

METRO EAST MIXED-USE OVERLAY ZONE

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1.0 Purpose and Objectives

1.1 PURPOSE

The purpose of the Metro East Mixed Use Overlay Zone, hereinafter referred to as the MEMU Overlay Zone, is to introduce development forms and uses that will provide for the creation of a high-intensity, mixed-use urban village within a previously developed mid-rise to high-rise office environment. This chapter establishes standards which will fulfill the following major objectives for this mixed-use urban village area.

1.2 OBJECTIVES



Mixed-use urban village prototype

The Metro East Mixed Use Overlay Zone is intended to create a unique urban environment that achieves the following objectives:

- a. Create an active, mixed-use urban village where it is possible to live, work, shop and play all within a short walk of each other.
- b. Facilitate well-designed new mixed-use development projects that combine residential and nonresidential uses through innovative and flexible design solutions.
- c. Achieve the harmonious integration of new mixed-use development projects within the existing fabric of the mid-rise and high-rise office environment and commercial land uses.
- d. Encourage urban form and architecture that incorporate contemporary design styles and solutions as well as the use of sustainable building and site design concepts such as green buildings, energy-conserving building materials, and landscaping designs that reduce water consumption.
- e. Create highly-amenitized streetscapes that provide items such as landscaping, street furniture, niche or linear parks, passive and active water features, public plazas and courtyards, public art, and public transportation shelters in a design that integrates the public realm with adjacent private development and serves to create a distinct identity for the district.
- f. Provide for an appropriate interface of land uses and development intensities relative to the Transit Zoning Code area, the Santa Ana (I-5) Freeway, and the Costa Mesa (SR-55) Freeway.
- g. Create a highly-integrated pedestrian system that provides for connectivity between the residential areas and public recreation amenities to the north and the MEMU Overlay Zone area.



Contemporary design solutions



Active urban mixed-use



Street-facing retail

- h. Provide for active street life through the inclusion of dedicated pedestrian-oriented design and active uses on the ground floor at strategic location.
- i. Provide for a mix of housing in order to encourage a continuum of living and a variety of household types.
- j. Ensure that each project includes exceptional site planning, unique architecture, high-quality building materials, extensive open space, indoor and outdoor amenities and first-rate public improvements.



Ground-floor commercial space at strategic corner

- k. Encourage parking solutions that provide for adequate parking to ensure the long-term quality of the project, but that are creative in their design thereby enhancing the area's urban form. Parking requirements are designed to create a level of scarcity that will discourage vehicle trips, increase pedestrian activity, and enhance the provision of high-quality building and site design.

- l. Facilitate project designs that encourage adequate amounts of retail or commercial space to service residents and/or employees within the development and the larger Metro East Overlay Zone area.
- m. Allow for the development of varied residential types in a mixed-use configuration including, but not limited to loft-style units, live/work units, attached row houses, and high-quality stacked flats.
- n. Provide adequate access for public safety services.
- o. Stimulate investment and reinvestment in the area through the provision of a comprehensive planning framework that facilitates private-market success.
- p. Provide for an alternative set of zoning regulations than is provided for by the underlying zoning district .
- q. Implement the City's General Plan.



Unique architecture



Contemporary building materials

2.0 Applicability

The MEMU Overlay Zone is comprised of the properties generally bounded by the Santa Ana (I-5) Freeway on the west, Park Court Place and its prolongation to Tustin Avenue on the north, First Street on the south, Tustin Avenue and the Costa Mesa State Route (SR-55) on the east, and also includes properties along First Street and Lyon Street between Grand Avenue to the West and the Santa Ana (I-5) Freeway to the East.



Figure 1 MEMU Overlay Zone Location

2.1 APPLICABLE REGULATIONS

These provisions shall apply to all properties within the MEMU Overlay Zone, but do not supersede the underlying zoning districts. All new development within the MEMU Overlay Zone will be subject to the requirements and improvements required in the Metro East Public Realm Amenity Plan, which is included as an Appendix. Definitions of terms not found within the MEMU Overlay Zone are found within SAMC Chapter 41, Article 1, Division 2.

2.2 CONTINUANCE OF DEVELOPMENT RIGHTS UNDERLYING ZONING DISTRICT STANDARDS

All regulations, development standards, and requirements in the underlying zoning districts shall continue to apply to those properties that are currently developed according to the existing standards. New properties may also be developed to the existing underlying zoning district provided that all standards and requirements of the underlying zone are met.

2.3 OPTION TO APPLY THE MEMU OVERLAY ZONE

Properties within the MEMU Overlay Zone may choose to develop to the standards of the underlying zoning districts or to the standards contained within this MEMU Overlay Zone. In order to exercise the option to develop under the MEMU Overlay Zone district, a MEMU Site Plan Review application approved by the Planning Commission shall be required. In granting such approval, the Planning Commission must find that the proposed development is in compliance with the provisions of the MEMU Overlay Zone and the Findings in Section 8.2 of this code section.

In order to facilitate project and site design consistent with the provisions of this MEMU Overlay Zone, any development proposal for exercising use of the MEMU Overlay Zone shall be designed so that any primary use within the development is located within a newly constructed building. No primary use or uses shall be located in a building that has been rehabilitated or is otherwise the reuse of an existing building.

2.4 USE OF PHOTOGRAPHS

Photographs are used liberally throughout this document and are intended for illustrative purposes only. Specific development standards and regulations contained in this document are the controlling language for purposes of development regulation.

3.0 Development Districts

This section delineates the development districts within the MEMU Overlay Zone (Figure 2) and describes the overall form and character of each District (Table 1). There are four development districts in the MEMU Overlay zone that differ in the permitted land uses and development intensity as determined by their location and adjacency to sensitive land uses and freeways as shown on Figure 2. The Village Center District contains a pedestrian-oriented area designed to serve a central core for the Metro East area. Developments within the Village Center core are allowed at a slightly greater intensity to facilitate the role and function of this area as an activity node within the Metro East area.

Table 1 provides general descriptions and representative images of the character of each District within the Metro East Mixed-Use Overlay Zone.

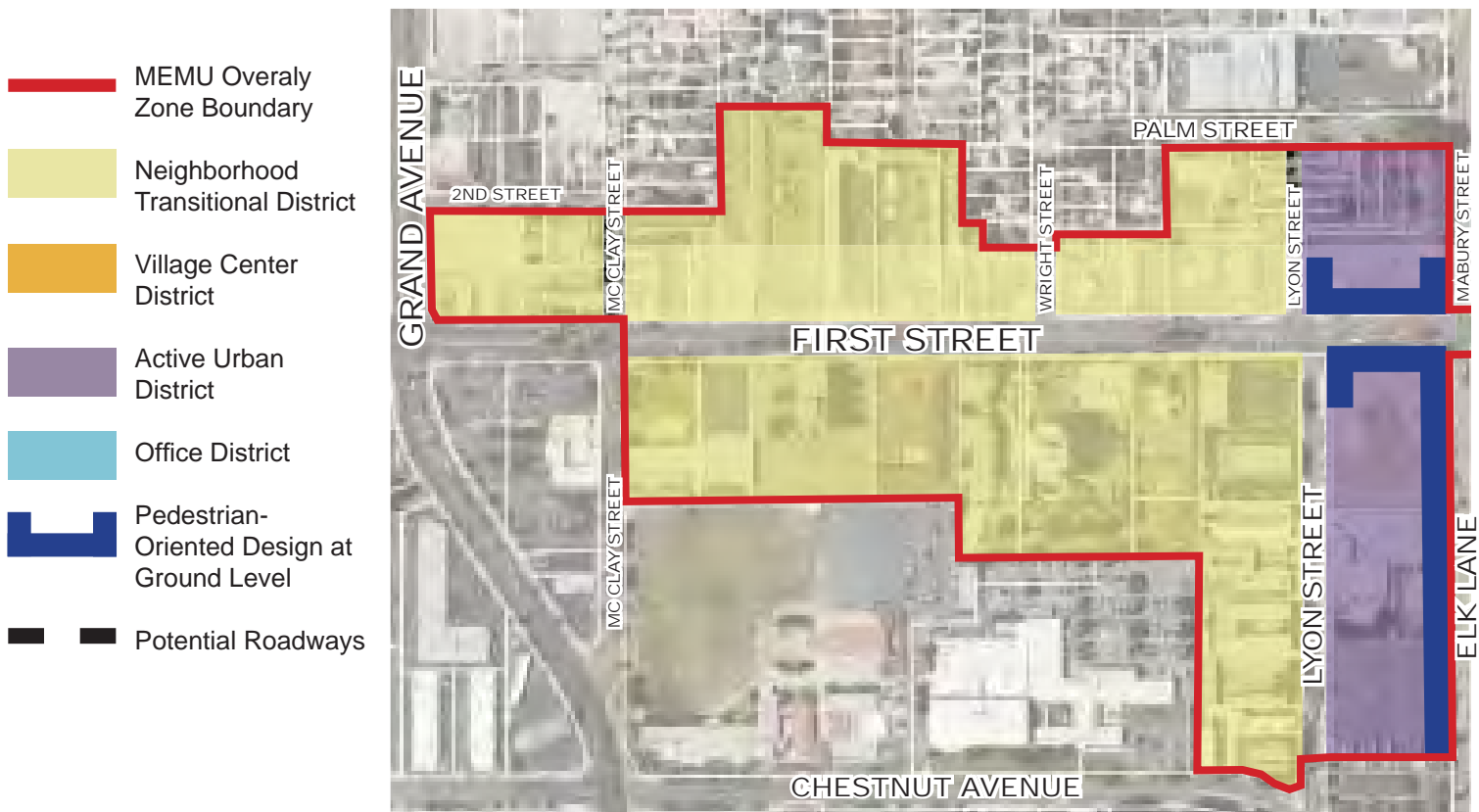


Figure 2 MEMU Overlay Zone Development Districts

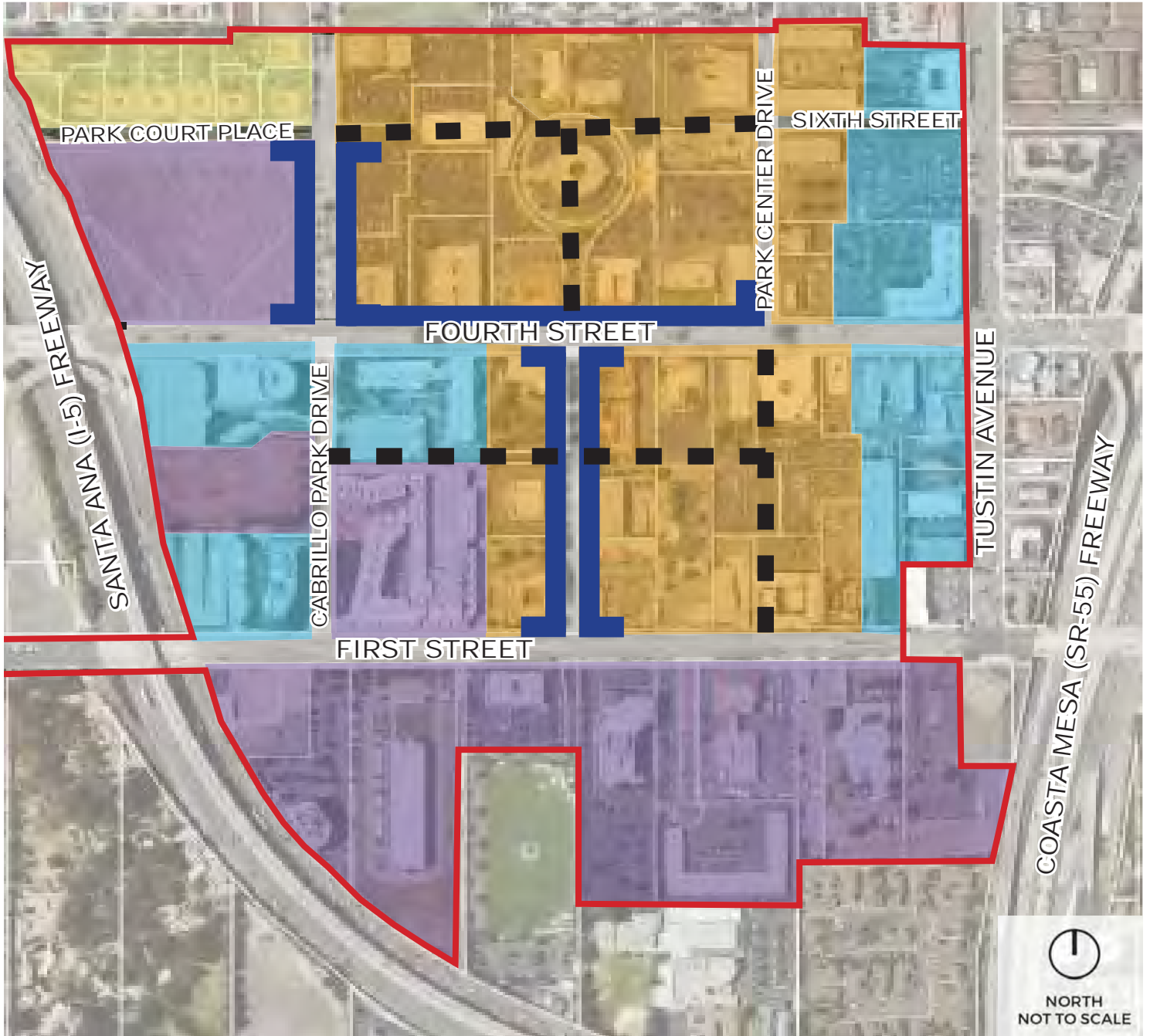


Table 1 MEMU Overlay District Description

Neighborhood Transitional District



The Neighborhood Transitional District is intended to provide opportunities for development that acts as a transition between the single-family residential to the north and the adjacent high-intensity Active Urban District. Designated for the lowest scale and the lowest intensity of uses in Metro East, development in this district is limited to residential, live/work, small scale neighborhood serving commercial or office uses. These uses may combine office on the ground floor with residential above or in freestanding single-use buildings on the same site at between two and four (2-4) stories in height. New development in this area will be designed to provide an appropriate interface with high levels of landscaping and design features that minimize impacts to the adjacent single family residential area to the north.



Village Center District



The Village Center District is intended to serve as the focal point and central gathering place within Metro East in well-designed highly connected development sites and public spaces. The District will provide a high level of neighborhood identity and activity through its central location and emphasis on creating a vibrant, attractive, and highly-interconnected pedestrian environment. Opportunities will be provided for shopping, dining, recreation, entertainment and services accessed by extensively landscaped, wide sidewalks that allow free flow between jobs, housing, and retail and commercial services, or opportunities for leisure walking within the District. The Village Center will provide commercial, office, and residential uses in the same building or on the same site in mid-rise buildings of six to ten (6-10) stories in height, in settings that provide open spaces, niches, and areas for gatherings and activities along streets, paseos, and interconnecting walkways that link the Village Center to adjacent districts and nearby public parks to the north. This District contains a pedestrian-oriented area designed to serve as a core activity center for the Metro East area. Developments within this core area along Fourth Street are allowed up to ten (10) stories in height to enhance the function of this area as a major activity center.



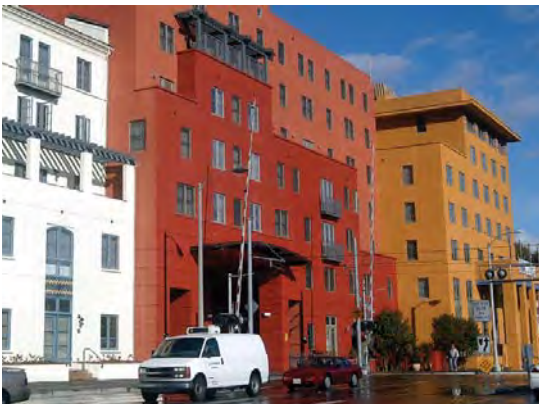
Active Urban District



The Active Urban District is intended as the location for well-designed high rise mixed use developments in a highly urbanized environment that capitalizes on the exposure and access provided by two adjacent freeways, the Santa Ana (I-5) and Costa Mesa (SR-55), and three major arterials (First and Fourth Streets and Tustin Avenue). Development in this District is envisioned to reflect signature architecture that reinforces the identity and character of Metro East as a vibrant urban village that serves as a regional employment and activity center. The Active Urban District will include major office, residential, commercial, hotel, and entertainment opportunities that are more intensive in scale and design than the adjacent Village Center. Developments in this District may combine office, commercial, and residential uses within one vertical mixed use-building with commercial on the ground floor and office or residential on the upper floors or a mix of uses within freestanding buildings on the same site. Developments will be designed to showcase an amenity-enhanced environment that provides numerous open space opportunities within this urban environment for the enjoyment of residents, employees, and visitors, and to promote pedestrian connections between this District and the Village Center as well as Cabrillo Park located north of Metro East area.



Office District



The Office District contains existing low-to high-rise office development along Tustin Avenue and adjacent to the Santa Ana (I-5) Freeway. These developments were in place prior to establishment of the MEMU Overlay Zone and will be maintained exclusively as office to promote and maintain a healthy balance between office, commercial and residential land uses within the Metro East area.



4.0 Development Standards

The MEMU Overlay Zone establishes development standards for each District, as summarized below in Table 2. Additional requirements for the standards listed in Table 2 are specified in Sections 4.1 through 4.8.

Table 2: Development Standards Summary				
Development Standards by District				
	Neighborhood Transitional District	Village Center District	Active Urban District	Office District
4.1 Land Uses	(See Table 3)			
4.2 Maximum No. Stories	4	6 10 stories in Village Core (See Section 4.2)	No Maximum 3 Stories Minimum	10
4.3 Minimum Development Site Area	20,000 sq. ft.	30,000 sq. ft.	1 acre	1.5 acre
4.4 Permitted Street Level Building Frontages				
Front Porch	Yes	Yes	No	No
Stoop	Yes	Yes	No	No
Forecourt	Yes	Yes	Yes	Yes
Shop Front	Yes	Yes	Yes	Yes
Gallery	Yes	Yes	Yes	Yes
Arcade	Yes	Yes	Yes	Yes
4.5 Publicly Accessible Open Space	5% of Total Lot Area	10% of Total Lot Area	15% of Total Lot Area	15% of Total Lot Area
4.6 Private/Common Open Space	90 sq. ft. per unit	90 sq. ft. per unit	90 sq. ft. per unit	10% of Total Lot Area
4.7 Building Setbacks	(See Table 4 on Page 21)			
4.8 Parking *	Residential: 2.25 spaces per unit Non-Residential: Per Code	Residential: 2-2.25 spaces per unit Non-Residential: Per Code	Residential: 2-2.25 spaces per unit Non-Residential: Per Code	Office: 3 spaces per 1,000 sq. ft. Other: Per Code

* For further detail, refer to Section 4.8, Parking and Access.

4.1 LAND USES

The MEMU Overlay Zone sets forth specific uses to be allowed within each district, as shown in Table 3, subject to a MEMU Site Plan Review approval by the Planning Commission. Any use that is not specified as permitted or conditionally permitted within the MEMU Overlay Zone is prohibited unless a determination is

made by the Executive Director of Planning that the proposed use is compatible with the overall intent and character of the MEMU Overlay Zone as specified in Section 41-601(c) of the Santa Ana Municipal Code (SAMC).

Table 3: Permitted Land Uses					The allowable uses are established by letter designations as follows:
Neighborhood Transitional District Village Center District Active Urban District Office District*					P Permitted by right N Prohibited NA Not Applicable CUP Conditional Use Permit required LUC Land Use Certificate required
	Neighborhood Transitional	Village Center	Active urban	Office	Special Provisions (references to other applicable code sections or limitations)
Residential Uses					
Live/Work Units	P	P	P	N	Subject to Sec. 4.1.2 of this division
Multiple-Family Residential	P	P	P	N	Subject to Sec. 4.1.3 of this division
Non-Residential Uses					
Art galleries and studios including, but not limited to: photography; fine art; fiber art; printing, lithography, and calligraphy; ceramic and pottery; glass blowing and sculpting	P	P	P	P	
Eating establishments, (cafes, restaurants)	P	P	P	P	CUP for liquor sales, after hours operation
Bakeries	P	P	P	P	
Retail and Service uses	P	P	P	P	
Medical and Dental offices	P	P	P	P	
Professional, administrative and business offices	P	P	P	P	
Child care facilities	P	P	P	P	
Gymnasiums and health clubs	N	P	P	CUP	
Hotels	N	P	P	CUP	
Indoor/outdoor entertainment	N	P	P	P	As defined in Chapter 41 of the SAMC
Religious Institutions	CUP	CUP	CUP	CUP	
Schools	N	P	P	N	
Tattoo Establishments	N	P	P	N	Subject to Sec. 41-199.3 of the SAMC
Temporary outdoor activities	LUC	LUC	LUC	LUC	Subject to Sec. 41-195.5 of the SAMC
Theaters and cinemas	N	P	P	P	
Use/Operational Standards					
Drive through establishments	N	N	N	N	

In addition to the land use categories listed in Table 3, the following regulations and operational standards shall apply to development within the MEMU Overlay Zone.

4.1.1 Village Center Core: Pedestrian-Oriented Design and Active Ground-Floor Uses

In order to generate pedestrian use and contribute to an active street life, developments located in the Village Center core, properties fronting on Fourth Street will be allowed to develop up to a height of ten stories for a depth of 150'. The ground floor of buildings fronting on Fourth Street between Cabrillo Park Drive and Park Center Drive, and Golden Circle Drive south of Fourth Street as shown in Figure 2 shall have commercial uses and pedestrian-oriented designs. The pedestrian-oriented design features include, but are not limited to, visually transparent and architecturally articulated building facades, or designing arcades or courtyards for activities such as outdoor dining or active open-space areas. Active uses that are visually accessible to the public also may satisfy this requirement and may include, but are not limited to, uses such as a workout room for a health club, a cooking class, the nonresidential portion of live/work units, or other active-artisan use.



Two-story live/work units

- a. Residential use is permitted only in combination with individual studios in a manner which provides an integrated working and living environment.
- b. At least one (1) off-street parking space shall be provided for each dwelling unit.
- c. A live/work unit shall be at least one thousand (1,000) square feet in size.
- d. It shall comply with all Housing Code requirements as modified by section 8-2700 of the SAMC.
- e. The residential component of a live/work unit shall meet the following standards:
 1. It shall have a space of at least seven hundred fifty (750) square feet.
 2. It shall have access to separate bathroom facilities, including a water closet, a wash basin, and a bathtub or shower.
 3. It shall have separate kitchen facilities including a kitchen sink, cooking appliances and refrigerator. All such facilities shall have a clear working space of at least thirty (30) inches in front.
- f. On-site laundry facilities are required within each unit.



