, FOLLOWING SURFACE ROUGHENING. PERFORM ALL OPERATIONS ACROSS OR AT RIGHT ANGLES TO THE SLOPE. 8. FERTILIZERS ARE TO BE USED ACCORDING TO SUPPLIERS RECOMMENDATIONS. AMOUNTS USED

9. HYDROSEED SHALL NOT BE APPLIED TO AREAS SUBJECT TO WEAR BY CONSTRUCTION TRAFFIC.

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Project Title:

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER **WEDDERMANN** 2302 A STREET

TACOMA, WA 98402

<u>Project No.</u>

2190242.11

Issue Set & Date:

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JUNE 17 2020





<u>Revisions:</u>

Sheet Title:

TESC AND DEMOLITION NOTES AND DETAILS

<u>Designed by:</u> <u>Drawn by:</u> AMT AMT

<u>Checked by:</u> WJF

<u>Sheet No.</u>





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Call Before You Dig. 8-1-1 or 1-800-424-5555 Underground Service (USA)

GRADING NOTES

1. THE EXISTING SITE SOILS REQUIRE PROPER COMPACTION PRIOR TO PLACEMENT OF PAVING AND UTILITY IMPROVEMENTS.

2. CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL PAVED AND NON-PAVED AREAS, AND SHALL GRADE ALL AREAS TO PRECLUDE PONDING OF SURFACE

3. THE SPOT ELEVATIONS INDICATED ON THESE PLANS REPRESENTS THE DESIGN TOP OF PAVEMENT UNLESS NOTED OTHERWISE.

4. VEGETATED AREAS LOCATED ADJACENT TO THE BUILDING SHALL BE 6" MIN BELOW FINISH FLOOR ELEVATION UNLESS NOTED OTHERWISE AND RECEIVE POST CONSTRUCTION SOIL PER (C3.01) 5. SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.

6. ADA ROUTES SHALL NOT EXCEED 5% IN THE DIRECTION OF TRAVEL UNLESS HANDRAILS ARE PROVIDED. LANDINGS AT NO MORE THAN 2% SHALL BE PROVIDED FOR EVERY 2.5 FEET OF RISE. CROSS SLOPE SHALL NOT EXCEED 2%.

7. FOR BEDDING AND BACKFILL STORM PIPE TRENCHES DETAIL SEE $\begin{pmatrix} 2 \\ C3.01 \end{pmatrix}$

8. ALL STORM PIPE SHALL BE LINED CORRUGATED POLYETHYLENE PIPE UNLESS OTHERWISE

9. SPOT ELEVATIONS AND CONTOURING SHOWN REPRESENT FINISH GRADE ELEVATIONS. SUBGRADE ELEVATIONS WILL VARY DEPENDING UPON TYPE OF SURFACING REQUIRED. REFER TO SPECIFICATIONS FOR TOPSOIL AND MULCH DEPTHS.

CONTRACTOR SHALL VERIFY ALL EXISTING AND PROPOSED GRADES PRIOR TO CONSTRUCTION ACTIVITIES. CONTACT ENGINEER IF PROPOSED GRADES ARE BELIEVED TO BE IN ERROR.

11. FINISHED GRADES ARE INDICATED. CONTRACTOR SHALL EXCAVATE AND FILL TO ACHIEVE

12. ALL FILL MATERIALS SHALL BE PLACED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT

13. ALL FOOTING DRAINS SHALL BE 4 INCH IN DIAMETER AND PLACED AROUND BUILDING. FOOTING DRAINS SHALL CONNECT INTO STORM DRAIN CATCH BASIN. FOOTING DRAINS PER DETAIL

14. ROOF DRAIN LINES SHALL BE 8-INCH PIPE MINIMUM AT A MINIMUM 0.5 PERCENT SLOPE UNLESS OTHERWISE INDICATED. CONNECT ALL DOWNSPOUTS AND ROOF LEADERS TO THE NEAREST ROOF DRAIN. ALL ROOF DRAINS SHALL BE CONNECTED TO A STORM DRAINAGE CATCH BASIN. PROVIDE AS-BUILTS TO OWNER. SEE ARCHITECTURAL AND MECHANICAL PLANS FOR EXACT LOCATIONS AND NUMBER OF DOWNSPOUTS.

