

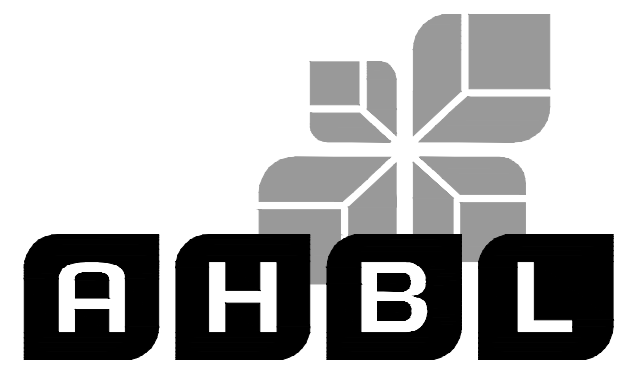
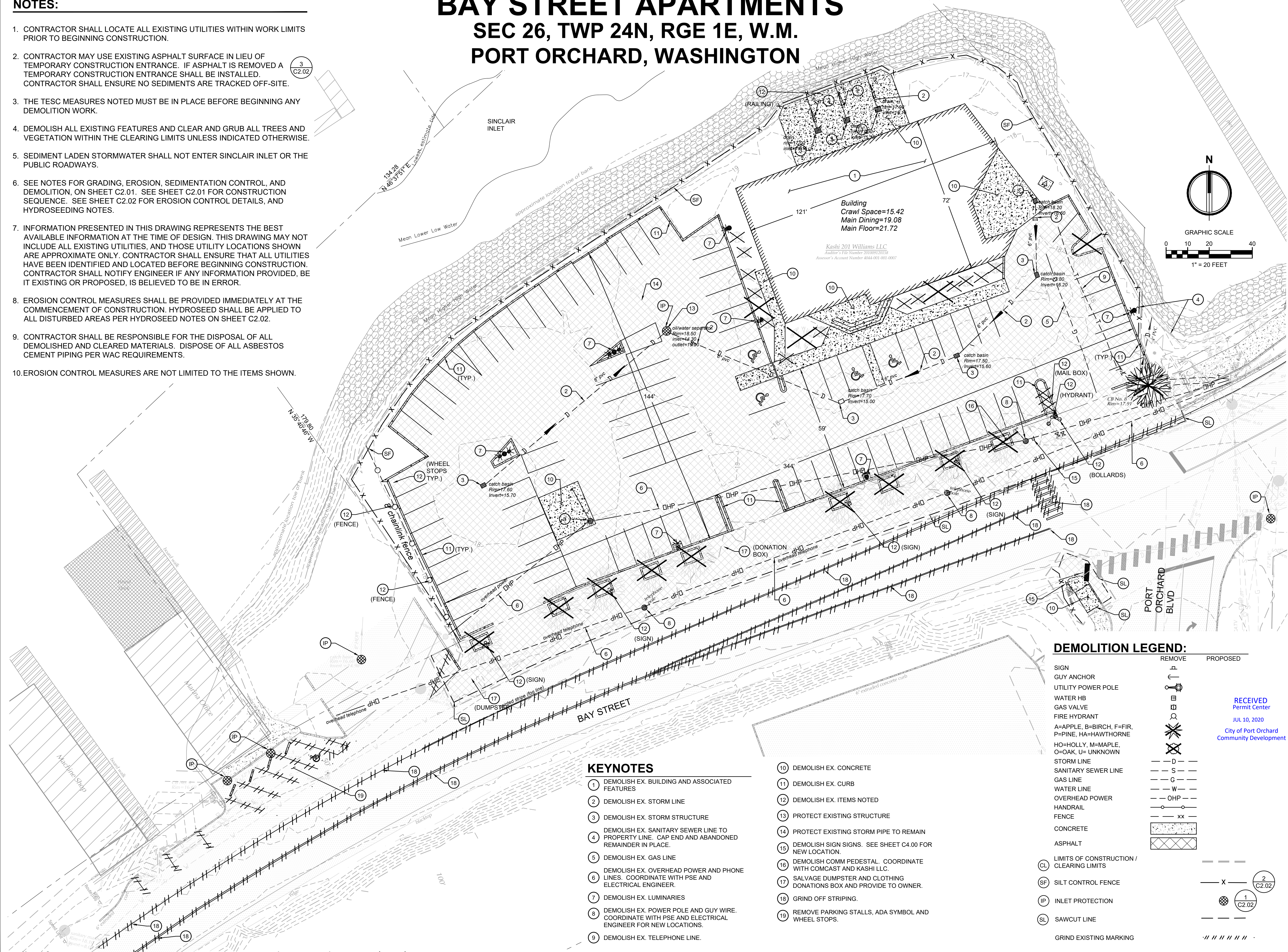
NOTES:

- CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES WITHIN WORK LIMITS PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR MAY USE EXISTING ASPHALT SURFACE IN LIEU OF TEMPORARY CONSTRUCTION ENTRANCE. IF ASPHALT IS REMOVED A TEMPORARY CONSTRUCTION ENTRANCE SHALL BE INSTALLED. CONTRACTOR SHALL ENSURE NO SEDIMENTS ARE TRACKED OFF-SITE.
- THE TESC MEASURES NOTED MUST BE IN PLACE BEFORE BEGINNING ANY DEMOLITION WORK.
- DEMOLISH ALL EXISTING FEATURES AND CLEAR AND GRUB ALL TREES AND VEGETATION WITHIN THE CLEARING LIMITS UNLESS INDICATED OTHERWISE.
- SEDIMENT LADEN STORMWATER SHALL NOT ENTER SINCLAIR INLET OR THE PUBLIC ROADWAYS.
- SEE NOTES FOR GRADING, EROSION, SEDIMENTATION CONTROL, AND DEMOLITION, ON SHEET C2.01. SEE SHEET C2.01 FOR CONSTRUCTION SEQUENCE. SEE SHEET C2.02 FOR EROSION CONTROL DETAILS, AND HYDROSEEDING NOTES.
- INFORMATION PRESENTED IN THIS DRAWING REPRESENTS THE BEST AVAILABLE INFORMATION AT THE TIME OF DESIGN. THIS DRAWING MAY NOT INCLUDE ALL EXISTING UTILITIES, AND THOSE UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. CONTRACTOR SHALL ENSURE THAT ALL UTILITIES HAVE BEEN IDENTIFIED AND LOCATED BEFORE BEGINNING CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER IF ANY INFORMATION PROVIDED, BE IT EXISTING OR PROPOSED, IS BELIEVED TO BE IN ERROR.
- EROSION CONTROL MEASURES SHALL BE PROVIDED IMMEDIATELY AT THE COMMENCEMENT OF CONSTRUCTION. HYDROSEED SHALL BE APPLIED TO ALL DISTURBED AREAS PER HYDROSEED NOTES ON SHEET C2.02.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF ALL DEMOLISHED AND CLEARED MATERIALS. DISPOSE OF ALL ASBESTOS CEMENT PIPING PER WAC REQUIREMENTS.
- EROSION CONTROL MEASURES ARE NOT LIMITED TO THE ITEMS SHOWN.

BAY STREET APARTMENTS

SEC 26, TWP 24N, RGE 1E, W.M.

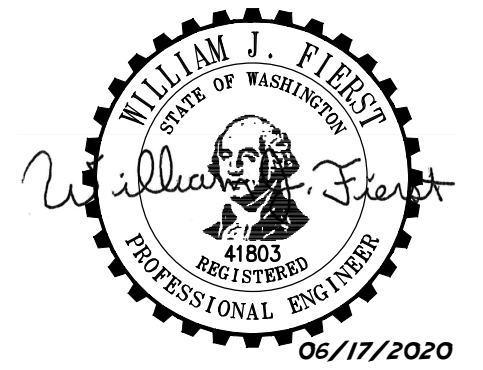
PORT ORCHARD, WASHINGTON



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Project Title:
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 C/O JENNIFER WEDDERMANN
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PERMIT SUBMITTAL
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DEMOLITION LEGEND:

- | | | |
|---|--------|----------|
| SIGN | REMOVE | PROPOSED |
| GUY ANCHOR | ↑ | P |
| UTILITY POWER POLE | ⊕ | ⊕ |
| WATER HB | ⊕ | ⊕ |
| GAS VALVE | ⊕ | ⊕ |
| FIRE HYDRANT | ⊕ | ⊕ |
| A=APPLE, B=BIRCH, F=FIR, P=PINE, HA=HAWTHORNE | ⊕ | ⊕ |
| HO=HOLLY, M=MAPLE, O=OAK, U= UNKNOWN | ⊕ | ⊕ |
| STORM LINE | --- | D |
| SANITARY SEWER LINE | --- | S |
| GAS LINE | --- | C |
| WATER LINE | --- | W |
| OVERHEAD POWER | --- | OHP |
| HANDRAIL | --- | xx |
| FENCE | --- | --- |
| CONCRETE | --- | --- |
| ASPHALT | --- | --- |
| CL LIMITS OF CONSTRUCTION / CLEARING LIMITS | --- | --- |
| SF SILT CONTROL FENCE | --- | --- |
| IP INLET PROTECTION | --- | --- |
| SL SAWCUT LINE | --- | --- |
| GRIND EXISTING MARKING | --- | --- |

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KEYNOTES

- | | |
|--|--|
| 1 DEMOLISH EX. BUILDING AND ASSOCIATED FEATURES | 10 DEMOLISH EX. CONCRETE |
| 2 DEMOLISH EX. STORM LINE | 11 DEMOLISH EX. CURB |
| 3 DEMOLISH EX. STORM STRUCTURE | 12 DEMOLISH EX. ITEMS NOTED |
| 4 DEMOLISH EX. SANITARY SEWER LINE TO PROPERTY LINE. CAP END AND ABANDONED REMAINDER IN PLACE. | 13 PROTECT EXISTING STRUCTURE |
| 5 DEMOLISH EX. GAS LINE | 14 PROTECT EXISTING STORM PIPE TO REMAIN |
| 6 DEMOLISH EX. OVERHEAD POWER AND PHONE LINES. COORDINATE WITH PSE AND ELECTRICAL ENGINEER. | 15 DEMOLISH SIGN SIGNS. SEE SHEET C4.00 FOR NEW LOCATION. |
| 7 DEMOLISH EX. LUMINARIES | 16 DEMOLISH COMM PEDESTAL. COORDINATE WITH COMCAST AND KASHI LLC. |
| 8 DEMOLISH EX. POWER POLE AND GUY WIRE. COORDINATE WITH PSE AND ELECTRICAL ENGINEER FOR NEW LOCATIONS. | 17 SALVAGE DUMPSTER AND CLOTHING DONATIONS BOX AND PROVIDE TO OWNER. |
| 9 DEMOLISH EX. TELEPHONE LINE. | 18 GRIND OFF STRIPING. |
| | 19 REMOVE PARKING STALLS, ADA SYMBOL AND WHEEL STOPS. |

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Revisions:
 Sheet Title:
TESC AND DEMOLITION PLAN
 Designed by: AMT Drawn by: AMT Checked by: WJF

Sheet No.
C2.00
 2 of 12 Sheets

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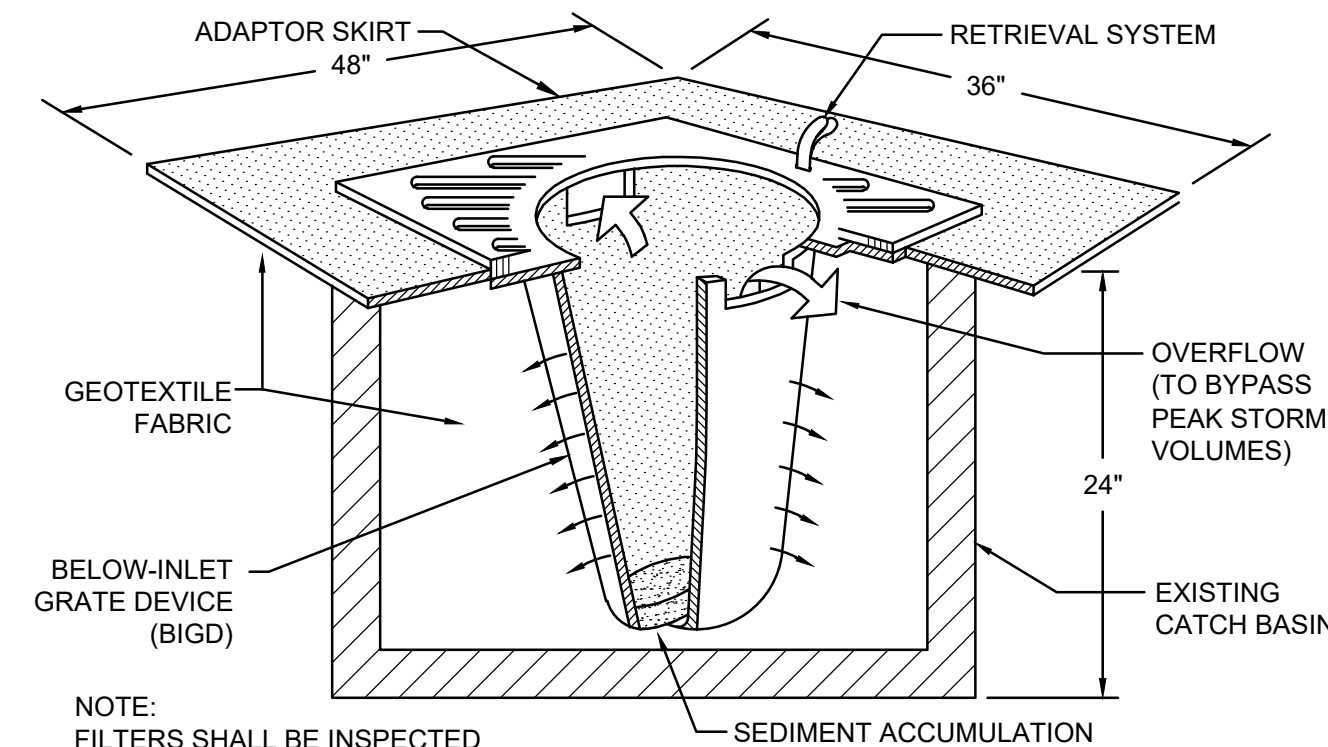
PORT ORCHARD, WASHINGTON

PLASTIC COVERING NOTES

1. PLASTIC SHEETING SHALL HAVE A MINIMUM THICKNESS OF 6 MILS AND SHALL MEET THE REQUIREMENTS OF THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS SECTION 9-14.5(3).
2. COVERING SHALL BE INSTALLED AND MAINTAINED TIGHTLY IN PLACE BY USING SANDBAGS OR TIRES ON ROPES WITH A MAXIMUM 10-FOOT GRID SPACING IN ALL DIRECTIONS. ALL SEAMS SHALL BE TAPED OR WEIGHTED DOWN FULL LENGTH. SEAMS SHALL HAVE AT LEAST A 12 INCH OVERLAP.
3. CLEAR PLASTIC COVERING SHALL BE INSTALLED IMMEDIATELY ON AREAS SEEDED BETWEEN NOVEMBER 1 AND MARCH 31 AND REMAIN UNTIL VEGETATION IS FIRMLY ESTABLISHED.
4. WHEN COVERING IS USED ON UN-SEEDED SLOPES, IT SHALL BE KEPT IN PLACE UNTIL THE NEXT SEEDING PERIOD.
5. PLASTIC COVERING SHALL BE BURIED TWO FEET AT THE TOP OF SLOPES IN ORDER TO PREVENT SURFACE WATER FLOW BENEATH SHEETS.
6. PROPER MAINTENANCE OF COVERING INCLUDES, BUT IS NOT NECESSARILY LIMITED TO, REGULAR CHECKS FOR AND REPAIRS OF RIPS AND DISLODGED ENDS.