Three-story live/work units

4.1.2 Live/Work Units

The residential component of live/work units shall be contiguous with and integral to the working space with direct access between the two areas and shall not be designed as a separate stand-alone dwelling unit. This requirement does not preclude a separate access for the residential area as long as there is not a physical separation between the living area and the work areas of the unit. In addition, the following regulations shall apply to live/work units:

4.1.3 Stand Alone Residential Development

Consistent with the objectives of the MEMU Overlay Zone to encourage and facilitate mixed-use, no stand-alone residential building shall be permitted except in the Neighborhood Transitional District or as a component of a mixed-use project that has different uses in multiple buildings designed as an integrated development on a single development site. An integrated development is a project that is built on a site comprised of more than one parcel or contained in more than one building when the uses and support facilities such as parking or open space are designed to function in a cohesive interactive manner throughout the development site.

4.2 MAXIMUM NUMBER OF STORIES

A. The overall scale and massing of development within the MEMU Overlay Zone should transition from the existing low-scale intensity of the Neighborhood Transitional District, which is adjacent to an existing single-family residential neighborhood, to mid-rise development in the Village Center and high-rises in the Urban Core adjacent to the Santa Ana Freeway. To create a varied skyline and enhance the form of development sites throughout the Metro East area, building heights will be regulated based on the maximum number of stories permitted. The following heights are typical for the uses within a building as calculated from floor to floor:

1. Residential: 9–15 feet (includes lofts and live work unit designs)
2. Retail: 18 feet
3. Retail with mezzanine: 22 feet
4. Specialty retail: 22–24 feet
5. Office: 13.5 feet

B. In compliance with the Airport Land Use Commission (ALUC) of Orange County, the following conditions shall apply to all new development with the MEMU Overlay Zone.

1. For development of structures that exceed 200 feet in height above ground level at a development site, applicants shall file a Notice of Proposed Construction or Alteration with the FAA (FAA Form 7460-1). Following the FAA's Aeronautical Study of the project, projects must comply with conditions of approval imposed or recommended by the FAA. Subsequent to the FAA findings, the City shall refer the project to the ALUC for consistency analysis.
2. Buildings within any District of the Overlay Zone shall not penetrate the FAA FAR Part 77 imaginary obstruction surface for John Wayne Airport.
3. Applicants shall file a Notice of Proposed Construction or Alteration with the FAA (Form 7460-1) for any construction cranes that exceed 200 feet in height above ground level.

C. New development will be required to be within the following maximum number of stories based upon their location within the MEMU Overlay Zone except as specified below:

1. Neighborhood Transitional District: Four stories maximum

New development shall be of a low scale, and should serve as a visual transition between the MEMU Village and adjacent residential areas to the north. New development



Three-story live/work units

in this District shall be compatible in height, scale, and mass with adjacent residential development to the north, with heights ranging between two and four stories.

2. Village Center District: Six stories maximum; ten stories maximum within core area (See Section 4.1.1)

New development in the Village Center District is more intensive in scale than Neighborhood Transitional, and shall serve as a visual transition from the low-intensity of the Neighborhood Transitional District to the intensive development in the Active Urban District. The height, scale, mass and bulk of new development shall be designed to achieve a “human scale” at the ground level to foster a pedestrian-friendly atmosphere, with buildings located along and oriented to the street frontages and common plazas. In order to encourage and maintain active streets and function as a central gathering place and 18-hour activity center for residents, employees, and visitors, a core area along Fourth Street will be allowed to develop at a height of up to ten stories, for a depth of 150’.



Five-story mixed-use corner building

(See also Section 4.1.1). Properties outside the core of the Village Center will be allowed to develop up to a height of six stories.

3. Active Urban District: No maximum height; minimum three stories required

The Active Urban District is the most intensive in scale within the MEMU area, and is designed to create a highly urbanized environment. New development in this District shall relate in scale, height, and configuration with adjacent buildings. New development shall be designed and oriented to promote intensive public activity at the ground level that integrates and establishes a cohesive transition to adjacent Districts.



Front street building orientation

Larger buildings shall be broken down in scale through changes in massing, changes in plane and profile, façade subdivision, as well as other architectural means to ensure that the building is well proportioned and creates a satisfactory composition. Sites in this District shall be designed so that tower elements do not impact adjacent lower intensity developments but rather are designed to maximize exposure to the freeway or to adjacent developments of a similar intensity.

A minimum building height of three stories is required within this District to ensure that a level of intensity is achieved on any development site even when there are multiple buildings of various heights. Any new development that is ten stories or greater in the Active Urban District and is proposed at a location adjacent to a multiple-family residential use or zone outside of the MEMU area shall be set back at least 150' from the property line of the adjacent residentially used or zoned property.

4. Office District: Ten stories maximum

Existing development within the Office District varies from two-story garden offices to the high-rise Xerox Center adjacent to the I-5 Freeway. Development in this District is limited to office uses to maintain a core of offices uses within the Metro East area and therefore is least likely to utilize the provisions of the MEMU Overlay Zone. New development in this District under the Overlay Zone will vary from low-rise garden office to mid-rise office buildings with ancillary uses and may not redevelop as quickly as other Metro East areas.



Higher intensity mixed-use building

The height, scale, mass and bulk of new development shall be designed to achieve a "human scale" at the ground level to foster a pedestrian-friendly atmosphere, with buildings located along and oriented to the street frontages and common plazas. In order to maintain a consistent level of development intensity within this District, a minimum height of three stories is encouraged. If a project is on a large site with multiple buildings, an average of four stories should be used as a guide.

4.3 MINIMUM DEVELOPMENT SITE AREA

As a means to encourage development that achieves the objectives of the MEMU Overlay Zone, all new development shall have a minimum lot size that facilitates the use of design principles that achieve adequate setbacks, open spaces, connections with adjacent properties in the district, and a high level of site design. Required development site area is required as follows:

- a. Neighborhood Transitional District
20,000 square feet
- b. Village Center District
30,000 square feet

- c. Active Urban District
1 Acre (43,560 square feet)
- d. Existing Office District
30,000 square feet

4.4 PERMITTED STREET LEVEL BUILDING FRONTAGES

A. Appropriate building frontages reinforce a continuous urban street wall defining the street edge and encouraging public-private interaction which results in active pedestrian urban spaces. In order to encourage a stronger public-private interaction within the Metro East area, new development will be required to include one of the building frontages specified by district below:

- Front Porch
- Stoop
- Forecourt
- Shop Front
- Gallery
- Arcade

1. Neighborhood Transitional District:
 - a. Front Porch
 - b. Stoop
 - c. Forecourt building frontages are permitted
 - d. Shop Front
2. Village Center District
 - a. Front Porch
 - b. Stoop
 - c. Forecourt
 - d. Shop Front
 - e. Gallery
 - f. Arcade
3. Active Urban District
 - a. Forecourt
 - b. Shop Front
 - c. Gallery
 - d. Arcade
4. Office District
 - a. Forecourt
 - b. Shop Front
 - c. Gallery
 - d. Arcade

B. Street level building frontages shall be designed according to the following descriptions and requirements.

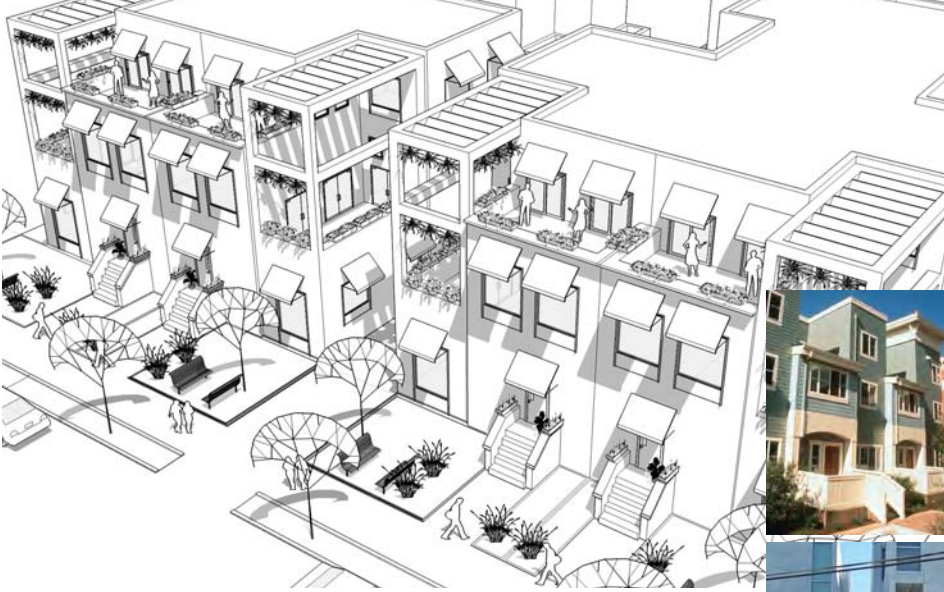
Front Porch



1. Front Porch

A front porch is frontage wherein the façade is set back from the frontage line with an attached roofed porch that extends into the front open space area. Porches shall have a minimum dimension of 8 feet in any direction.

Stoop



2. Stoop

A stoop is frontage wherein the façade is aligned close to the frontage line with the first story elevated from sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing. This type is recommended for ground floor residential uses.

Forecourt



3. Forecourt

A forecourt is a frontage wherein a portion of the façade is close to the frontage line and the central portion is set back. The forecourt created is suitable for vehicular drop offs and/or required open space areas. (Minimum dimension of a forecourt should be 20 x 20 feet). This type of frontage should be allocated in conjunction with other frontage types. Large trees within the forecourts may overhang the sidewalks.

Shopfront



4. Shop Front

A shop front is frontage wherein the façade is aligned close to the frontage line with the building entrance at sidewalk grade. This frontage is conventional for retail use. It has substantial non-reflective glazing at the sidewalk level and should include awnings at main entrances that may overlap the sidewalk consistent with the City's overhead sidewalk encroachment provisions.

Gallery



5. Gallery

A gallery is frontage wherein the façade is aligned close to the frontage line with an attached cantilevered shed or a lightweight colonnade which may overlap front open space areas. This type is convenient for retail use. The gallery shall be no less than 10 feet wide and may overlap adjacent open space areas up to within 5 feet of the parkway or sidewalk area.



Arcade



6. Arcade

An arcade is frontage wherein the façade may abut the sidewalk at the frontage line. This frontage type is conventional for retail use. The arcade shall be no less than 12 feet wide and may not overhang the sidewalk but may be within 5 feet of the parkway or sidewalk.

4.5 PUBLICLY ACCESSIBLE OPEN SPACE

In order to provide relief from the intensity of development within the MEMU Overlay Zone and provide for a high level of pedestrian connectivity and activity throughout the Metro East area, it is necessary to provide a variety of open space opportunities. Publicly accessible open space areas shall be designed to optimize linkages and connections with adjacent properties within the District. Further, open space areas shall encourage active use and pedestrian activity between the public and the private realm.

A. To achieve well-designed and highly amenitized open space areas that are accessible to the public, new development will be required to include an open space component that is accessible to the public through the main street-facing façade entry of the project. These areas may be inclusive of a front setback or driveway areas, but may not include parking areas or rear setbacks to satisfy a private open space requirement. All new development within the MEMU Overlay Zone shall provide publicly accessible open spaces as a percentage of the total development site area as follows:



Public plaza

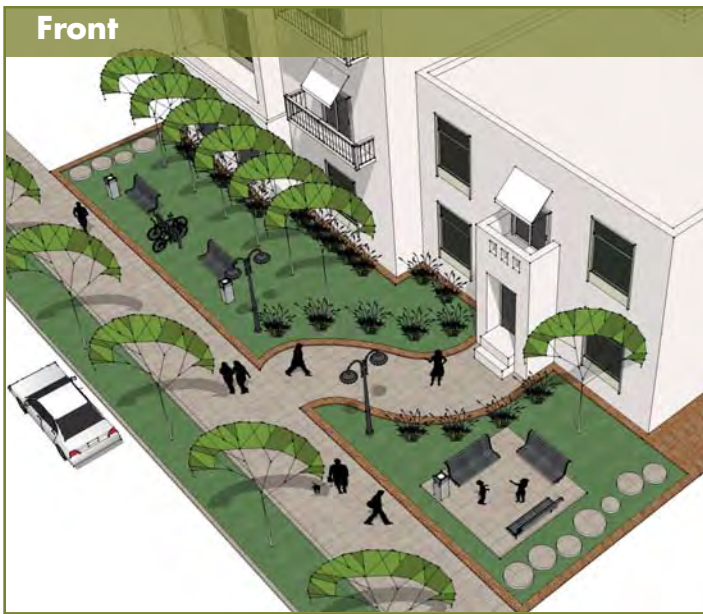
1. Neighborhood Transitional District: 5 percent
2. Village Center District: 10 percent
3. Active Urban District: 15 percent
4. Office District: 15 percent



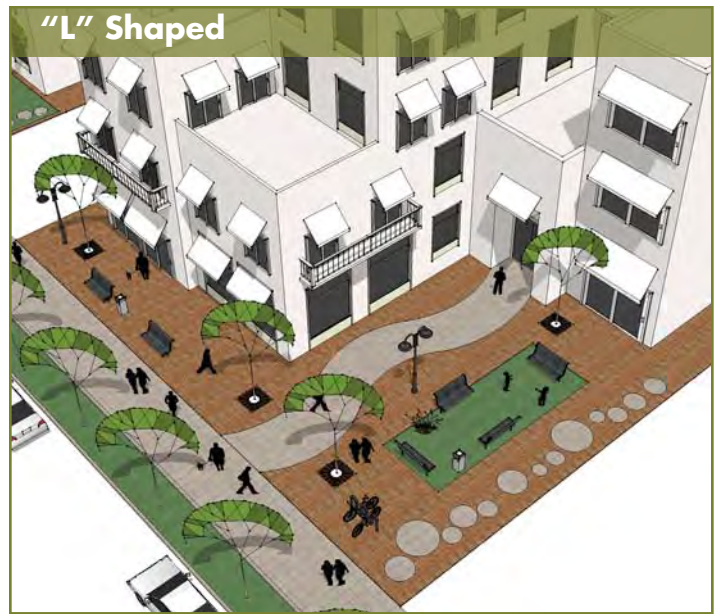
Active public/open space areas

B. In addition to the above requirements, the following provisions shall apply to the design, location, and materials used in these open space areas:

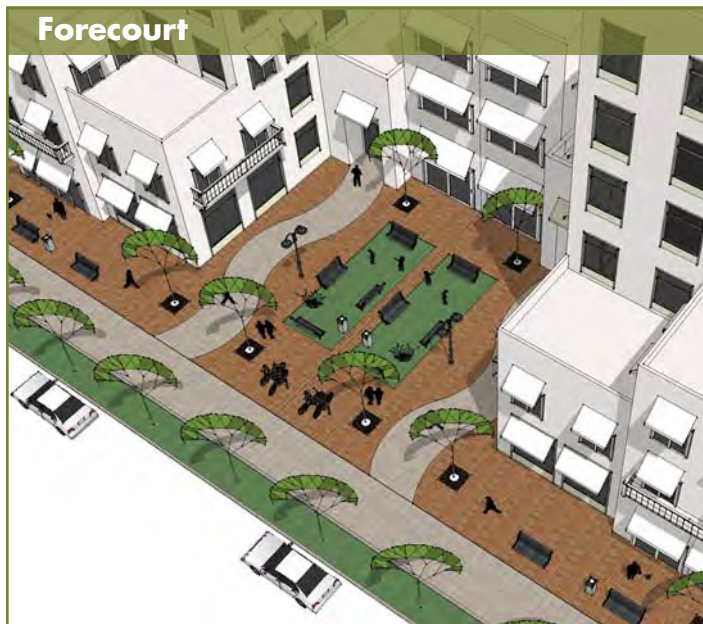
1. Plazas, courtyards, or other publicly accessible open space areas at the ground level may be used to satisfy this requirement, and shall be incorporated into the design of the development.
2. Parking and rear setback areas shall not be considered to meet this requirement. Front and side setback areas that are integrated into the design of the public open space may be considered to satisfy this requirement.
3. Public open space areas shall be visible and accessible from the public rights-of-way to engage the interest of pedestrians and encourage public use.
4. A combination of landscape and hardscape materials shall be used in the design of these areas to satisfy the following requirements:
 - a. Hardscape paving may include brick, stone, interlocking concrete pavers, textured concrete, and/or impressed patterned concrete. Hardscape elements may include, but are not limited to, seating areas, potted plant materials, water features, and public art installations.
 - b. The balance of the open space areas shall be landscaped with turf, shrubs, or groundcover, and trees. All plant materials shall be in proportion to the height and mass of the building, and shall be permanently maintained.
5. In order to achieve sunlight and air circulation in required open space areas, the following minimum height to width ratios shall be provided:
 - a. Enclosed Open Space: 2 to 1 ratio
 (Open space that is enclosed on four sides, such as a courtyard). The required open space shall have a width of at least one-half the height of the adjacent building façade (measured perpendicularly from the façade). This requirement shall apply to all sides of the required open space.
 - b. Open Space which is open on one or more sides: 3 to 1 ratio
 The required open space shall have a width of at least one-third the height of the adjacent building facade (measured perpendicularly from the façade). This requirement shall apply to all sides of the required open space.
6. The required publicly accessible open space areas shall be located and configured as one of the following:



a. Front: The publicly accessible open space area is located along the street facing frontage of the building as illustrated.



c. "L" Shaped: The publicly accessible open space area is located along the front and side of the lot as illustrated.



b. Forecourt: The publicly accessible open space area is located along a recessed center section of the front façade of the building as illustrated.



d. Paseo or Central Courtyard: The publicly accessible open space area is located on the side of the building or along a center pedestrian paseo or courtyard as illustrated.

4.6 PRIVATE/COMMON OPEN SPACE

To support and enhance the publicly accessible open space, new development within the MEMU Overlay Zone will be required to include private and/or common open space for resident, tenant and visitor use. Private and/or common open space shall be provided in addition to the required publicly accessible open space in Section 4.5.

Residential developments: Private or common open space shall be provided on a per unit basis for residential projects and shall be a combination of the total required space divided between private areas such as balconies or patios or common areas such as courtyards, recreation facilities, multi-purpose room or other areas designed for the common use of residents as specified below.

Nonresidential or mixed-use developments: Private or common open space shall be provided as a percentage of the total lot area for nonresidential projects and may be used to provide site amenities such as rooftop decks, courtyards, or similar features. Mixed use developments shall combine the residential standards and the nonresidential standard to satisfy this provision.

A. Private or common open space shall be provided in the following amounts:

1. Neighborhood Transitional District: 90 square feet per unit
2. Village Center District: 90 square feet per unit and 5 percent of the total development site area for nonresidential uses.
3. Active Urban District: 90 square feet per unit and 5 percent of the total development site area for nonresidential uses.
4. Office District: 10 percent of the total development site area

B. In addition to the above standards, all private and/or common open space areas are also subject to the following provisions in their design, location, and selection of amenities:

1. Private and common open space areas shall be designed for the exclusive use of the residents or tenants of the property and their guests and shall not be publicly accessible except in the Neighborhood Transitional District where the required private/common open space may be designed in combination with the required publicly accessible open space specified in Section 4.5.

2. In the Neighborhood Transitional, Village Center, and Active Urban Districts, a combination of private and common open space shall be provided to satisfy the following requirements:

- a. Common open space amenities shall include, but are limited to, one or more of the following amenities: courtyards, plazas, tennis courts, swimming pools, spas, permanently equipped gym/exercise rooms, or other permanent amenity. Rooftop decks and terraces may be used to satisfy this requirement, however, these areas shall be easily accessible to all residents within the building, and face the public rights-of-way where possible.
 - b. Private open space areas may include balconies, patios, terraces, or rooftop decks. These areas shall be carefully integrated into the overall architectural design of the building. Architectural elements such as railings, trellises, short walls, or roof top enclosures shall be consistent with the architectural style of the structure to which they are attached.
3. Open space areas shall be constructed of permanent materials and be permanently integrated into the design of the building.

4. In order to achieve sunlight in outdoor common open space areas, the following building height to open space width ratios are required: 1 to 1; and 2 to 1 if the space is open on one or more sides.



Private rooftop deck



Private balcony

4.7 BUILDING SETBACKS

A. Setback area requirements facilitate the incorporation of publicly accessible open space while reinforcing a continuous urban street frontage that defines the street edge. Setback requirements are designed as a range so that variety may be achieved in the public/private interface area depending on the site and building design and its relationship to adjacent developments. Setbacks are designed to coordinate with the public improvements identified in the Metro East Public Realm Amenity Plan (Appendix). New development within the MEMU Overlay Zone shall be designed to provide setback areas from private or public streets as follows:

Setbacks or Separation (1)(2)(3)		Min.	Max.
Building to Street and Parking			
Adjacent to public street right-of-way or public easement		--	10 ft (4)
Adjacent to alley or internal aisle		3 ft	--
Adjacent to parking		7 ft	--
Clear Zone		25 ft	--
Building to Property Line			
Adjacent to any residential use	1st and 2nd floor	15 ft	--
	3rd floor and above (5)	20/30 ft	--
Adjacent to all other uses	All floors	5 ft	--
Building to Building			
Up to 3rd floor		6 ft	--
4th floor and above		15 ft	--

Notes

1. Setbacks are measured from the closest point of a building to the property line or public easement, except for clear zones. If a front yard/porch frontage is used, the building facade may be setback a maximum of 15 feet.
2. Additional setbacks for entry plazas or courtyards, or to meet adjacent structures, may be permitted subject to additional design review. Frontage types may be used to satisfy setback requirements.
3. Building planes above 3rd floor shall have, at a minimum, a 50% variation in setback within the specific standards in Table 4.
4. If a development project has multiple frontages, the maximum setback along one street frontage may be exceeded if the area is used for publicly-accessible open space.
5. For building with seven or more stories, the third floor and above must be set back a minimum of 30 feet from any residential use.

6. Clear Zones:

- a. Clear zones are required at driveway, street, or alley intersections to provide adequate line of sight for drivers, bicyclists, and pedestrians as they approach intersections. The clear zone shall consist of an isosceles right triangle with 25-foot sides along the property line.
- b. The clear zone shall not be occupied by a ground floor building facade. Site and building features that are taller than 30 inches in height, including utilities, mechanical equipment, fences, and landscaping, are prohibited in the clear zone.
- c. Upper floors may extend over the clear zone. Awnings must maintain a vertical clearance of at least 15 feet within the clear zone.

B. In addition to the above setback requirements, the following provisions shall also apply to new development projects:

1. In order to encourage flexibility and provide an organic feel in the design and building-street interaction, there is no minimum setback requirement within the MEMU Overlay Zone, with the exception that all structures shall maintain the maximum rear setback specified if the property abutting on the rear property line is developed solely as a residential use.
2. Setbacks abutting public right-of-way may be further recessed from the maximum setback specified for the purpose of a public open space, dining/gathering or special entry area. These areas may not satisfy private open space requirements.
3. Setbacks abutting public rights-of-way shall be landscaped with lawn, trees, shrubs, or other plants and/or decorated as a hard surface expansion of the sidewalk. A combination of landscape and hardscape materials shall be provided as follows:
 - a. Hardscape paving may include brick, stone, interlocking concrete pavers, textured concrete, and/or impressed patterned concrete. Hardscape elements shall also be provided when setback area is being used to satisfy publicly accessible open space areas. These elements may include, but are not limited to, seating areas, potted plant materials, water features, and public art installations.
 - b. The balance of the setback areas shall be landscaped with turf, shrubs, or groundcover, and trees. All plant materials shall be in proportion to the height and mass of the building, and shall be permanently maintained.