STORM DRAIN CLEANOUTS PER DETAIL DOWNSPOUT CONNECTION PER DETAIL C3.01

15. ADJUST EXISTING STORM AND UTILITY LIDS FOR FINISHED GRADE.

16. PROVIDE BUILDING FLOOR SLAB SUPPORT PER

C3.01

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LEGEND

	RIGHT-OF-WAY / PROPERTY LINE
	- CENTERLINE
	BUILDING OUTLINE
123	- PROPOSED MINOR CONTOUR
123	- PROPOSED MAJOR CONTOUR
-123.45	SPOT ELEVATION
1.0% OR3:1	SLOPE
	– – RIDGE OR VALLEY
LP 123.45	LOW POINT

OW POINT HP 123.45 **HIGH POINT**



KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET

TACOMA, WA 98402

<u>Project No.</u>

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<u>Revisions:</u> Sheet Title:

GRADING AND DRAINAGE PLAN

C3.00

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<u>Designed by:</u> <u>Drawn by:</u> AMT

<u>Sheet No.</u>

<u>Checked by</u> W.IF

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AREA DRAIN SHALL BE NYLOPLAST DRAIN BASIN WITH PEDESTRIAN

PEDESTRIAN GRATES SHALL BE ADA-COMPLAINT AND MADE FROM

DRAIN BASIN TO BE CUSTOM MANUFACTURED ACCORDING TO

DRAINAGE CONNECTION STUB JOINT TIGHTNESS SHALL CONFORM

5. MANUFACTURER TO PROVIDE ADAPTERS FOR CONNECTING PIPES

6. DIRECT ALL PIPES SHOWN ON PLAN TO AREA DRAIN. DO NOT TEE



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C3.0

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- SEE MECHANICAL PLANS FOR BUILDING INFORMATION.

- 7. CONNECTION TO EXISTING GAS LINE BY PSE. CONTRACTOR SHALL PROVIDE NEW GAS LINE TRENCH AND BED AND BACKFILL FOR THE NEW PIPING. GAS METER AND GAS PIPE BY PSE.



WATER NOTES:

WATER SERVICE MATERIALS AND INSTALLATION SHALL BE PER PORT ORCHARD WATER DISTRICT STANDARDS.

2. THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS OF STATE AND LOCAL HEALTH AUTHORITIES.

3. ALL LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE AND IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXACT LOCATION TO AVOID DAMAGE OR DISTURBANCE.

4. THE MINIMUM COVER FOR WATER MAINS SHALL BE 3' AND SHALL BE MAINTAINED AT ALL POINTS.

5. ALL CUTTING, TAPPING, REMOVAL AND DISPOSAL OF ASBESTOS CEMENT PIPE SHALL BE IN CONFORMANCE WITH THE CURRENT POLICIES AND PROCEDURES OF THE KITSAP COUNTY HEALTH DEPARTMENT, THE PUGET SOUND AIR POLLUTION CONTROL AGENCY AND OTHER PUBLIC OFFICES WITH JURISDICTION IN THIS MATTER. AT THIS TIME THE POLICY IS THAT:

A. A PIPE SHALL BE "SNAPPED OFF" RATHER THAN BEING CUT WITH ABRASIVE SAW. B. ABANDONED PIPE SHALL BE BE LEFT IN THE TRENCH OR DISPOSED OF IN AN APPROVED METHOD.