HYDROSEEDING NOTES

1. HYDROSEEDING SHALL BE APPLIED TO ALL DISTURBED NON-PAVED SURFACES, UNLESS NOTED OTHERWISE.
2. HYDROSEEDING SHALL CONSIST OF BENTGRASS AND SHALL BE APPLIED AT A RATE OF 120 LBS/ACRE.
3. HYDROSEED SHALL INCLUDE A MINIMUM OF 1500 LBS/ACRE OF MULCH WITH 3% TACKIFIER. IT SHALL ALSO INCLUDE A 10-4-6 FERTILIZER AT A RATE OF 90 LBS/ACRE.
4. SEED BEDS PLANTED BETWEEN MAY 1 AND OCTOBER 31 WILL REQUIRE IRRIGATION AND OTHER MAINTENANCE AS NECESSARY TO FOSTER AND PROTECT THE ROOT STRUCTURE.
5. FOR SEED BEDS PLANTED BETWEEN OCTOBER 31 AND APRIL 30, ARMORING OF THE SEED BED WILL BE NECESSARY TO FOSTER AND PROTECT THE ROOT STRUCTURE (E.G. GEOTEXTILES, JUTE MAT, CLEAR PLASTIC COVERING).
6. BEFORE SEEDING, INSTALL ALL REQUIRED SURFACE RUNOFF CONTROL MEASURES SUCH AS GRADIENT TERRACES, INTERCEPTOR DIKES, SWALES, LEVEL SPREADERS, AND SEDIMENTATION BASINS.
7. THE SEED BED SHALL BE FIRM WITH A FAIRLY FINE SURFACE, FOLLOWING SURFACE ROUGHENING. PERFORM ALL OPERATIONS ACROSS OR AT RIGHT ANGLES TO THE SLOPE.
8. FERTILIZERS ARE TO BE USED ACCORDING TO SUPPLIERS RECOMMENDATIONS. AMOUNTS USED SHOULD BE MINIMIZED, ESPECIALLY ADJACENT TO WATER BODIES AND WETLANDS.
9. HYDROSEED SHALL NOT BE APPLIED TO AREAS SUBJECT TO WEAR BY CONSTRUCTION TRAFFIC.

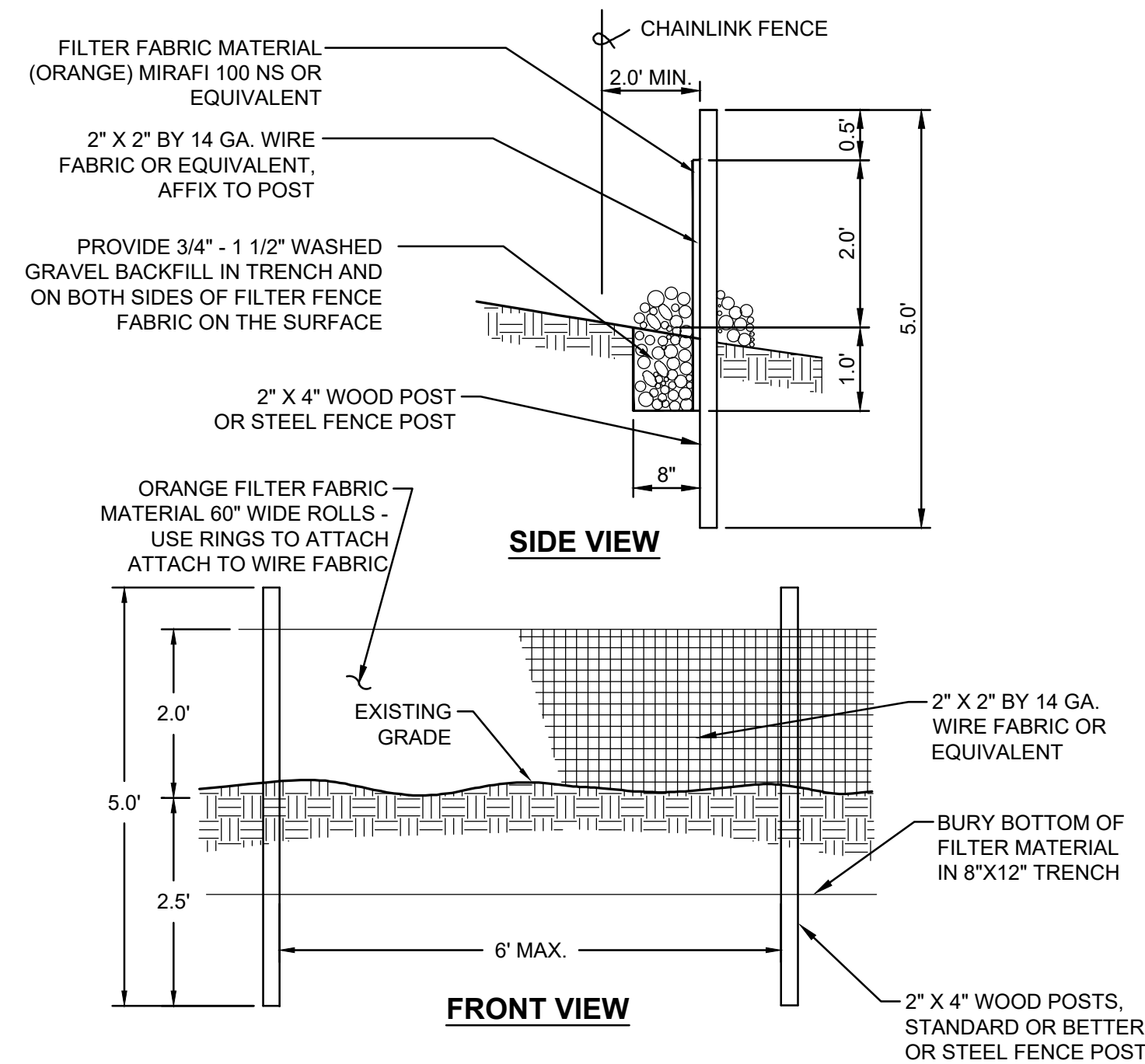


INLET PROTECTION NOTES

1. INLET PROTECTION PER WSDOT STANDARD PLAN I-40.20-00.
2. SIZE THE BELOW-INLET GRATE DEVICE (BIGD) FOR THE STORMWATER STRUCTURE IT WILL SERVE.
3. THE BIGD SHALL HAVE A BUILT-IN HIGH-FLOW RELIEF SYSTEM (OVERFLOW BYPASS).
4. THE RETRIEVAL SYSTEM MUST ALLOW FOR THE REMOVAL OF THE BIGD WITHOUT SPILLING THE COLLECTED MATERIAL.
5. PERFORM MAINTENANCE IN ACCORDANCE WITH WSDOT STANDARD SPECIFICATION 8-01.3(5).
6. FILTERS SHALL BE INSPECTED AFTER EACH STORM EVENT AND CLEANED OR REPLACED WHEN 1/3 FULL.
7. ALL FILTERS MUST BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED.

1 INLET SEDIMENT PROTECTION

NOT TO SCALE

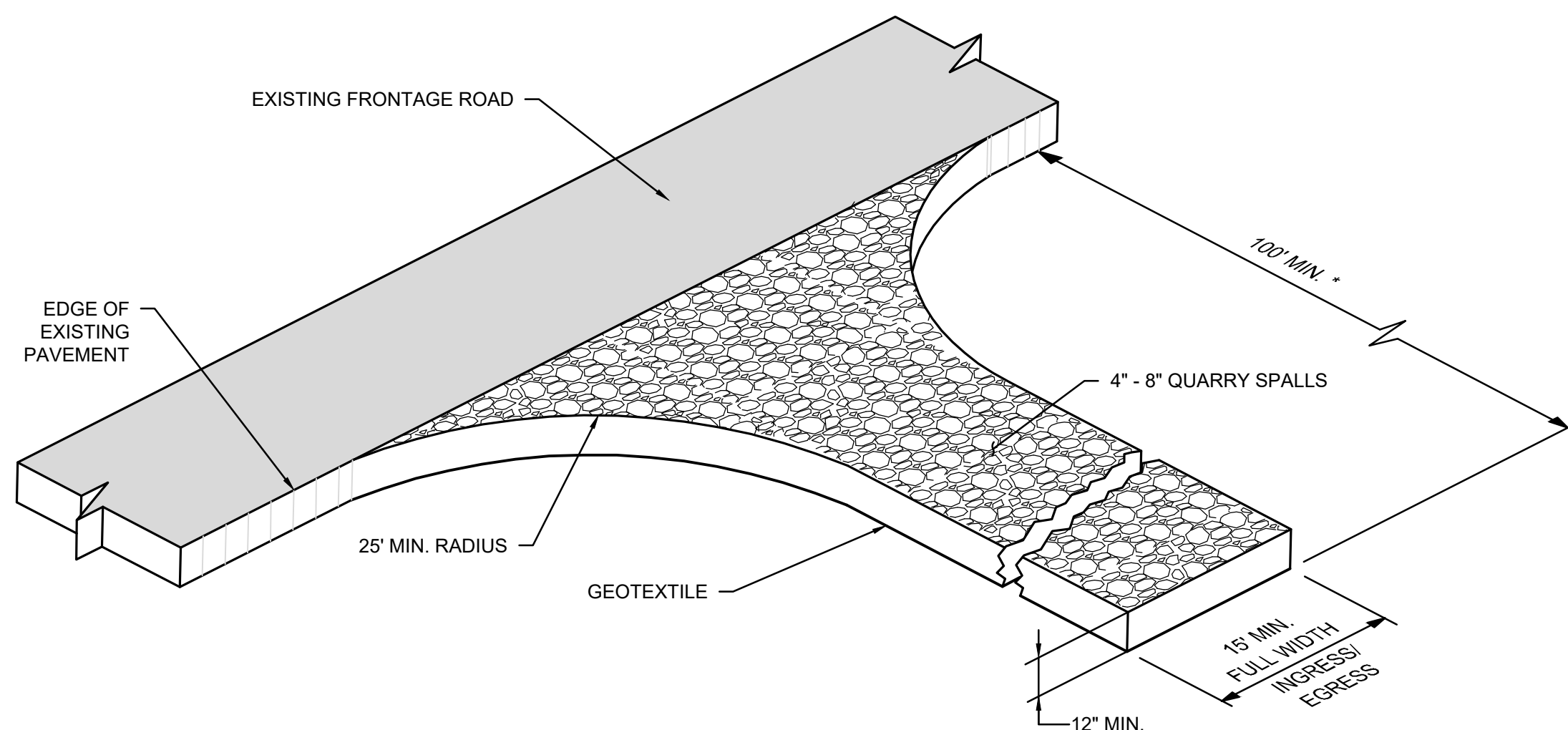


NOTES:

1. THE FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL CUT TO THE LENGTH OF THE BARRIER TO AVOID USE OF JOINTS. WHEN JOINTS ARE NECESSARY, FILTER CLOTH SHALL BE SPICED TOGETHER ONLY AT A SUPPORT POST WITH A MINIMUM SIX-INCH OVERLAP, AND BOTH ENDS SECURELY FASTENED TO THE POST.
2. THE FILTER FABRIC FENCE SHALL BE INSTALLED TO FOLLOW THE CONTOURS (WHERE FEASIBLE). THE FENCE POSTS SHALL BE SPACED A MAXIMUM OF SIX FEET APART AND DRIVEN SECURELY INTO THE GROUND (MINIMUM OF 30").
3. A TRENCH SHALL BE EXCAVATED, ROUGHLY EIGHT INCHES WIDE AND TWELVE INCHES DEEP, UPSLOPE AND ADJACENT TO THE WOOD POST TO ALLOW THE FILTER FABRIC TO BE BURIED.
4. WHEN STANDARD STRENGTH FILTER FABRIC IS USED, A WIRE MESH SUPPORT FENCE SHALL BE FASTENED SECURELY TO THE UPSLOPE SIDE OF THE POSTS USING HEAVY-DUTY WIRE STAPLES AT LEAST ONE INCH LONG, TIE WIRES, OR HOG RINGS. THE WIRE SHALL EXTEND INTO THE TRENCH A MINIMUM OF FOUR INCHES AND SHALL NOT EXTEND MORE THAN THIRTY SIX INCHES ABOVE THE ORIGINAL GROUND SURFACE.
5. THE STANDARD STRENGTH FILTER FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE, AND TWENTY INCHES OF THE FABRIC SHALL BE EXTENDED INTO THE TRENCH. THE FABRIC SHALL NOT EXTEND MORE THAN THIRTY SIX INCHES ABOVE THE ORIGINAL GROUND SURFACE. FILTER FABRIC SHALL NOT BE STAPLED TO EXISTING TREES.
6. WHEN EXTRA-STRENGTH FILTER FABRIC AND CLOSER POST SPACING ARE USED, THE WIRE MESH SUPPORT FENCE MAY BE ELIMINATED. IN SUCH A CASE, THE FILTER FABRIC IS STAPLED OR WIRED DIRECTLY TO THE POSTS WITH ALL OTHER PROVISIONS OF STANDARD NOTE (S) APPLYING.
7. THE TRENCH SHALL BE BACKFILL WITH 3/4 INCH MINIMUM DIAMETER WASHED GRAVEL.
8. FILTER FABRIC FENCES SHALL BE REMOVED WHEN THEY HAVE SERVED THEIR USEFUL PURPOSE, BUT NOT BEFORE THE UPSLOPE AREA HAS BEEN PERMANENTLY STABILIZED.
9. FILTER FABRIC FENCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND AT LEAST DAILY DURING PROLONGED RAINFALL. ANY REQUIRED REPAIRS SHALL BE MADE IMMEDIATELY.
10. CONTRIBUTING LENGTH TO FENCE SHALL NOT BE MORE THAN 100 FEET.
11. DO NOT INSTALL BELOW AN OUTLET PIPE OR WEIR.
12. DO NOT DRIVE OVER OR FILL OVER FILTER FABRIC FENCE.

2 SILT FENCE

NOT TO SCALE



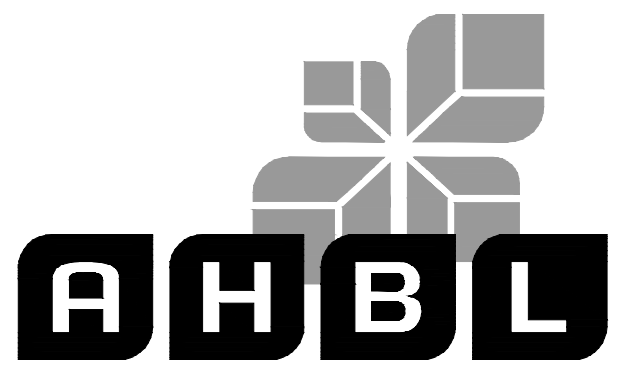
CONSTRUCTION ENTRANCE NOTES:

1. MATERIAL SHALL BE 4-INCH TO 8-INCH QUARRY SPALLS AND MAY BE TOP-DRESSED WITH 1-INCH TO 3-INCH ROCK. (STATE STANDARD SPECIFICATIONS, SECTION 8-15.)
 2. THE ROCK PAD SHALL BE AT LEAST 12 INCHES THICK AND 100 FEET LONG. WIDTH SHALL BE THE FULL WIDTH OF THE VEHICLE INGRESS AND EGRESS AREA.
 3. ADDITIONAL ROCK SHALL BE ADDED PERIODICALLY TO MAINTAIN PROPER FUNCTION OF THE PAD.
 4. IF THE PAD DOES NOT ADEQUATELY REMOVE THE MUD FROM THE VEHICLE WHEELS, THE WHEELS SHALL BE HOSED OFF BEFORE THE VEHICLE ENTERS A PAVED STREET. THE WASHING SHALL BE DONE ON AN AREA COVERED WITH CRUSHED ROCK AND WASH WATER SHALL DRAIN TO A SEDIMENT RETENTION FACILITY OR THROUGH A SILT FENCE.
 5. GEOTEXTILE SHALL MEET THE FOLLOWING: GRAB TENSILE STRENGTH 200 SI MIN. GRAB TENSILE ELONGATION 30% MAX. MULLEN BURST STRENGTH 400 PSI MIN. AOS 2-45(U.S. STANDARD SIEVE)
- * USE OF EXISTING PAVEMENT IS PERMISSIBLE UNTIL PAVEMENT FAILS. ANY FAILED PAVEMENT MUST BE REPLACED PER UTILITY AND SURFACING SERIES C5.X.

3 CONSTRUCTION ENTRANCE

NOT TO SCALE

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BAY STREET APARTMENTS

Client:

KASHI 201 WILLIAMS LLC
ALI KASHI
C/O JENNIFER WEDDERMANN
2302 A STREET
TACOMA, WA 98402

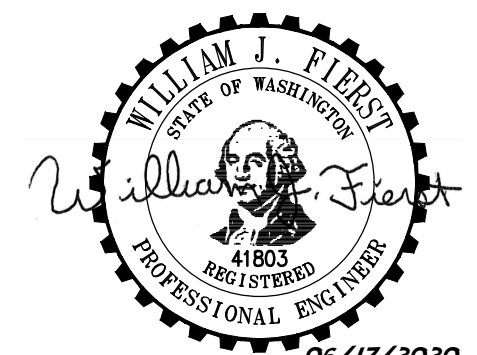
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TESC AND DEMOLITION NOTES AND DETAILS

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C2.02

4 of 12 Sheets

BAY STREET APARTMENTS

SEC 26, TWP 24N, RGE 1E, W.M.