4.8 PARKING AND ACCESS

Adequate parking is necessary to service the full-range of uses permitted within a development project. In order to enhance the appearance and character of the Metro East area, parking should not be a part of the pedestrian level streetscape. The goal of the Metro East area is to provide adequate parking that encourages people to “park once” and conveniently walk to multiple destinations in the same area. Standards below reflect this concept except in the Neighborhood Transition District where parking is established at a rate that does not impact the adjacent residential neighborhood to the north.

The parking standards for the MEMU Overlay Zone are designed to accommodate the wide variety of uses in the area while providing an incentive for mixed-use projects that encourage an active street life and convenient services and leisure activities for Metro East residents and visitors. A mixed-use development is defined as any development that contains more than one type of use in a building or set of buildings and contains within the development a residential component. Stand alone uses are defined as operations or uses that are not a component of an integrated mixed-use development project. Dimensions of parking spaces and drive aisles shall be consistent with SAMC Article XV, Division 1.

A. Parking within the MEMU Overlay Zone shall be required for new development in each district as follows:

1. Neighborhood Transitional District

- a. Stand-alone residential: 2.25 spaces per residential unit, inclusive of guest parking
- b. Office or other permitted nonresidential uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.

2. Village Center District

- a. Stand-alone nonresidential Uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.
- b. Stand-alone Residential Uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.
- c. Mixed-use Developments with less than 10 percent of the gross floor area devoted to a commercial activity: A minimum of 2.0 spaces per residential or live/work unit inclusive of guest parking and any nonresidential uses.

- d. Mixed-use developments with 10 percent or greater of the gross floor area devoted to a commercial activity: Any development proposal that devotes 10 percent or more of the development's gross floor area to a nonresidential use shall be required to provide a parking study by a city approved consultant to establish an adequate parking requirement for the mixture of uses in the proposed development. In no case, however, shall a standard of less than 2.25 spaces per unit inclusive of guest parking and any nonresidential uses be established.

3. Active Urban District

- a. Stand-alone Nonresidential Uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.
- b. Stand-alone Residential Uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.
- c. Mixed-use Developments with less than 10 percent of the gross floor area devoted to a commercial activity: A minimum of 2.0 spaces per residential or live/work unit inclusive of guest parking and any nonresidential uses.
- d. Mixed-use developments with 10 percent or greater of the gross floor area devoted to a commercial activity: Any development proposal that devotes 10 percent or more of the development's gross floor area to a nonresidential use shall be required to provide a parking study by a city approved consultant to establish an adequate parking requirement for the mixture of uses in the proposed development. In no case, however, shall a standard of less than 2.25 spaces per unit inclusive of guest parking and any nonresidential uses be established.

4. Office District

- a. Office or other permitted nonresidential uses: As required by Division 3 of Article 15, Off-Street Parking Requirements, SAMC.

B. In addition to the parking requirements above, new developments in the MEMU Overlay Zone shall provide parking in compliance with the following:

1. Guest parking at a rate of 10 percent of the total required parking spaces shall be set aside and assigned for the exclusive use of guests in any development project within the MEMU Overlay Zone.
2. All residential units shall be provided a minimum of one (1) assigned space per unit.

In order to provide well-designed parking areas that enhance the pedestrian experience and adequately provide for the mixture of uses that may occur on a development site, the following standards shall be required for all new developments within the MEMU Overlay Zone:

3. Parking facilities (surface or structures) shall be located below grade, behind buildings, and/or screened from the main right of way and all active public areas, except for areas designed for and devoted to vehicular access, drop off, or valet parking. Those portions of new parking structures that face a public street shall include commercial spaces or other pedestrian oriented design features at the ground floor level.



Screening of parking structures along main streets

4. Parking spaces specifically designated for nonresidential and residential uses shall be clearly marked by the use of posting, pavement markings, and/or physical separation. Parking area design shall incorporate a separation of the parking for nonresidential and residential uses, except that guest parking may be combined with nonresidential parking as long as the total required parking for the development is not reduced.
5. Driveways shall be a maximum of 24 feet wide, in order to enhance the pedestrian experience, except as required to satisfy fire and/or waste collection requirements. A maximum of one curb cut shall be permitted for each parcel with up to 150 feet of street frontage. Development sites exceeding 150 feet of street frontage shall be permitted a maximum of two curb cuts. Three curb cuts shall be permitted only if a separate residential entrance is required.

5.0 Design Principles

The MEMU Overlay Zone establishes qualitative design principles that will be used by Staff and decision makers in evaluating proposed projects. This section establishes qualitative design standards intended to guide the developer to achieve the image and design desired for the Metro East Overlay Zone. Staff and decision makers will use these principles to review proposed development projects and determine their approval based on the extent to which the plans address the intent of the City for the area as part of the MEMU Overlay Zone Site Plan Review process.



Architectural/massing variety

5.1 INTEGRATED DESIGN

The design of buildings, signs, landscaping, and other structures or elements shall feature a unified and integrated theme.

5.2 ARCHITECTURAL QUALITY

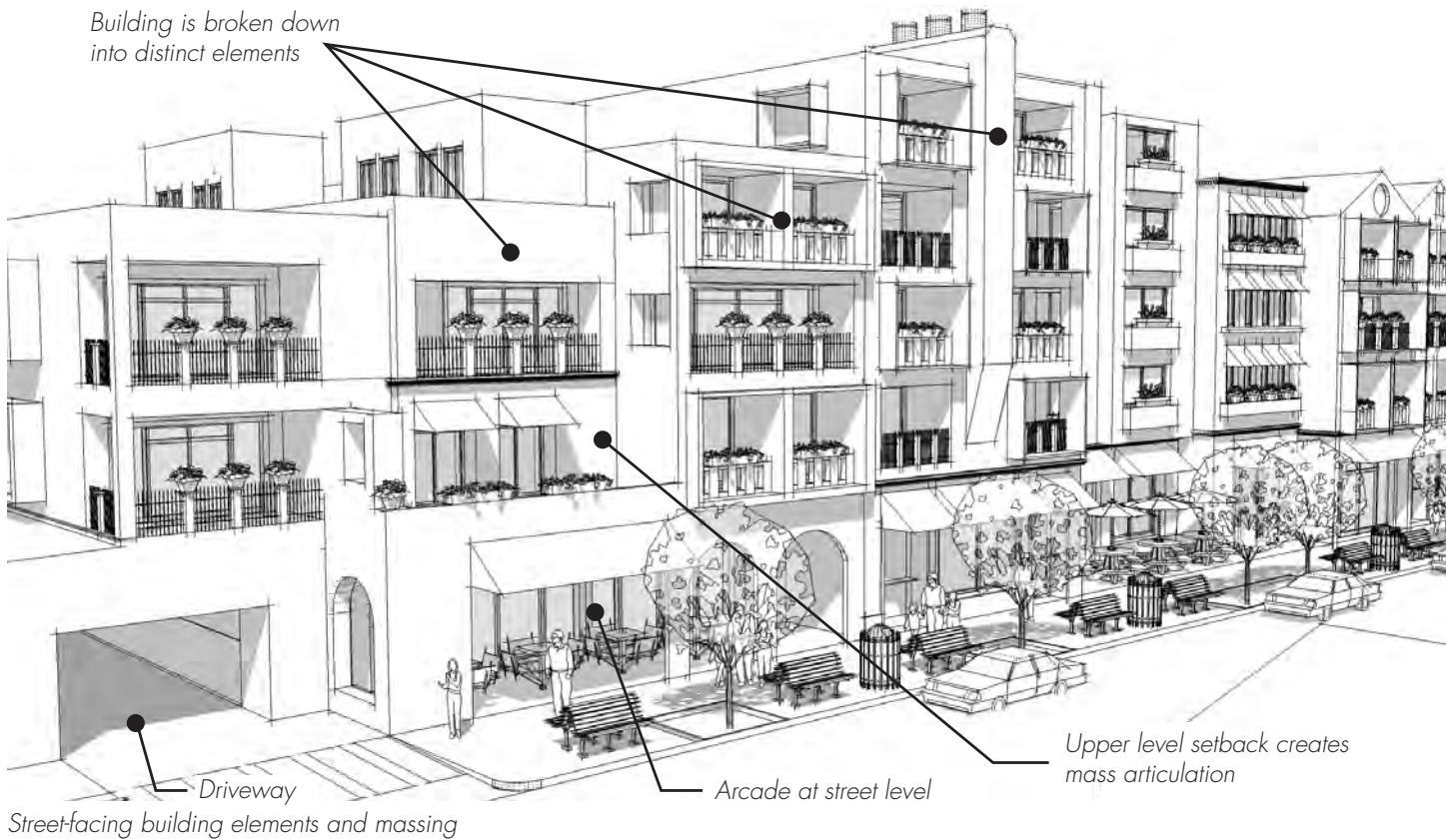
Projects shall utilize materials, window and door systems, hardscape, softscape, and water features, and architectural details and components throughout that convey a distinct sense of endurance and quality.

5.3 ARCHITECTURAL VARIETY

Projects shall utilize architectural means such as varied massing, breaks in plane, a diverse palette of high-quality materials, and changes in form, proportion and detail, as well as a variety of unit types to create variety and differentiation between and within project components.

In order to further promote variety and interest within the project as a whole, the use of more than one architect is required for projects on development sites with an area that exceeds two (2) acres. This requirement can be met by compliance with any one of the following as approved by the Planning Manager at the time of the initial Site Plan Review:

- a. Architectural design is completed by two different firms
- b. Architectural design is peer reviewed by a second architectural firm
- c. Architectural design is completed by different studios within one architectural firm
- d. Applicant conducts an architectural design workshop with City staff in the early phase of design



5.4 MASSING

- In general, project massing should be broken down into discrete building elements and components that reinforce a continuous street-facing urban block pattern. This pattern should be broken by intervening the required publicly accessible open space areas. See Section 4.5.
- Within this block pattern, individual buildings shall be massed such that individual building planes or architectural masses do not exceed one-hundred-fifty (150) feet in length.
- The design of buildings shall incorporate a variety of massing and forms to introduce variety at the ground plane and skyline of the project.

5.5 FACADES

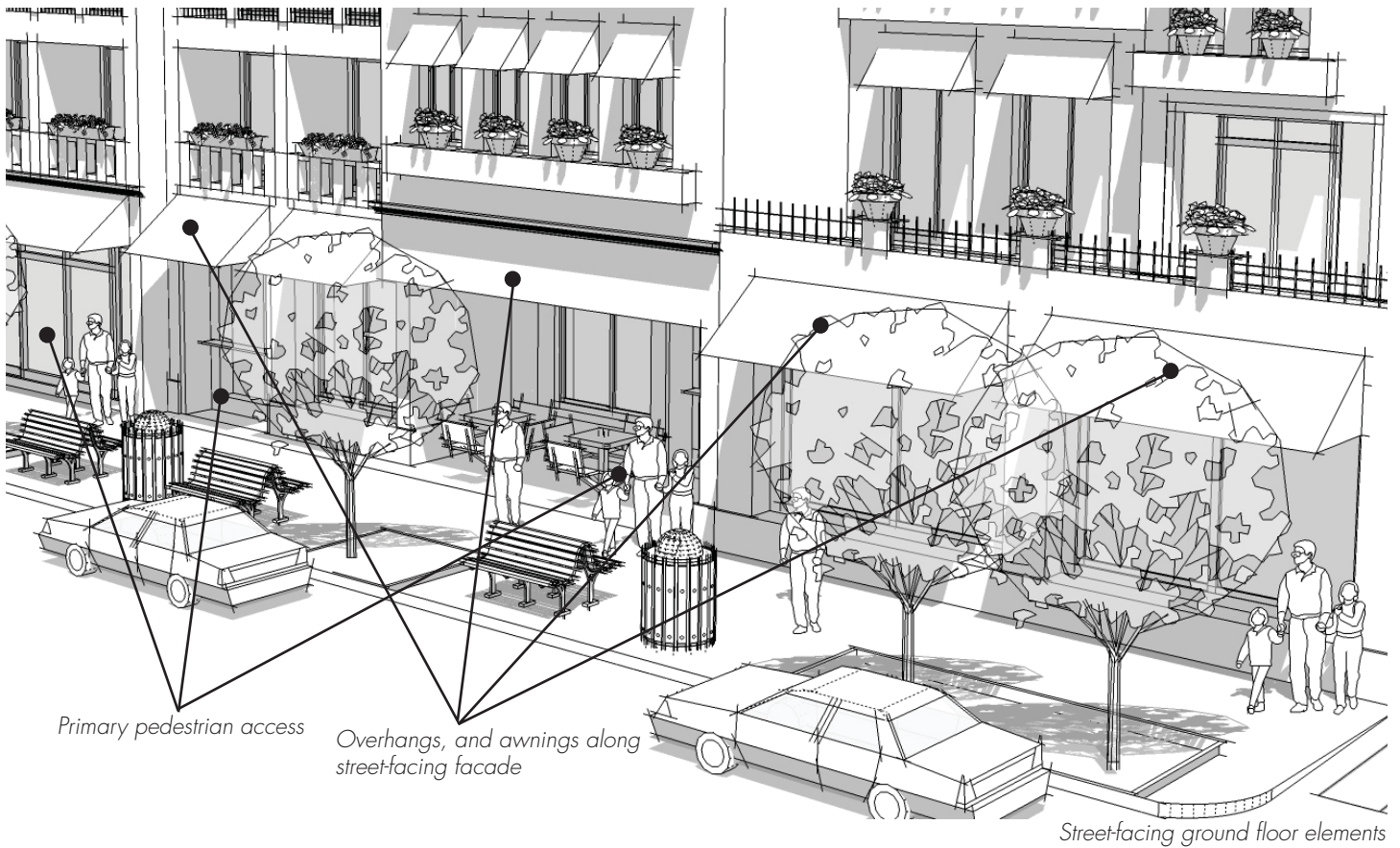
- Street facing facades shall be architecturally enhanced through the use of arcades, colonnades, recessed entrances, window details, bays, variation in building materials, and other details such as cornices and contrasting colors.
- Building step backs at the upper level of buildings along public rights-of-ways in the Neighborhood Transitional, Village Center, and Active Urban Districts shall be provided as a means to provide building articulation.



Street-facing facade elements

The building articulation may include private/common open space terraces or roof decks. The minimum upper level setback area shall be 10 Ft wide (parallel to the building façade) by 6 Ft deep.

- Buildings facades shall be articulated through the use of separated wall surfaces, contrasting colors and materials, variations in building setbacks, and attractive window fenestrations.



Contrasting colors and materials

d. Street facing facades shall be integrated with public plazas, mini parks, outdoor dining, and other pedestrian-oriented amenities where feasible.

5.6 GROUND FLOOR USES

- a. Ground floor commercial or retail uses oriented to the street shall provide primary pedestrian access directly from the adjacent public street frontage.
- b. Ground floor commercial or retail uses oriented to the street shall incorporate the use of overhangs, awnings, or trellis work for at least 60 percent of the frontage.

5.7 COMMUNITY CONNECTIONS

Project site plans shall create community connections and linkages between developments within the various Districts within the MEMU Overlay Zone area, as well as facilitate comfortable and visually interesting pedestrian and bicycle movements.

- a. *Pedestrian- and sidewalk-oriented buildings.*
Projects as a whole, as well as individual building components, shall reinforce activity and pedestrian scale along adjacent sidewalks through the use of active building entries and frontages, and/or publicly accessible open space fronting the streets.

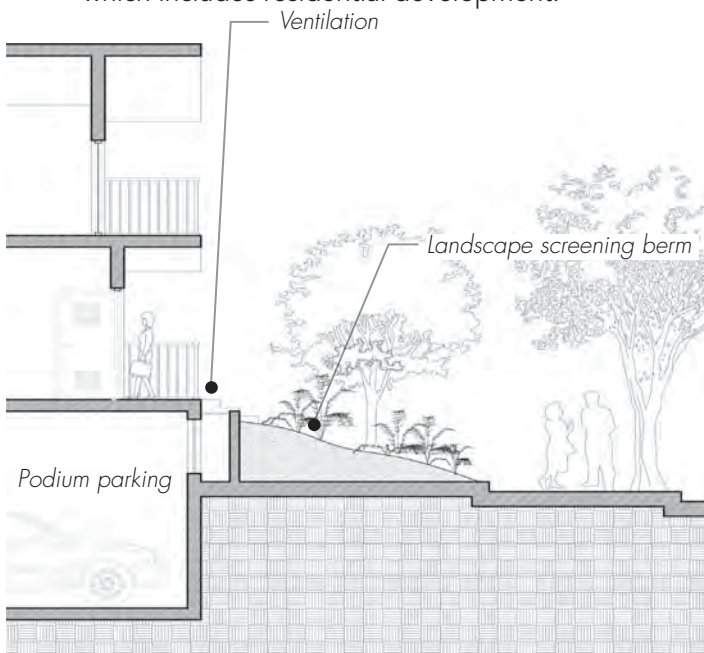


Pedestrian/bicycle oriented streetscape

- b. *Pedestrian-oriented sidewalks and streetscapes.* Projects as a whole shall incorporate design and development features that facilitate pedestrian and bicycle-scale orientation at the street level. These features include, but are not limited to, street trees, improved medians with trees, widened sidewalks, pedestrian-scale lighting, integrated bicycle paths, curbside parkways, and feasible curbside parking to create a streetscape that accommodates increased and comfortable pedestrian and bicycle use.

5.8 LANDSCAPE BUFFERS

- a. In order to establish a strong public street and sidewalk edge, building massing shall establish variation in relationship of the building frontages to the street. Landscape buffers should be provided along driveways which are adjacent to any project which includes residential development.



Podium parking screening

- b. Landscape buffers in addition to other screening techniques should also be provided as a means to screen trash collection areas, and above grade surface parking.

5.9 PARKING

Parking areas, whether at grade or in a structure, shall be completely screened from the public street view. Above-grade parking structures shall be completely screened at the street level by architectural features including retail spaces, stoops, porches, landscaping or other pedestrian oriented design features.

5.10 ENERGY CONSERVATION—SHADE AND SHADOW

Energy consumption shall be minimized using the following features where feasible: cogeneration, solar access, south facing windows with eave coverage, double glazed windows, deciduous shade trees, good ventilation, efficient lighting, and day lighting.

Individual units, where possible, should be oriented such that they receive a minimum of two (2) hours of direct sunlight in major living spaces such as living rooms, dining rooms, dens, great rooms, or kitchens on the day of the winter solstice.

5.11 RESIDENTIAL UNIT SIZE

An average unit size of 1,200 square feet per unit will be used as a guideline to assess the overall quality and livability of any residential units. The mix of unit sizes within a project should be more heavily weighted towards units larger than 1,000 square feet in size.

6.0 Signs

Building identification and signage can be used as a creative tool to enhance the look and design of a development project. When signs are designed to complement the form and function of the building and its users, it contributes to the aesthetics of the project and aids in way finding. The City desires to encourage creative sign proposals where the standard provisions of Article XI do not well suit a particular need, situation, or circumstance. In exchange, certain signs that otherwise would be allowed by Article XI might not be allowed.



Main access signage

6.1 MASTER SIGN PROGRAM

Building identification and signage can be used as a creative tool to enhance the look and design of a development project. When signs are designed to complement the form and function of the building and its users it contributes to the aesthetics of the project and aids in way finding. The provisions of this section encourage creative sign proposals where the standard provisions of the sign code may not well suit a particular need, situation, or circumstance. In exchange, certain signs that otherwise would be allowed might not be allowed in the MEMU Overlay Zone.

Prior to issuance of any sign permits or certificates of occupancy for any building or portion thereof, a master sign program for the entire site, including directional signs and graphics for any parking structure, shall be submitted to the Planning Commission as part of the MEMU Site Plan Review application. The purpose of the Master Sign Program is as follows:

- a. The primary purpose of a master sign program is to provide a mechanism by which the sign regulations established in the SAMC can be modified to ensure that signs for a uniquely planned or designed development are most appropriate for that development or area.
- b. A master sign program may include sign regulations that are, at the same time, both more restrictive in some respects and less restrictive in other respects than the regulations established in the SAMC. Less restrictive provisions in a master sign program may also include signs that are otherwise prohibited by the SAMC.
- c. Each master sign program application shall show to the Planning Commission's satisfaction why the modifications requested are warranted and how the total sign proposal for the development meets the general purpose and intent of the SAMC.



Secondary access sign

- d. The Planning Commission shall approve, conditionally approve, or disapprove any application for a master sign program based on the signage and its compatibility with the proposed development project based on the following findings:
 - 1. Compliance with the objectives of the MEMU Overlay Zone as specified in Section 1.2.
 - 2. Compliance with the purpose and objectives outlined in the Citywide Design Guidelines; and
 - 3. Compliance with the purpose and intent of Section 41-860 et seq. except for the variations specified in the Master Sign Program application.
- e. Applications for a Master Sign Program that are denied by the Planning Commission may be appealed to the City Council. Appeals from decision of the Planning Commission shall be processed and considered consistent with SAMC Chapter 41, Article 5.

7.0 Compatibility/Operational Standards

The following standards are intended to ensure the compatibility of the mix of residential and nonresidential uses that may occur within one building or on a development site in the MEMU Overlay Zone. These standards are intended to mitigate the potential impacts that may occur as a result of the interface between nonresidential and residential uses within one project.



Compatible design: Public open space, retail on first level, and residential uses on top

7.1 SECURITY

Residential units shall be designed to ensure the security of residents through the provision of secured entrances and exits that are separate from the nonresidential uses and are directly accessible to parking areas. Nonresidential and residential uses shall not have common entrances that provide direct access to residential units. The separation of entrances shall be shown on the development plan and the separations shall be permanently maintained.

7.2 RESTRICTION ON ACTIVITIES

Commercial uses shall be designed and operated, and hours of operation limited, so that neighboring residents are not exposed to offensive noise, especially from traffic, trash collection, routine deliveries, or late night activity. There shall be no loading or unloading of trucks between the hours of 9:00 P.M. and 9:00 A.M.

7.3 LOADING AND UNLOADING

Every building involving the receipt or distribution by vehicle of materials or merchandise incidental to carrying on such activity shall provide adequate area for the loading and unloading of vehicles, include moving vans, garbage trucks, and other delivery vehicles.



Mixed use along corridor

7.4 VIBRATIONS AND ODORS

No use, activity, or process shall produce vibrations or noxious odors that are perceptible without instruments by the average person at the property lines of the site or within the interior of residential units on the site.

7.5 LIGHTING

Outdoor lighting associated with commercial uses shall not adversely impact surrounding residential uses, but shall provide sufficient illumination for access and security purposes consistent with the lighting standards in Chapter 8 of the SAMC (Security Ordinance).