6. CONTRACTOR SHALL COORDINATE AND PROVIDE ALL THE NECESSARY POWER/FIRE SYSTEM MONITORING WIRING.

7. FDC AND PIV ARE BUILDING MOUNTED. SEE MECHANICAL PLANS.

PAVING NOTES

- 1. THE EXISTING SITE SOILS REQUIRE PROPER COMPACTION PRIOR TO PLACEMENT OF PAVING AND UTILITY IMPROVEMENTS.
- 2. CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL PAVED AND NON-PAVED AREAS, AND SHALL GRADE ALL AREAS TO PRECLUDE PONDING OF SURFACE WATER.

FIRE PROTECTION NOTES

- . THE FIRE PROTECTION SYSTEM SHOWN ON THESE DRAWINGS, INCLUDING MATERIALS, SIZING, AND CONFIGURATION, IS SUBJECT TO APPROVAL BY A FIRE PROTECTION ENGINEER. THE DRAWINGS CONTAINING FIRE PROTECTION WORK, INCLUDING DETAIL DRAWINGS, SHALL BE STAMPED FOR APPROVAL BY A FIRE PROTECTION ENGINEER AND SUBMITTED TO THE FIRE MARSHAL FOR APPROVAL.
- 2. CONTRACTOR SHALL COMPLY WITH ALL NATIONAL FIRE PROTECTION ASSOCIATION REQUIREMENTS AS SET FORTH BY "NFPA 13", ITS AMENDMENTS, AND LOCAL REQUIREMENTS, INCLUDING BUN NOT LIMITED TO ACQUISITION OF ALL PERMITS.
- 3. THE FIRE PROTECTION SYSTEM SHALL BE INSTALLED BY A WASHINGTON STATE CERTIFIED LEVEL "U" CONTRACTOR IN ACCORDANCE WITH WAS 212-80-010.
- 4. CONTRACTOR'S BID SHALL REFLECT THESE CONDITIONS AND REQUIREMENTS.



<u>Project Title:</u>

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET

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<u>Project No.</u>

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WATER KEYNOTES

(1) 10" X 6" TEE (MJ X FL)

(2) THRUST BLOCK

(1) 4" 90° BEND (MJ) (1) THRUST BLOCK

2

4

5

ASPHALT SECTION

C4.01

	<u>A</u>
- M + M	\\$
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TER KEYNOTES	<u>Revisions:</u>
(1) 4" 90° BEND (MJ) (1) THRUST BLOCK	PAVING, UTILITIES
WET TAP CONNECTION TO EXISTING 10' WATER MAIN. (1) 10" X 6" TEE (MJ) (1) THRUST BLOCK (1) GATE VALVE (MJ X FL)	AND HORIZONTAL CONTROL PLAN
WET TAP CONNECTION TO EXISTING 10' WATER MAIN. (1) 10" X 4" TEE (MJ) (1) 4" WATER METER (1) THRUST BLOCK	<u>Designed by:</u> <u>Drawn by:</u> <u>Checked by:</u> AMT AMT WJF
(1) FIRE HYDRANT ASSEMBLY (1) 10" X 6" TEE (MJ X EL)	<u>Sheet No.</u>

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BAY STREET APARTMENTS SEC 26, TWP 24N, RGE 1E, W.M. **PORT ORCHARD, WASHINGTON**

CLASS 4000 CEMENT CONCRETE PAVEMENT CRUSHED SURFACING BASE COURSE PER WSDOT 9.03-9(3) COMPACTED SUBGRADE TO 95%

2. DUMMY JOINTS CONSISTING OF 3/16" BE 2" PREMOLDED JOINT MATERIAL SHALL

3. PROVIDE A TOOLED CONSTRUCTION JOINT AT MAXIMUM 6 FOOT INTERVAL.

4. THE CONTRACTOR SHALL EXPOSE EXISTING FILL, PROOF ROLL AND COMPACT TO 95%. IF YIELDING CONDITIONS ARE ENCOUNTERED, YIELDING MATERIALS SHALL BE REPLACED WITH IMPORTED STRUCTURAL FILL MATERIAL. THE DEPTH OF MATERIAL REPLACEMENT SHALL BE DETERMINED BY THE GEOTECHNICAL