PORT ORCHARD, WASHINGTON

GRADING NOTES

- THE EXISTING SITE SOILS REQUIRE PROPER COMPACTION PRIOR TO PLACEMENT OF PAVING AND UTILITY IMPROVEMENTS.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL PAVED AND NON-PAVED AREAS, AND SHALL GRADE ALL AREAS TO PRECLUDE PONDING OF SURFACE WATER.
- THE SPOT ELEVATIONS INDICATED ON THESE PLANS REPRESENTS THE DESIGN TOP OF PAVEMENT UNLESS NOTED OTHERWISE.
- VEGETATED AREAS LOCATED ADJACENT TO THE BUILDING SHALL BE 6" MIN BELOW FINISH FLOOR ELEVATION UNLESS NOTED OTHERWISE AND RECEIVE POST CONSTRUCTION SOIL PER 5 C3.01
- SEE LANDSCAPE PLANS FOR ADDITIONAL INFORMATION.
- ADA ROUTES SHALL NOT EXCEED 5% IN THE DIRECTION OF TRAVEL UNLESS HANDRAILS ARE PROVIDED. LANDINGS AT NO MORE THAN 2% SHALL BE PROVIDED FOR EVERY 2.5 FEET OF RISE. CROSS SLOPE SHALL NOT EXCEED 2%.
- FOR BEDDING AND BACKFILL STORM PIPE TRENCHES DETAIL SEE 2 C3.01
- ALL STORM PIPE SHALL BE LINED CORRUGATED POLYETHYLENE PIPE UNLESS OTHERWISE NOTED.
- SPOT ELEVATIONS AND CONTOURING SHOWN REPRESENT FINISH GRADE ELEVATIONS. SUBGRADE ELEVATIONS WILL VARY DEPENDING UPON TYPE OF SURFACING REQUIRED. REFER TO SPECIFICATIONS FOR TOPSOIL AND MULCH DEPTHS.
- CONTRACTOR SHALL VERIFY ALL EXISTING AND PROPOSED GRADES PRIOR TO CONSTRUCTION ACTIVITIES. CONTACT ENGINEER IF PROPOSED GRADES ARE BELIEVED TO BE IN ERROR.
- FINISHED GRADES ARE INDICATED. CONTRACTOR SHALL EXCAVATE AND FILL TO ACHIEVE PROPER SUBGRADES.
- ALL FILL MATERIALS SHALL BE PLACED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT.
- ALL FOOTING DRAINS SHALL BE 4 INCH IN DIAMETER AND PLACED AROUND BUILDING. FOOTING DRAINS SHALL CONNECT INTO STORM DRAIN CATCH BASIN. FOOTING DRAINS PER DETAIL 6 C3.01
- ROOF DRAIN LINES SHALL BE 8-INCH PIPE MINIMUM AT A MINIMUM 0.5 PERCENT SLOPE UNLESS OTHERWISE INDICATED. CONNECT ALL DOWNSPOUTS AND ROOF LEADERS TO THE NEAREST ROOF DRAIN. ALL ROOF DRAINS SHALL BE CONNECTED TO A STORM DRAINAGE CATCH BASIN. PROVIDE AS-BUILTS TO OWNER. SEE ARCHITECTURAL AND MECHANICAL PLANS FOR EXACT LOCATIONS AND NUMBER OF DOWNSPOUTS.
- STORM DRAIN CLEANOUTS PER DETAIL 3 C3.01
- DOWNSPOUT CONNECTION PER DETAIL 1 C3.01
- ADJUST EXISTING STORM AND UTILITY LIDS FOR FINISHED GRADE.
- PROVIDE BUILDING FLOOR SLAB SUPPORT PER 3 C4.01

STORM STRUCTURE TABLE

STRUCTURE NAME	STRUCTURE DETAILS
AD 01 AREA DRAIN N 202088.99 E 1193598.62	RIM = 20.00 IE = 17.72 (6" E) IE = 17.72 (6" SW)
AD 02 AREA DRAIN N 202112.86 E 1193666.30	RIM = 20.19 IE = 18.44 (6" E) IE = 18.44 (6" W)
AD 03 AREA DRAIN N 202122.38 E 1193691.40	RIM = 20.11 IE = 18.71 (6" W)

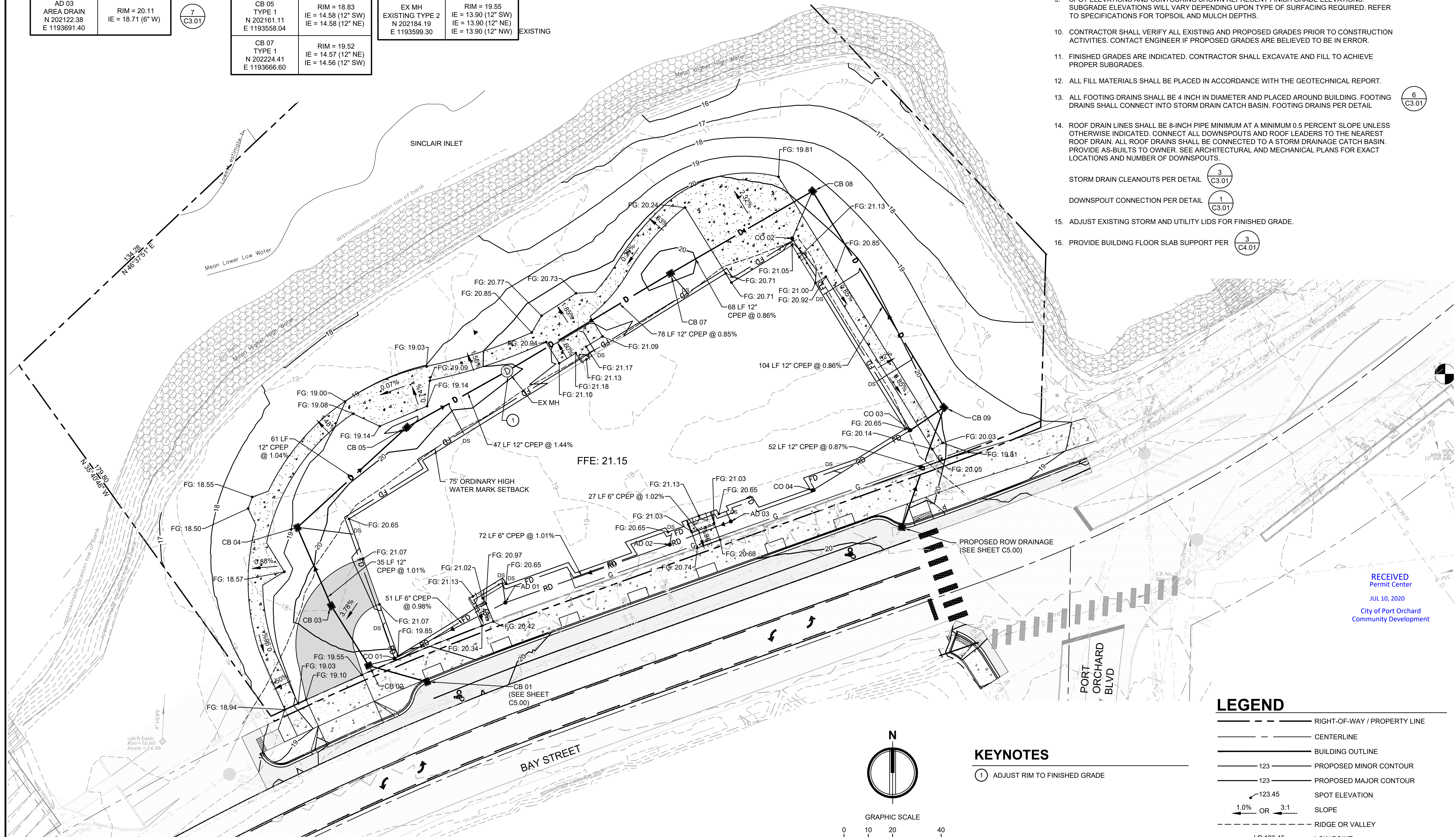
STORM STRUCTURE TABLE

STRUCTURE NAME	STRUCTURE DETAILS
CB 03 TYPE 1 N 202087.66 E 1193526.98	RIM = 20.47 IE = 15.57 (12" NW) IE = 15.57 (12" SE)
CB 04 TYPE 1 N 202119.86 E 1193512.98	RIM = 19.12 IE = 15.21 (12" SE) IE = 15.21 (12" NE)
CB 05 TYPE 1 N 202161.11 E 1193558.04	RIM = 18.83 IE = 14.58 (12" SW) IE = 14.58 (12" NE)
CB 07 TYPE 1 N 202224.41 E 1193666.60	RIM = 19.52 IE = 14.57 (12" NE) IE = 14.56 (12" SW)

STORM STRUCTURE TABLE

STRUCTURE NAME	STRUCTURE DETAILS
CB 08 TYPE 1 N 202258.28 E 1193725.05	RIM = 19.39 IE = 15.15 (12" SE) IE = 15.15 (12" SW) IE = 17.34 (6" SW)
CB 09 TYPE 1 N 202169.32 E 1193779.14	RIM = 19.81 IE = 16.05 (12" NW) IE = 16.47 (6" SW) IE = 16.05 (12" S)
EX MH EXISTING TYPE 2 N 202184.19 E 1193599.30	RIM = 19.55 IE = 13.90 (12" SW) IE = 13.90 (12" NE) IE = 13.90 (12" NW)

EXISTING

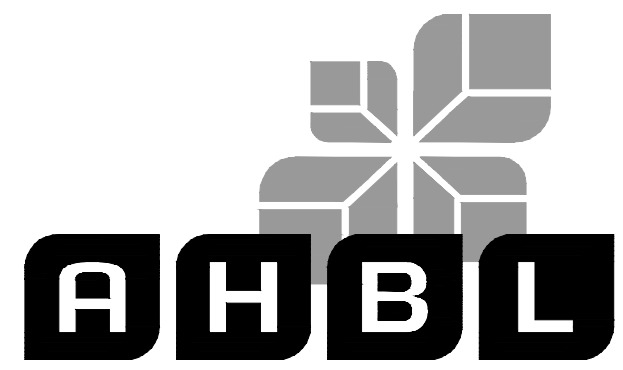


KEYNOTES

- ADJUST RIM TO FINISHED GRADE

LEGEND

	RIGHT-OF-WAY / PROPERTY LINE
	CENTERLINE
	BUILDING OUTLINE
	PROPOSED MINOR CONTOUR
	PROPOSED MAJOR CONTOUR
	SPOT ELEVATION
	SLOPE
	RIDGE OR VALLEY
	LOW POINT
	HIGH POINT



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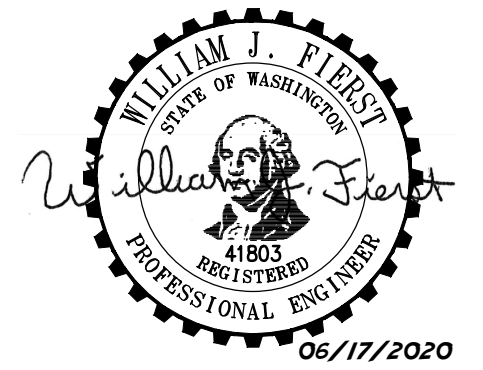
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Sheet No.

C3.00

5 of 12 Sheets

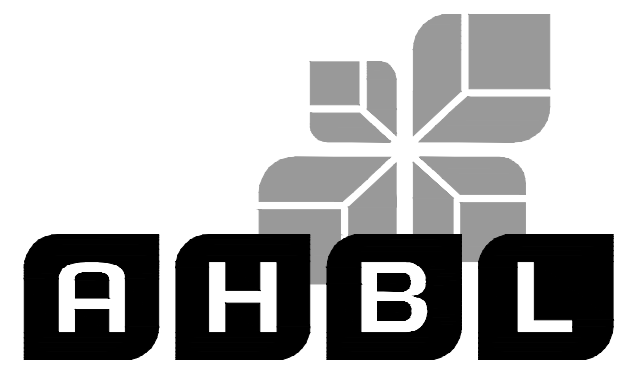
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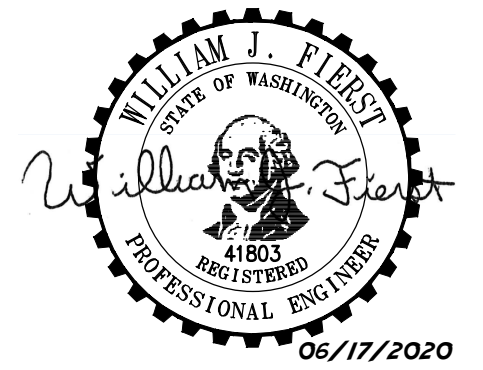
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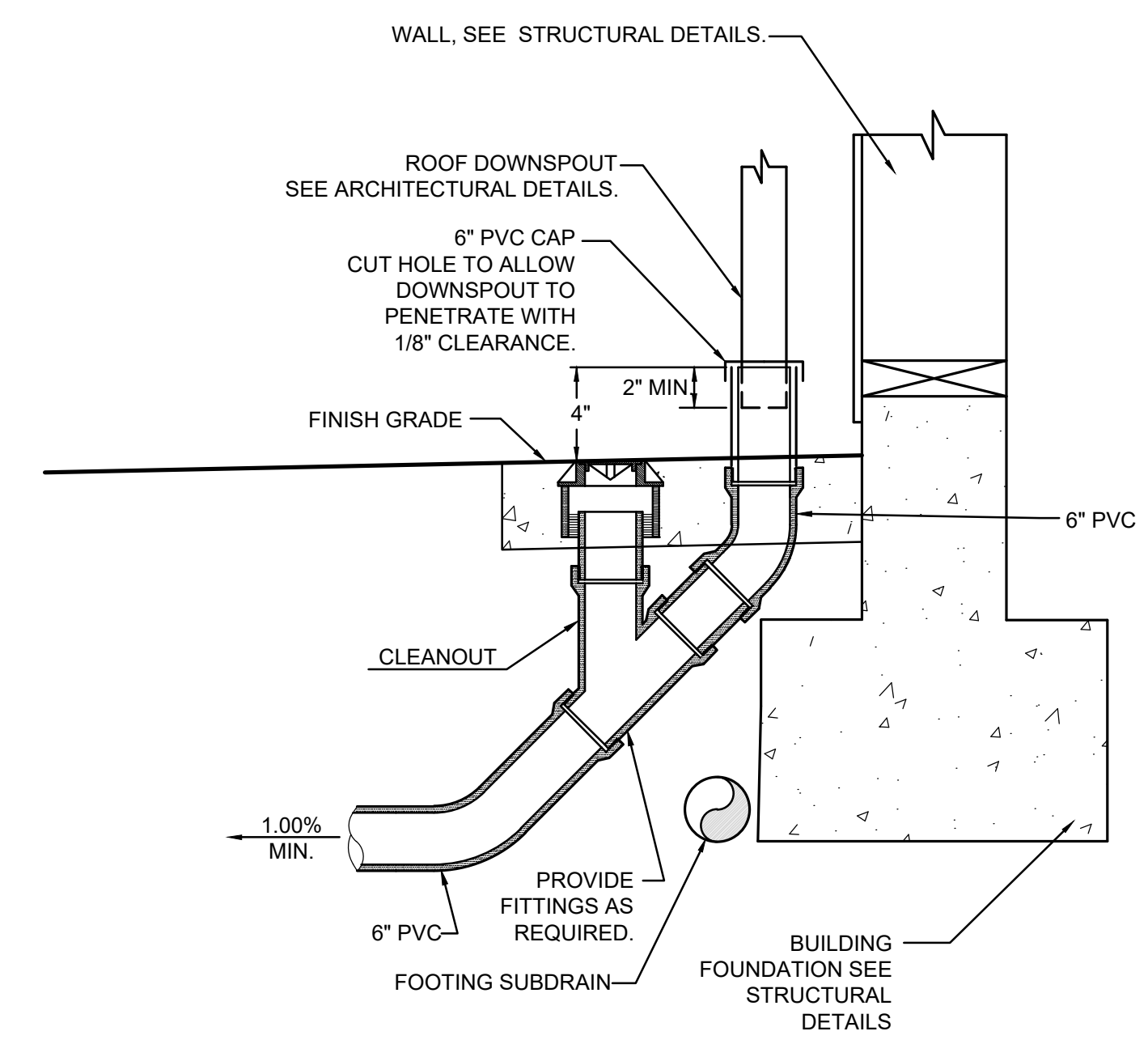
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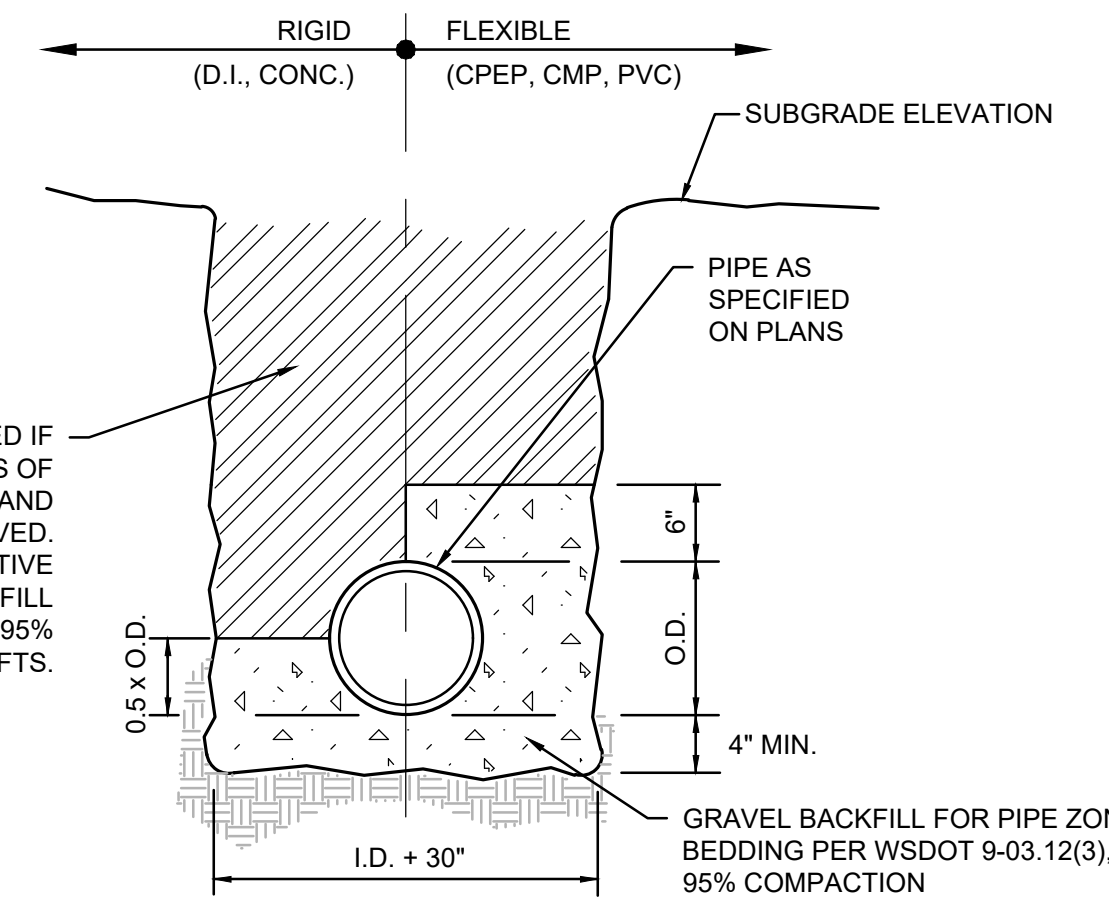
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1 DOWNSPOUT CONNECTION
 NOT TO SCALE