7.6 WINDOWS

Residential windows shall not directly face loading areas and docks. To the extent windows of residential units face each other, the windows shall be offset to maximize privacy.

8.0 Implementation

This Division includes development Standards and design principles that are designed to work in concert to facilitate the vision for the long-term development of the MEMU Overlay Zone. Successful implementation of this Division requires effective and coordinated administration of these required standards and principles by professional staff on a cooperative basis with developers and property owners with the goal of expediting the entitlement/permit process for projects consistent with the plan.

8.1 MEMU SITE PLAN REVIEW BY PLANNING COMMISSION

A. Any application for development subject to the provisions of the MEMU Overlay Zone shall require filing an application with the Planning Manager for approval of a MEMU Site Plan Review by the Planning Commission. The application and development plan shall contain the materials outlined in the Planning Division's Development Review Committee Checklist which includes but is not limited to the following materials that shall demonstrate conformance with the provisions of this section and other required sections of the SAMC:

1. A site plan consisting of architectural drawings or sketches and plot plans, all to a workable scale and fully dimensioned, showing the elevations of the proposed building or structure, off-street parking and other physical features such as trees, hydrants, poles, and other installations, and any other such information as determined necessary by the Planning Manager to fully evaluate any requirement of a building permit.
2. A landscape design plan, pursuant to the requirements of Section 41-1507, showing fully dimensioned private, common, and public open space areas.
3. A master sign program, pursuant to the requirements of Section 6.

B. Upon receipt of a MEMU Site Plan Review application, the same shall be reviewed by City staff to determine conformance with the provisions of the MEMU Overlay Zone and referred to the Planning Commission for review and recommendations. The Planning Commission shall review said plans for the purpose of ensuring that buildings, structures, and grounds will be in keeping with the compatibility standards and design principles of this division, and

will not be detrimental to the harmonious development of the city or impair the desirability of investment or occupation in the MEMU Overlay Zone. Depending on the level of conformance with the findings of the MEMU Overlay Zone, the Planning Commission may take any of the following actions:

1. Approve the development plan
2. Approve the development plan with conditions
3. Deny the development plan

8.2 FINDINGS FOR GRANTING A MEMU OVERLAY ZONE SITE PLAN REVIEW APPROVAL

The Planning Commission may grant a MEMU Overlay Zone Site Plan Review approval subject to finding the following:

- a. That the proposed development plan is consistent with and will further the objectives outlined in Section 1.2 for the MEMU Overlay Zone.
- b. That the proposed development plan is consistent with the development standards specified in Section 4 of the MEMU Overlay Zone.
- c. That the proposed development plan is designed to be compatible with adjacent development in terms of similarity of scale, height, and site configuration and otherwise achieves the objectives of the Design Principles specified in Section 5 of the MEMU Overlay Zone.
- d. That the land use uses, site design, and operational considerations in the proposed development plan have been planned in a manner that will result in a compatible and harmonious operation as specified in Section 7 of the MEMU Overlay Zone.

8.3 CONDITIONAL USE PERMITS, VARIANCES AND MINOR EXCEPTIONS

Conditional use permits, variances and minor exceptions in the MEMU Overlay Zone shall be processed and considered pursuant to SAMC Chapter 41, Article 5.

8.4 TIME LIMITS

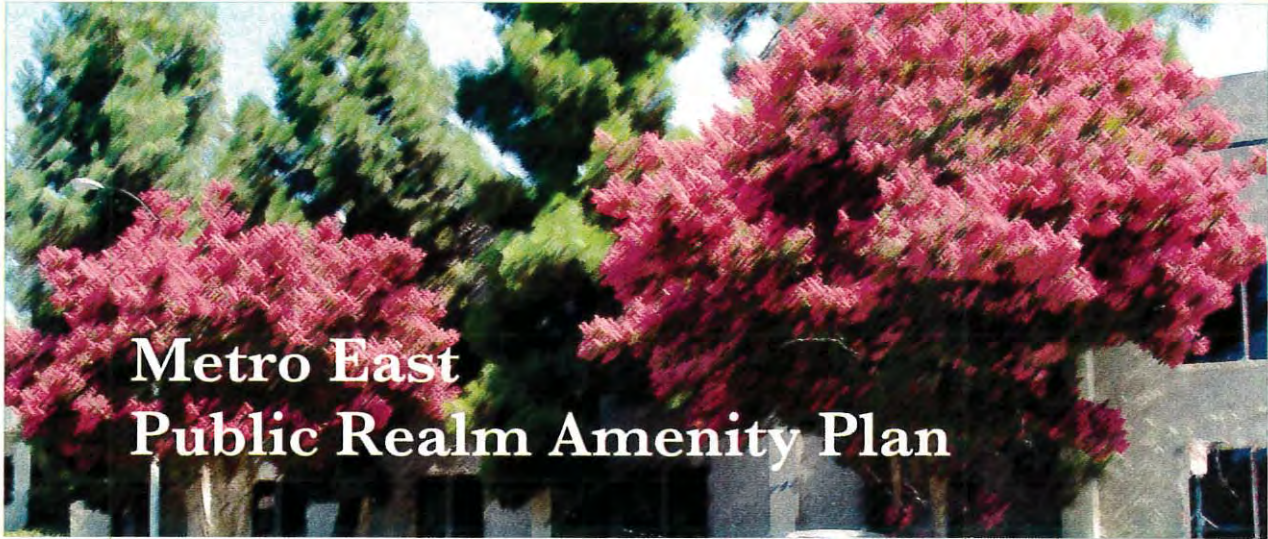
MEMU Site Plan Review shall automatically become void after two (2) years from the effective date of such approval when the owner fails to institute an action to erect, build, alter, move or maintain the use of the property as specified in the terms and conditions of the MEMU Site Plan Review. City Council may, by resolution, extend the date on which a MEMU Site Plan Review becomes void for a period or periods not exceeding three (3) years in total beyond the date it would otherwise become void.

Acceptable action shall be considered to be actual construction, alteration, repairs and use of the structures and land. Preparation of plans, financial negotiations, or change of property owners are not considered sufficient evidence of action.

8.5 DEVELOPMENT CAPACITY MONITORING AND ENVIRONMENTAL REVIEW

In order to maintain conformity with the provisions of the environmental analysis completed for this project under the California Environmental Quality Act (CEQA), development capacity within the MEMU Overlay Zone shall be limited to 1.27 million gross square feet of commercial (retail and service) and 3.41 million gross square feet of office space. This corresponds to a potential net increase of 963,000 square feet of commercial, and 690,000 square feet of office space, and the potential for 5,551 residences above existing development constructed prior to March of 2007. When new development within the MEMU Overlay Zone reaches 80 percent of the allowable increased capacity, shall be entitled until an environmental review, including a traffic study, has been completed pursuant to CEQA. These development capacity "triggers" are as follows:

- a. Commercial—770,400 square feet
- b. Office—552,000 square feet
- c. Residential—4,440 units



**Metro East
Public Realm Amenity Plan**

City of Santa Ana
February 2007

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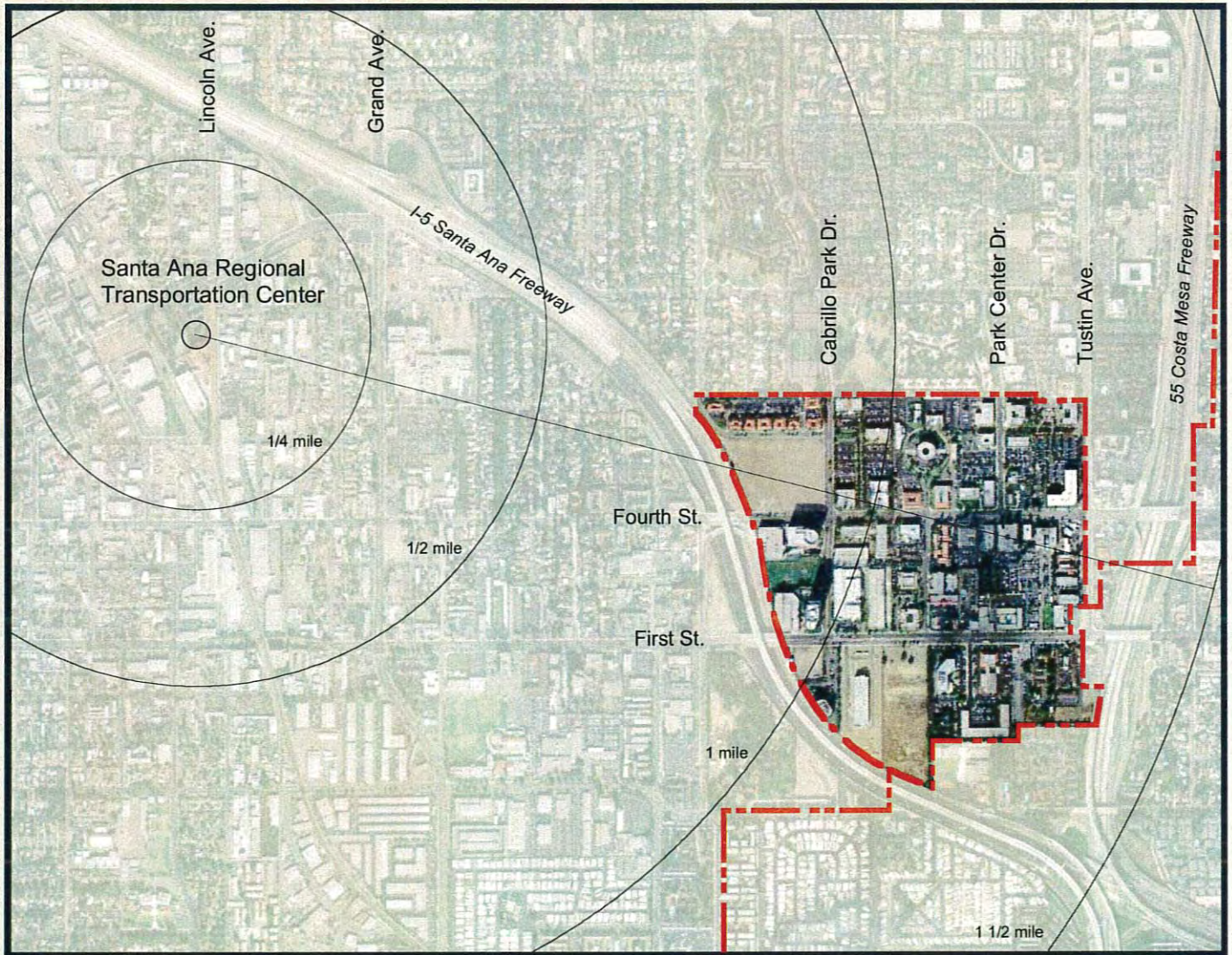
Introduction

The Metro East Public Realm Amenity Plan is an outline of streetscape and open space improvements that reinforce the new uses proposed for the Metro East Mixed Use Overlay Zone. The improvements of this Plan are in addition to the normal infrastructure improvements that are consequent to new development. These improvements are proposed to ensure that the public realm complements and supports the introduction of residential uses into this formerly commercial area. Additional street trees, pedestrian scale lighting, district identity signs, water features, improvements to the character of Golden Circle Drive, and the acquisition of new open space amongst other improvements are all proposed to establish the amenities required to both attract and support quality residential and mixed use development. The goal of this public realm plan is to ensure that appropriate high quality streetscape and open space improvements are realized as this area emerges as a mixed-use urban village.

Site and Planning Context

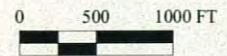
1.1

Bounded by the Santa Ana Freeway to the East and the Costa Mesa Freeway to the West, the Metro East district covers approximately 147 acres of land in the north-eastern corner of the City of Santa Ana.



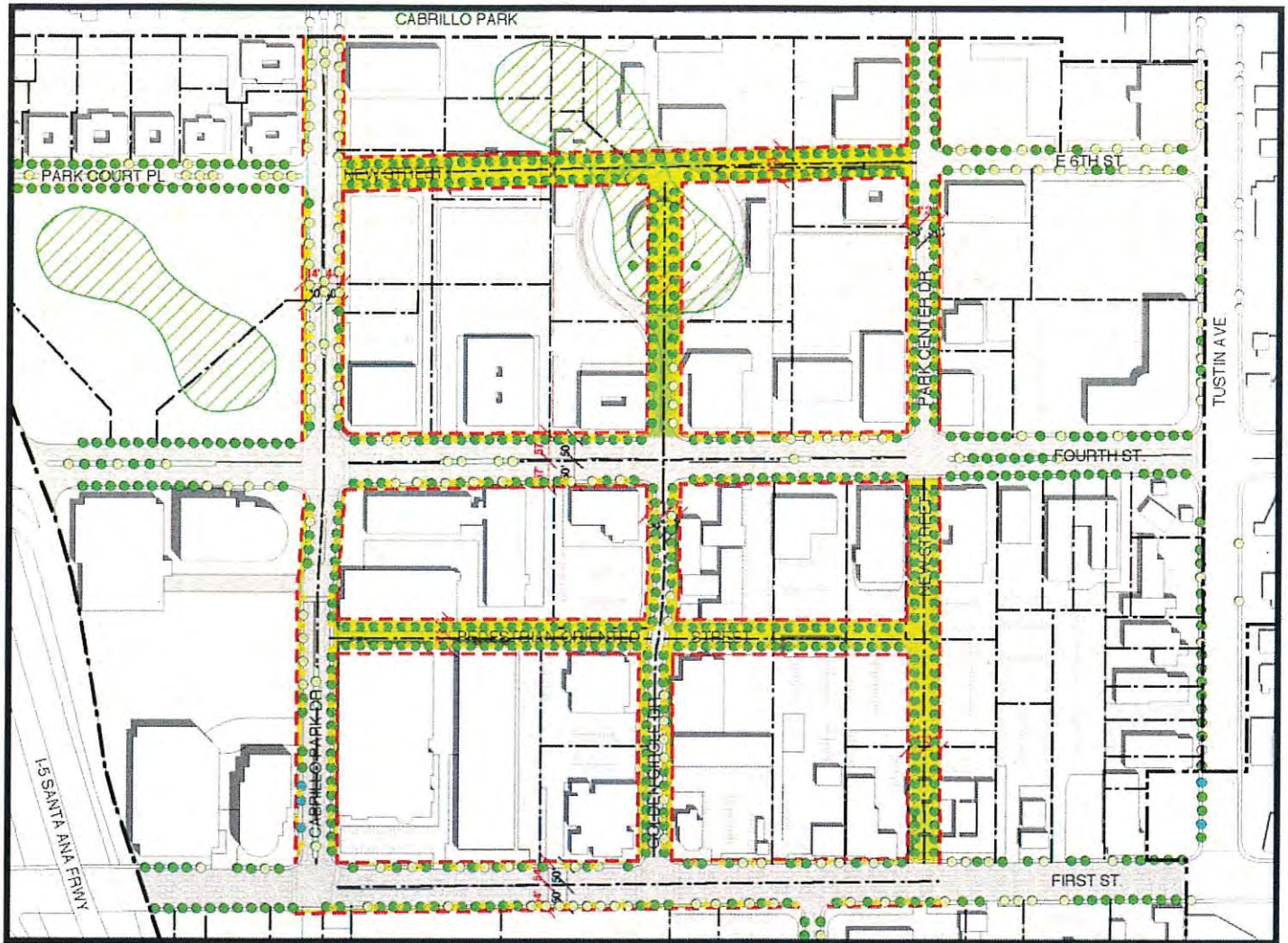
Key

- Study Area
- City Boundary



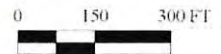
Metro East Public Realm Amenity Plan

In conjunction with the Metro East Mixed-Use Overlay Zone, this amenity plan provides a framework from which to build a clear identity for the district.



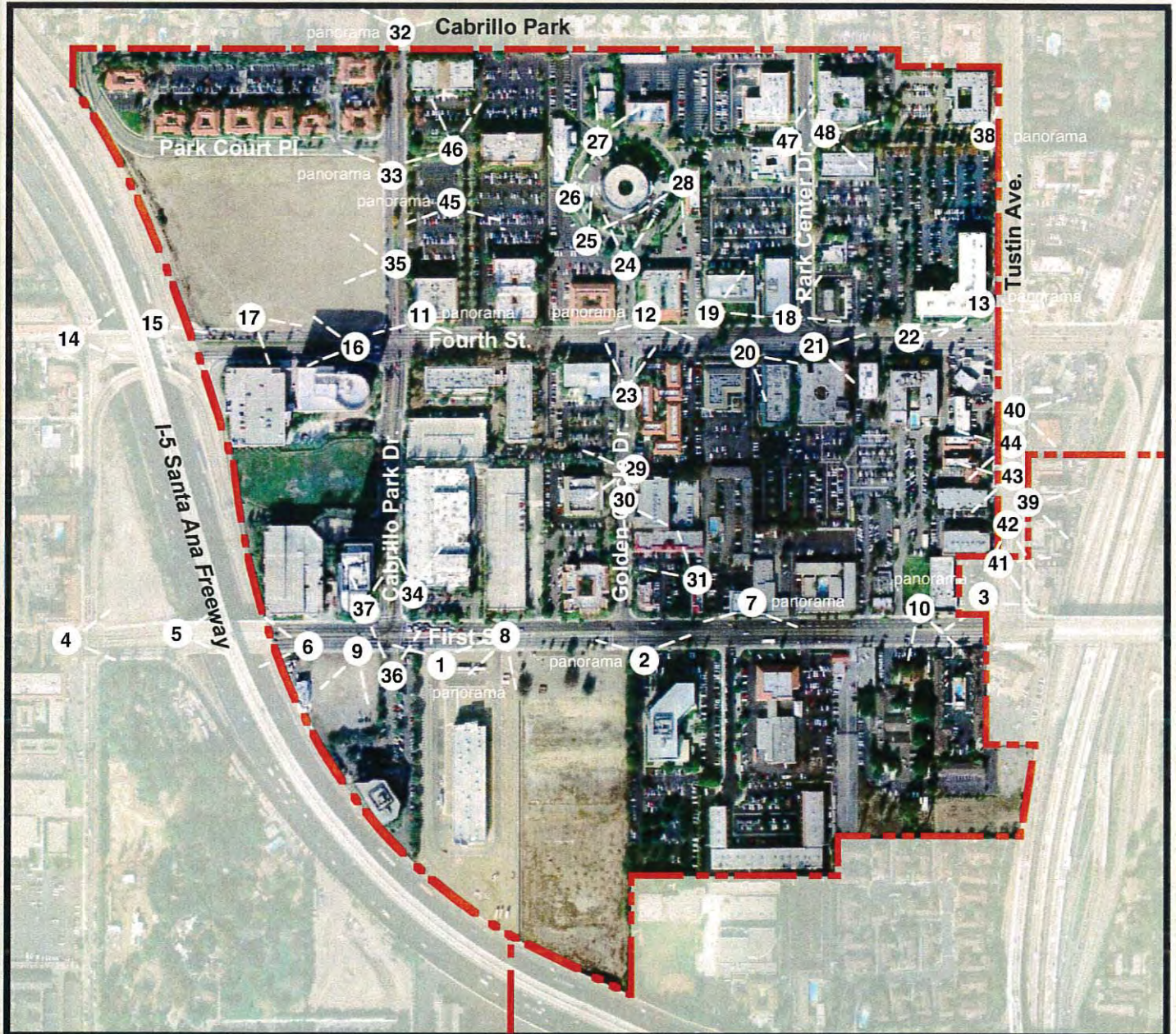
Key

- Study Area
- City Boundary
- Existing Parcel Lines
- Setback Lines
- Existing Streets
- Additional Setback Easements
- Public Open Space
- Existing Street Trees
- Existing Palm Trees
- New Street Trees
- New Pedestrian Street Lights



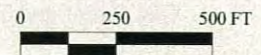
2.0 Existing Conditions Photo Survey Locations

Urban Studio documented conditions in the field, the streets, sidewalks and open spaces that define the area's public resources. In general, district open space character has an incomplete sense of place with few fully realized streetscapes and little connectivity to the surrounding area.



Key

- - - Study Area
- · - · - City Boundary



Existing Conditions First Street

First Street is automobile oriented. There is no sense of arrival or signage that marks the district. Street tree plantings are inconsistent and in many locations non-existent.



1 First Street and Cabrillo Park Drive Looking West to East



2 First Street and Golden Circle Drive Looking North



3 First Street and Tustin Avenue Looking North

**Existing Conditions
First Street
(continued)**

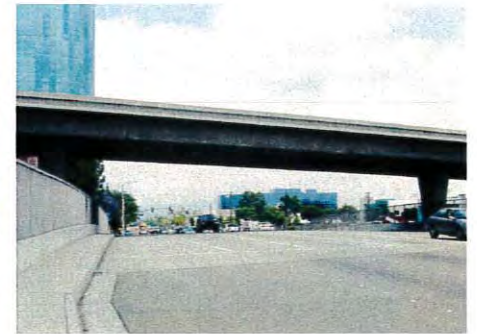
The First Street environment mainly addresses the functional requirements of the automobile.



4 First Street View East



5 First Street and Freeway Looking East



6 First Street View West to Freeway



2 First Street Panorama South Side



8 First Street South Side



9 First Street South Side



10 First Street South Side

Existing Conditions Fourth Street

Fourth Street is the central east-west street that bisects the district, but has missed median opportunities and no sense of arrival from the surrounding area.



11 Fourth Street and Cabrillo Park Drive Looking East to West



12 Fourth Street and Golden Circle Drive Looking North



13 Fourth Street and Tustin Avenue Looking West to East

**Existing Conditions
Fourth Street
(continued)**

Inconsistent street tree plantings along Fourth Street and underutilized landscaped medians provide little sense of identity.



14 Fourth Street Looking East to District



15 Fourth Street Looking South at Freeway



16 Fourth Street Looking West to Freeway Overpass



17 Fourth Street Looking South to Commercial Building



18 Fourth Street and Park Center Drive Looking East



19 Fourth Street Looking Northwest from Intersection on Golden Circle Drive



20 Fourth Street and Tustin Avenue Looking West to East



21 Fourth Street Looking Southeast at Golden Circle Drive



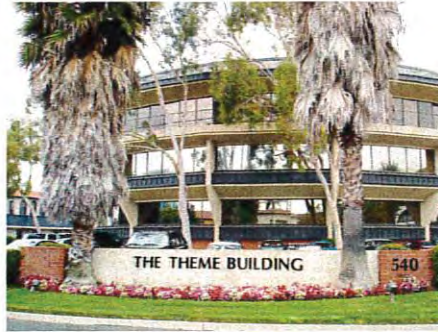
22 Fourth Street Looking East Towards Tustin Avenue

Existing Conditions Golden Circle Drive

Golden Circle Drive is the main centrally located north-south street that serves the district.