CONCRETE (ONSITE)

(SEE ROADWAY PLANS FOR PAVING SECTION WITHIN THE RIGHT-OF-WAY)



<u>Sheet No.</u>

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KEYNOTES:

2

- VERTICAL CURB AND GUTTER PER CITY OF PORT ORCHARD STANDARD DRAWING 300
- PEDESTRIAN CURB PER CITY OF PORT ORCHARD STANDARD DETAIL 301
- COMMERCIAL DRIVEWAY PER CITY OF PORT ORCHARD
- STANDARD DETAIL 322 SIDEWALK PER CITY OF PORT ORCHARD STANDARD
- (4)DETAIL 340
- DETECTABLE WARNING PATTERN PER CITY OF PORT 5 ORCHARD STANDARD_DETAIL 341
- 6
- (7)SAWCUT EXISTING PAVEMENT
- 8 MATCH EXISTING SIDEWALK
- (9)MATCH EXISTING CURB
- (10)ADJUST UTILITY STRUCTURE TO FINISH GRADE
- TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02
- WITH HERRINGBONE GRATE PER B-30.50-03 TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02 (12)
- WITH COMBINATION INLET PER B-25.20-02 BEVELED END SECTION PER WSDOT STD PLAN
- (13) B-70.20-00



<u>Project Title:</u>

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET TACOMA, WA 98402

<u>Project No.</u>

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_	EXISTING GRADE AT ALIGNMENT (0).0' L)			
	EXISTING GRADE AT SAWCUT LINE	(26.0' L)			
	AT FLOWLINE (32.0' L)				
	CURB TRANSITI OFFSET	ON TO EXISTING			
		_			
			ΤΔ· 20+21 45		
		F States	I FV: 17 94		
		C	URB & GUTTER		
TYPE 1 ATIONS					
(10) ' RT					
		EXISTING GRADE AT SAWC	UT LINE (26.0' L)		
		- FINISHED GRADE AT FLOW	LINE (OFFSET VARIES)		
~					
0	.6 .6	88	7.6	7.5	
	18	18	1. -	1.	0
С Ш	LO L	19 19	о Ш	O E	
S	TA 18+75 TO STA 21+0 BAY STREET PROFILE)0 :	204	500 	
	VERTICAL SCALE: $1" = 2'$				

KEYNOTES:

- VERTICAL CURB AND GUTTER PER CITY OF PORT ORCHARD STANDARD DRAWING 300
- PEDESTRIAN CURB PER CITY OF PORT ORCHARD (2)
- STANDARD DETAIL 301 COMMERCIAL DRIVEWAY PER CITY OF PORT ORCHARD
- STANDARD DETAIL 322
- SIDEWALK PER CITY OF PORT ORCHARD STANDARD DETAIL 340
- DETECTABLE WARNING PATTERN PER CITY OF PORT ORCHARD STANDARD_DETAIL 341
- ASPHALT PAVING $\begin{pmatrix} 2 \\ CE 02 \end{pmatrix}$ (6)
- SAWCUT EXISTING PAVEMENT
- (8) MATCH EXISTING SIDEWALK
- MATCH EXISTING CURB

21+00

17 FG

С Ш

21+00

- ADJUST UTILITY STRUCTURE TO FINISH GRADE
- TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02
- WITH HERRINGBONE GRATE PER B-30.50-03 TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02
- WITH COMBINATION INLET PER B-25.20-02
- BEVELED END SECTION PER WSDOT STD PLAN (13) B-70.20-00



<u>Project Title:</u>

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET TACOMA, WA 98402

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<u>Revisions:</u> <u>Sheet Title:</u> **BAY STREET ROAD** AND DRAINAGE PLAN AND PROFILE <u>Drawn by:</u> <u>Checked by:</u> <u>Designed by:</u> AMT AMT WJF <u>Sheet No.</u>