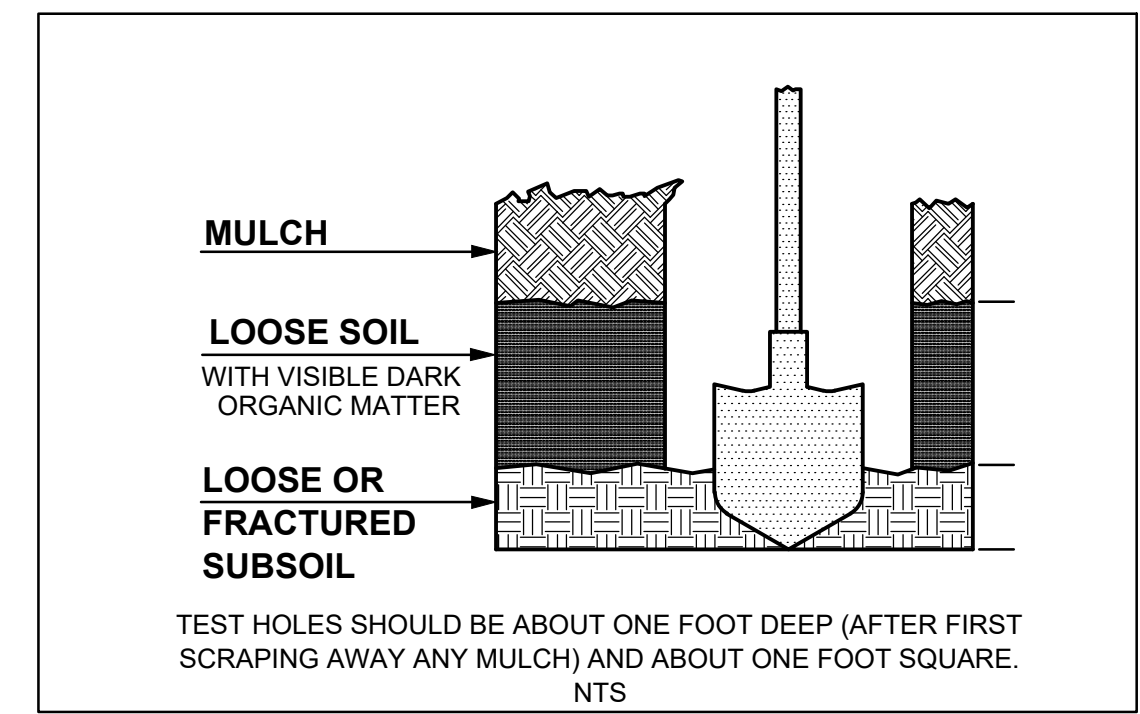
2 BEDDING AND BACKFILL PIPE TRENCHES
 NOT TO SCALE

- PREPARATION METHOD**
1. TILL THE SUB GRADE SOIL TO AN 8-INCH DEPTH; THOSE AREAS SUBJECTED TO VEHICLE TRAFFIC SHALL BE TILLED TO A DEPTH OF 12 INCHES. DO NOT SCARIFY OR TILL WITHIN THE DRIP LINE OF EXISTING TREES THAT WILL BE RETAINED.
 2. PLACE 3 INCHES OF COMPOST MATERIAL AND TILL INTO 5 INCHES OF SOIL (A TOTAL AMENDED DEPTH OF ABOUT 9.5 INCHES, FOR A SETTLED DEPTH OF 8 INCHES).
 3. RAKE THE AREA SMOOTH AND REMOVE SURFACE ROCKS LARGER THAN 1 INCH IN DIAMETER.
 4. PLACE 2 INCHES OF AA COMPOST AND RAKE.

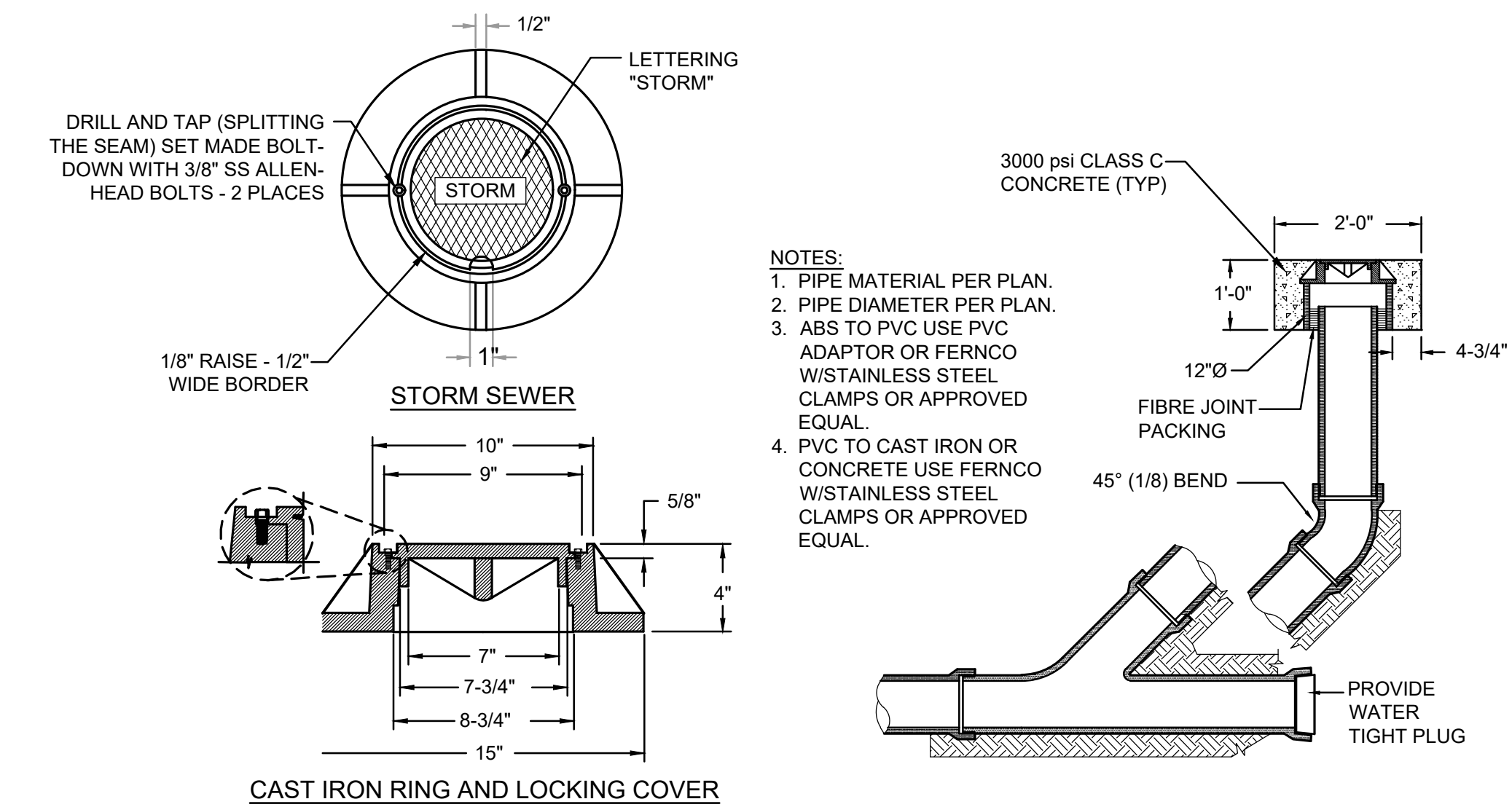
- COMPOST AMENDMENT QUALITY STANDARDS**
1. THE ORGANIC CONTENT FOR "PRE-APPROVED" AMENDMENT RATES CAN BE MET ONLY BY USING COMPOST THAT MEETS THE DEFINITION FOR "COMPOSTED MATERIALS" IN WAC 173-350-220, AVAILABLE ONLINE AT <http://www.ecy.wa.gov/programs/swfa/compost/>
 2. THE COMPOST MUST ALSO HAVE AN ORGANIC MATTER CONTENT OF 35 TO 65 PERCENT, CARBON TO NITROGEN RATIO BELOW 25:1.
 3. THE CARBON TO NITROGEN RATIO MAY BE AS HIGH AS 35:1 FOR PLANTINGS COMPOSED ENTIRELY OF PLANTS NATIVE TO THE PUGET SOUND LOWLANDS REGION.

- PREPARATION FOR INSPECTION**
- PROVIDE SIX HOLES PER SITE; ALL HOLES SHALL BE EVENLY PLACED THROUGHOUT THE SITE TO DEMONSTRATE AUGMENTATION HAS BEEN COMPLETED.
 - TEST HOLES SHALL BE A MINIMUM OF 1 FOOT DEEP AND 1 FOOT SQUARE.
 - CONTRACTOR SHALL PROVIDE DOCUMENTS FOR THE AA COMPOST VOLUME DELIVERED TO THE SITE TO CONFIRM COMPOST ORIGIN AND QUALITY.

- INSPECTION VERIFICATION**
- 2 INCHES OF COMPOST ON ALL DISTURBED SURFACES.
 - LOOSE SOIL FOR 8 INCHES WITH VISIBLE DARK ORGANIC MATTER.
 - LOOSE FRACTURED SUB SOILS SHOULD NOT BE EASY TO PENETRATE WITH A SHOVEL 4 INCHES BELOW THE 8 INCHES. TOTAL DEPTH OF AUGMENTED AND LOOSENEED SOIL SHALL BE 12 INCHES.