23 Fourth Street Looking North at Golden Circle Drive



24 The Theme Building Looking North at Golden Circle Drive



25 The Theme Building from the Southwest



26 Golden Circle Drive: Western Section Looking North



27 Golden Circle Drive: Northern Section



28 Fourth Street Looking Northwest from Intersection with Golden Circle Drive



29 Golden Circle Drive West Side



30 Golden Circle Drive East Side



31 Golden Circle Drive East Side

Existing Conditions Cabrillo Park Drive

Cabrillo Park Drive connects the existing uses to the major public amenity in the area, Cabrillo Park.



32 Cabrillo Park



33 Cabrillo Park Drive and Park Court Place



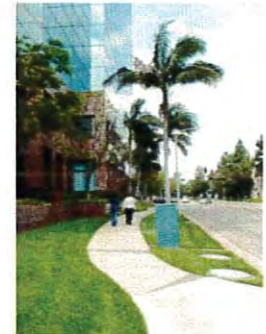
34 Cabrillo Park Drive
East Side



35 Cabrillo Park Drive View to Freeway



36 Cabrillo Park Drive and First Street



37 Golden Circle Drive
East Side

Existing Conditions Tustin Avenue

Tustin Avenue provides a connection to the public bus transportation lines running north and south through Orange County.



38 Tustin Avenue and East 6th Street Looking North to South



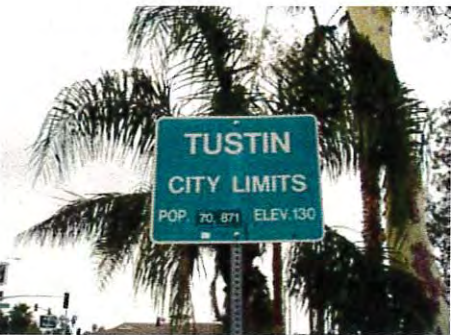
39 Tustin Avenue East Side



40 Tustin Avenue East Side



41 Tustin Avenue East Side



42 Tustin Avenue West Side



43 Tustin Avenue West Side



44 Tustin Avenue West Side

Existing Conditions
Tustin Avenue
(continued)

Street lights along the eastern side of Tustin Avenue are infrequently placed.



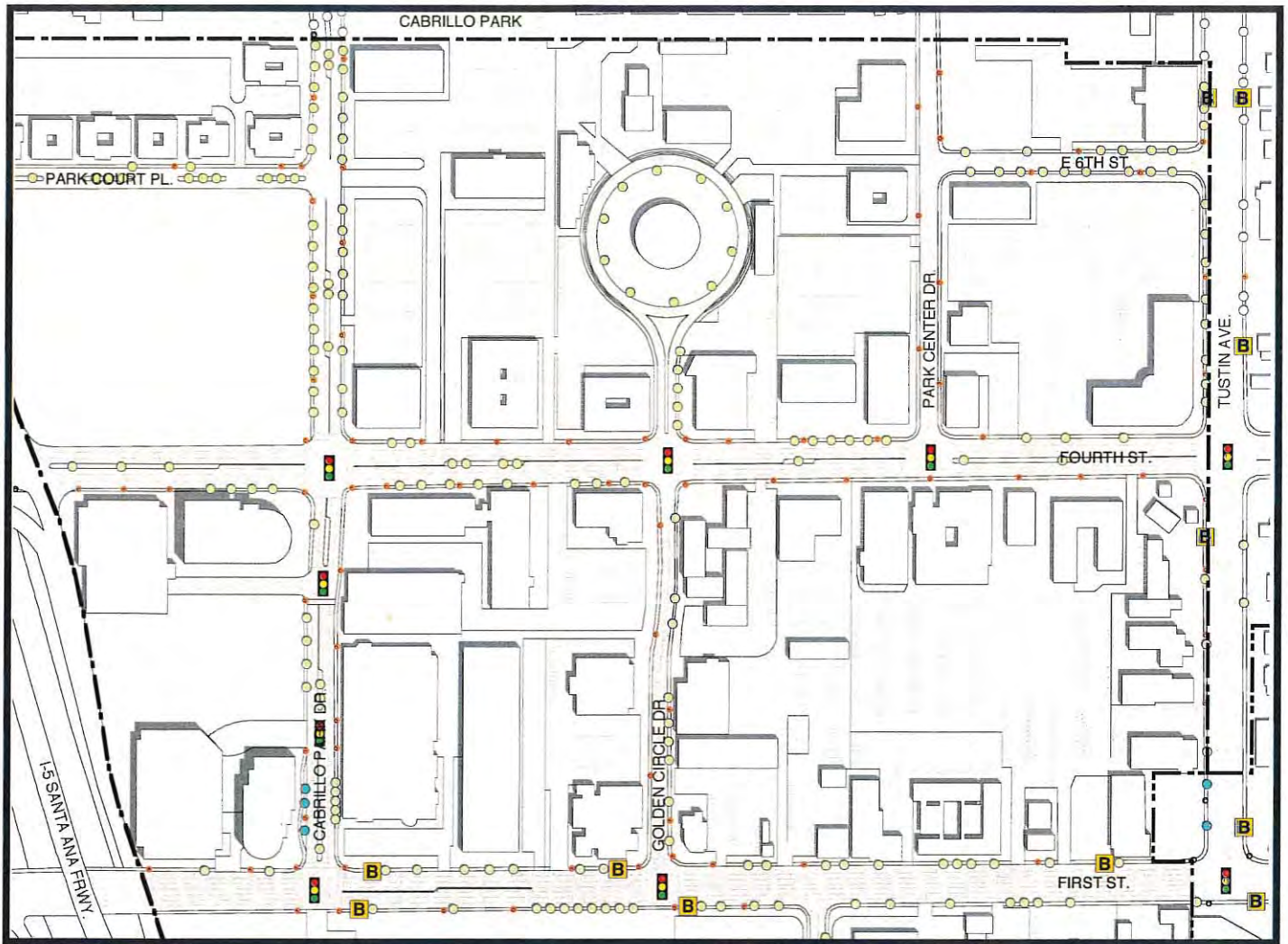
47 Tustin Avenue East Side



48 Tustin Avenue East Side

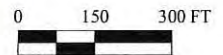
Existing Conditions Base Map

The existing conditions of the Metro East district include limited street tree plantings, a lack of distinctive markers, and lighting oriented towards the automobile.



Key

- Study Area
- City Boundary
- Bus Stops (Routes 64 & 71)
- Existing Street Lights
- Existing Street Trees
- Existing Palm Trees
- Signalized Intersection

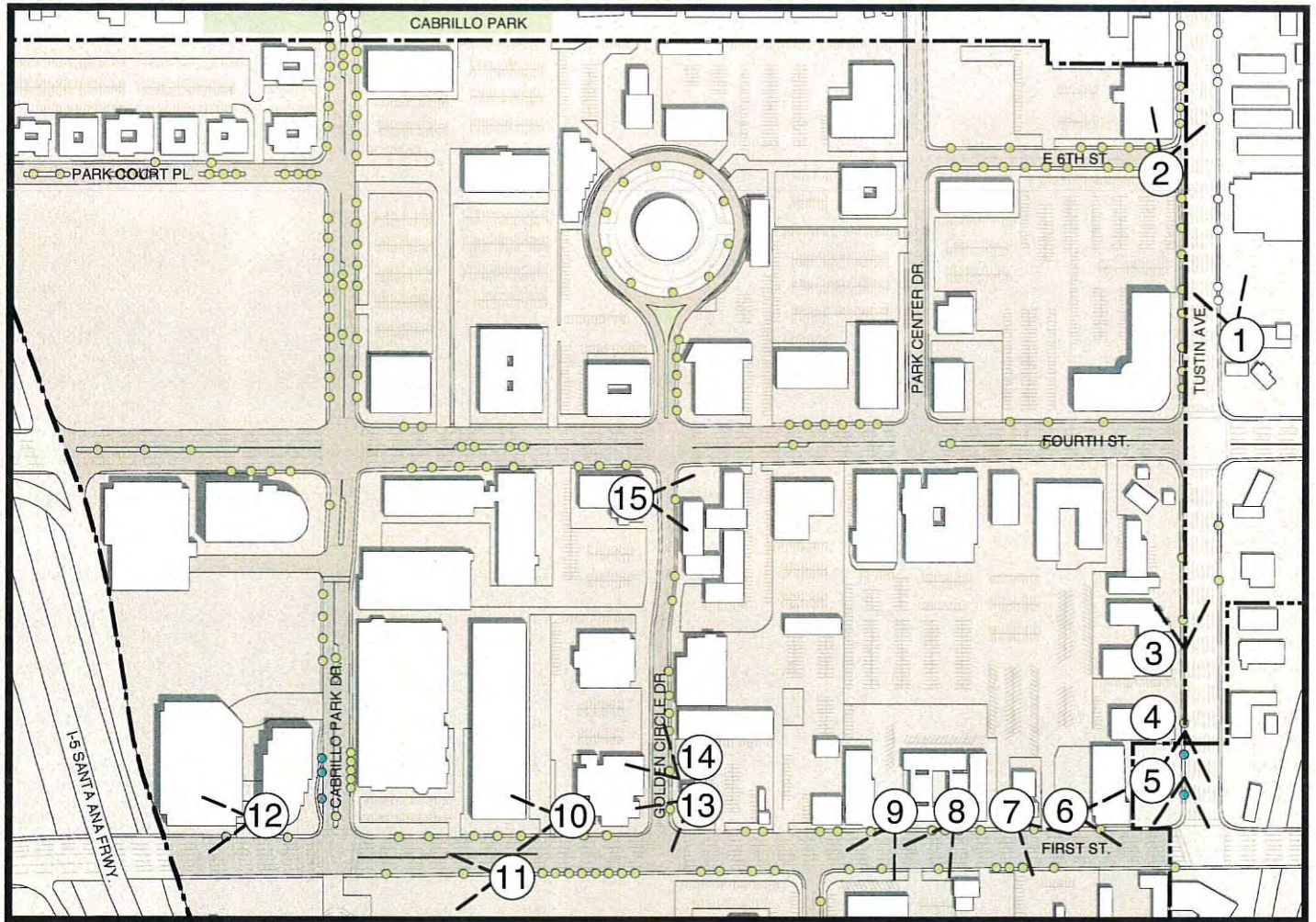


Notes

- Existing bus stops along 1st Street and Tustin Avenue.
- Existing street trees vary in distance.
- Only existing street lights; no pedestrian lighting.

3.1 Existing District Street Tree Types

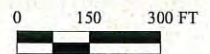
The existing trees along Tustin Avenue vary widely in species.



Key

- Study Area
- City Boundary

- Existing Street Trees
- Existing Palm Trees



1 *Pyrus Kawakamii*



2 *Ligustrum Lucidum*



3 *Liquidambar*



4 *Syagrus Romanzoffianum*

Existing District Street Tree Types

Along First Street, the predominant type of street tree is the *Leptospermum*.



5 *Brahea Edulis*



6 *Leptospermum*



7 *Leptospermum*



8 *Leptospermum*



9 *Leptospermum*



10 *Leptospermum*



11 *Leptospermum*



12 *Leptospermum*



13 *Callistemon Citrinus*



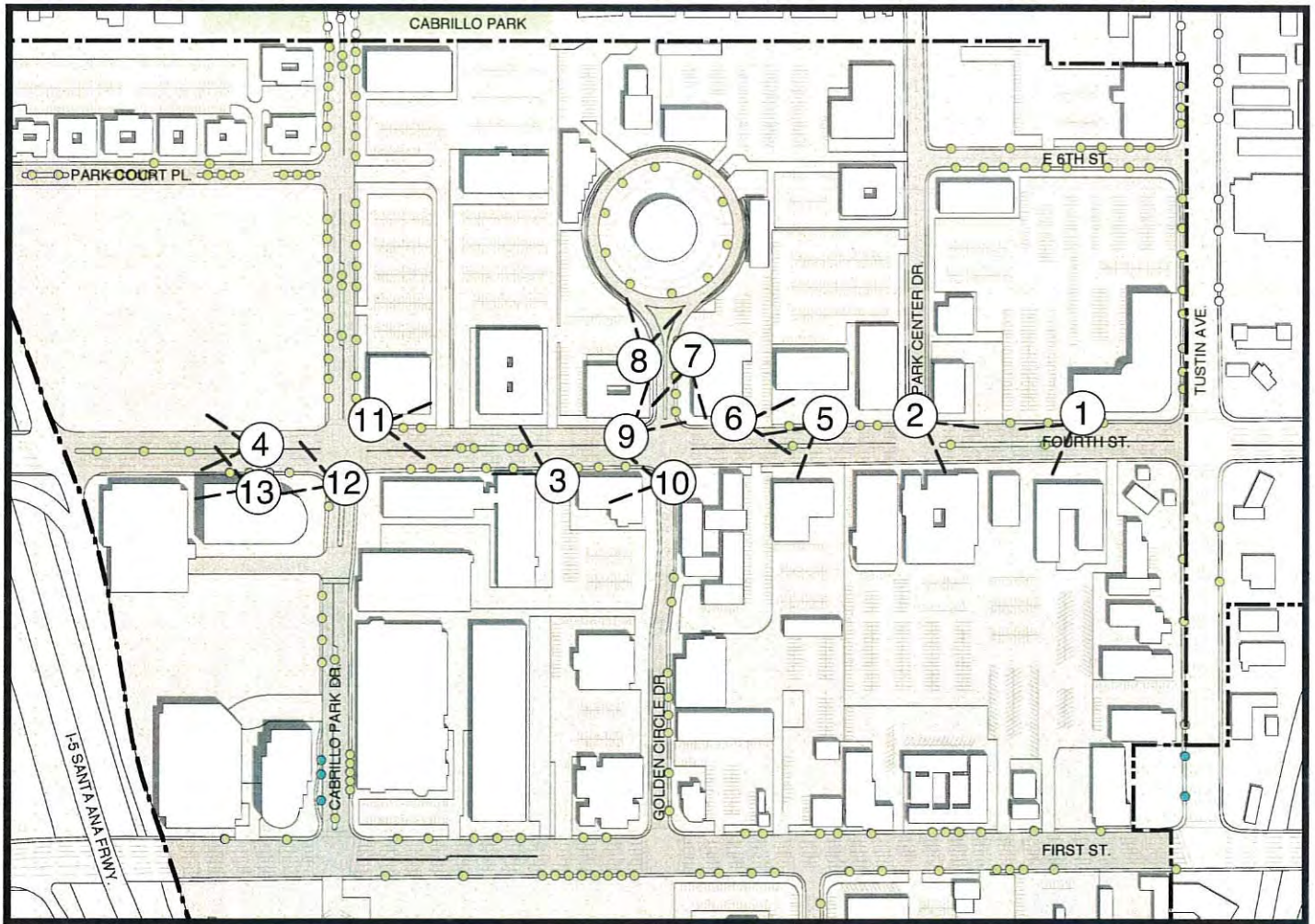
14 *Callistemon Citrinus*



15 *Callistemon Citrinus*

Existing District Street Tree Types

Pinus Canariensis and the Ficus tree occur primarily along the existing medians.



Key

- Study Area
- City Boundary

- Existing Street Trees
- Existing Palm Trees

0 150 300 FT



1 Pinus Canariensis



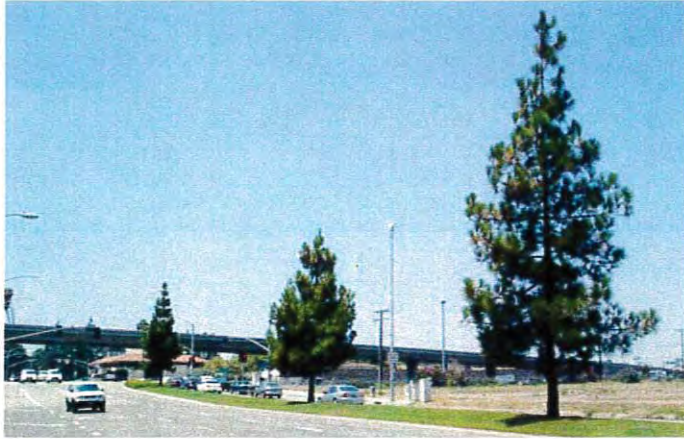
2 Ficus



3 Ficus

Existing District Street Tree Types

Long portions of sidewalk along Fourth Street contain little or no landscaping.



4 Pinus Canariensis



5 Ficus



6 Magnolia Grandiflora



7 Callistemon



8 Washingtonia/Queen Palm



9 Washingtonia



10 Magnolia Grandiflora



11 Magnolia Grandiflora



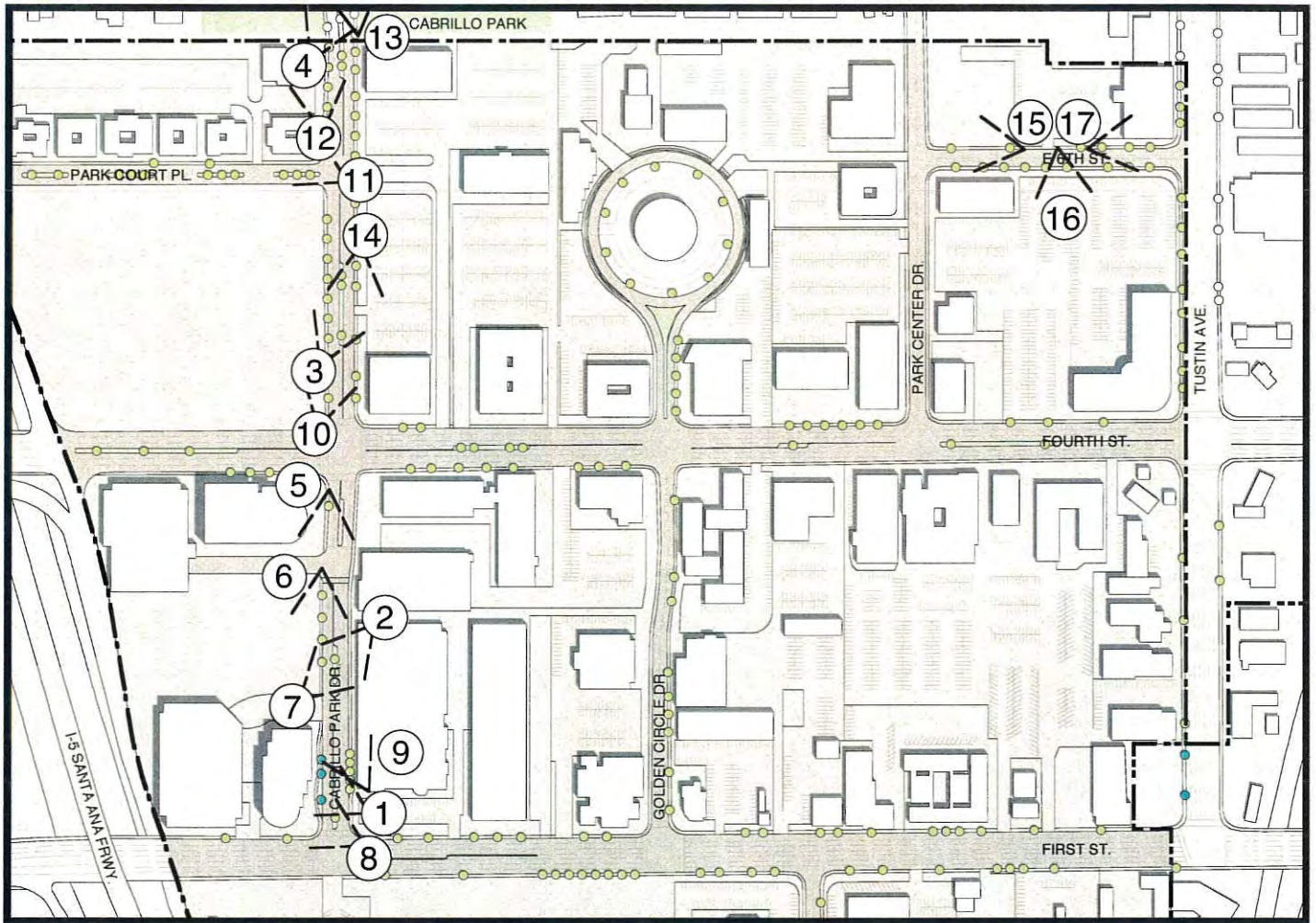
12 Platanus



13

Existing District Street Tree Types

Lagerstroemia Indica is the existing median tree along Cabrillo Park Drive.



Key

- Study Area
- City Boundary

- Existing Street Trees
- Existing Palm Trees

0 150 300 FT



1 King Palms



2 Lagerstroemia Indica



3 Lagerstroemia Indica

Existing District Street Tree Types

Platanus and Liquidambar trees define the street
along Cabrillo Park Drive.



4 Lagerstroemia Indica



5 Platanus



6 Platanus



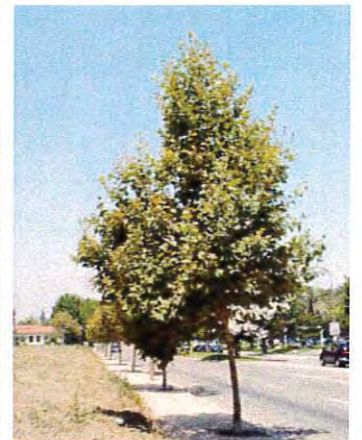
7 Platanus



8 Lagerstroemia Indica



9 Pinus Canariensis



10 Platanus



11 Olea Eoropaea



12 Liquidambar



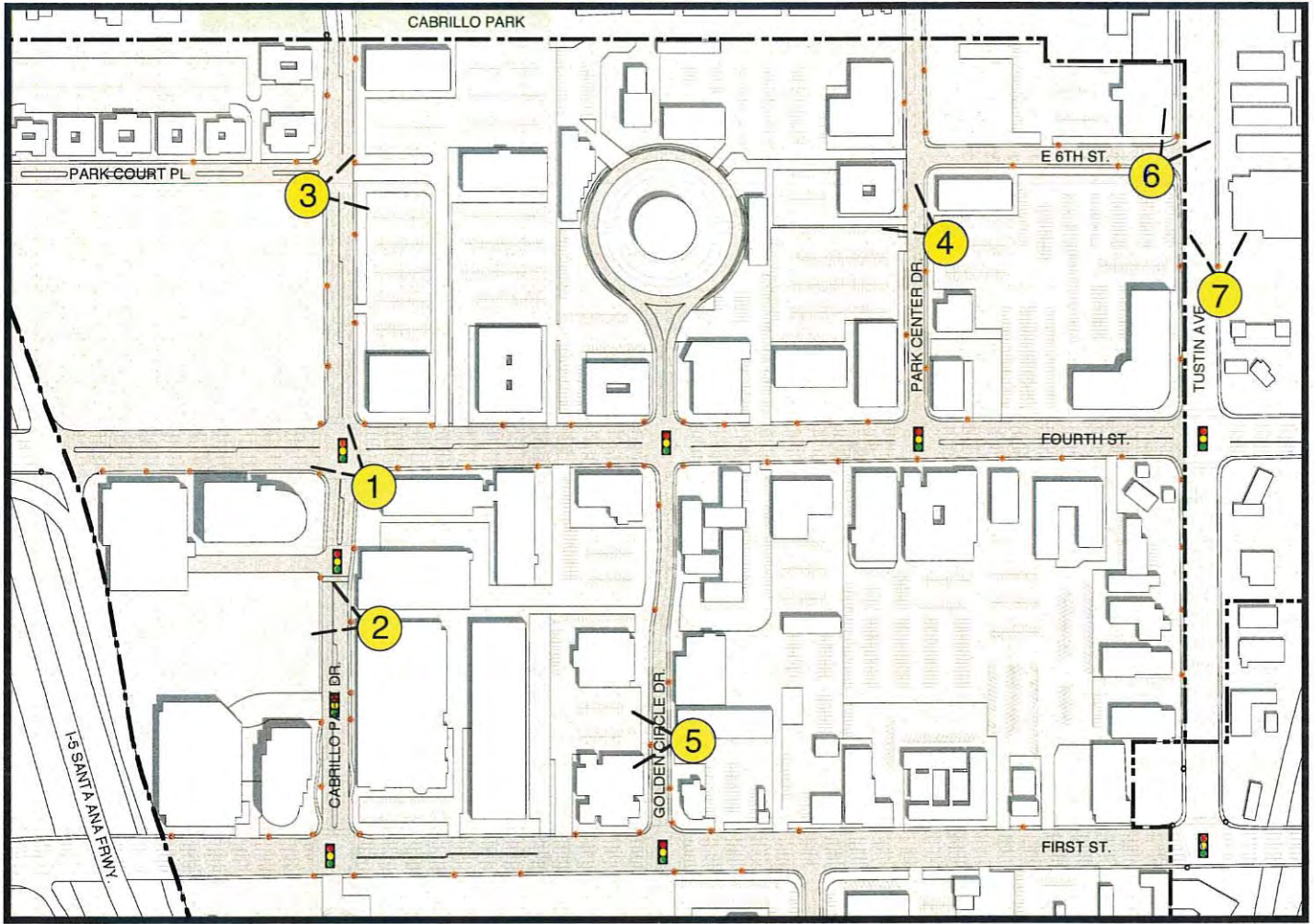
13 Liquidambar



14 Liquidambar

Existing Street Lights Locations and Types

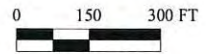
All of the street light types in the area are cobrahead variants.