> C5.0⁴ 10 of 12 Sheets



06/1//2020

WJF

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BAY STREET APARTMENTS SEC 26, TWP 24N, RGE 1E, W.M. **PORT ORCHARD, WASHINGTON**



NOTES:

- 1. PROVIDE PAVEMENT SECTION PER CITY OF PORT ORCHARD STANDARD DETAIL 400.
- 2. ALL DEPTHS SHOWN ARE COMPACTED DEPTHS.
- 3. THE CONTRACTOR SHALL EXPOSE EXISTING FILL, PROOF ROLL AND COMPACT TO 95%. IF YIELDING CONDITIONS ARE ENCOUNTERED, YIELDING MATERIALS SHALL BE REPLACED WITH IMPORTED STRUCTURAL FILL MATERIAL. THE DEPTH OF MATERIAL REPLACEMENT SHALL BE DETERMINED BY THE GEOTECHNICAL ENGINEER.





<u>Project Title:</u>

BAY STREET APARTMENTS

<u>Client:</u> KASHI 201 WILLIAMS LLC ALI KASHI C/O JENNIFER WEDDERMANN 2302 A STREET

TACOMA, WA 98402

<u>Project No.</u>

2190242.11

<u>Issue Set & Date:</u>

PERMIT SUBMITTAL

JUNE 17 2020





RECEIVED Permit Center JUL 10, 2020 City of Port Orchard **Community Development**

<u>Sheet Title:</u> **BAY STREET NOTES**

AND DETAILS

<u>Drawn by:</u> <u>Designed by:</u> AMT AMT

<u>Checked by:</u> WJF

<u>Sheet No.</u>

<u>Revisions:</u>

C5.03 12 of 12 Sheets

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					Machine Shap	wall	
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				6000	15	A-LINE (SR	(66) 12.0'±
/				T	1		edge
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<u>م</u>							
	:12+98.87						
		DESIGN D	ATA FOR	NON-NHS ROU	ITE		
DESIGN		A-L	INE	B-LIN	∎	C-LINE	
		SR-166 (BAY ST)	PORT ORCHA		DRIVEWAY	/
		URBAN PI	RINCIPAL	TWO-WAY S			
FUNCTIONAL		ARTE					
			ULASS 5			UNCONTROLLEI	
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ACCES TE DESI POST DESIG PERCE	IN VEHICLE INT TRUCKS	(DELTA		ATA S TANGENT	LENGTH	% SUPER	
	Image: Normal State Image: Normal State Image: Normal State Image: Normal State <td>(DELTA 24°23'00"</td> <td>CURVE DA RADIU: 1146.00</td> <td>ATA S TANGENT 247.60 07.04</td> <td>LENGTH 487.70'</td> <td>% SUPER</td> <td></td>	(DELTA 24°23'00"	CURVE DA RADIU: 1146.00	ATA S TANGENT 247.60 07.04	LENGTH 487.70'	% SUPER	
ACCES TE DESI POST DESIG PERCE	Image: Normal Sector	DELTA 24°23'00" 19°20'00"	CURVE DA RADIUS 1146.00 573.00	ATA S TANGENT 247.60 97.61	LENGTH 487.70' 193.35'	% SUPER - -	

DATE: __ CONCUR: ____ OLYMPIC REGION DEVELOPMENT SERVICES ENGINEER GEOMETRICS APPROVED WITHIN STATE HIGHWAY RIGHT-OF-WAY ONLY

Call Before You Dig. 8-1-1 or 1-800-424-5555 Underground Service (USA)

DATE: June 17, 2020 FILENAME: Q:\2019\2190242\10_CIV\CAD\EXHIBITS\WSDOT PFA\2190242-SH-PFA.dwg

BAY STREET REDEVELOPMENT F GOV'T LOTS 2 & 3, SEC 26, TWP 24N, RGE 1E, W.M. **PORT ORCHARD, WASHINGTON**