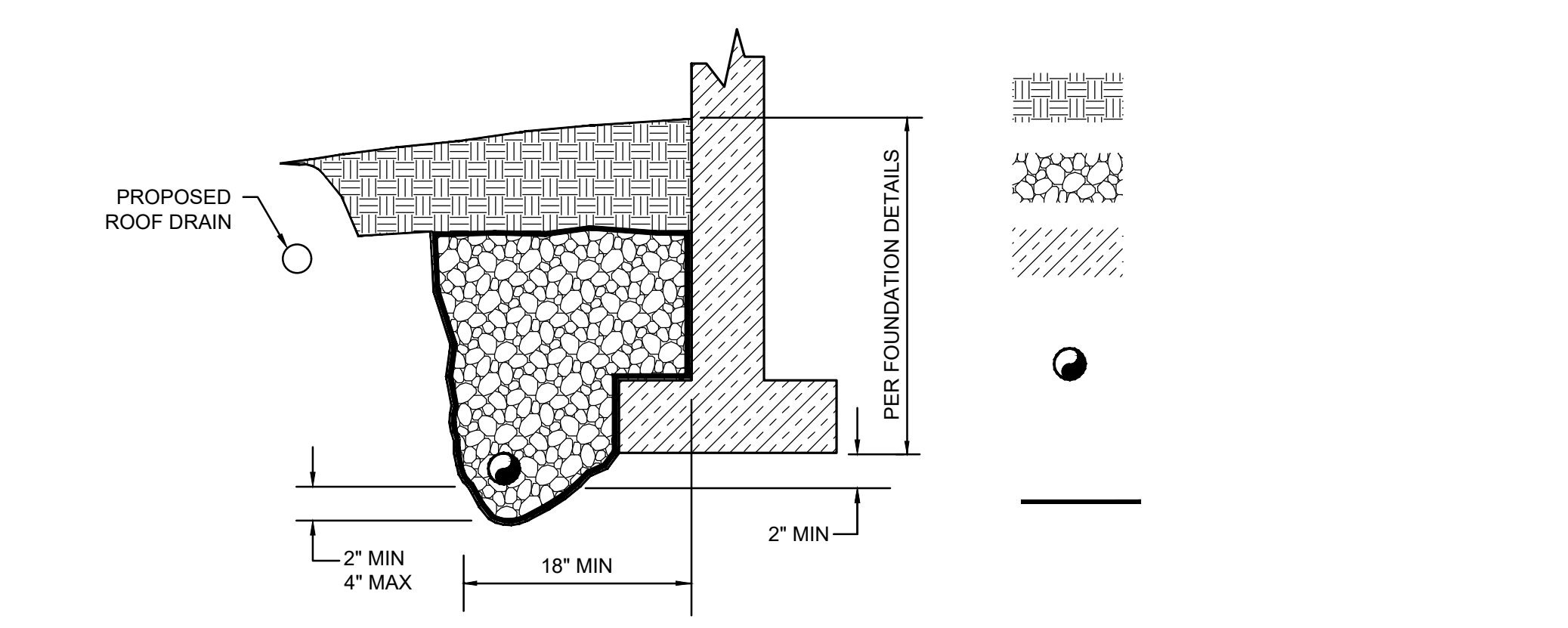


5 POST CONSTRUCTION SOIL QUALITY AND DEPTH
 NOT TO SCALE

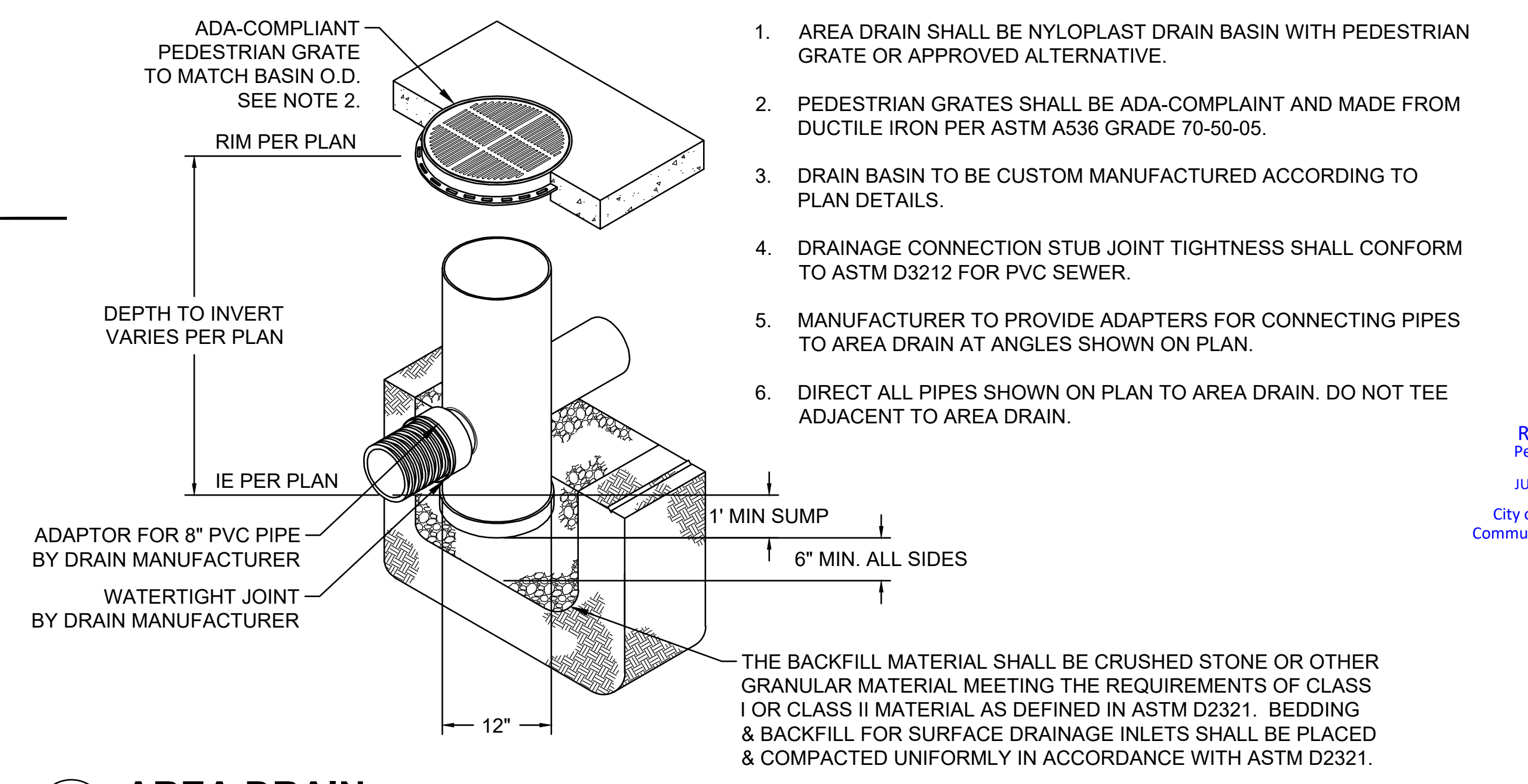


3 STORM DRAIN CLEANOUT
 NOT TO SCALE

4 NOT USED
 NOT TO SCALE



6 FOOTING DRAIN
 NOT TO SCALE



7 AREA DRAIN
 NOT TO SCALE

1. AREA DRAIN SHALL BE NYLOPLAST DRAIN BASIN WITH PEDESTRIAN GRATE OR APPROVED ALTERNATIVE.
2. PEDESTRIAN GRATES SHALL BE ADA-COMPLIANT AND MADE FROM DUCTILE IRON PER ASTM A536 GRADE 70-50-05.
3. DRAIN BASIN TO BE CUSTOM MANUFACTURED ACCORDING TO PLAN DETAILS.
4. DRAINAGE CONNECTION STUB JOINT TIGHTNESS SHALL CONFORM TO ASTM D3212 FOR PVC SEWER.
5. MANUFACTURER TO PROVIDE ADAPTERS FOR CONNECTING PIPES TO AREA DRAIN AT ANGLES SHOWN ON PLAN.
6. DIRECT ALL PIPES SHOWN ON PLAN TO AREA DRAIN. DO NOT TEE ADJACENT TO AREA DRAIN.

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Revisions:

Sheet Title:
GRADING AND DRAINAGE NOTES AND DETAILS

Designed by: AMT Drawn by: AMT Checked by: WJF

Sheet No.

C3.01
 6 of 12 Sheets

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BAY STREET APARTMENTS

SEC 26, TWP 24N, RGE 1E, W.M.

PORT ORCHARD, WASHINGTON

GENERAL NOTES:

- SEE MECHANICAL PLANS FOR BUILDING INFORMATION.
- WATER METER PER CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 863.
- THRUST BLOCKS PER CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 803 A.
- TRENCH AND BACKFILL PER CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 900.
- PIPE CONNECTIONS PER CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 901.
- SEWER CLEANOUT PER CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 960.
- CONNECTION TO EXISTING GAS LINE BY PSE. CONTRACTOR SHALL PROVIDE NEW GAS LINE TRENCH AND BED AND BACKFILL FOR THE NEW PIPING, GAS METER AND GAS PIPE BY PSE.
- SEE CITY OF PORT ORCHARD STANDARD DETAIL, DRAWING NUMBER 800-B, FOR INFORMATION ON UTILITY SEPARATION.

WATER NOTES:

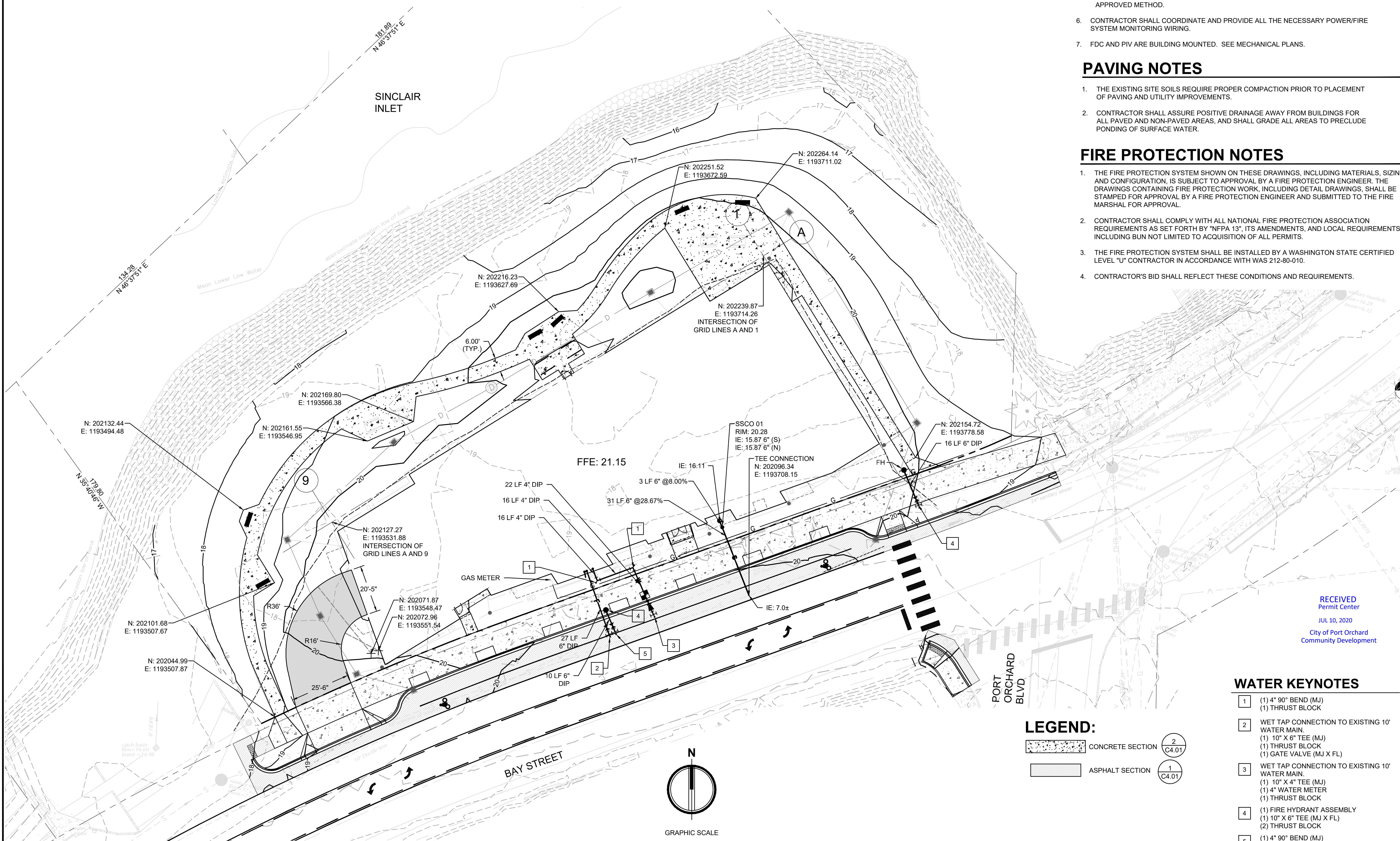
- WATER SERVICE MATERIALS AND INSTALLATION SHALL BE PER PORT ORCHARD WATER DISTRICT STANDARDS.
- THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS OF STATE AND LOCAL HEALTH AUTHORITIES.
- ALL LOCATIONS OF EXISTING UTILITIES ARE APPROXIMATE AND IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXACT LOCATION TO AVOID DAMAGE OR DISTURBANCE.
- THE MINIMUM COVER FOR WATER MAINS SHALL BE 3' AND SHALL BE MAINTAINED AT ALL POINTS.
- ALL CUTTING, TAPPING, REMOVAL AND DISPOSAL OF ASBESTOS CEMENT PIPE SHALL BE IN CONFORMANCE WITH THE CURRENT POLICIES AND PROCEDURES OF THE KITSAP COUNTY HEALTH DEPARTMENT, THE PUGET SOUND AIR POLLUTION CONTROL AGENCY AND OTHER PUBLIC OFFICES WITH JURISDICTION IN THIS MATTER. AT THIS TIME THE POLICY IS THAT:
 - A PIPE SHALL BE "SNAPPED OFF" RATHER THAN BEING CUT WITH ABRASIVE SAW.
 - ABANDONED PIPE SHALL BE LEFT IN THE TRENCH OR DISPOSED OF IN AN APPROVED METHOD.
- CONTRACTOR SHALL COORDINATE AND PROVIDE ALL THE NECESSARY POWER/FIRE SYSTEM MONITORING WIRING.
- FDC AND PIV ARE BUILDING MOUNTED. SEE MECHANICAL PLANS.

PAVING NOTES

- THE EXISTING SITE SOILS REQUIRE PROPER COMPACTION PRIOR TO PLACEMENT OF PAVING AND UTILITY IMPROVEMENTS.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL PAVED AND NON-PAVED AREAS, AND SHALL GRADE ALL AREAS TO PRECLUDE PONDING OF SURFACE WATER.

FIRE PROTECTION NOTES

- THE FIRE PROTECTION SYSTEM SHOWN ON THESE DRAWINGS, INCLUDING MATERIALS, SIZING, AND CONFIGURATION, IS SUBJECT TO APPROVAL BY A FIRE PROTECTION ENGINEER. THE DRAWINGS CONTAINING FIRE PROTECTION WORK, INCLUDING DETAIL DRAWINGS, SHALL BE STAMPED FOR APPROVAL BY A FIRE PROTECTION ENGINEER AND SUBMITTED TO THE FIRE MARSHAL FOR APPROVAL.
- CONTRACTOR SHALL COMPLY WITH ALL NATIONAL FIRE PROTECTION ASSOCIATION REQUIREMENTS AS SET FORTH BY "NFPA 13", ITS AMENDMENTS, AND LOCAL REQUIREMENTS, INCLUDING BUT NOT LIMITED TO ACQUISITION OF ALL PERMITS.
- THE FIRE PROTECTION SYSTEM SHALL BE INSTALLED BY A WASHINGTON STATE CERTIFIED LEVEL "U" CONTRACTOR IN ACCORDANCE WITH WAS 212-80-010.
- CONTRACTOR'S BID SHALL REFLECT THESE CONDITIONS AND REQUIREMENTS.



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Project Title:

BAY STREET APARTMENTS

Client:
KASHI 201 WILLIAMS LLC
ALI KASHI
C/O JENNIFER WEDDERMANN
2302 A STREET
TACOMA, WA 98402

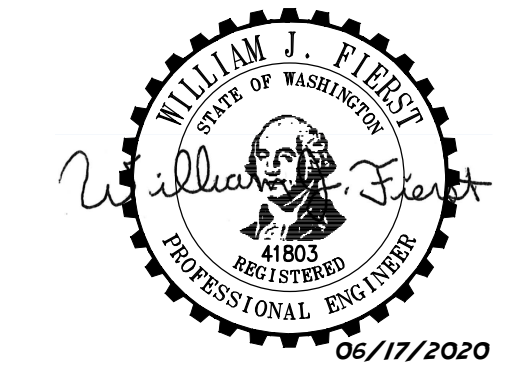
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PAVING, UTILITIES AND HORIZONTAL CONTROL PLAN

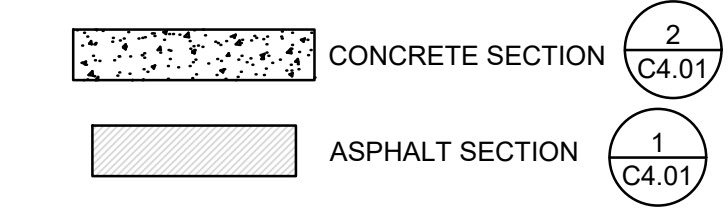
Designed by: AMT Drawn by: AMT Checked by: WJF

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C4.00

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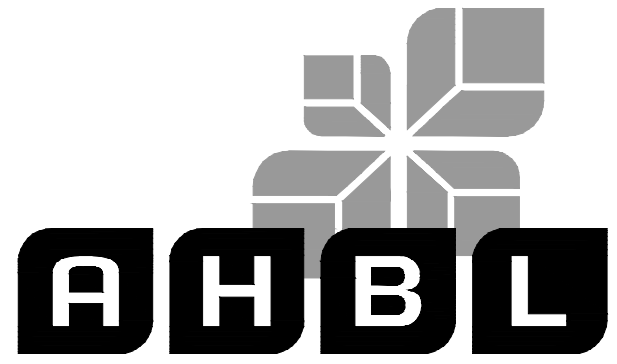
WATER KEYNOTES

- (1) 4" 90° BEND (MJ)
(1) THRUST BLOCK
- WET TAP CONNECTION TO EXISTING 10" WATER MAIN.
(1) 10" X 6" TEE (MJ)
(1) THRUST BLOCK
(1) GATE VALVE (MJ X FL)
- WET TAP CONNECTION TO EXISTING 10" WATER MAIN.
(1) 10" X 4" TEE (MJ)
(1) 4" WATER METER
(1) THRUST BLOCK
- (1) FIRE HYDRANT ASSEMBLY
(1) 10" X 6" TEE (MJ X FL)
(2) THRUST BLOCK
- (1) 4" 90° BEND (MJ)
(1) THRUST BLOCK

BAY STREET APARTMENTS

SEC 26, TWP 24N, RGE 1E, W.M.