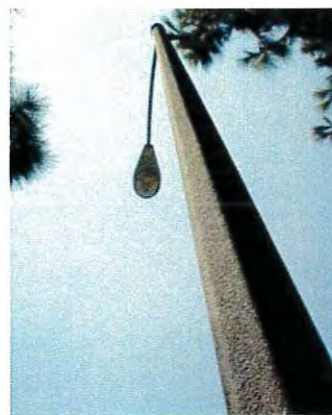
Key

- Study Area
- City Boundary

- Existing Street Lights
- Signalized Intersection



1



2



3

Existing Street Lights Locations and Types

Street lights along the First Street corridor are irregular.



4



5



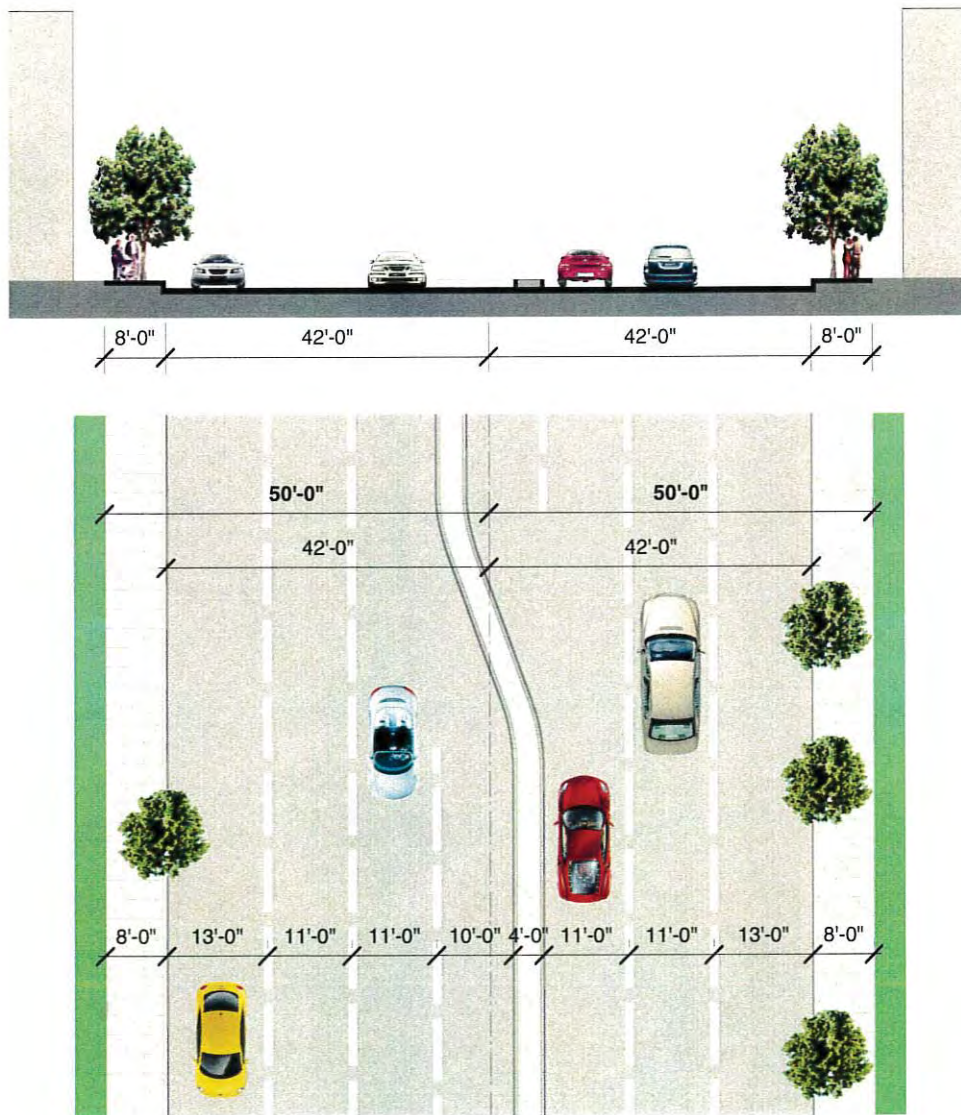
6



7

Existing Streets Typical Plan and Section

Wide streets and inconsistent street lights provide low visibility for passing traffic and the surrounding environment.



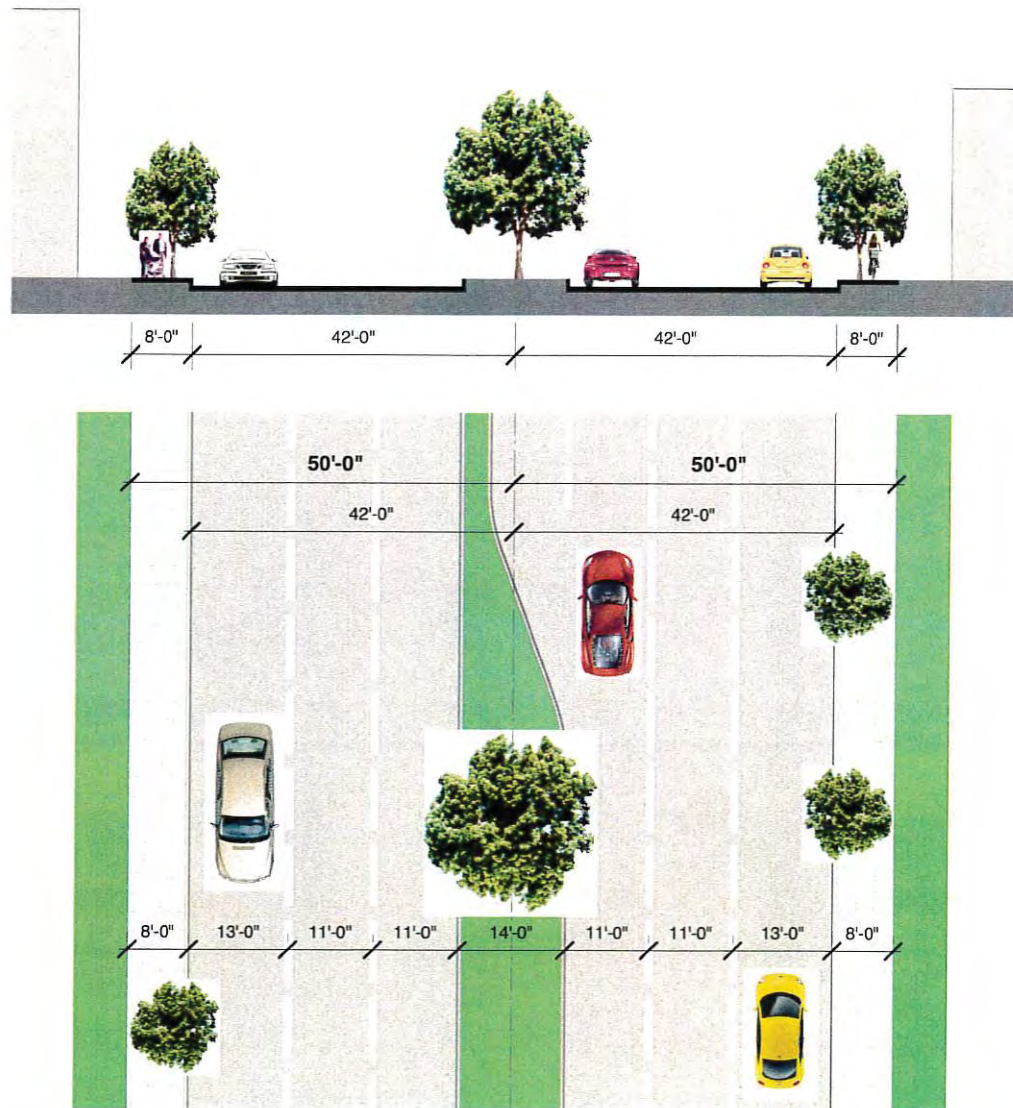
A

Main Traffic Corridor

Scale: 1"=25'

Existing Streets Typical Plan and Section

The existing medians and sidewalks along Fourth Street are sparsely landscaped and make little use of the potential gateway qualities of this major street.



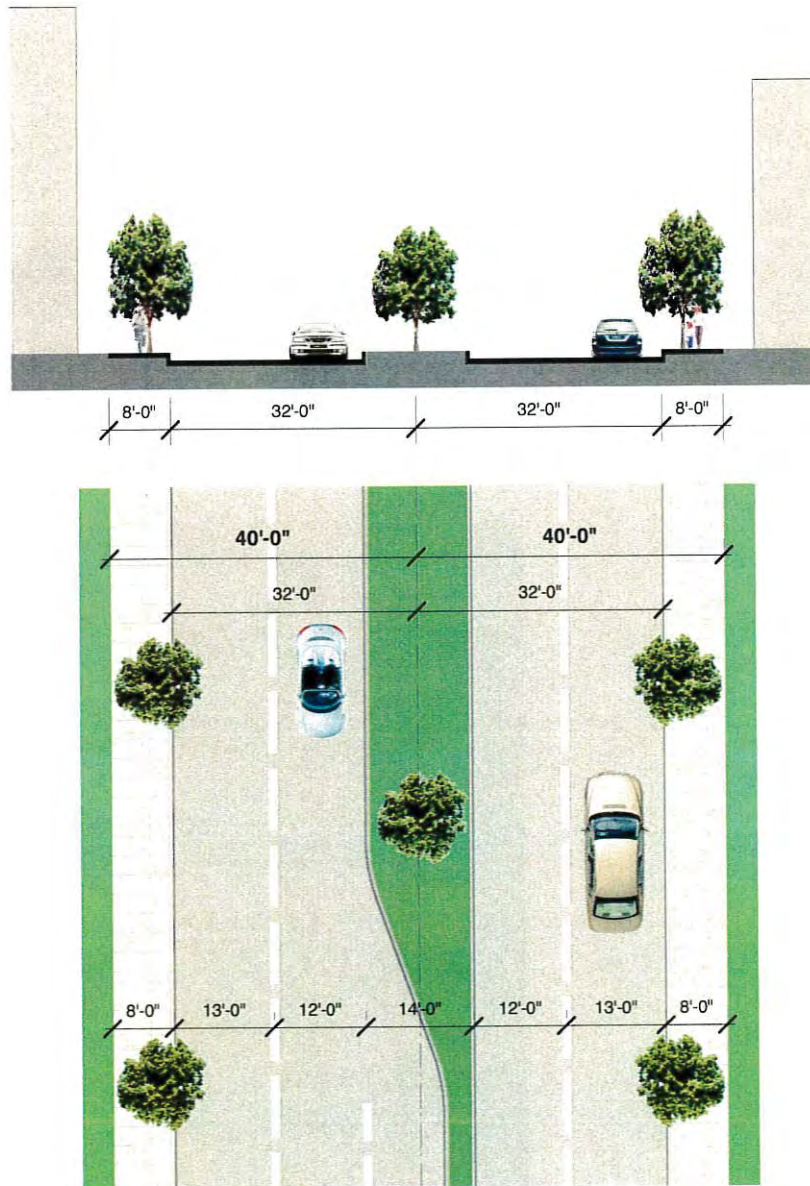
B

Central Street Corridor

Scale: 1"=25'

Existing Streets Typical Plan and Section

Cabrillo Park Drive maintains a fairly uniform row of street tree planting north of Fourth Street. However, the southern portion of the street breaks with the regularity.



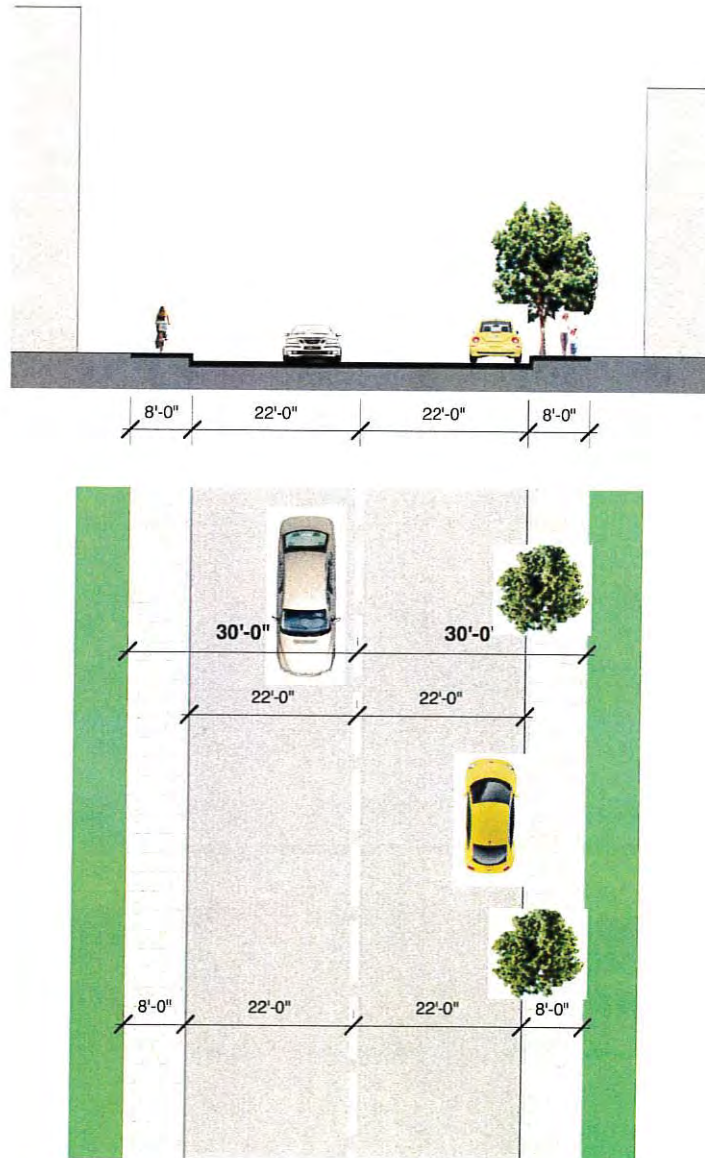
C

Typical Local Access Street

Scale: 1"=25'

Existing Streets Typical Plan and Section

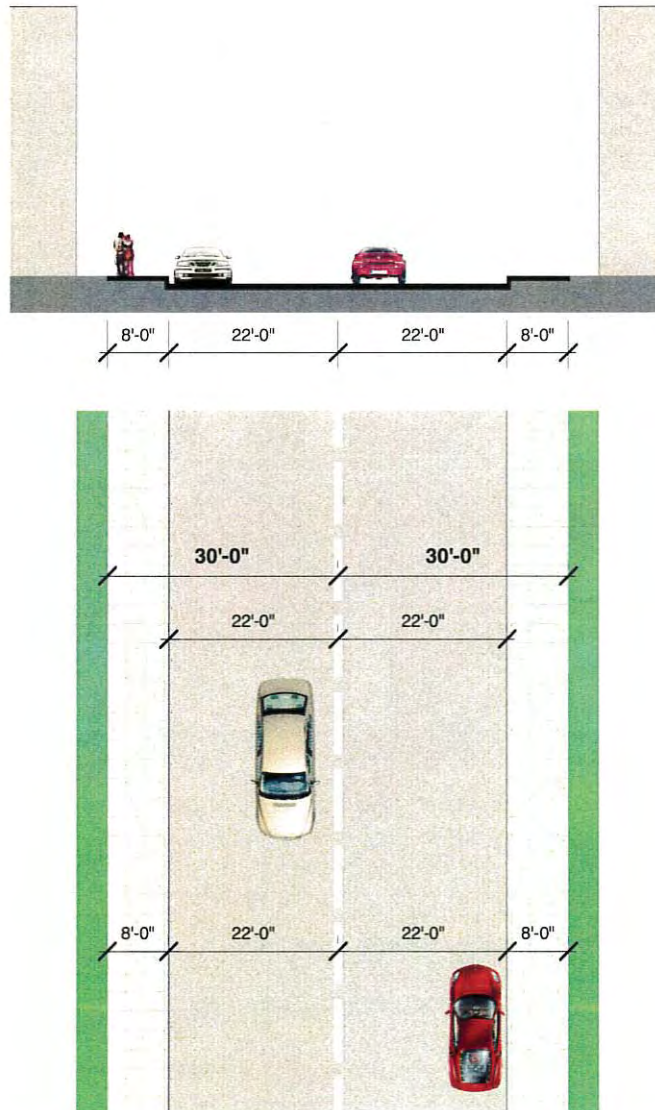
Golden Circle Drive is a narrow two lane street providing access from First to Fourth Street.



D Typical Main Street
Scale: 1"=25'

Existing Streets Typical Plan and Section

Street tree plantings along Park Center Drive are non-existent.

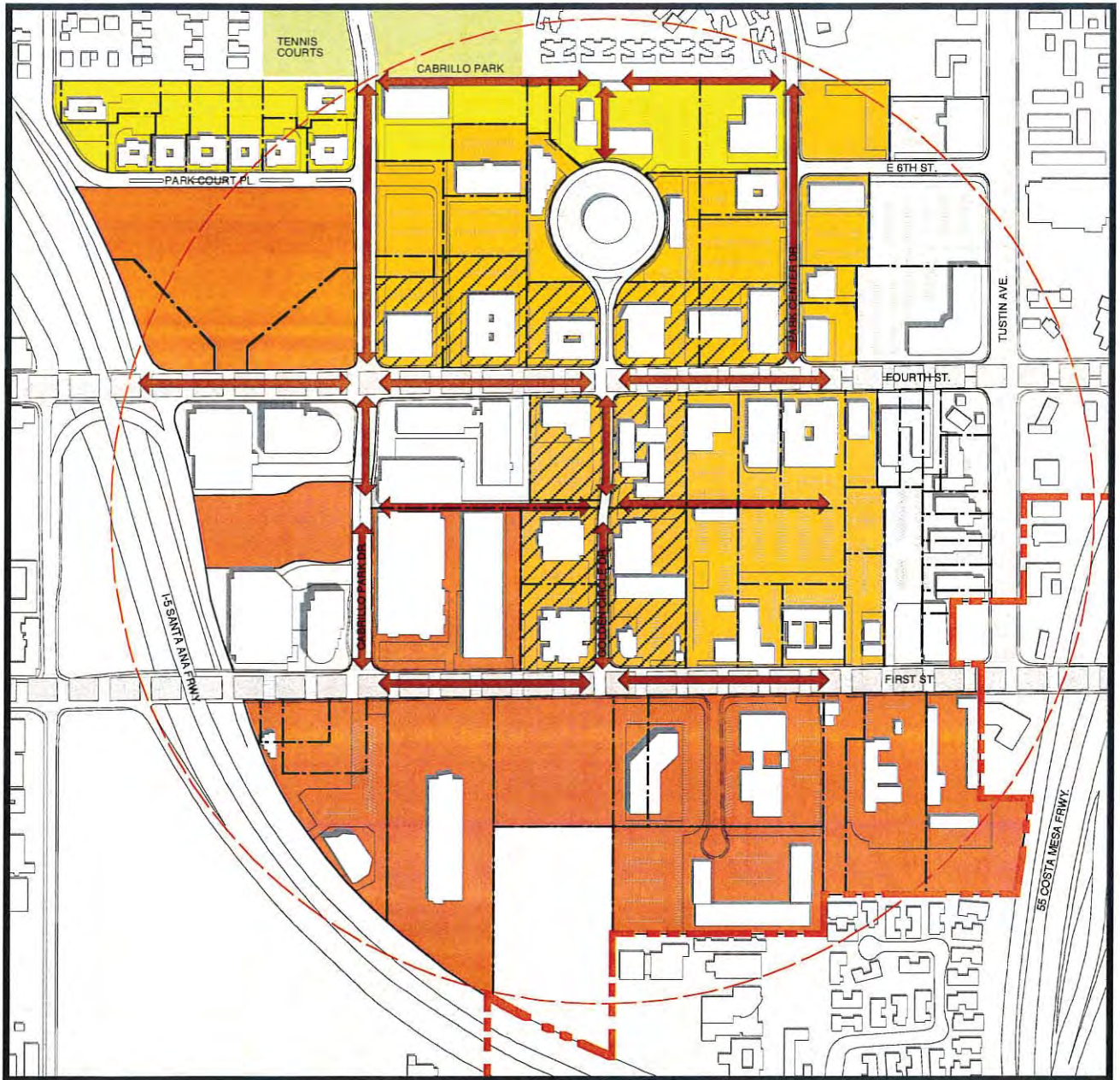


E

Secondary Local Access Streets

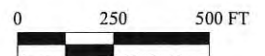
Scale: 1"=25'

MEMU Overlay Development Zone Districts



Key

- Study Area
- City Boundary
- Existing Parcel Lines
- Improved Pedestrian and Bicycle Corridor
- Pedestrian Linkage
- 1,500 ft. Radius Walking Distance
- District 1: Neighborhood Transitional
- District 2: Village Center
- District 3: Active Urban
- Pedestrian-Oriented Uses on Ground Level
- Existing Office Uses

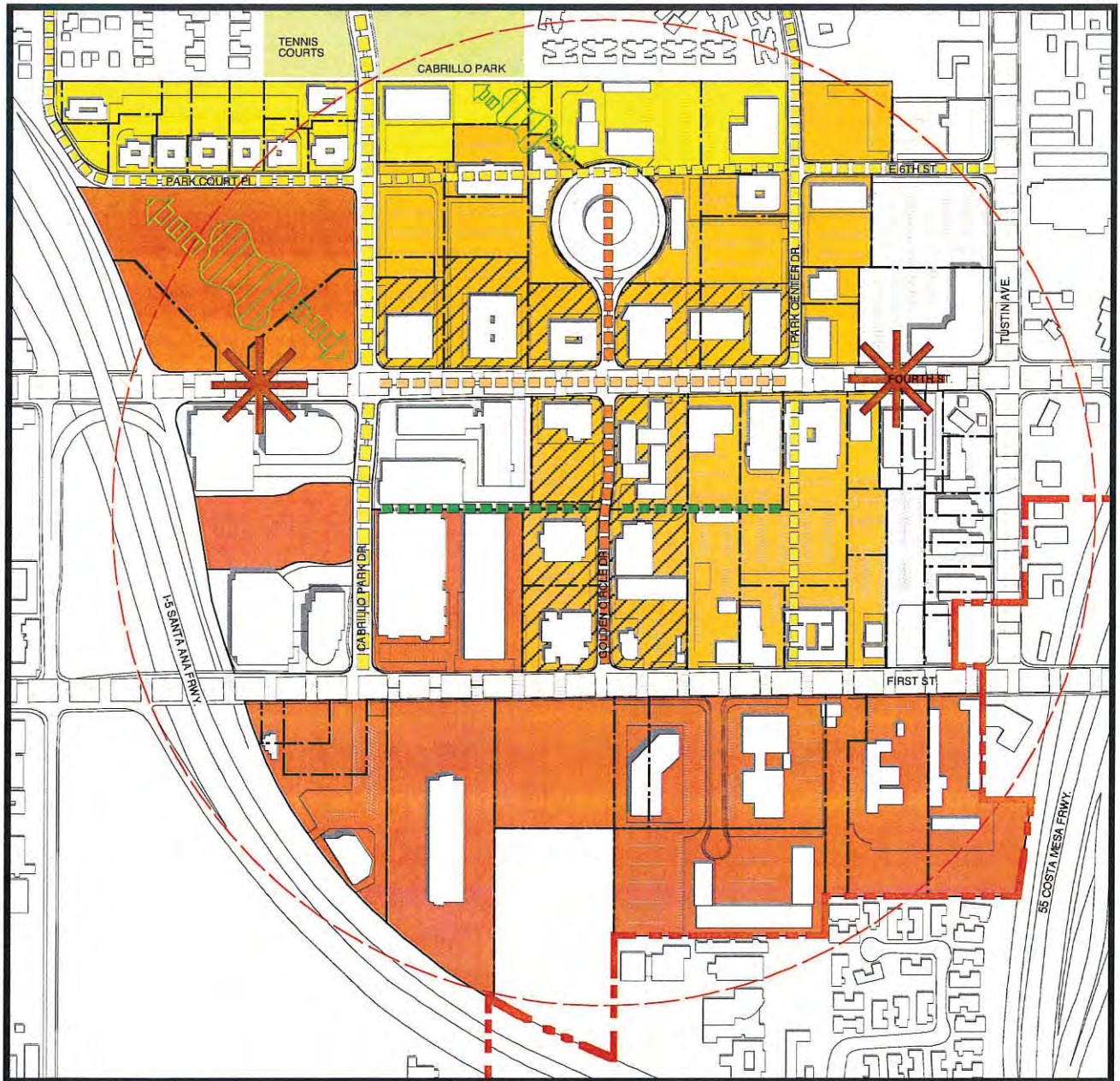


MEMU Overlay Development Zone District Descriptions

	District 1 Neighborhood Transitional	District 1 is intended to provide opportunities for low-intensity development that acts as a transition between the single-family residential to the north and the adjacent high-intensity Active Urban District.
	District 2 Village Center	District 2 is intended to serve as the focal point and central gathering place in well-designed highly connected development sites and public sites.
	District 3 Active Urban	District 3 is intended as the location for well-designed high rise mixed-use developments in a highly urbanized environment.
	Pedestrian-Oriented Uses on Ground Level	Pedestrian-oriented uses are intended to generate pedestrian activity and provide uses that are neighborhood serving and contribute to an active street life.
	Existing Office Uses	Existing office uses include low and high rise office development along Tustin Avenue and the Santa Ana (I-5) Freeway Corridor.

Public Realm Concept

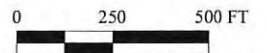
The proposed public realm amenities will enhance the pedestrian scale and quality of living in the area.



Key

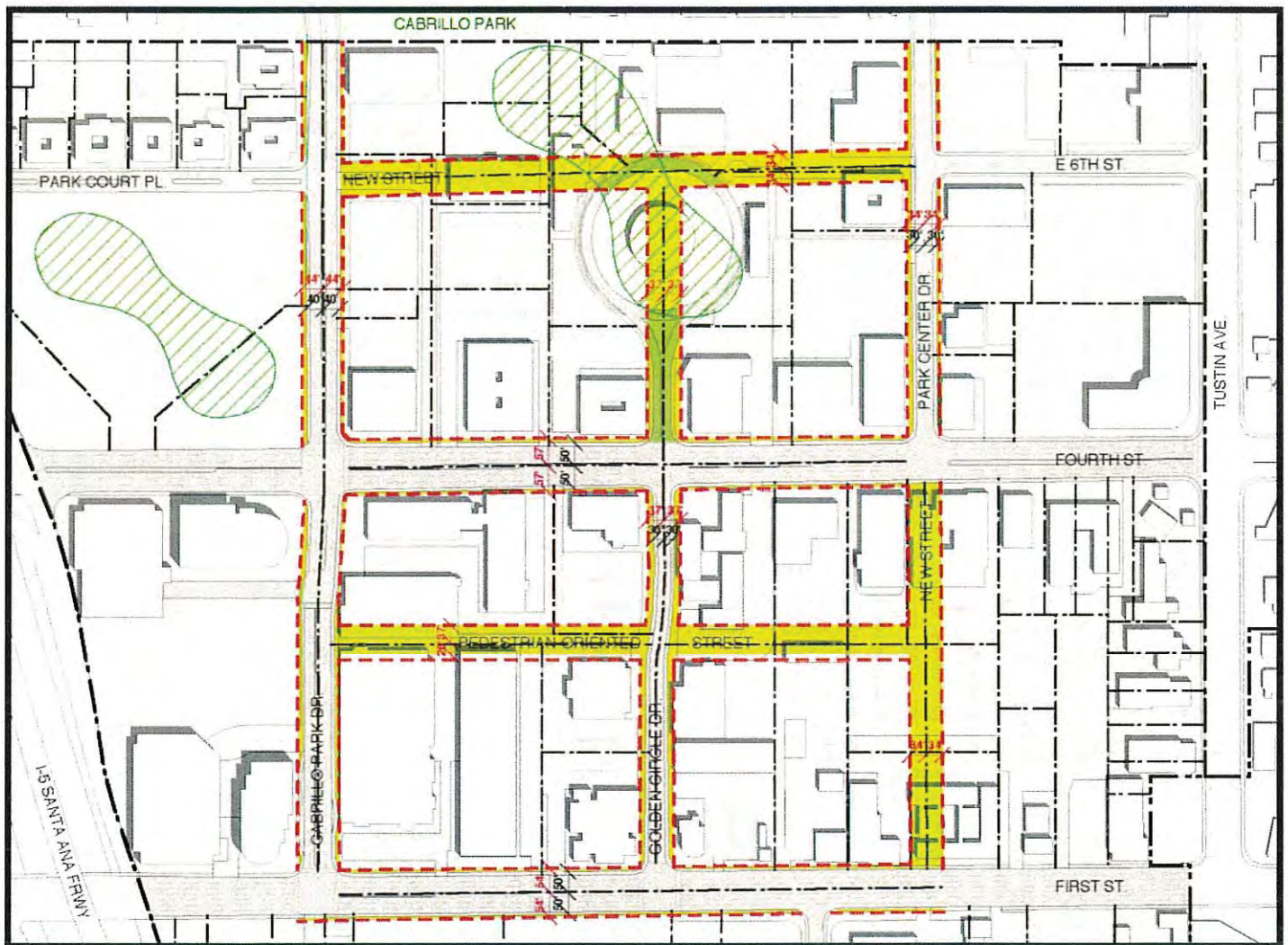
- - - Study Area
- - - City Boundary
- - - Existing Parcel Lines
- * Gateway Opportunities
- Main Traffic Corridors
- Local Access Streets (existing & new)
- Central Street Corridor
- Main Street
- Alley Street with Pedestrian Orientation

- District 1: Neighborhood Transitional
- District 2: Village Center
- District 3: Active Urban
- Pedestrian-Oriented Uses on Ground Level
- Existing Office Uses
- 🌿 Open Space/Pedestrian Linkage Component
- 1,500 ft. Radius Walking Distance



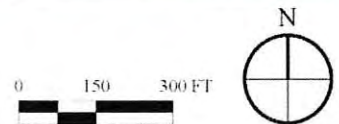
Proposed Easements

By allowing for greater easements, this plan provides an opportunity to create a walkable environment and a friendly pedestrian atmosphere.



Key

- Study Area
- City Boundary
- Existing Parcel Lines
- Setback Lines
- Additional Setback Easements
- Public Open Space



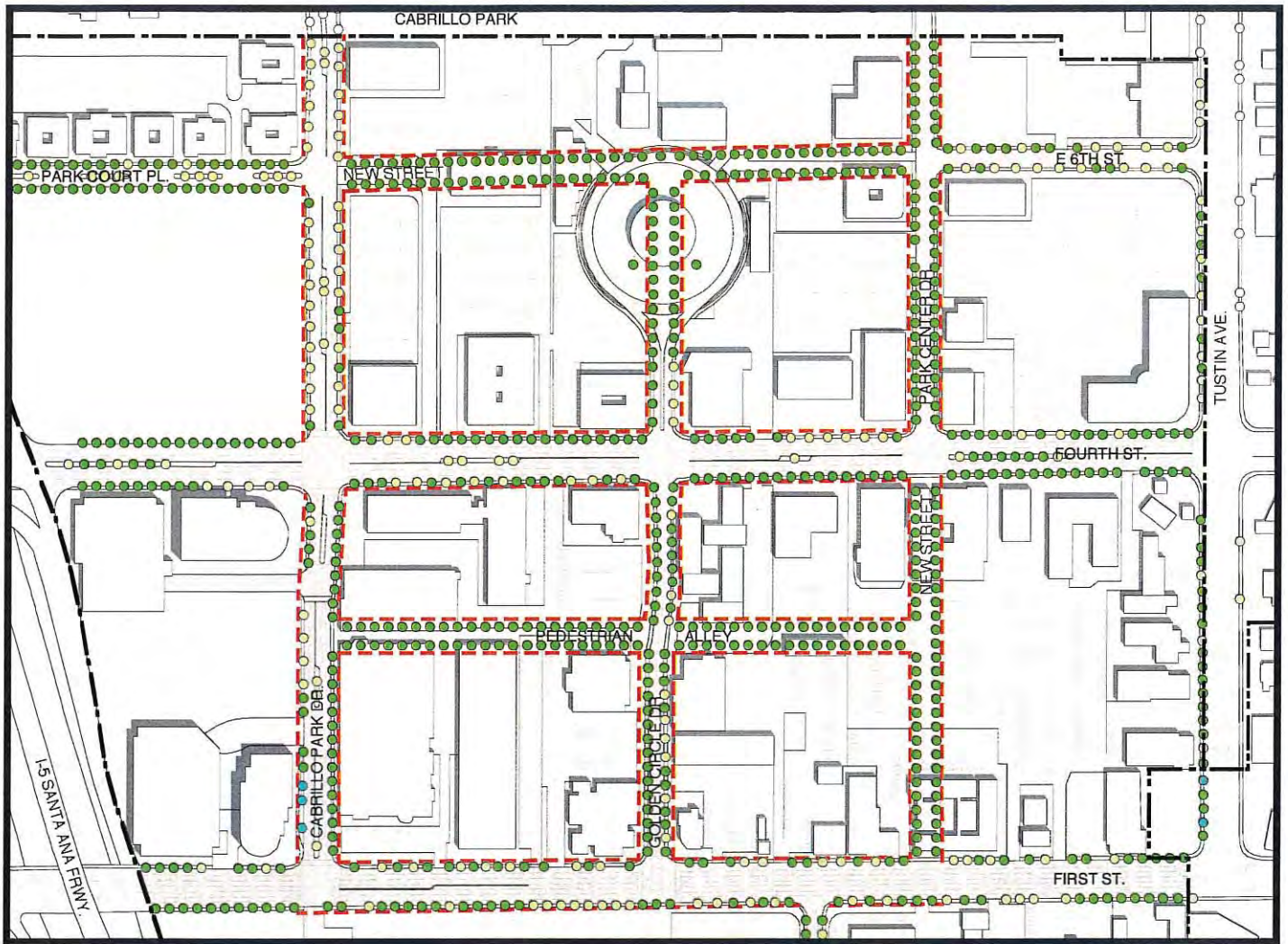
Notes

- Minimum 35,000 SF of public open space including roads shall be created within this area.
- NEW and EXISTING Dimensions from Center of Right-of-Way.



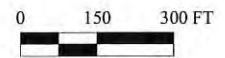
Improved Condition Base Map Street Trees

Consistent street tree plantings throughout the district provide a coherent visual identity and an attractive streetscape for visitors, workers, and residents.



Key

- Study Area
- City Boundary
- Setback Lines
- Existing Street Trees
- Existing Palm Trees
- New Street Trees

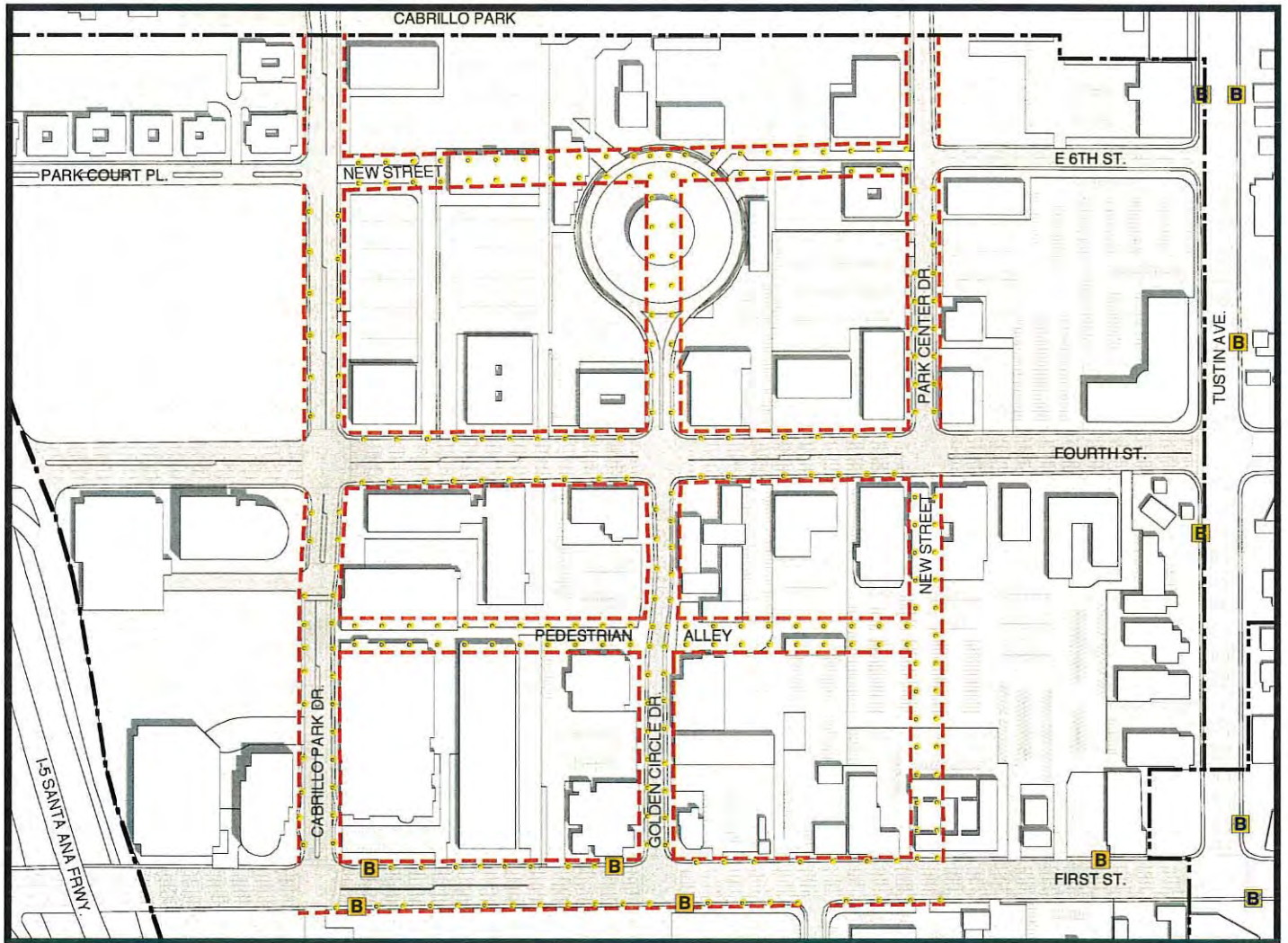


Notes:

- New street trees should average 30' on center.
- Flowering trees at medians create seasonal color.

**Improved Condition
Base Map
Pedestrian Street Lighting**

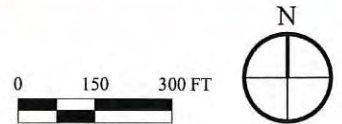
Regular pedestrian street lighting throughout the district increases night-time visibility and safety for pedestrians.



Key

- Study Area
- City Boundary
- Setback Lines

- Bus Stops (Routes 64 & 71)
- New Pedestrian Street Lights



Notes

- Pedestrian lightpoles in the Central Area every 60 ft.

Improved Streetscape Typical Plan and Section

District signage and flowering trees mark Fourth Street as the major gateway into the district.

Improvements:

- District signage & identity with flowering trees at remainder of median
- 10' vs. 8' sidewalk
- 5' parkway at curb

Infill:

- Magnolia Grandiflora or similar at ± 30' o.c. along sidewalk
- Pinus Canariensis or similar at ± 30' o.c. on median
- Pyrus Kawakamii or similar at ± 30' o.c. on median



Pyrus Kawakamii



Magnolia Grandiflora

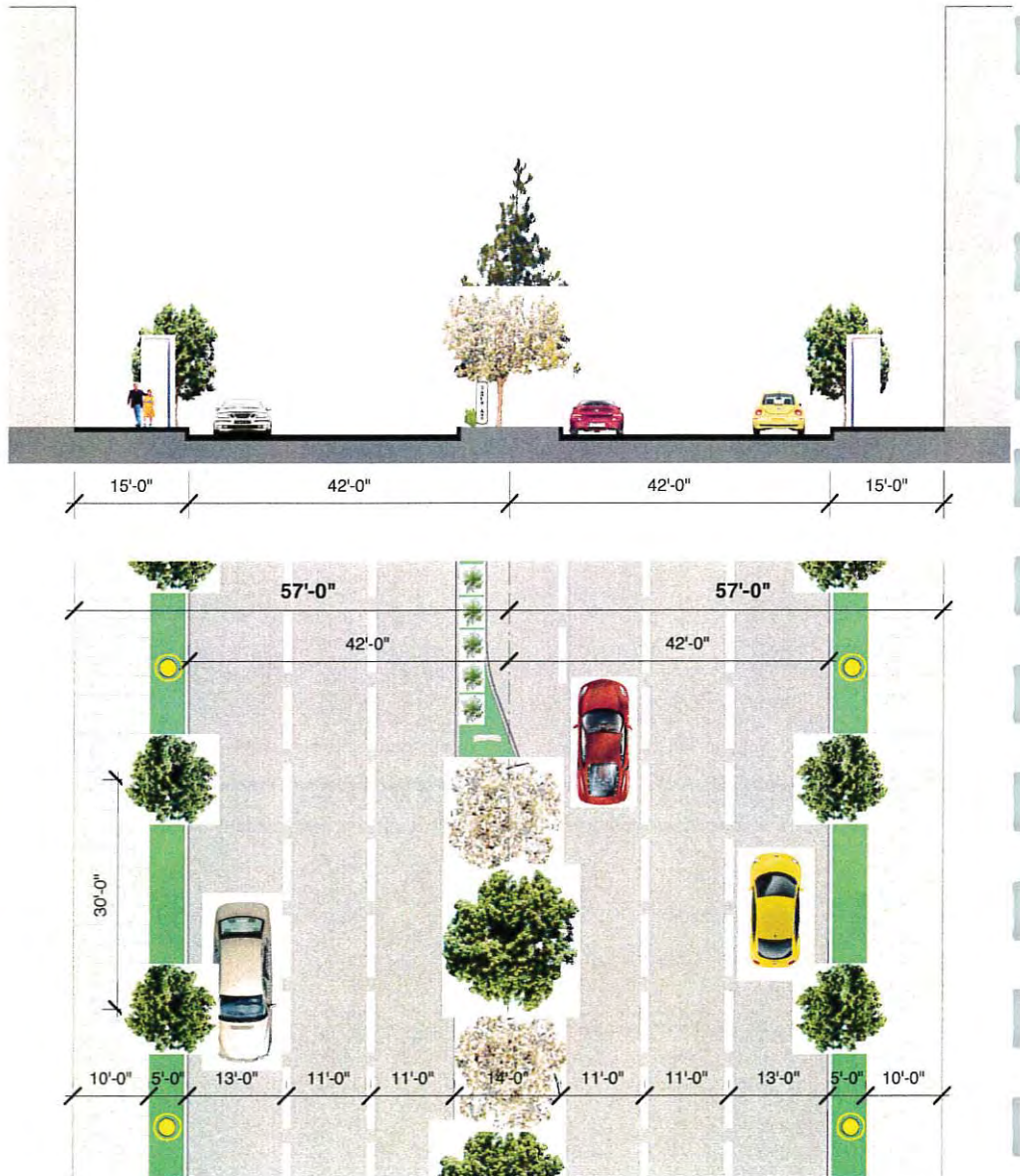
Pedestrian Light Poles:



Alternative 1



Alternative 2



F Gateway Condition
Scale: 1"=25'

Improved Streetscape Typical Plan and Section

Wider sidewalks in conjunction with the parkway enhance the overall quality of the urban environment for both pedestrians and automobiles.