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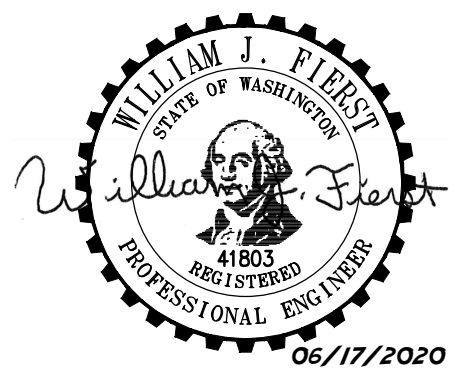
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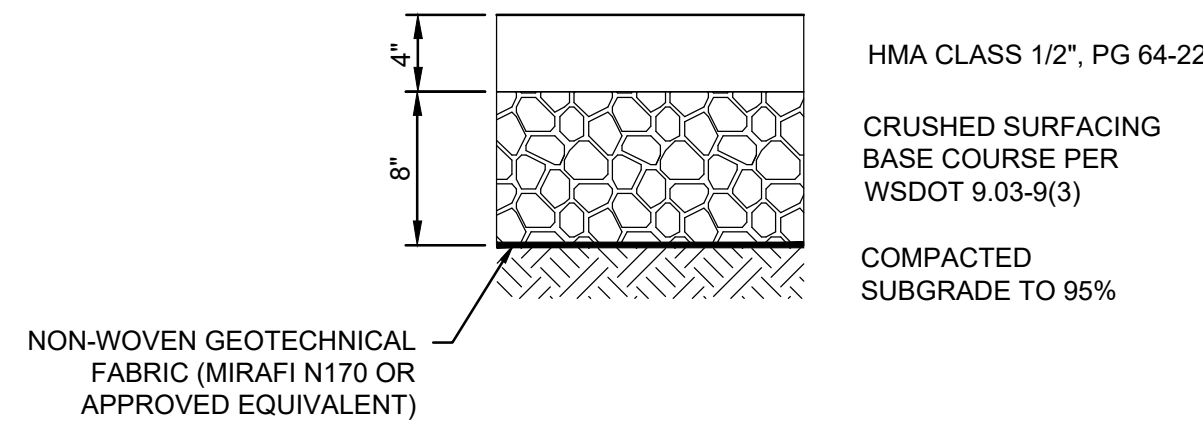
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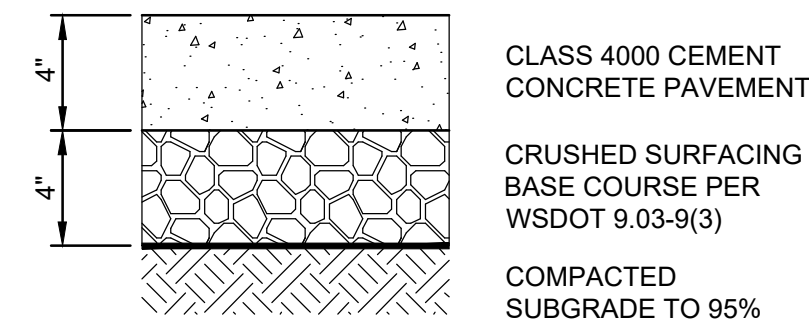
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NOTES:

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2. THE CONTRACTOR SHALL EXPOSE EXISTING FILL, PROOF ROLL AND COMPACT TO 95%. IF YIELDING CONDITIONS ARE ENCOUNTERED, YIELDING MATERIALS SHALL BE REPLACED WITH IMPORTED STRUCTURAL FILL MATERIAL. THE DEPTH OF MATERIAL REPLACEMENT SHALL BE DETERMINED BY THE GEOTECHNICAL ENGINEER.

1 ASPHALT PAVEMENT (ONSITE)
NOT TO SCALE
(SEE ROADWAY PLANS FOR PAVING SECTION WITHIN THE RIGHT-OF-WAY)



NOTES:

1. DEPTHS ARE COMPACTED THICKNESS.
2. DUMMY JOINTS CONSISTING OF 3/16" BE 2" PREMOLDED JOINT MATERIAL SHALL BE PLACED AT MAXIMUM 18 FOOT INTERVALS.
3. PROVIDE A TOOLED CONSTRUCTION JOINT AT MAXIMUM 6 FOOT INTERVAL.
4. THE CONTRACTOR SHALL EXPOSE EXISTING FILL, PROOF ROLL AND COMPACT TO 95%. IF YIELDING CONDITIONS ARE ENCOUNTERED, YIELDING MATERIALS SHALL BE REPLACED WITH IMPORTED STRUCTURAL FILL MATERIAL. THE DEPTH OF MATERIAL REPLACEMENT SHALL BE DETERMINED BY THE GEOTECHNICAL ENGINEER.

2 CONCRETE (ONSITE)
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(SEE ROADWAY PLANS FOR PAVING SECTION WITHIN THE RIGHT-OF-WAY)

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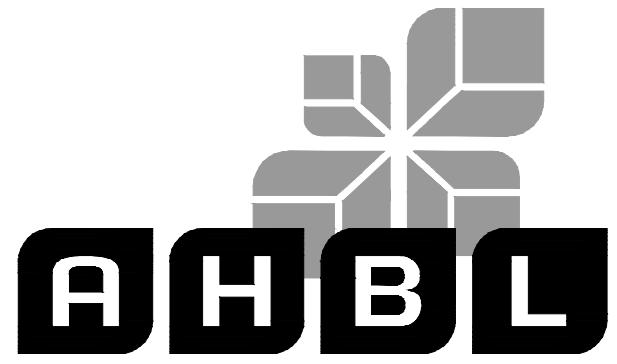
BAY STREET APARTMENTS

SEC 26, TWP 24N, RGE 1E, W.M.

PORT ORCHARD, WASHINGTON

KEYNOTES:

- 1 VERTICAL CURB AND GUTTER PER CITY OF PORT ORCHARD STANDARD DRAWING 300
- 2 PEDESTRIAN CURB PER CITY OF PORT ORCHARD STANDARD DETAIL 301
- 3 COMMERCIAL DRIVEWAY PER CITY OF PORT ORCHARD STANDARD DETAIL 322
- 4 SIDEWALK PER CITY OF PORT ORCHARD STANDARD DETAIL 340
- 5 DETECTABLE WARNING PATTERN PER CITY OF PORT ORCHARD STANDARD DETAIL 341
- 6 ASPHALT PAVING (C5.03)
- 7 SAWCUT EXISTING PAVEMENT
- 8 MATCH EXISTING SIDEWALK
- 9 MATCH EXISTING CURB
- 10 ADJUST UTILITY STRUCTURE TO FINISH GRADE
- 11 TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02 WITH HERRINGBONE GRATE PER B-30.50-03
- 12 TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-02 WITH COMBINATION INLET PER B-25.20-02
- 13 BEVELED END SECTION PER WSDOT STD PLAN B-70.20-00



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 ALI KASHI
 C/O JENNIFER WEDDERMANN
 2302 A STREET
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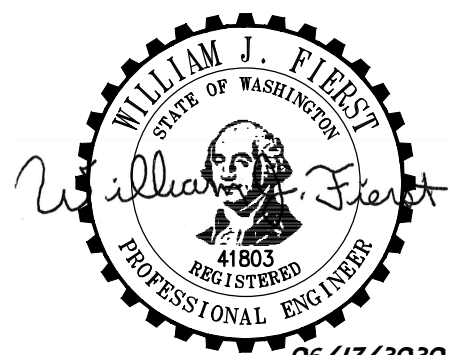
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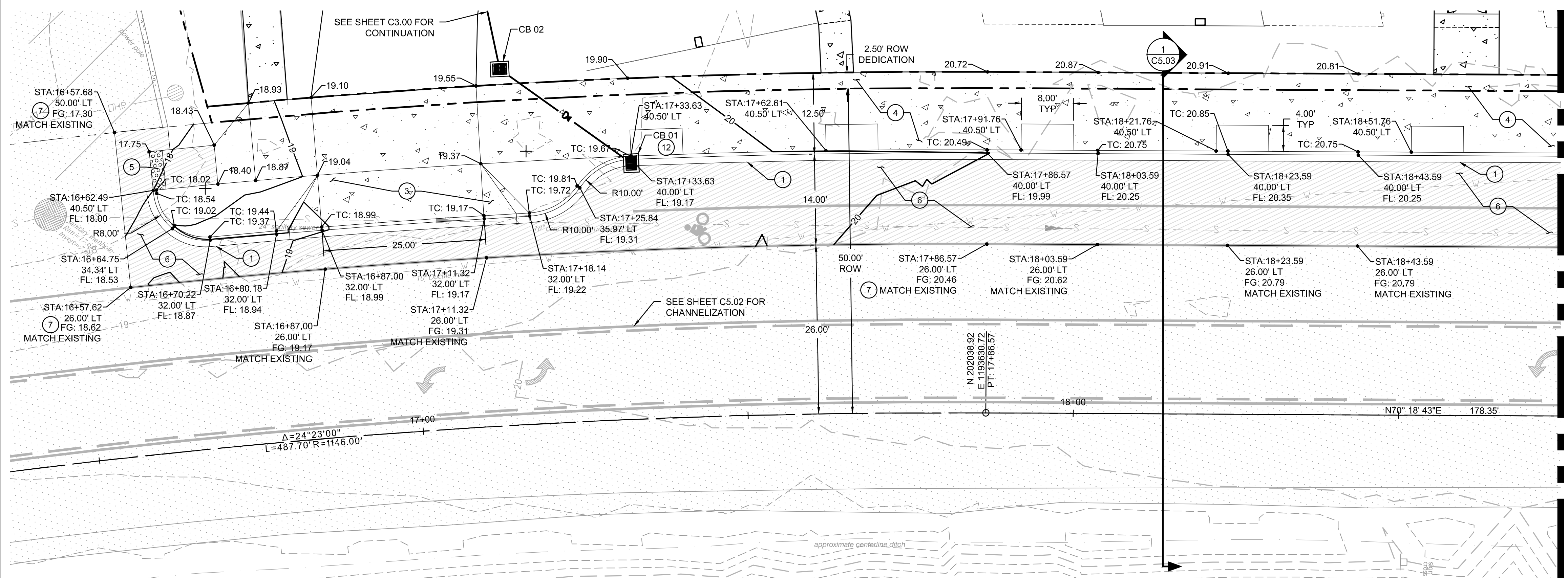
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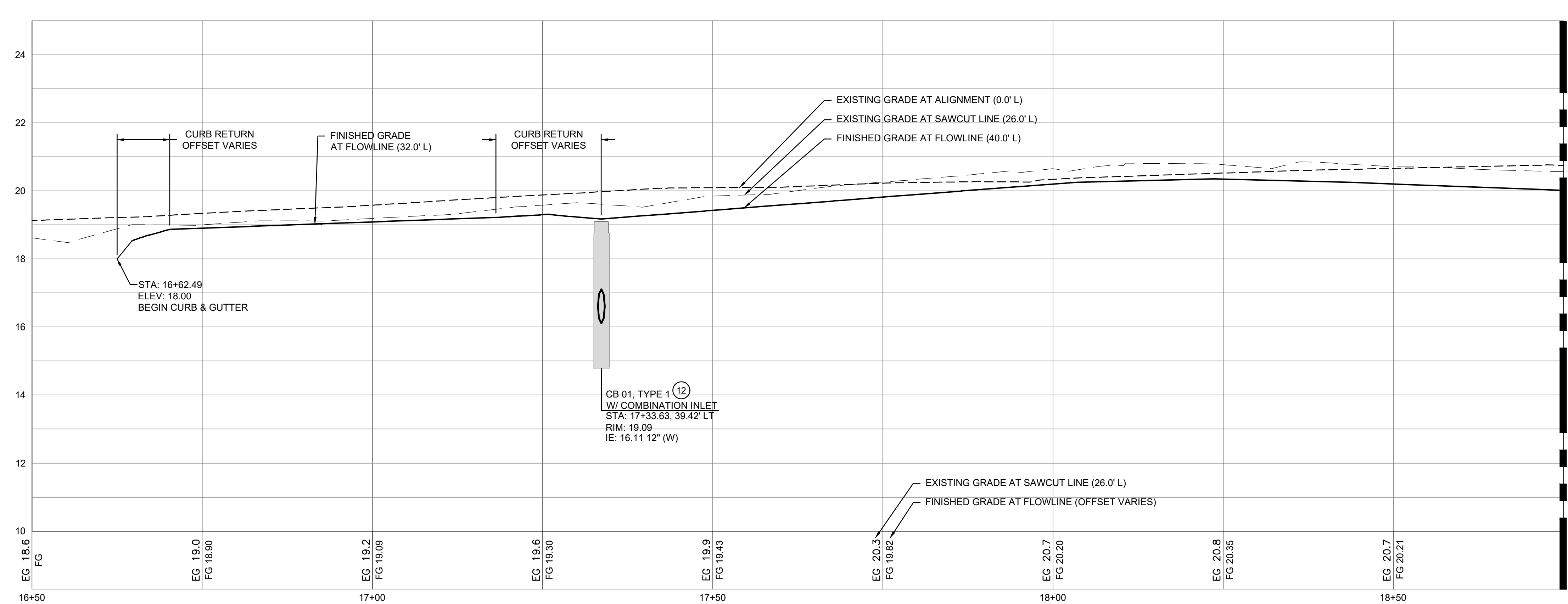
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MATCHLINE STA 18+75 - SEE SHEET C5.01 FOR CONTINUATION



MATCHLINE STA 18+75 - SEE SHEET C5.01 FOR CONTINUATION

STA 16+50 TO STA 18+75
 BAY STREET PROFILE
 VERTICAL SCALE: 1" = 2'

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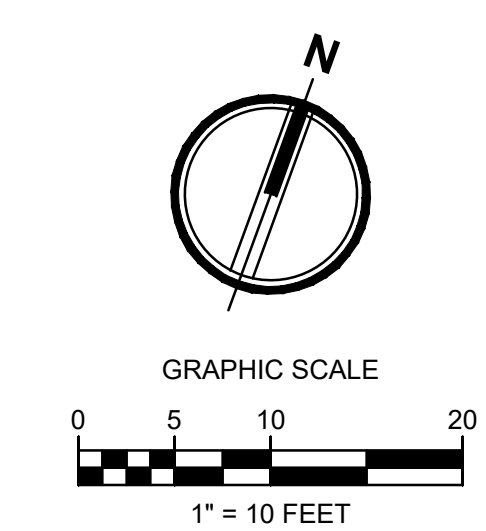
BAY STREET ROAD AND DRAINAGE PLAN AND PROFILE

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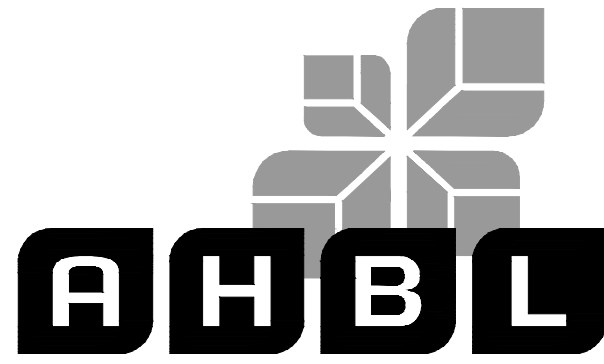
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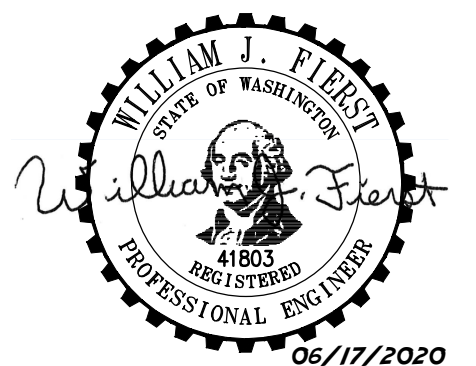
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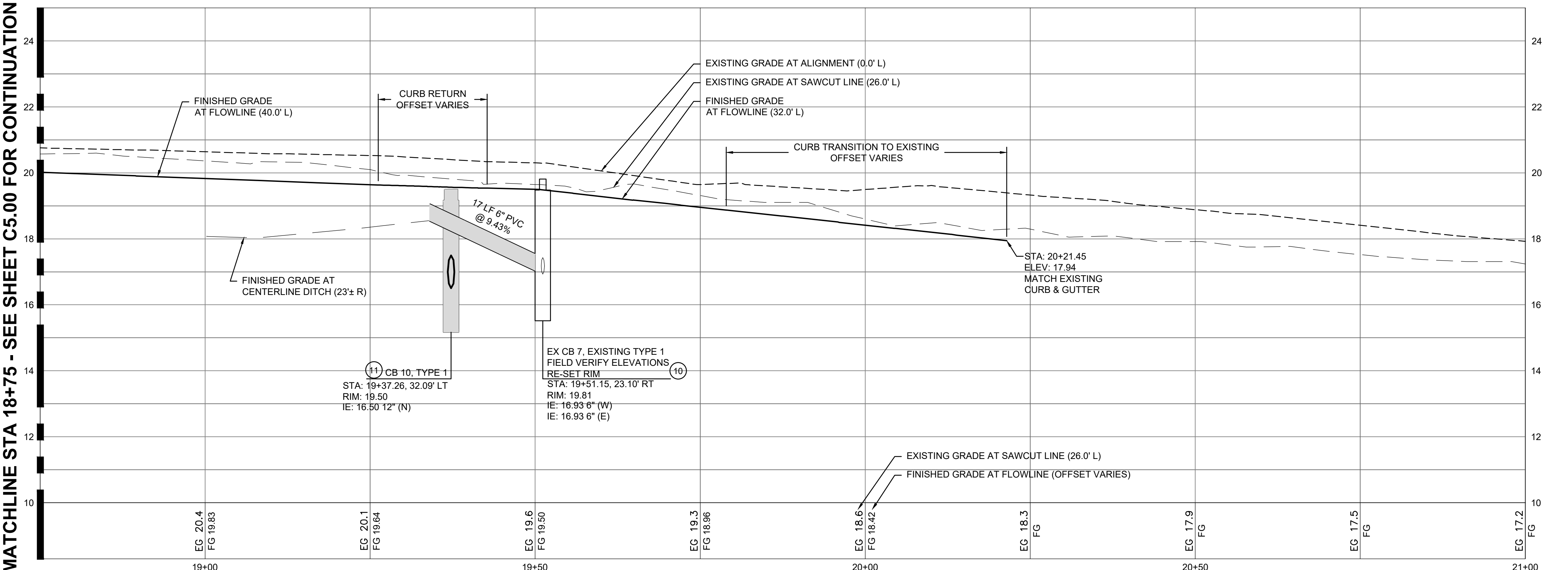
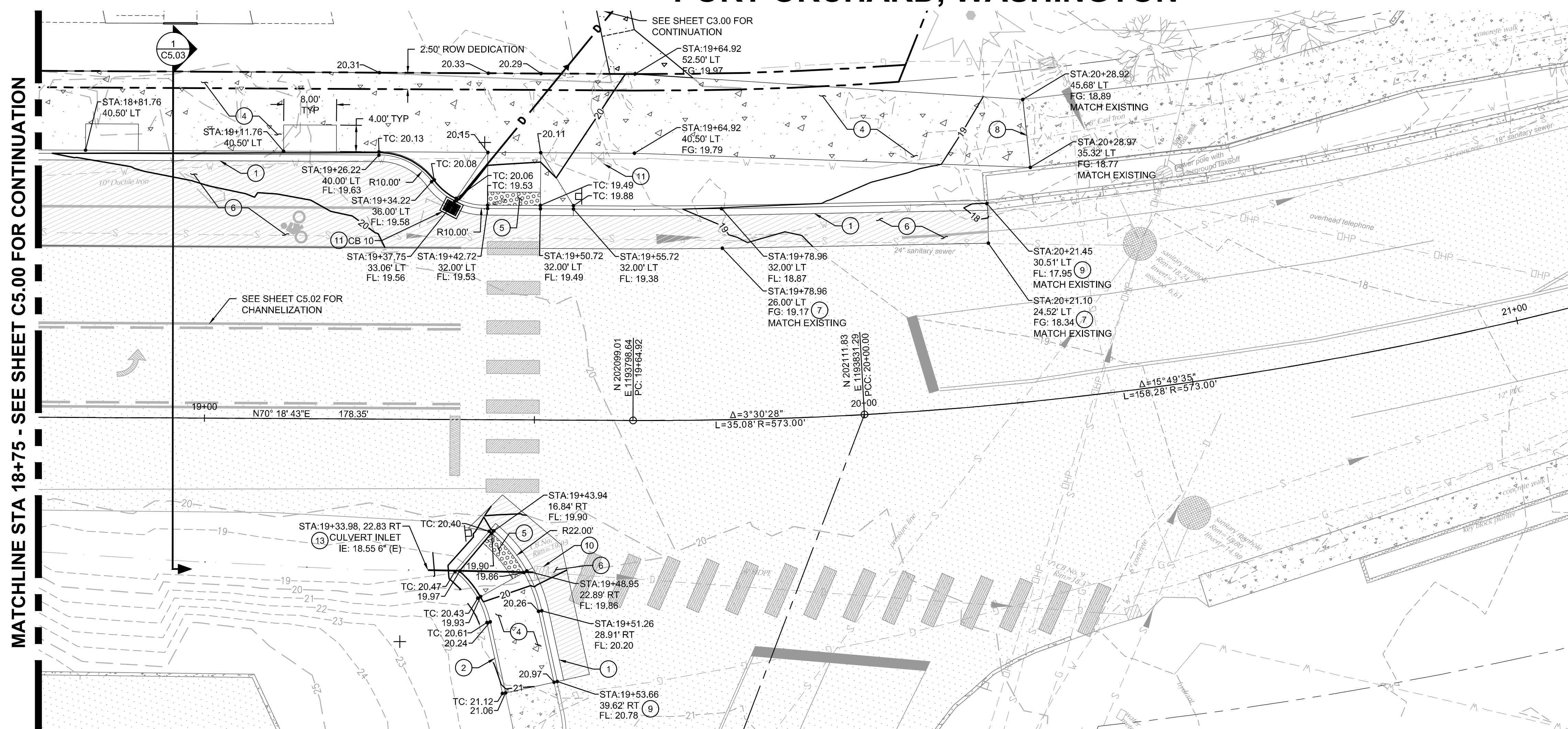
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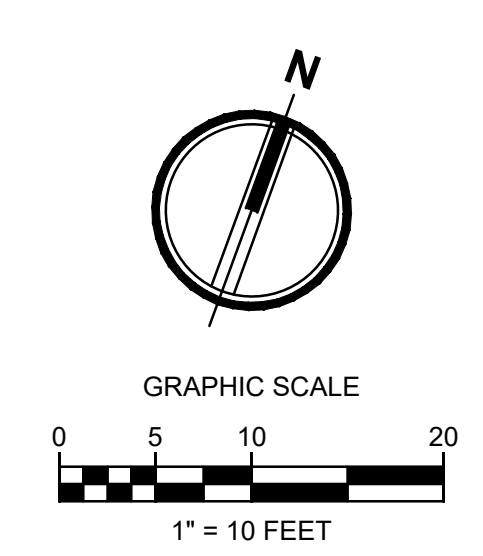
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BAY STREET ROAD AND DRAINAGE PLAN AND PROFILE

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Sheet No.

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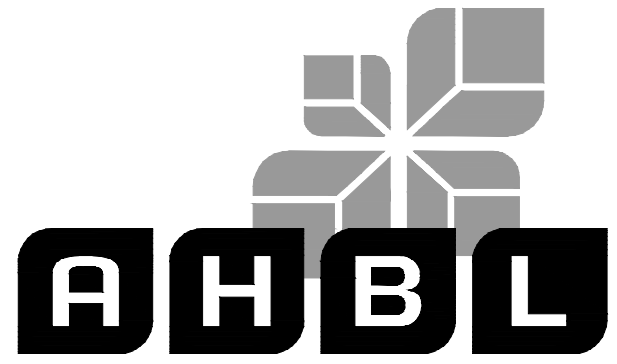
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STA 18+75 TO STA 21+00
 BAY STREET PROFILE
 VERTICAL SCALE: 1" = 2'

BAY STREET APARTMENTS

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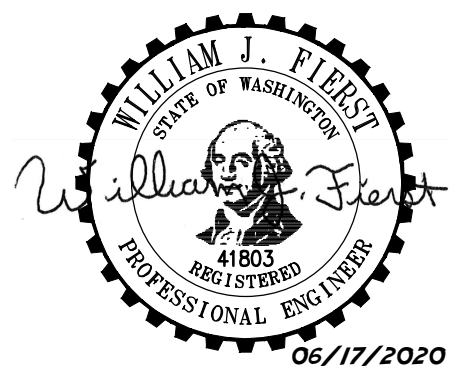
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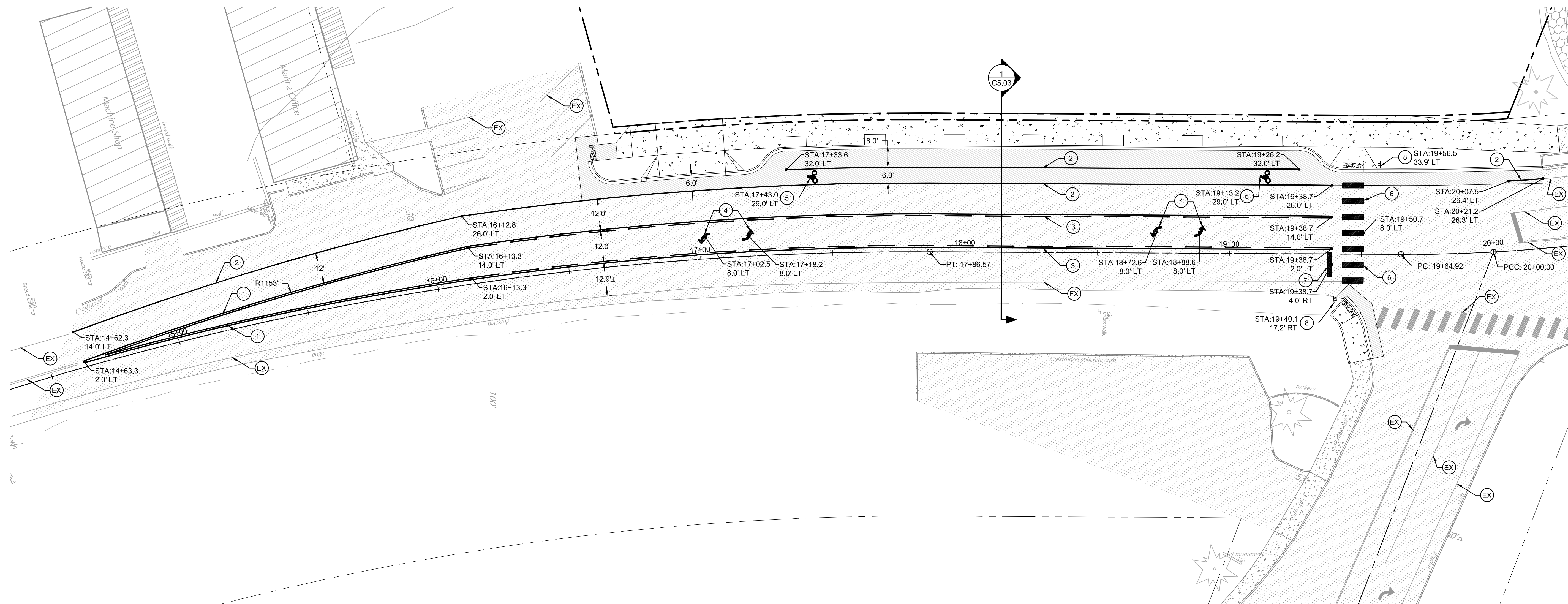
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KEYNOTES:

- ① DOUBLE CENTERLINE, YELLOW, NARROW PATTERN, PER WSDOT STD PLAN M-20.10-02
- ② SOLID LANE LINE, WHITE, PER WSDOT STD PLAN M-20.10-02
- ③ TWO-WAY LEFT TURN CENTERLINE, YELLOW, PER WSDOT STD PLAN M-20.10-02
- ④ TYPE 2SL TRAFFIC ARROW, WHITE, PER WSDOT STD PLAN M-24.20-02
- ⑤ BIKE LANE MARKINGS, WHITE, PER WSDOT STD PLAN M-9.50-02
- ⑥ CROSSWALK, WHITE, PER WSDOT STD PLAN M-15.10-01
- ⑦ STOP LINE, WHITE, PER WSDOT STD PLAN M-24.60-04
- ⑧ ADVANCE PEDESTRIAN WARNING SIGN W11-2 PER WSDOT SIGN FABRICATION MANUAL; WITH DOWN ARROW PLACARD W16-7P; WITH PEDESTRIAN CROSSING SAFETY FLAGS
- EX EXISTING PAVEMENT MARKINGS TO REMAIN

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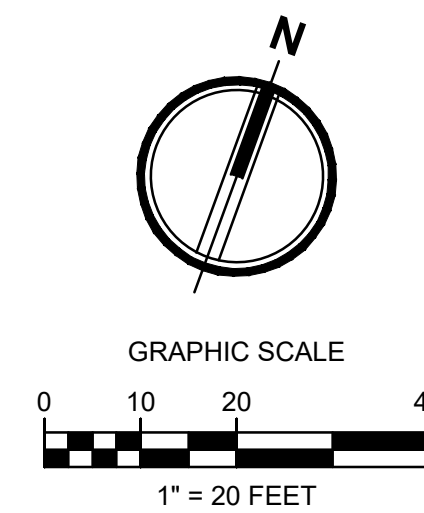
BAY STREET CHANNELIZATION PLAN

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AMT AMT WJF

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C5.02

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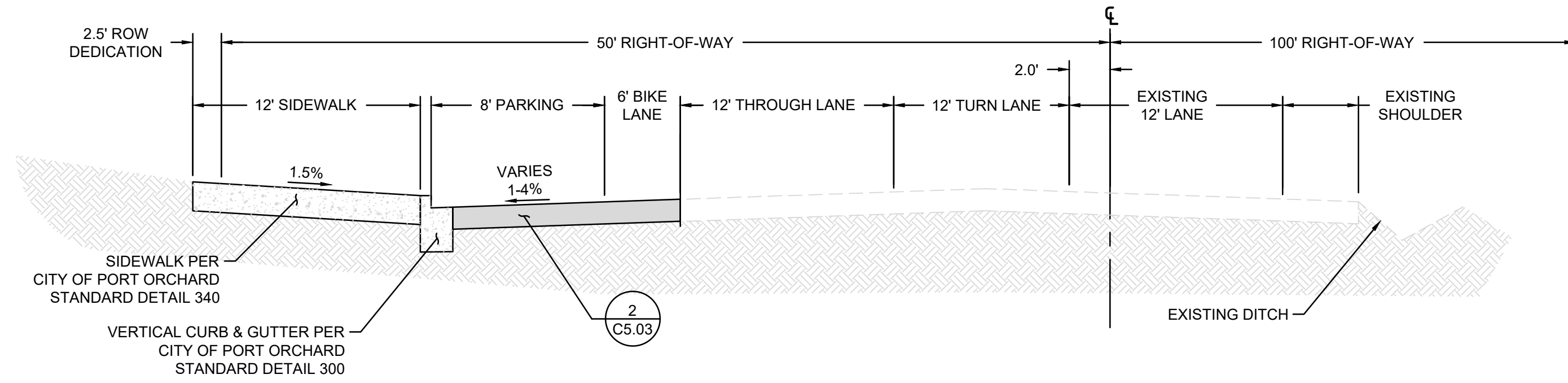


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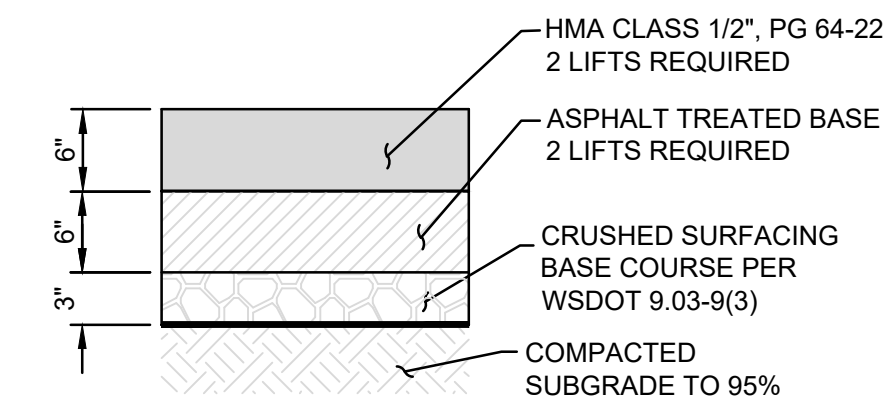
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PORT ORCHARD, WASHINGTON



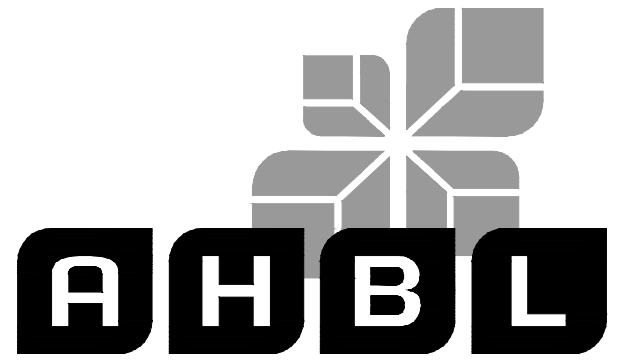
1 BAY STREET TYPICAL SECTION
NOT TO SCALE



NOTES:

1. PROVIDE PAVEMENT SECTION PER CITY OF PORT ORCHARD STANDARD DETAIL 400.
2. ALL DEPTHS SHOWN ARE COMPACTED DEPTHS.
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2 BAY STREET PAVING SECTION
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ALI KASHI
C/O JENNIFER WEDDERMANN
2302 A STREET
TACOMA, WA 98402

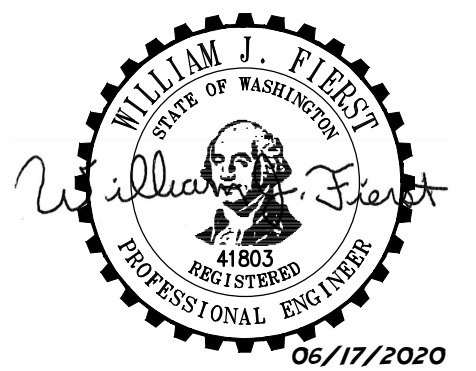
Project No.