Improvements:

- 10' vs. 8' sidewalk
- 5' parkway at curb
- New pedestrian light poles at $\pm 60'$ o.c. along sidewalk

Infill:

- Magnolia Grandiflora or similar at $\pm 30'$ o.c. along sidewalk
- Pinus Canariensis or similar at $\pm 30'$ o.c. on median



Magnolia Grandiflora

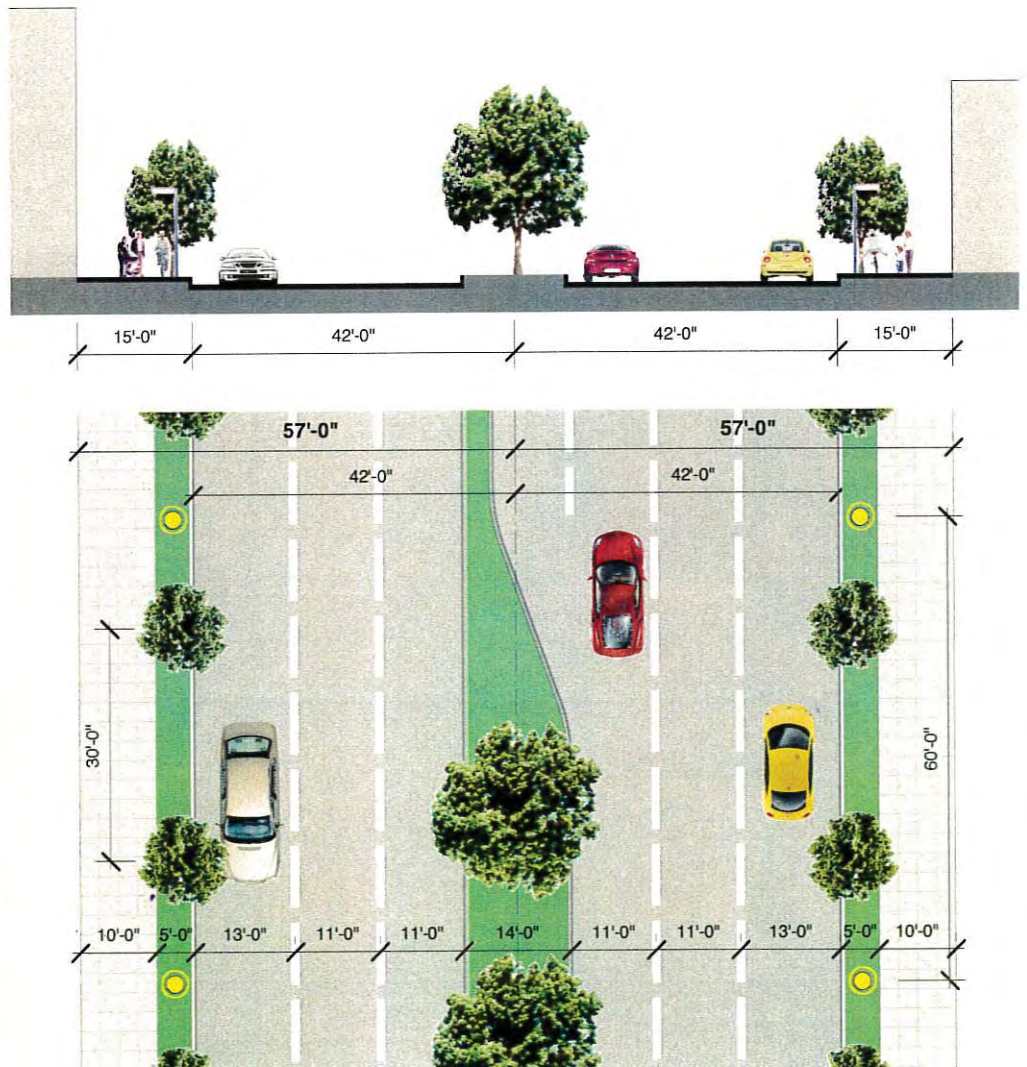
Pedestrian Light Poles:



Alternative 1



Alternative 2



B Central Street Corridor
Scale: 1"=25'

Improved Streetscape Typical Plan and Section

Decorative paving and pedestrian scaled lighting place an emphasis on the walkable nature of the district.

Improvements:

- One lane in each direction with parking on both sides
- 8' sidewalk
- 4' parkway at curb
- Pedestrian light poles at ± 60' o.c. in line with trees

Infill:

- Liquidambar or similar at ± 30' o.c. along sidewalk



Liquidambar

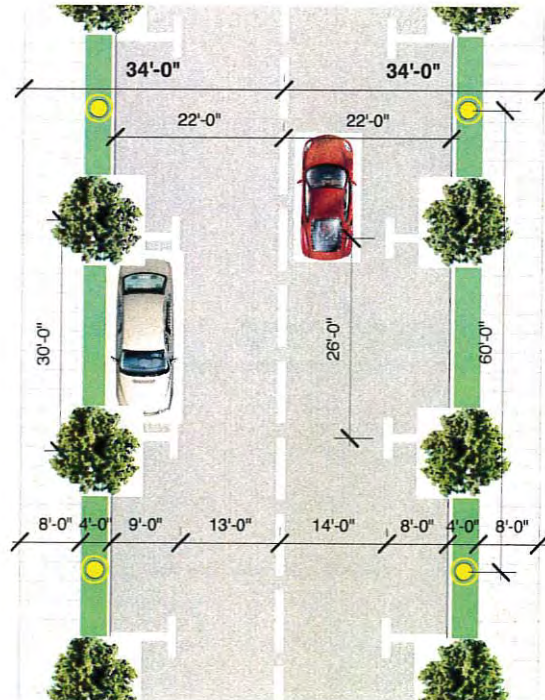
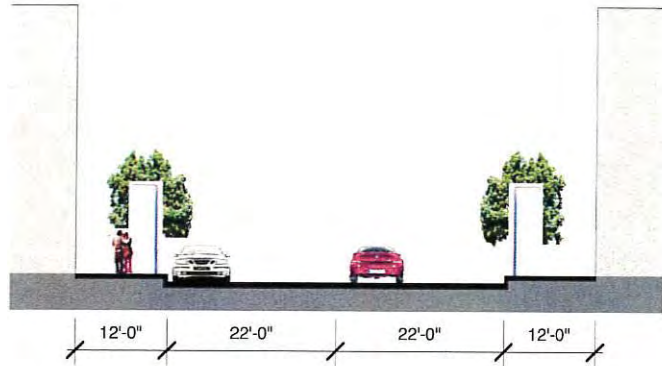
Pedestrian Light Poles:



Alternative 1



Alternative 2



E Secondary Local Access Streets
Scale: 1"=25'

Proposed Streetscape Typical Plan and Section

Greater allowances for the parkway provide a physical buffer between pedestrians and automobile traffic. In turn, this buffer increases the comfort level for pedestrians.

Improvements:

- 8' sidewalk
- 4' parkway at curb
- Pedestrian light poles at ± 60' o.c. in line with trees

Infill:

- Leptospermum or similar at ± 30' o.c. along sidewalk
- Pyrus Kawakamii or similar at ± 30' o.c. along sidewalk



Leptospermum

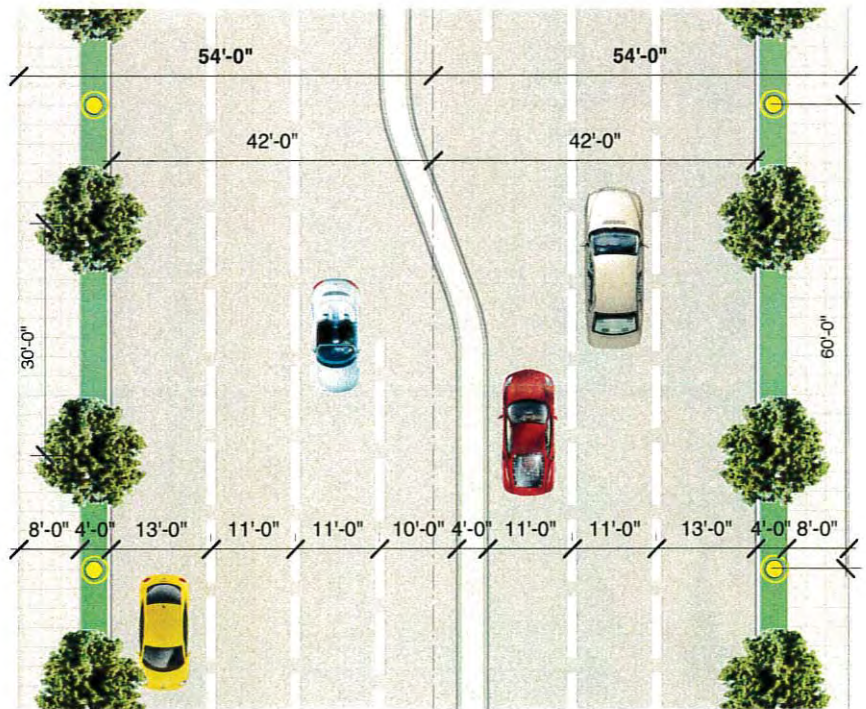
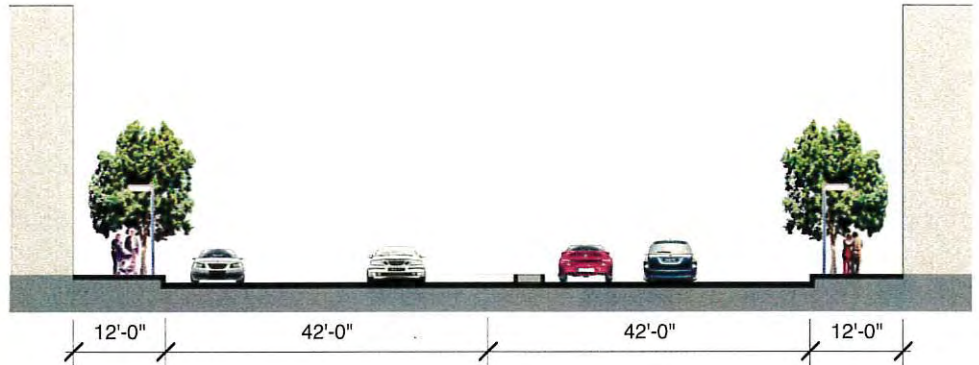
Pedestrian Light Poles:



Alternative 1



Alternative 2



Main Traffic Corridor

Scale: 1"=25'

Proposed Streetscape Plan and Section

A variety of trees located at the curb and the median give the street a diverse range of colors and textures as the seasons progress.

Improvements:

- 8' sidewalk
- 4' parkway at curb
- New pedestrian light poles at ± 60' o.c. in line with trees

Infill:

- Platanus or similar at ± 30' o.c. along sidewalk
- Pyrus Kawakamii or similar at ± 30' o.c. along median
- Lagerstroemia Indica or similar at ± 20' o.c. along median



Pyrus Kawakamii



Platanus

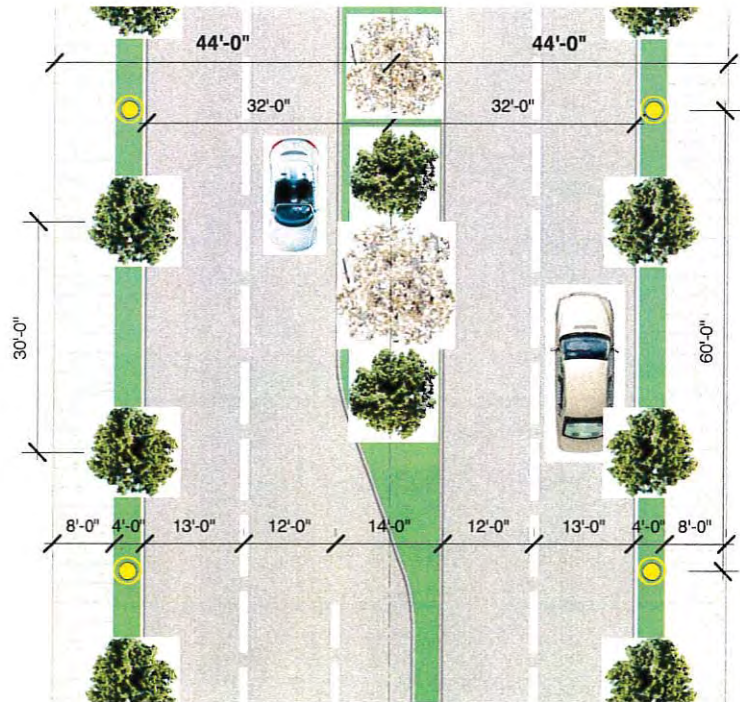
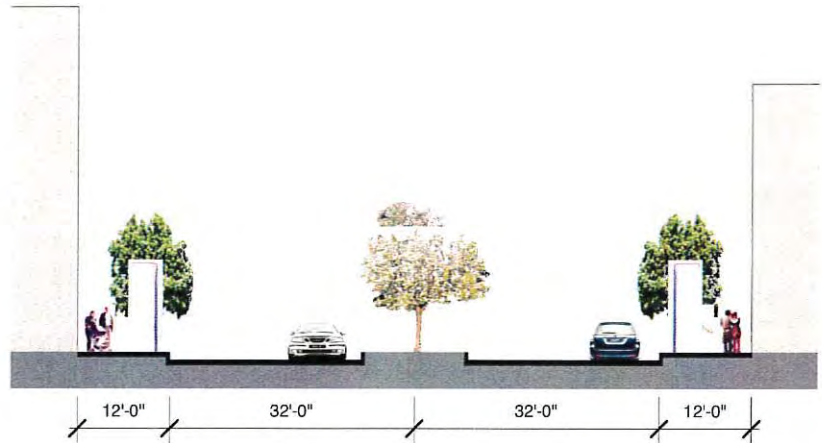
Pedestrian Light Poles:



Alternative 1



Alternative 2



C Typical Local Access Street
Scale: 1"=25'

Proposed Streetscape Typical Plan and Section

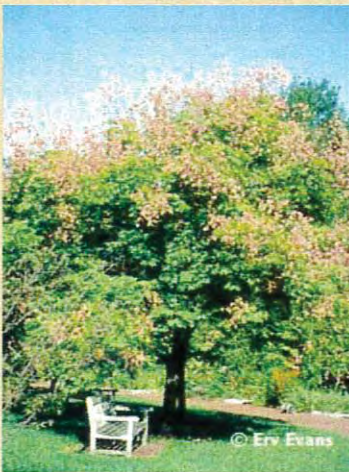
Generous decoratively paved sidewalks provide a textured surface and visual appeal for the area.

Improvements:

- 15' decorative paved sidewalk
- Pedestrian light poles at ± 60' o.c. in line with trees

Infill:

- Koelreuteria Bipinnata or similar at ± 30' o.c. along sidewalk



Koelreuteria Bipinnata

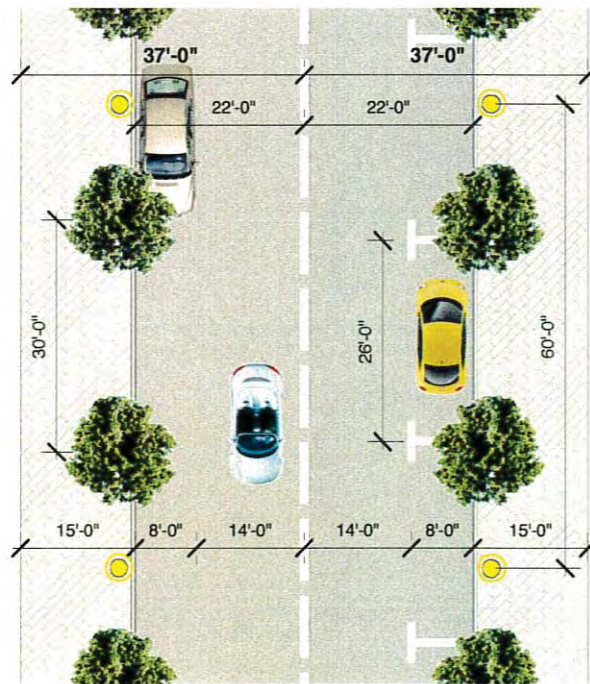
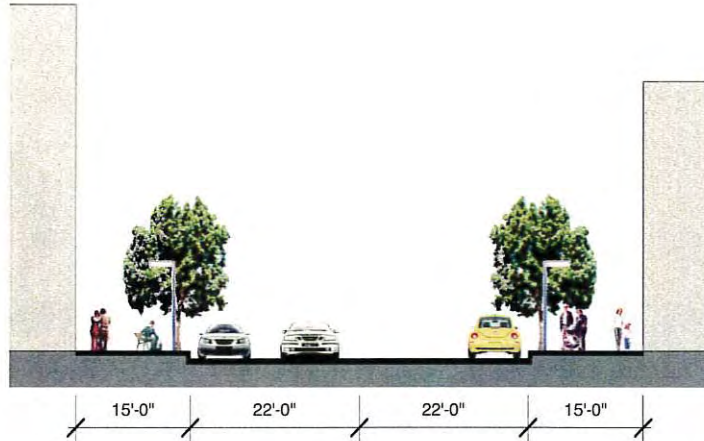
Pedestrian Light Poles:



Alternative 1



Alternative 2



D Typical Main Street
Scale: 1"=25'

Proposed Streetscape Typical Plan and Section

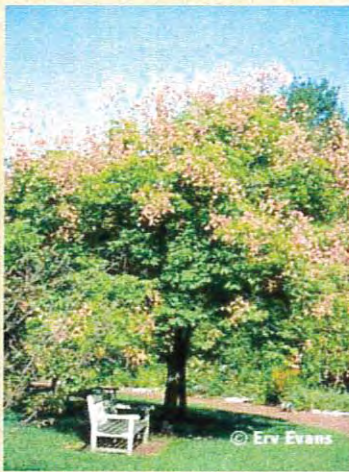
Located between First and Fourth Street, the pedestrian-oriented street creates a natural gathering place for the community.

Improvements:

- 37' decorative paved street with pedestrian orientation
- Bollards 5' from one-way alley at ± 10' o.c.
- Pedestrian light poles at ± 60' o.c. in line with trees

Infill:

- Koelreuteria Bipinnata 21' from curb ± 30' o.c.



Koelreuteria Bipinnata

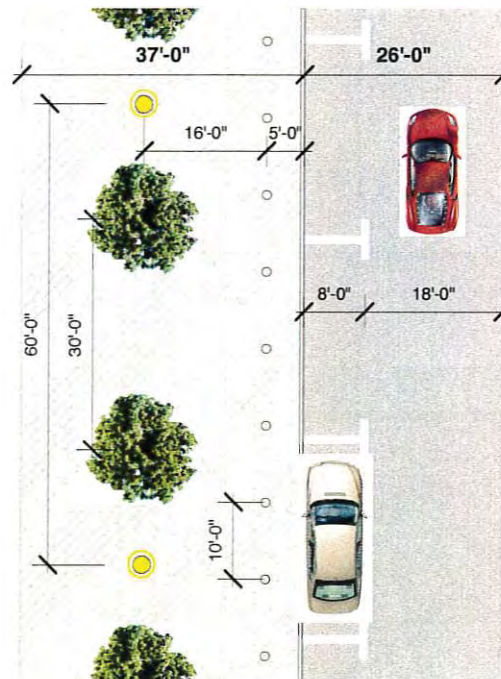
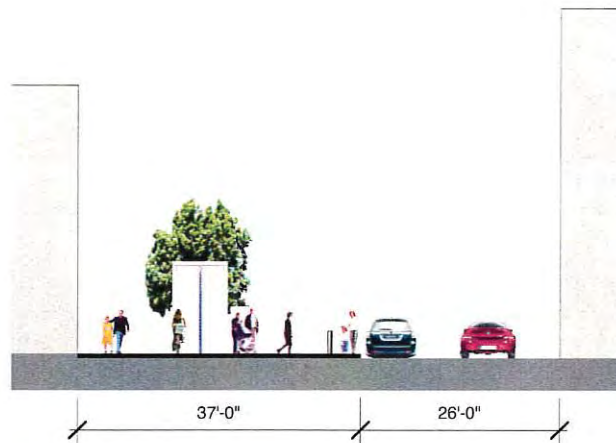
Pedestrian Light Poles:



Alternative 1



Alternative 2



G Pedestrian-Oriented Street
Scale: 1"=25'

Metro East Public Realm Amenity Plan Cost Estimate

**METRO EAST PUBLIC REALM AMENITY PLAN
UNIT COST ESTIMATE FOR AMENITY IMPROVEMENTS**

	Unit Type	Unit Cost	Number of Units	Improvement Cost	Notes
(N) Roadways (acquisition)					cost of development
(N) Roadways (construction)					cost of development
(N) Sidewalks (acquisition)					cost of development
(N) Sidewalks (construction)					cost of development
(N) Paved Sidewalks @ Golden Circle Drive	square feet	\$16	35,000	\$560,000	
(N) Paved Surface at (N) Alley				-	cost of development
(N) Public Open Space (acquisition)	square feet	\$125	43,500	\$5,437,500	assumes purchase of approximately one acre of new open space in district
(N) Public Open Space (hardscape and softscape)	square feet	\$20	43,500	\$870,000	assumes improvement of one acre of new open space
(N) Parkways (landscape improvements)				-	cost of development
Medians (hardscape and softscape improvements)	square feet	\$6	23,500	\$141,000	
Irrigation	square feet	\$3	23,500	\$70,500	allowance
(N) Trees (evergreen, deciduous, and flowering)	each	\$1,000	800	\$800,000	
(N) Tree Guards	each	\$1,000	100	\$100,000	Golden Circle Drive only
(N) Tree Grates	each	\$1,000	100	\$100,000	Golden Circle Drive only
(N) Pedestrian Street Lighting / Poles	each	\$3,500	275	\$962,500	
(N) Bollards	each	\$750	200	\$150,000	
(N) Trash Receptacles	each	\$750	100	\$75,000	
(N) Benches	each	\$1,250	100	\$125,000	
(N) Bicycle Racks	each	\$1,250	100	\$125,000	
(N) Newspaper Racks	each	\$1,000	20	\$20,000	
(N) Bus Shelters	each	\$30,000	7	\$210,000	allowance
(N) Wayfinding / District Identity	each	\$250,000.00		\$250,000	allowance
Gateway Monuments	each	\$25,000.00	2	\$50,000	allowance
Water Features / Fountains	each	\$50,000.00	4	\$200,000	allowance
Subtotal Unit Costs				\$10,246,500	includes Contractor O & P
Contingency @ 20%				\$2,049,300	
Engineering and Design @ 5%				\$614,790	
Construction Management @ 5%				\$614,790	
Subtotal Metro East Public Real Amenity Costs				\$13,525,380	
Assume Inflation During Project Duration @ 20%				\$2,705,076	
Metro East Public Realm Amenity Costs				\$16,230,456	