2190242.11

Issue Set & Date:

PERMIT SUBMITTAL

JUNE 17 2020



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RECEIVED
Permit Center
JUL 10, 2020
City of Port Orchard
Community Development

Revisions:

BAY STREET NOTES AND DETAILS

Designed by: Drawn by: Checked by:
AMT AMT WJF

Sheet No.

C5.03

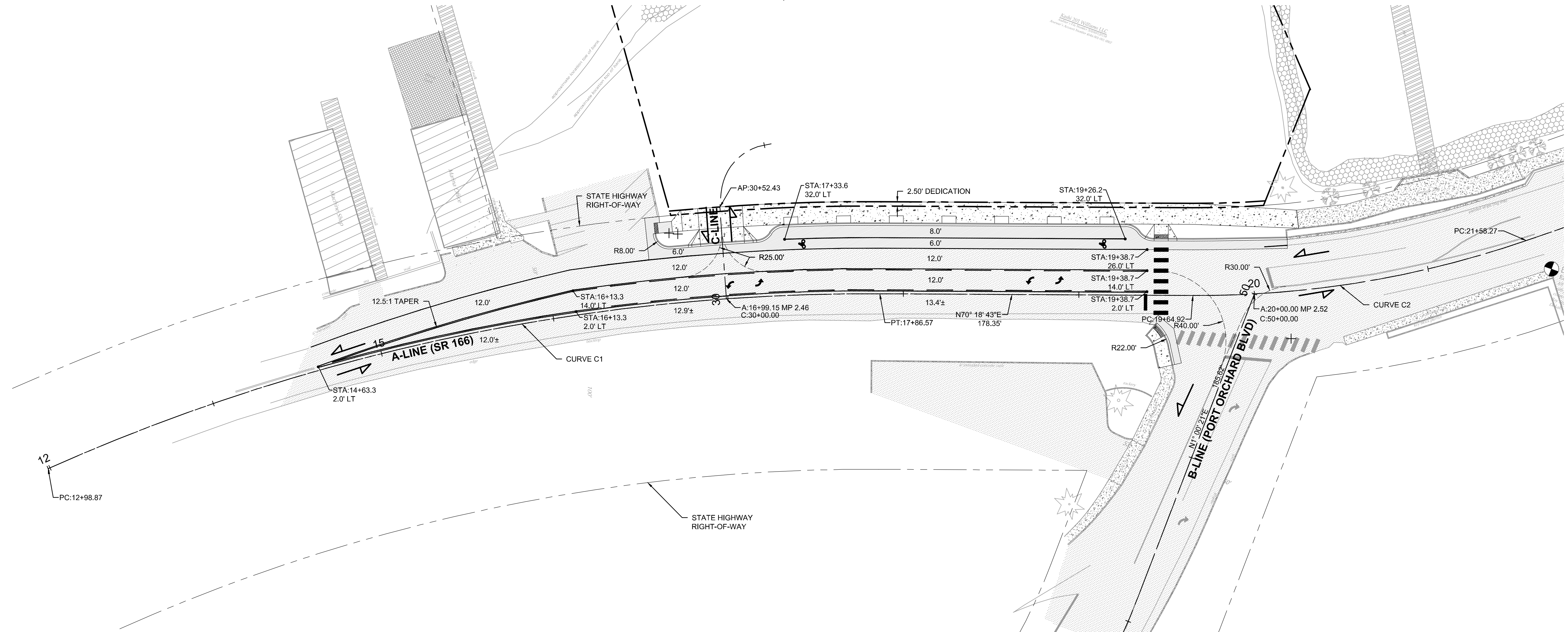
12 of 12 Sheets

Call Before You Dig. 8-1-1 or 1-800-424-5555 Underground Service (USA)

429 BAY STREET REDEVELOPMENT

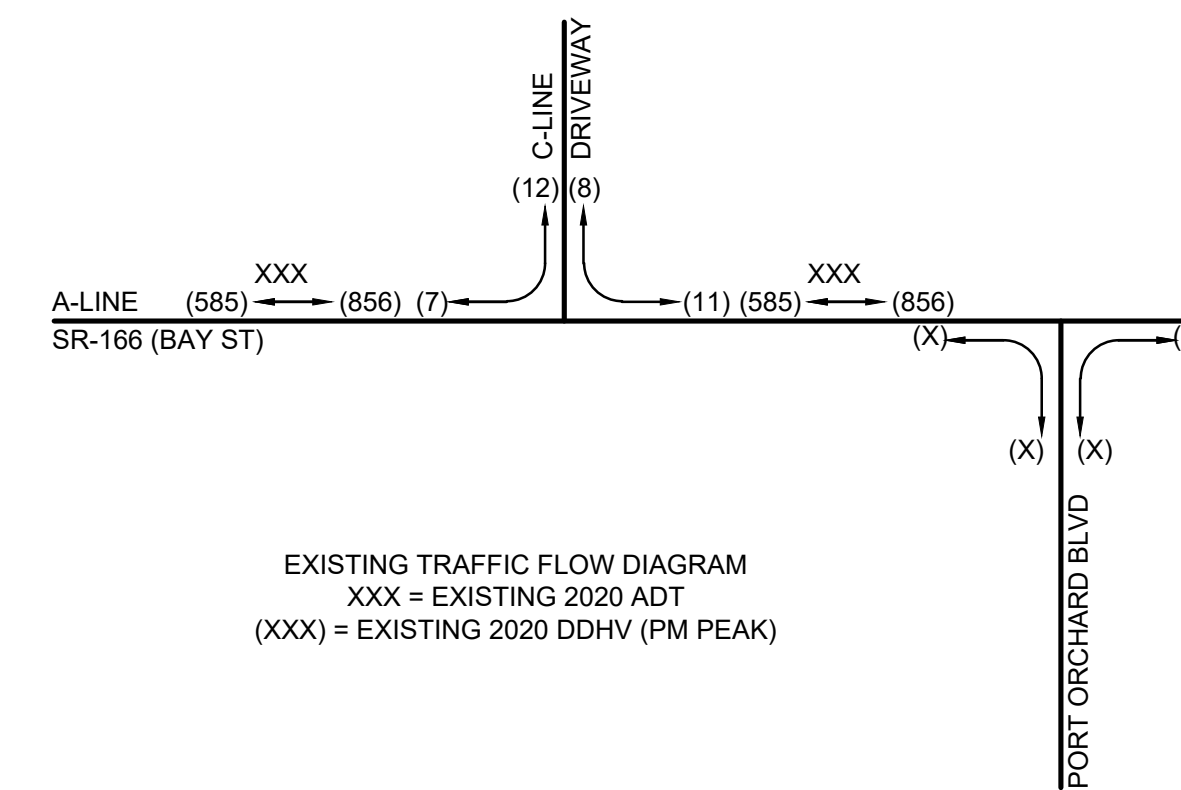
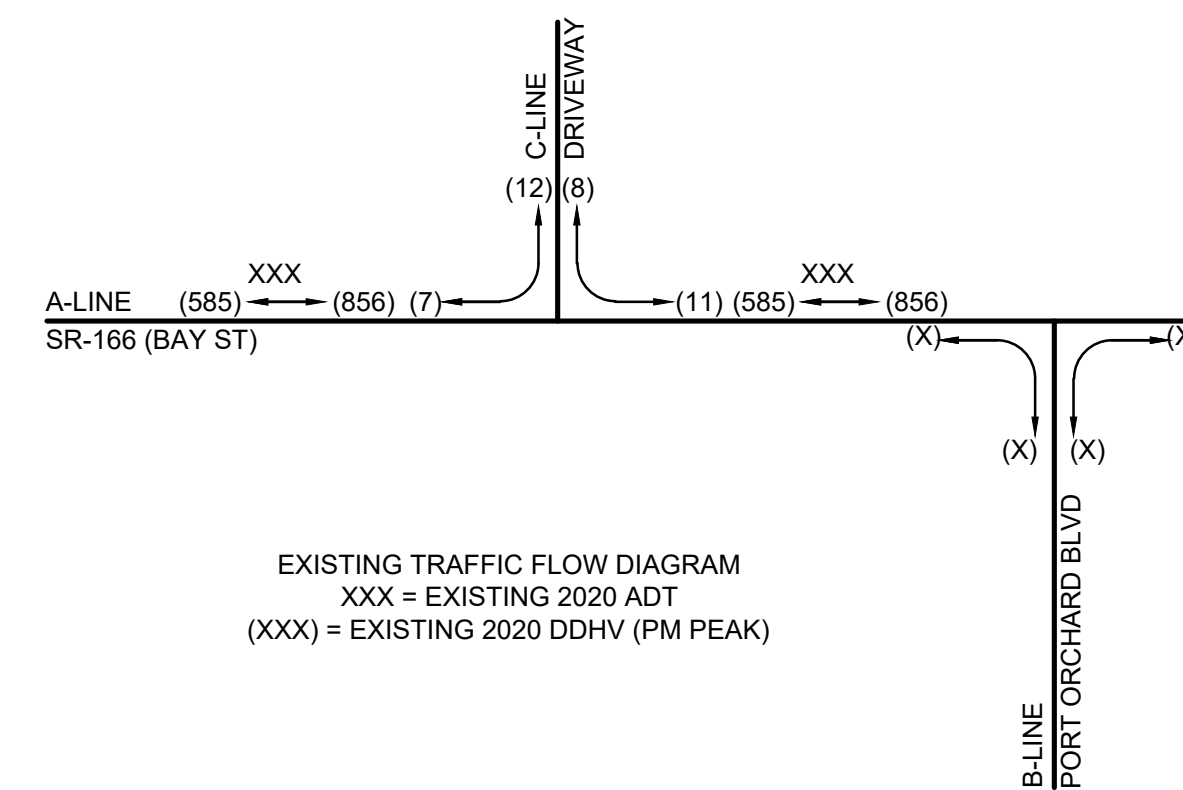
A PORTION OF GOV'T LOTS 2 & 3, SEC 26, TWP 24N, RGE 1E, W.M.

PORT ORCHARD, WASHINGTON



DESIGN DATA FOR NON-NHS ROUTE			
DESIGN CRITERIA	A-LINE SR-166 (BAY ST)	B-LINE PORT ORCHARD BLVD	C-LINE DRIVEWAY
INTERSECTION CONTROL TYPE		TWO-WAY STOP	TWO-WAY STOP
FUNCTIONAL CLASSIFICATION	URBAN PRINCIPAL ARTERIAL	URBAN MAJOR COLLECTOR	UNCLASSIFIED
ACCESS CONTROL	MANAGED CLASS 5	UNCONTROLLED	UNCONTROLLED
TERRAIN	ROLLING	ROLLING	FLAT
DESIGN SPEED	25 MPH	25 MPH	-
POSTED SPEED	25 MPH	25 MPH	15 MPH
DESIGN VEHICLE	CITY BUS	CITY BUS	P
PERCENT TRUCKS	2	2	0

CURVE DATA						
CURVE	PI	DELTA	RADIUS	TANGENT	LENGTH	% SUPER
C1	15+42.72/26.44 L	24°23'00"	1146.00	247.60	487.70'	-
C2	20+61.60/8.25 R	19°20'00"	573.00	97.61	193.35'	-



RECEIVED
Permit Center
JUL 10, 2020
City of Port Orchard
Community Development

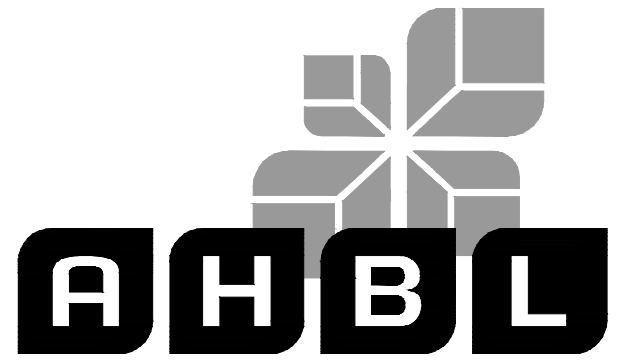
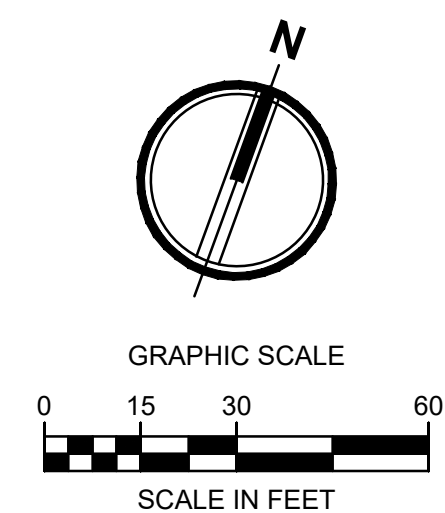
CONCUR: _____ DATE: _____
ARA FOR MULTI-MODAL DEVELOPMENT AND DELIVERY

CONCUR: _____ DATE: _____
OLYMPIC REGION TRAFFIC ENGINEER

CONCUR: _____ DATE: _____
OLYMPIC REGION DEVELOPMENT SERVICES ENGINEER

GEOMETRICS APPROVED WITHIN STATE HIGHWAY RIGHT-OF-WAY ONLY

Call Before You Dig. 8-1-1 or 1-800-424-5555 Underground Service (USA)



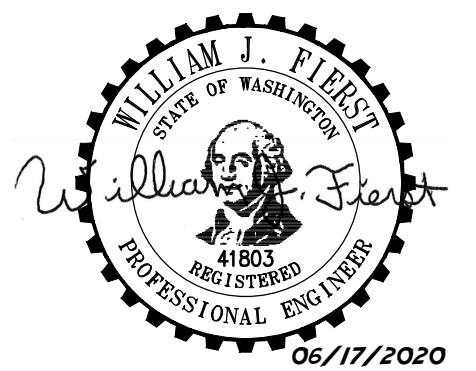
TACOMA · SEATTLE · SPOKANE · TRI-CITIES
2215 North 30th Street, Suite 300, Tacoma, WA 98403
253.383.2422 TEL 253.383.2572 FAX www.ahbl.com WEB

Project Title:
**SR 166
429 BAY STREET
MP 2.5**

Client:
**WEDDERMANN
ARCHITECTURE**
2302 A STREET
TACOMA, WA 98402

Project No.
2190242.10

Issue Set & Date:
WSDOT PFA
JANUARY 15 2020



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Revisions:

Sheet Title:
**WSDOT PLAN FOR
APPROVAL**

Designed by: BJS Drawn by: AJB Checked by: WJF

Sheet No.
PFA-1
1 of 1 Sheets